

Planning Commission

**February 14, 2019
City Hall, Council Chambers
749 Main Street
6:30 PM**

For agenda item detail see the Staff Report and other supporting documents included in the complete meeting packet.

Public Comment will be limited to three (3) minutes per speaker.

1. Call to Order
2. Roll Call
3. Approval of Agenda
4. Approval of Minutes
 - a. January 10, 2019
5. Public Comment on Items Not on the Agenda
6. New Business – Public Hearing Items
 - a. **The Foundry PUD Amendment:** A request to amend the phasing plan requiring that both commercial structures be built concurrently with the residential structures. The applicant requests that only one commercial structure be required with the residential structures (Resolution 4, Series 2019)
 - Applicant : Foundry Builders
 - Case Manager: Rob Zuccaro, Planning and Building Safety Director
7. Discussion of the September 26, 2018 Planning Commission Development Review Audit
8. Discussion of the 2019 Planning Commission work plan
9. Planning Commission Comments
10. Staff Comments
 - a. Public Notice Posting Locations (Resolution No. 3, Series 2019)
 - City Hall, 749 Main Street
 - Library, 951 Spruce Street
 - Recreation/Senior Center, 900 Via Appia

- Police Department/Municipal Court, 992 Via Appia
- City Web Site: www.LouisvilleCO.gov

11. Items Tentatively Scheduled for the regular meeting March 14, 2019:

- Vaisala PUD Amendment and Replat
- 468 S Arthur Wireless Facility
- Business Center at CTC – GDP Amendment F
- Draft Sign Code discussion

12. Adjourn

***Planning Commission
Meeting Minutes
January 10th, 2019
City Hall, Council Chambers
749 Main Street
6:30 PM***

Call to Order – Chair Brauneis called the meeting to order at 6:30 PM.

Roll Call was taken and the following members were present:

Commission Members Present: Steve Brauneis, Chair
David Hsu, Vice Chair
Dietrich Hoefner
Jeff Moline
Keaton Howe
Debra Williams

Commission Members Absent: Tom Rice

Staff Members Present: Rob Zuccaro, Dir of Planning & Building Safety
Lisa Ritchie, Senior Planner
Amelia Brackett, Planning Clerk

COMMISSION ELECTIONS

Moline made a motion to re-elect the chair, vice chair, and secretary. Howe seconded. Voice vote. All in favor.

APPROVAL OF AGENDA

Moline moved and Williams seconded a motion to approve the January 10th, 2018 agenda. Motion passed unanimously by voice vote.

APPROVAL OF MINUTES

Williams recommended that the City Council members watch the YouTube recording of the December 13th, 2018 minutes.

Hoefner moved and Hsu seconded a motion to approve the December 13th, 2018 minutes. Motion passed unanimously by voice vote.

PUBLIC COMMENTS ON ITEMS NOT ON THE AGENDA

None.

NEW BUSINESS – PUBLIC HEARING ITEMS

Terraces on Main (712 & 722 Main Street): A request for a PUD to allow for a 22,020 sf commercial building, and a 5,802 sf parking garage at 712 & 722 Main ST, and a final plat to consolidate two lots, and a special review use to allow an

automobile garage and outdoor sales for retail goods and eating and drinking establishments (Resolution 1, Series 2019).

- Applicant: 712 Main Street, LLC and 722 Main Street, LLC
- Case Planner: Rob Zuccaro, Director of Planning and Building Safety

Public notice was published in the Boulder *Daily Camera* on December 23rd, 2018 and in all other required postings on December 21st, 2018.

Brauneis asked for conflicts of interest. None disclosed.

Zuccaro noted that there had been substantial changes to the original application from last year. The new proposal included a 22,020 square-foot commercial building and a 5,802 square-foot parking garage, designed with the intent to have commercial uses on the first floor and office space on the second. The overall height was still 45 feet. The main changes between the original proposal and the current one dealt with the third story.

Zuccaro addressed parking and other development criteria. There was a 23-space parking requirement of which the proposal covered 18. The remaining 5 spaces would be paid as a fee in lieu for the Downtown Parking Fund. Zuccaro reviewed the Downtown Louisville Framework Plan, Design Handbook for Downtown Louisville, the Comprehensive Plan, and the zoning code, all of which regulate development downtown.

Staff finds that the proposal meets the requirements and review criteria.

Zuccaro presented the rear setback waiver request. The majority of the building met the setback requirement. All of the alley setback requirements are met in the proposal, plus the balconies and the staircase are well-designed and add to the architectural interest of the building. Staff found that the proposal met the waiver criteria by enhancing the design of the building.

Zuccaro presented the view angles for the third story. The third story is 1,000 square feet and the applicants provided a view analysis, showing that the third story would be behind the second story from straight across the street. Some view angles would be able to see portions of the third story.

Staff finds the project meets all applicable PUD criteria as outlined in the staff report.

Zuccaro addressed the SRU compliance criteria for outdoor sales. The second-floor deck and the patio area were included in the SRU. The proposal includes limits to the uses under the SRU typical of similar SRUs staff has seen and staff finds that this SRU request meets the criteria.

On the subdivision plat, the proposal moved the lot line to allow for a single building on the property. Staff finds that it meets all criteria.

Staff recommends the proposal overall.

Howe asked how many proposed retail spaces could fit on the ground floor.

Zuccaro replied that it was being designed for two retail spaces with a lobby area.

Hsu asked what space should be included in the parking calculations, particularly whether the basement space should be included in the calculation.

Zuccaro stated that the PUD limited the basement space to storage, which could not be occupied. Zucca and Voltage had similar garage allowances on their PUDs. The applicant would have to come back through the PUD process if they wanted to use the basement for something other than storage.

Hsu asked how the loading time limit would be enforced.

Zuccaro replied that the PUD required the applicant to put up a sign. Violations would go through the typical enforcement process.

Howe asked for the width of the alley beyond the projections.

Brauneis clarified that the projections were encroaching into the setback space, not the alley. Brauneis asked if any other buildings came up to the property line.

Zuccaro replied that there were structures that were not set back as far as 20 feet.

Brauneis asked for additional questions. Seeing none, he invited the applicant to speak.

Erik Hartronft, 950 Spruce Street in Louisville, stated that they had made significant changes to the project and called the project an opportunity to transform the ground floor from office space to a more retail-friendly storefront. He pointed out that there was a historic structure south of 712 & 722 Main, which the design responded to. The design also carried forward the architectural simplicity of the existing buildings from the 1960s and 1970s while reflecting current architectural styles.

Hartronft noted that the building may be too small at this point to contain its current owner, Boulder Creek Neighborhoods. Other occupants were interested in the second-floor addition. Hartronft noted that offices generated sales for the downtown area and that the first floor could attract new retail.

Hartronft described the materials. The proposal included warm and inviting materials on the ground floor, architectural nods to classic western architecture, elements that divided the mass of the building, and a two-story appearance from a sidewalk view. He also noted the proposal's attention to the rhythm and street music of varying height facades on the block. The back of the building featured a wall of stucco along the alley, which could display a mural and encourage pedestrian use and interest in the alley.

Brauneis asked for questions of the applicant.

Hsu asked if there was a stairwell on the rear south side.

Hartronft confirmed.

Hsu asked why the second and third floors were a different look than the first floor.

Hartronft replied that they did not want to create a monolithic building by carrying through the same materials and design from the second and first stories to the third story. There was also a specific design guideline that says a third floor should look different than the rest of the building and should look like an addition that was put on later. They thought a more transparent third floor would feel lighter and would tend to recede more than if it were a more massive, solid design.

Williams asked if the office space was designed for multiple tenants.

Hartronft responded that it was currently designed for a single tenant, but it could accommodate more.

Williams asked if Boulder Creek Neighborhoods was planning to stay in the new building.

David Sinkey, Founder and President of Boulder Creek Neighborhoods, 712 Main Street in Louisville, replied that the company had not made a decision. They were looking for bigger buildings so they could accommodate all their employees. They were far enough along on this proposal that they wanted to see it taken forward whether Neighborhoods ended up occupying it or not. Sinkey added that the ground floor could accommodate as many as three retail spaces. He explained that they created a lobby to make an entry environment for the second floor.

Williams asked what the third floor could be used for.

Sinkey replied that it was essentially an amenity space for employees and could be used as a kitchen or for company events space. It would not be used as restaurant or retail space.

Williams stated that the design was well thought-out and an improvement on the previous proposal. She pointed to the amount of glass was an improvement over the last application and noted the appeal of the street music, skyline-feel of the varying heights. On the parking, she thought that the ingress/egress separation was an improvement over the last proposal and she understood why the parking was about half. She asked for the total square footage of the current buildings.

Sinkey responded that 712 Main was roughly 5,600 square feet and 722 Main was roughly 1,700-1,900 square feet.

Howe asked for clarification on the movement of cars in and out of the garage and if there was enough room for delivery trucks, pedestrians, and cars.

Hartronft responded that the ramp ended at the property line. Hartronft added that they used a turning radius template to ensure there would be enough space. He acknowledged that a delivery truck parked in front of the ramp, it would block the

garage, but that was already the reality downtown. It was not currently a big issue since the delivery trucks move quickly.

Williams asked if the proposal would allow for a summer patio.

Hartronft replied that it would allow for the patios and on the south building the setback was large enough for tables. Public Works wanted them to make sure that there was enough space for a public walkway, since it was a narrow sidewalk.

Williams clarified that the parking was not a shared or public parking space.

Hartronft confirmed.

Hoefner moved to enter the materials board into the record. Howe seconded.

Williams asked for a description of the materials and where they would be used.

Hartronft showed the materials that would be used for accents and to divide up the mass of the building.

Williams noted that there were a number of guidelines that pertain to size, mass, stepdown of the alley, and the ratio of the windows. She noted that she thought all of these measurements were an improvement on the previous proposal. She would not call the proposed step-down a true step-down.

Howe asked how much farther back the building was compared to the Singing Cook and the Huckleberry and if the alley between the Singing Cook and the proposed building would remain in place.

Hartronft responded that the setbacks were farther back than the Huckleberry. He added that the alley was on the neighbor's property so it would not be affected by the proposal.

Brauneis asked for public comment.

John Leary, 1116 Lafarge Avenue in Louisville, stated that he believed the mass and scaling of the building were now consistent with city standards. However, the project illuminated policy issues that the City needed to address. First, the public twice passed a tax to preserve the character of downtown, yet there were municipal incentives for the redevelopment of the downtown, directives that pulled in opposite directions. Second, the parking in downtown was not based on estimated parking demand, it was based on policy meant for adaptive reuse of existing buildings. Currently, the Terrace on Main was required to have 23 spaces. If you were to apply the standards for an office to this building, it would require 40, plus about 32 parking spots for the retail spaces. Leary did not think that an office being downtown should exempt it from the higher requirement. Over the past decades, job growth in Louisville has been at a faster rate than population growth, but fewer people are working in Louisville percentage-wise than ever before. Leary summarized that the parking and development requirements were not sustainable.

Cindy Bedell, 662 West Willow Street in Louisville, stated that the mass and scale of this proposal was better than the past application. She still did not think that the two-story section fit in the downtown area. She noted that there was a jolting height contrast between the two-story structure and the one-story neighbors. Bedell suggested that the eye line could be improved by softening the roof on the northern section with a gabled roof or architectural features added to the top like the State Mercantile building. This was an iconic location and she thought we should take our time to get a design that fits and enhances the historic downtown.

Moline asked about parking lots being obsolete by 2030 and wondered if the Transportation Management Plan would be looking at parking.

Zuccaro replied that the Transportation Master Plan would not be looking at parking policy, however the update to the Commercial and Industrial Design Guidelines will look at parking, but not for downtown. Policy work downtown would require a City Council work plan to look at that item.

Moline asked if staff had done downtown parking studies recently.

Zuccaro replied that there had been several studies over the years to make recommendations on acquisitions and promoting biking and walking to implement the previous planning efforts to provide more public parking.

Brauneis asked where most of the newly purchased parking spots came from.

Zuccaro responded that the numbers came from the three areas around DELO, Lucky Pie, and the Corner.

Brauneis asked Mr. Sinkey for his thoughts on the fee-in-lieu amount of \$18,000 per space.

Sinkey replied that creating a parking space in a small garage downtown would cost about \$40,000 per space. However, the fee-in-lieu payments were for parking spaces that did not have leasable benefits for the tenants. He noted that there were many approved PUDs in town that were not being built largely because the economics did not support it. But in the scheme of things, \$18,000 for a space was probably fair.

Brauneis closed public comment and opened commissioner discussion.

Hoefner stated that the Commission did not have a lot of direction from Council about what they did not like about the previous proposal. That said, the changes to the application do respond to the concerns from the Commission and the public from the last proposal.

Moline appreciated the street music concept. He agreed that the northern portion did feel a bit out of place, but thought that it might help offset the two-story building from the historic buildings. He thought the massing had been improved and was no longer a problem.

Hsu stated that he had voted for the application previously even though he had been concerned about the mass and the new proposal addressed that issue. He thought that the process and the public comment made the project better, even without specific direction from Council. He liked the southern section more than the northern section, but that was an aesthetic opinion and the architecture met the criteria. Hsu noted that the third-floor windows could be judged not to be traditional according to the language in the guidelines, but he thought that could be waived since there were benefits to having windows there. He felt that the calculations for the parking spaces were correct.

Williams stated that one of the reasons she voted yes the first time was to keep Boulder Creek Neighborhoods in Louisville and she was disappointed to hear that they might leave downtown. She asked the applicant to remember to consider their neighbors during the construction process.

Howe stated that it was an opportunity to improve what was there currently.

Brauneis stated that the project stitched the fabric of the block together and replaces a current dead zone downtown. He also thanked Mr. Leary for his long-term insights.

Hsu moved to approve Resolution 1, Series 2019. Hoefner seconded. Roll call vote. All in favor.

Office Zoned Property Zone Change – Rezone to Agricultural and Administrative Office – A request to rezone certain property from the Office zone district to the Agricultural and Administrative Office zone district (Resolution No 2, Series 2019).

- Applicant: City of Louisville
- Case Manager: Lisa Ritchie, Senior Planner

Public notice was published in Boulder *Daily Camera* on November 25, 2018 and at all other required postings at November 23, 2018. The notice was continued to this meeting.

Howe recused himself due to a conflict of interest.

Ritchie presented the history of the Office Zone District, which was established in 1973 and repealed in 1984. This item was the last bit of clean-up to address historic zoning irregularities in the city.

Ritchie showed the areas that staff proposed to rezone agricultural. Staff worked with Parks and Open Space to determine the proposed zoning, which was consistent with the properties immediately to the north and east. The agricultural zone is consistent with current use.

Ritchie showed the areas that staff proposed to rezone to Administrative Office Properties. These areas contain smaller office buildings with medical, dental, and other professional office users. Staff has official consent in writing from three out of the four property owners and a conversation with the final property owner, even though the City

is not required to get that consent before rezoning. The proposed rezoning is consistent with the properties to the west as well as the uses on the site.

Staff finds that the application meets criterion 1 in Section 17.44.050 and that the other criteria are not applicable. Criterion 1 reads, "The land to be rezoned was zoned in error and as presently zoned is inconsistent with the policies and goals of the city's comprehensive plan." It also appears that the original zoning was done in error since it was repealed later.

Hsu asked why staff was proposing to change the zone to Agriculture instead of Business Office, which was also near the property.

Ritchie stated that the Business Office zone was inconsistent with the current use and the projected use of the area.

Hsu asked if there were any agricultural uses, such as crop-growing, in the area.

Ritchie replied that the zone was the closest fit among the zoning options, but no crop-growing is occurring in the area.

Hsu asked why they were not recommending the Open Space zone district.

Ritchie replied that OS staff thought that the agricultural zone was a better fit at this time. Also, the Open Space zone was very restrictive to change and any future zoning changes from Open Space would require a vote from Louisville residents.

Brauneis asked for additional questions of staff. Seeing none, he asked for commissioner comments.

Hsu did not think that agriculture was a perfect fit but it made more sense than any other zone. Office space was a suitable use for the other area.

Williams made a motion to approve Resolution 19, Series 2018. Roll call vote. All in favor.

Davidson Highline Replat 2 – A request for a replat of Davidson Highline Replat subdivision, Lots 1A and 2A to adjust the lot boundaries of Lots 1A and 2A, vacate Tract Q, Takoda subdivision, and create Outlot A (Resolution No 2, Series 2019).

- Applicant: City of Louisville.
- Case Manager: Lisa Ritchie, Senior Planner

Williams disclosed that she lived near the block in question but confirmed that she could be fair.

The application met all public notice requirements on December 23rd and December 21st, 2018.

Ritchie presented the proposal, one of the purposes of which was to create an outlot that the City could purchase from the property owner in accordance with the Regional

Trail Improvement Plan for the construction of an underpass. She noted that the property was privately owned, however the City of Louisville was the applicant and staff had the owner's consent to conduct this application. The application included moving a lot line, creating an outlot, and creating a dedication for state highway 42.

Staff recommends approval of Resolution 2, Series 2018 with the following condition: Concurrent with the recordation of the plat, deeds shall be recorded which reflect the revised legal description of each affected property.

Moline asked if the highway reservation was part of the replat and why it did not have a label as a tract or something similar.

Ritchie replied staff would make sure the new version had a label.

Hsu made a motion to enter Sheet 2 into the record. Williams seconded. Voice vote. All in favor.

Hsu asked if Outlot A was being created as part of the proposal.

Ritchie replied that it was an outlot, not a lot, so it was not for development per se. It was an acceptable shape and location related to the subdivision ordinance.

Zuccaro added that staff used outlots on other developments for drainage and were not measured against minimum lot widths, et cetera.

Hsu asked why the creation of an outlot need the change in the lot line between lots 2A and 1A.

Ritchie replied that they were unrelated requests, however the property owner and the City desired to set forth the intent to link up Kaylix Avenue should the property be sold or transferred.

Brauneis asked why it the lot line not straight.

Ritchie replied that the two parts of the avenue did not line up in a straight line.

Moline asked if in a future situation Lot 1A would have to be modified for Kaylix Avenue development.

Ritchie confirmed that it would have to be modified and the City would have to go through a right-of-way process.

Hsu asked if it would be possible to do the subdivision as part of a PUD.

Ritchie confirmed.

Hsu asked Ritchie to explain more of staff's reasoning for the modification criteria. He stated that he was concerned the staff report was stretching the definition of "physical circumstances or conditions" under modification criterion number one. Since the street

was not on the property, he did not think “peculiar to the specific property” applied, either.

Williams asked if there was a condition requiring the future development of a street on the lot line.

Zuccaro replied that there was no such condition related to this plat application. The City has had long-range plans to extend Kaylix Avenue and had approached the property owner to acquire the necessary right-of-way. The property owner supported those plans, but did not want to go through the right-of-way process at the moment. This replat was trying to make it easier in the future so no one will build where the City wants to put the future road, while also creating two lots that can be developed or sold.

Ritchie added that the Comprehensive Plan addressed connecting the two parts of Kaylix Avenue.

Zuccaro responded to Commissioner Hsu’s concern about modification criteria number one. He acknowledged that it was not black and white, but they considered the lot to be “unique” because the property was in the middle of two sides of a road that the City wanted to connect in the future.

Hsu stated that having a right-of-way would not affect the future sale of the lot. The modification review criteria was pretty stringent and was only to be waived with a PUD.

Zuccaro responded that another way to look at it was that the development had already taken place with the shed, which was “reasonable development.”

Ritchie added that the phrase “reasonable development” could address future development, including the desired road.

Brauneis asked for additional questions of staff. Seeing none, he asked for staff final statements.

Ritchie reminded the commissioners that they could ask for further information and that staff recommended the proposal.

Moline stated that he was in favor of both aspects of what the replat would let the City do: create a consistent street pattern for this portion of the town and create Outlot A, allowing the City to purchase the land to construct an underpass for the trail system in an area where there was a recent fatality.

Brauneis asked Commissioners Williams and Hsu if their reservations stemmed from the outcome or the procedure.

Williams and Hsu indicated that they were concerned with procedure.

Hsu confirmed that his issues were procedural. He supported the underpass, but he was not convinced that criteria 1, 2, or 5 were satisfied. For criterion 5, he thought there

were other alternatives, for example tearing down the shed or going through a PUD process.

Brauneis asked if Hsu had a different procedure in mind.

Hsu responded that he had not considered the application with the PUD criteria in mind, but that the present application seemed to involve a lot of legal wrangling that might be unnecessary.

Hoefner recommended that they add a note recommending that City Council waive the modification review criteria.

Hsu replied that he did not think the Council could do that.

Zuccaro added that the Council did have the authority but would have to pass an ordinance. He asked the Commission to consider what it meant to reasonably develop a property for criterion 2. Staff determined that it was not possible to reasonably develop the lot with both the shed and the road.

Hoefner agreed and stated that he did not see the friction point, since staff, the property owner, and the Commission agreed with the application in substance and outcome, even if there was disagreement over procedure.

Howe agreed and noted that the application was part of an effort to complete goals in the Comprehensive Plan and to build the underpass.

Hsu stated that there were two workarounds already. They could not move the line in the current application since it does not affect the underpass or they could wait for a PUD.

Williams agreed, stating that moving the lot line had nothing to do with the underpass. She understood that moving the lot line helped facilitate development, but the lot was developed as-is. She did not understand what the application was trying to solve right now other than the underpass.

Zuccaro responded that it was a fair analysis and Council would have to make that judgement based on the Commissions' recommendation.

Williams stated that not changing the lot line did not change anything for the future.

Brauneis responded that this was an opportunity to lay out the lot lines to make it easier in the future, since right now the property owner was amenable.

Williams replied that for all the City knew one person could buy the whole lot in the future.

Hoefner and Brauneis asked what the harm was in moving the line.

Hsu responded that they were stretching the law and he did not know what they were trying to achieve by doing that.

Hoefner stated that the replat did meet the criteria and that Council could recognize in this instance that everyone was in agreement that moving the lot line made some sense, notwithstanding the criteria in the code.

Williams asked what would happen if someone wanted to develop the shed on lot 1A.

Zuccaro replied that shed would become a legally non-conforming structure. Under the code, you can further that kind of structure but you could not further the encroachment.

Williams asked about the process for reviewing a non-conforming structure.

Ritchie stated that it depended on the incoming request. It could be reviewed by the Commission or by staff depending on what the owner asked for.

Williams asked if that made it a hardship for lot 1A.

Hsu added that right now the shed could be expanded more.

Brauneis pointed out that the owner had already agreed to the changes in the application.

Hsu replied that this application could actually harm the reasonable development of the shed.

Howe asked if Commissioner Hsu was suggesting resubmitting the proposal as a PUD.

Hsu responded that he was suggesting to keep outlot A in the application and leave the lot line as-is.

Howe asked what it would take to resubmit the application as a PUD.

Ritchie replied that there was no development that would be associated with a PUD at this time. She added that the shed was likely constructed prior to the Design Standards adopted today, so the design itself would be problematic to develop without bringing it up to design standards. She noted that staff would have to confirm with the property owner with this request to relocate the lot line and staff would have to confirm that he was comfortable moving forward with the application without moving the lot line.

Williams asked how much square footage Divine Canine would be losing off their lot.

Brauneis clarified that the Commission should not think about the current business but instead think of it as the entire lot, since the same owner owns both lots.

Williams asked for clarification on lot ownership.

The other commissioners confirmed that it was the same owner for all the lots.

Brauneis noted that they could not do what they were proposing to do if the lot had more than one owner. He appreciated Commissioners Hsu and Williams for their desire to follow the rules, but he felt those rules were to protect people from harm, which in this case was not a concern.

Ritchie responded to Commissioner William's earlier question and stated that the lot sizes were swapping 33,968 square feet, not including Tract Q. Staff also recognized that not moving the lot line now put the City at a bit of a risk for development on Kaylix Avenue.

Williams asked if the owner was aware of the lot-line change reason.

Zuccaro and Ritchie replied that they were aware that it was to accommodate a future right-of-way. Public Works have had direct conversations with the owner, though he had not.

Williams stated that there was no condition requiring the development of Kaylix Avenue in this application. If there was no such condition, the City may not be able to convince the owner for a right-of-way consideration through a future PUD process.

Zuccaro replied that the reason the City was trying to move the lot line now was to set it up for good future development. To redevelop it now would require a replat anyway to avoid creating oddly shaped lots in a future right-of-way process. He recommended that the Commission vote on the proposal based on the criteria and he asked commissioners to articulate their reasons for supporting or not supporting the criteria.

Hsu asked if the lot line could be moved anywhere west of where it is.

Zuccaro replied that the lot line was the property owner's preferred placement.

Ritchie added that there could be a modification on the west side.

Hsu stated that he was pretty convinced that at least one of the criteria was not met.

Williams stated that she would feel more comfortable if the proposal came forward from the property owner instead of the City or if there were confirmation of support from the property owner in writing.

Moline and Brauneis pointed out that the owner signed the application.

Williams stated that she was unconvinced by the signature.

Ritchie added that the property owner would also have to sign the plat.

Zuccaro stated that one possible reason for the property owner to support this could be that it made it easier to sell or develop one lot without having to deal with the right-of-way. He acknowledged that he could not speak for the applicant, but he imagined that there could be a logic of convenience.

Brauneis added that the application also grandfathered in the shed.

Hoefner addressed criteria 1, 2, and 5. He noted that for criterion 1, the road and the lot line did not match and that was a unique physical circumstance. For criterion 2, straight roads were important for public safety. Finally, for criterion 5, he noted that the language did not say no alternatives, it said no reasonable alternatives, and in this case it was much more reasonable to move a line on a piece of paper than to move the road to the line.

Brauneis agreed with Hoefner's assessment.

Ritchie responded to Commissioner Hsu's earlier question, stating that there was another shed further to the west of the proposed property line.

Moline appreciated the perspective from staff about taking the opportunity to work with an amenable property owner to make a huge improvement to trail infrastructure.

Hsu asked what the process would be if the City extended the right-of-way right now.

Zuccaro replied that there were a number of scenarios. It would be difficult to extend the road without a request for redevelopment. The City would have to negotiate to purchase the right-of-way. He could not imagine a scenario where they would not be applying to replat, anyway, since the right-of-way process would create a remnant lot.

Howe stated that the proposal had benefits as part of the Commission's goals for land use in the city and that the Comprehensive Plan supported it, as well.

Brauneis noted that the benefits were significant. Not doing this now, the City ran the risk of the lot getting sold off to someone else who was not amenable to the City's plan.

Hsu stated that regarding criterion 1 that he was not convinced that having a road somewhere outside the property met criterion 1. He agreed that there were benefits to the application, but the benefits were not part of his evaluation of the criteria. Making it easier for the property owner to sell property was not a reason to approve the application. He thought moving the lot line somewhere else was a reasonable alternative option.

Brauneis stated that if the proposal set off alarm bells or red flags, he would be concerned. He appreciated Commissioner Hsu's attention to procedure.

Williams stated that process was important to her. She did not think the criteria were met and she would have preferred to have the property owner present the proposal. She also did not think that the lot line had anything to do with the underpass. She felt that criterion 1 was absolutely not met, which negated the other criteria.

Moline noted a property created in 1990 well before the street was laid out and the lots were laid out around it. He wondered if there was some reasonableness that the City should accommodate the property owner's request to update the lot lines to be more in step with what is around it.

Brauneis stated that there was one place where the lot line needed to be and the application had it.

Hoefner moved to approve Resolution 2, Series 2019. Howe seconded. Roll call vote. Four in favor. Commissioners Hsu and Williams voted nay.

COMMISSIONER COMMENTS

Howe asked if it was appropriate for the Planning Commission to discuss long-term lot vacancies, which came up in the marijuana ordinance issue last meeting.

Zuccaro replied that one of the roles of the Planning Commission was to make comments on the Comprehensive Plan that dealt with future land use. Within that context, the City refreshes its Comprehensive Plan policy every 10 years. There could be more frequent discussions about land use changes and policy implications in a study session, for example.

Brauneis stated that it could be a proactive discussion about vacancies.

Hoefner added that he thought a study session made sense, where recommendations did not have to be necessary.

Hsu stated that it might be nice to organize the sessions around specific topics, like parking or affordable housing.

Zuccaro suggested bringing a general work plan discussion for 2019 on a future agenda. Staff could provide background information and analysis and the Commission could recommend policy changes at the end of those discussions if the commissioners wanted.

Hsu asked if the Commission would be commenting on budget issues for the City Council.

Zuccaro replied that the Council just adopted a budget for 2019 and 2020. The capital plan could be an opportunity to address budget issues. He stated that the City has a 6-year capital plan that may come up again in 2020.

STAFF COMMENTS

Open Government & Ethics Pamphlet – 2019 Edition

Zuccaro noted that the pamphlet was included the staff packet.

Public Notice Posting Locations (Resolution No 3, Series 2019)

State law requires that each year every municipal board or commission establish the location(s) where the notice of their public meetings will be posted. It is required the location be established at that body's first regular meeting of the year.

Staff recommends the following official locations for posting of Planning Commission agendas as follows:

- City Hall, 749 Main Street
- Library, 951 Spruce Street
- Recreation/Senior Center, 900 Via Appia
- Police Department/Municipal Court, 992 Via Appia
- City Web Site: www.LouisvilleCO.gov

Staff proposed to continue the item to February with a copy of the resolution to be included the staff packet.

Brauneis recommended that commissioners be more proactive in notifying staff if they were going to make it to the meetings or not.

2019 Meeting dates

Regular meetings are held at 6:30 p.m. on the 2nd Thursday of every month in the 2nd floor of City Hall, City Council Chambers. As needed, overflow meetings will be held at 6:30 p.m. on the 4th Thursday of every month. The 3rd Thursday of each month should be held for Study Sessions, as needed. Exceptions to these dates are in November and December.

ITEMS TENTATIVELY SCHEDULED FOR THE MEETING ON FEBRUARY 14TH, 2019

- Ascent Church Final PUD and St Louis Parish and Commercial Park Final Plat

Adjourn:

Howe made motion to adjourn. Hoefner seconded. Brauneis adjourned meeting at 9:33 PM.

ITEM: Case #PUD 0195-2019, Foundry Phasing Plan Amendment

PLANNER: Rob Zuccaro, Planning and Building Safety Director

APPLICANT: Foundry Builders
1209 Pearl Street, Suite 14
Boulder, CO 80302

OWNERS: Takoda Properties, Inc.

EXISTING ZONING: Planned Community Commercial/Residential (PCZD-C/R)

LOCATION: Southwest of Paschal Drive and Highway 42

TOTAL SITE AREA: 5.82 acres

REQUEST: A Request to Amend the Foundry PUD and Subdivision Phasing Plan to Modify the Requirement that Both Approved Commercial Buildings be Constructed Concurrent with the Residential Development

VICINITY MAP:



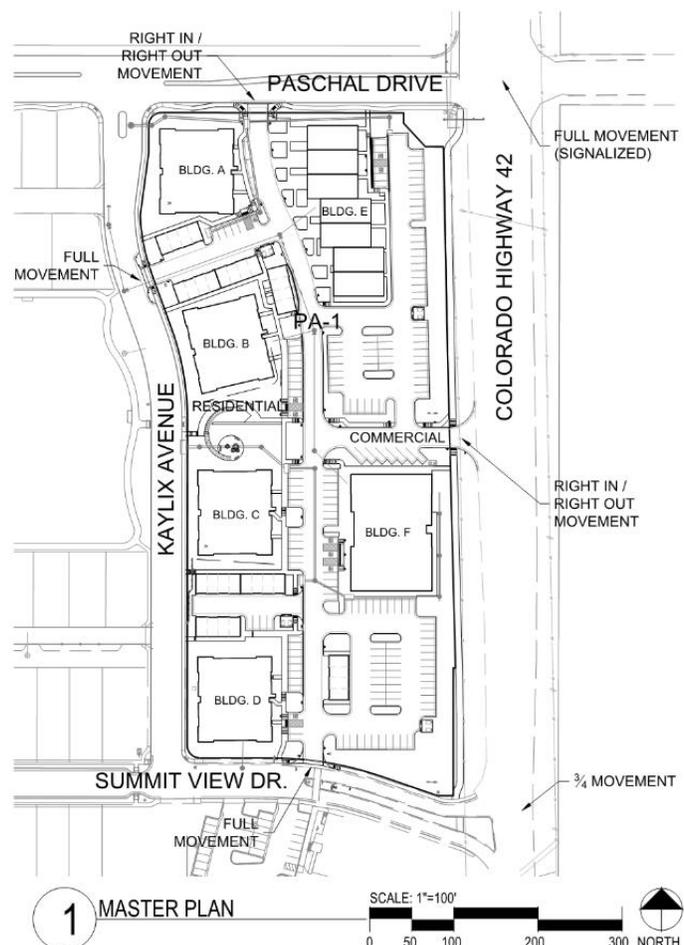
SUMMARY:

The applicant proposes to change the Foundry Planned Unit Development (PUD) phasing plan so that only one of the two proposed commercial buildings is required to be constructed concurrent with the residential development. The applicant states that change in phasing is needed due to current market and financing conditions that would make it difficult to construct both commercial buildings concurrent with the residential development. The proposed phasing language is as follows:

- Residential building permits for the condominiums shall be obtained concurrent with or subsequently with the building permit for one of the two commercial buildings, and
- The last certificate of occupancy for one of the residential 8-plex buildings shall be withheld until: 1) start of construction of the first commercial building commences, as defined by the 2018 International Building Code, which includes the first placement of permanent construction of a building, such as pouring of a slab or footings, installation of pilings or construction of columns; and 2) 30% of the net leasable space has identified tenants with proof being as and executed Letter of Intent coupled with a security deposit.

BACKGROUND:

The City approved the Foundry PUD, subdivision plat and a General Development Plan (GDP) Amendment on January 16, 2016 (see Attachment 3 for City Council approval resolution, Attachments 4-6 for approved plans, and Attachments 7 and 8 for City Council and Planning Commission minutes respectively). The proposed development included a rezoning of the 5.82-acre property from commercial (PCZD-C) to mixed commercial and residential (PCZD-C/R), and approved a PUD for 31,960 square feet of commercial development in two buildings (Buildings E and F), and 32 residential condominium units (24 restricted to senior housing) in four, eight-plex buildings (Buildings A through D). The two commercial buildings include a 17,850 sq. ft. in-line commercial building (Building E) and a 14,110 sq. ft. flex



commercial building (Building F). The Foundry is part of the Takoda (Steel Ranch) GDP, and was originally planned as a commercial hub for the Takoda development.

One of the conditions of approval in the City Council resolution of approval for the PUD, subdivision and GDP amendment (see Attachment 3, Condition No. 5, Resolution No. 3, 2016) was that the “Residential and Commercial Development shall be constructed concurrently.” The staff memo to City Council stated the following:

...staff believes it is important to require the applicant construct the commercial structures concurrent with the residential development and place a condition stating such. Planning Commission endorsed the condition as they are also concerned with the long-term reduction of commercially zoned property.

The condition of concurrent commercial and residential development would be enforced through the development agreement where the City can use the issuance of building permits and certificates of occupancy to ensure concurrent development.

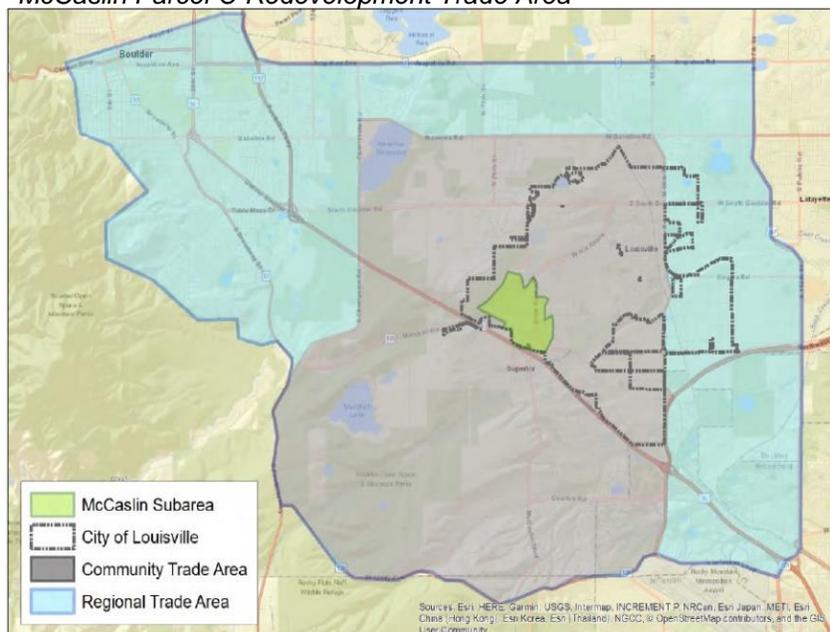
The concern with replacing commercial development with residential development is that it could have a negative fiscal impact on the City due to lower sales tax revenues. A requirement for concurrent commercial and residential development was intended to ensure fiscal balance for the Takoda/Steel Ranch development. The original GDP for the property anticipated 76,055 sq. ft. of commercial development on the Foundry property, which was reduced with the current development scenario to 31,960 sq. ft.

ANALYSIS:

The applicant notes in their request letter that there is limited market demand for new retail and commercial development, and cite as an example that the Delo Plaza development, constructed in 2017, continues to have high vacancy rates and undeveloped pad sites. The lack of strong retail market demand is supported by a recent [market analysis](#) conducted by the City for the McCaslin Corridor, which concluded that within the next ten years there is

anticipated market demand for 150,000 sq. ft. of new retail development in the regional market trade area. The amount that could be captured within any particular development is only a small portion of the total market demand. For example, in the

McCaslin Parcel O Redevelopment Trade Area



McCaslin Corridor the study concludes that approximately 20% of the market demand for new retail demand (30,000 sq. ft.) is likely to occur in the corridor. Other supportive uses, such as office, residential and entertainment could help make retail viable within any particular development.

Staff ran the City’s fiscal impact model under three scenarios to better understand the potential implications to City services as a result of changing the phasing. The first scenario shows the full absorption (time to build and occupy the space) of both commercial buildings in two years, which matches the fiscal analysis conducted when the City originally approved the PUD and GDP amendment in 2016. The second scenario reflects absorption of Building F between three and five years and Building E between eight and 10 years, which represents a possible scenario allowing the commercial phasing as proposed if both buildings end up being constructed. The third scenario reflects absorption of Building F between three and five years with Building E never being developed as a “worst case” scenario. With all scenarios, the commercial development is modeled with 30% office space and 70% retail space.

Fiscal Model Inputs

	Scenario 1	Scenario 2	Scenario 3
Residential Units	32	32	32
<i>Market Value</i>	\$600,000	\$600,000	\$600,000
<i>Construction Value</i>	\$480,000	\$480,000	\$480,000
<i>Household Income</i>	\$90,000	\$90,000	\$90,000
<i>Absorption</i>	years 2-5	years 2-5	years 2-5
Commercial Building E			
<i>Market Value/Sq. Ft.</i>	\$250	\$250	\$250
<i>Construction Value/Sq. Ft.</i>	\$175	\$175	\$175
<i>Retail Sales/Sq. Ft.</i>	\$200	\$200	\$200
<i>Absorption</i>	year 2	years 8-10	no development
Commercial Building F			
<i>Market Value</i>	\$250	\$250	\$250
<i>Construction Value</i>	\$175	\$175	\$175
<i>Retail Sales/Sq. Ft.</i>	\$200	\$200	\$200
<i>Absorption</i>	year 2	years 3-5	years 3-5

The fiscal model table on the following page provides the 20-year totals (per \$1,000) for revenue, expenditures and net fiscal impact. The model shows that all scenarios provide a net positive fiscal impact. Scenario 1 estimates a 20-year positive fiscal impact of \$2.2 million or an average of \$111,200 per year. Scenario 2 estimates a 20-year positive fiscal impact of \$1.8 million or an average of \$91,300 per year. Scenario 3 estimates a 20-year positive fiscal impact of \$954,000 or an average of \$47,000 per year

Fiscal Impact Model

Revenue by Fund 20-year totals (x\$1000)	SCENARIO					
	Scenario 1	%	Scenario 2	%	Scenario 3	%
General Fund	\$2,113	60%	\$1,687	59%	\$1,095	59%
Open Spaces & Parks Fund	\$350	10%	\$286	10%	\$189	10%
Lottery Fund	\$0	0%	\$0	0%	\$0	0%
Historic Preservation Fund	\$122	3%	\$100	3%	\$66	4%
Capital Projects Fund	\$956	27%	\$781	27%	\$514	28%
TOTAL REVENUE	\$3,541	100%	\$2,854	100%	\$1,865	100%
Expenditures by Fund						
General Fund	\$887	67%	\$681	66%	\$578	63%
Open Spaces & Parks Fund	\$22	2%	\$22	2%	\$21	2%
Lottery Fund	\$0	0%	\$0	0%	\$0	0%
Historic Preservation Fund	\$0	0%	\$0	0%	\$0	0%
Capital Projects Fund	\$408	31%	\$324	32%	\$312	34%
TOTAL EXPENDITURES	\$1,317	100%	\$1,027	100%	\$911	100%
NET FISCAL RESULT BY FUND						
General Fund	\$1,226		\$1,007		\$517	
Open Spaces & Parks Fund	\$328		\$264		\$168	
Lottery Fund	\$0		\$0		\$0	
Historic Preservation Fund	\$122		\$100		\$66	
Capital Projects Fund	\$548		\$456		\$202	
NET FISCAL IMPACT	\$2,224		\$1,826		\$954	

Criteria related to fiscal impact are by reference in the PUD approval criterion to policies of the City’s Comprehensive Plan.

Sec. 17.28.120B.1. – PUD Review Criteria

Development shall be in accordance with the adopted elements of the comprehensive development plan of the city, and in accordance with any adopted development design standards and guidelines.

The requested rezoning is located in the Highway 42 Urban Corridor of the Comprehensive Plan. The Comprehensive Plan states that all Urban Corridors should “demonstrate positive fiscal benefits to the City.”

Based on the City fiscal model results, staff finds that the request is likely to provide a positive fiscal benefit under the proposed phasing plan. Allowing a first phase of development could help activate the area as a local commercial hub, establishing a demand for the second commercial phase to take place.

PUBLIC COMMENTS:

Staff received one public comment in support of the project (see Attachment 9).

STAFF RECOMMENDATION:

Staff recommends approval of Resolution 4, Series 2019; recommending to City Council approval of an amendment to the Foundry PUD phasing plan to allow one of the two proposed commercial buildings to be constructed concurrent with the residential development.

ATTACHMENTS:

1. Resolution No. 4, Series 2019
2. Application Letter
3. City Council Resolution 3, 2016
4. Foundry PUD
5. Foundry Plat
6. Takoda GDP – 3rd Amendment
7. January 19, 2016 City Council Meeting Minutes
8. December 5, 2015 Planning Commission Minutes
9. Public Comments

**RESOLUTION NO. 4
SERIES 2019**

**A RESOLUTION RECOMMENDING APPROVAL OF A REQUEST TO AMEND THE
FOUNDRY PUD AND SUBDIVISION PHASING PLAN TO MODIFY THE
REQUIREMENT THAT BOTH APPROVED COMMERCIAL BUILDINGS BE
CONSTRUCTED CONCURRENT WITH THE RESIDENTIAL DEVELOPMENT**

WHEREAS, there has been submitted to the Louisville Planning Commission an application to amend the phasing plan for the Foundry PUD to allow construction of only one of two commercial buildings concurrent with the residential development approved under the PUD; and

WHEREAS, the City Council adopted a condition of approval through Resolution 4, Series 2016 requiring the residential and commercial development to be constructed concurrently; and

WHEREAS, the City Staff has reviewed the information submitted and found that the application complies with the Louisville zoning regulations and other applicable sections of the Louisville Municipal Code; and

WHEREAS, the Planning Commission has considered the application at a duly noticed public hearing on February 14, 2019, where evidence and testimony were entered into the record, including the findings in the Louisville Planning Commission Staff Report dated February 14, 2019.

NOW THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Louisville, Colorado does hereby recommend approval of the request to amend the Foundry PUD phasing plan to modify and replace the requirement that both commercial buildings be constructed concurrent with the residential development with the following:

1. Residential building permits for the condominiums shall be obtained concurrent with or subsequently with the building permit for one of the two commercial buildings, and
2. The last certificate of occupancy for one of the residential 8-plex buildings shall be withheld until: 1) start of construction of the first commercial building commences, as defined by the 2018 International Building Code, which includes the first placement of permanent construction of a building, such as pouring of a slab or footings, installation of pilings or construction of columns; and 2) 30% of the net leasable space has identified tenants with proof being as and executed Letter of Intent coupled with a security deposit.

PASSED AND ADOPTED this 14th day of February, 2019.

By: _____
Steve Brauneis, Chairperson
Planning Commission

Attest: _____
Debra Williams, Secretary
Planning Commission



January 18, 2019

Rob Zuccaro
Planning and Building Safety Director
City of Louisville
749 Main Street
Louisville, CO 80027

Via e-mail: rzuccaro@louisvilleco.gov

Mr. Zuccaro:

We are committed to completing the Foundry which will become the flagship project of Steel Ranch. The Foundry represents the final phase of the Steel Ranch neighborhood and will become an iconic gateway to the City of Louisville along Highway 42. The overall development plan for the Foundry at Steel Ranch envisions an approximate 14,100 square foot “Foundry” building consisting of retail and restaurant space (including a rooftop deck), 17,800 square feet of in-line commercial/retail space and 32 condominiums inclusive of a landscaped plaza, public and private parking including pedestrian connections throughout the community.

The Foundry Final Development Plan / Planned Unit Development (PUD) and final plat were approved by the Louisville City Council on January 19, 2016. Lack of clarity within the resolution of approval (Resolution No. 3, Series 2016) has led to a difference of opinion between City Staff and Foundry Builders regarding the timing of construction. Current market and financing conditions make it impossible to construct both the Foundry commercial building and in-line commercial space (which total approximately 32,000 square feet) prior to completing some or all of the condominiums. In response to these challenges we respectfully request an amendment to the PUD and associated resolution of approval that will provide clarity to the development schedule for both the commercial and residential components of the Foundry project.

Our primary concern is the market viability to secure tenants for all 32,000 square feet of commercial space at once. This proposed amendment will allow us to build out the commercial component in a phased approach by commencing with one of the commercial buildings first (prior to or concurrent with the residential condominiums) to be followed by the second commercial building in a later phase as market conditions and financing dictate. As an example, DeLo Plaza, the 14,000 square foot retail center, is still experiencing high retail vacancy since the building was completed in 2017. In addition to existing vacancy at DeLo Plaza, two approved pad commercial buildings have yet to be constructed on site.

This proposed amendment to the PUD and resolution of approval will provide clarity in the development schedule and allow us to commence construction on one commercial building and the residential portion of the Foundry project. Specifically, we request the following amendment to the Foundry PUD, and associated resolution of approval to reflect the following conditions:

- Residential building permits for the condominiums approved concurrent with or subsequently after approval of the building permit for one of the two commercial buildings; and
- Last Certificate of Occupancy (C/O) for one of the residential 8-plex buildings is withheld until: (1) start of construction of the first commercial building commences, which, per International Building Code utilized by the City of Louisville, is defined as the first placement of permanent construction of the principal building, such as pouring of a slab or footings, installation of pilings or construction of columns; and (2) 30% of the net leasable space has identified tenants with proof being an executed Letter of Intent (LOI) coupled with a security deposit
- An acknowledgement of an estoppel on the residential and commercial building permits CD's as submitted under the 2012 ICC Building Code that was stamped by the City, coupled with ongoing City review to work together to finalize building permit approvals

Please feel free to contact me anytime at 303-475-2106 or at justin@foundrybuilders.com with any questions or if we can be of further assistance.

We look forward to advancing the Foundry project and completing this final phase of the Steel Ranch neighborhood.

Best Regards,



1.18.19

Justin McClure
President

**RESOLUTION NO. 3
SERIES 2016**

A RESOLUTION APPROVING A REZONING, FINAL PLAT AND FINAL PLANNED UNIT DEVELOPMENT (PUD) TO CONSTRUCT A MULTI-USE DEVELOPMENT CONSISTING OF 24 AGE RESTRICTED CONDOMINIUMS, 8 NON-RESTRICTED CONDOMINIUMS, AND 38,000 SF COMMERCIAL/OFFICE.

WHEREAS, there has been submitted to the Louisville Planning Commission an application approving a rezoning, final Plat and final Planned Unit Development (PUD) to construct a multi-use development consisting of 24 age restricted condominiums, 8 non-restricted condominiums, and 38,000 sf commercial/office; and

WHEREAS, the City Staff has reviewed the information submitted and found that, subject to conditions, the application complies with the Louisville zoning and subdivision regulations and other applicable sections of the Louisville Municipal Code; and;

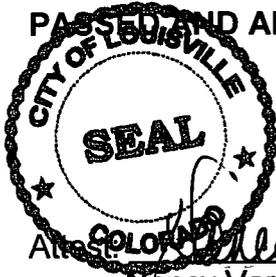
WHEREAS, after a duly noticed public hearing on December 10, 2015, where evidence and testimony were entered into the record, including the findings in the Louisville Planning Commission Staff Report dated December 10, 2015, the Planning Commission recommends the PUD for the Foundry to City Council, with the following conditions:

1. The 24 age restricted condominiums shall be for ages 55 and older. The 55 years and older age restriction shall be placed on the deed of each age restricted unit and shall also be included in the subdivision agreement, and a covenant agreement enforceable by the City of Louisville.
2. Staff recommends the wall signs of the In-line building, shown as vertical address numbers, be removed from the PUD and all wall signs must comply with Chapter 7 of the CDDSG and Chapter 17.24 of the LMC.
3. The applicant shall continue to work with the Parks Department on the type and location of additional trees along Highway 42, prior to recordation.
4. The applicant shall continue to work with the Public Works Department on the items listed in the October 25, 2015 memo. Each item shall be completed prior to recordation.
5. Residential and Commercial Development shall be constructed concurrently.

NOW THEREFORE, BE IT RESOLVED that the City Council of the City of Louisville, Colorado does hereby approve Resolution No. 9, Series 2016, a resolution approving a rezoning, final Plat and final Planned Unit Development (PUD) to construct a multi-use development consisting of 24 age restricted condominiums, 8 non-restricted condominiums, and 38,000 sf commercial/office, with the following conditions:

1. The 24 age restricted condominiums shall be for ages 55 and older. The 55 years and older age restriction shall be placed on the deed of each age restricted unit and shall also be included in the subdivision agreement, and a covenant agreement enforceable by the City of Louisville.
2. Staff recommends the wall signs of the In-line building, shown as vertical address numbers, be removed from the PUD and all wall signs must comply with Chapter 7 of the CDDSG and Chapter 17.24 of the LMC.
3. The applicant shall continue to work with the Parks Department on the type and location of additional trees along Highway 42, prior to recordation.
4. The applicant shall continue to work with the Public Works Department on the items listed in the October 25, 2015 memo. Each item shall be completed prior to recordation.
5. Residential and Commercial Development shall be constructed concurrently.
6. Add a sentence to the PA-1B General Notes, item 1, stating "This PUD authorizes only condominium project type development." Further, revise the phrase "a potential amount of units" to state instead "24 units."

PASSED AND ADOPTED this 19th day of January, 2016.



Nancy Varra

Nancy Varra, City Clerk
City of Louisville, Colorado

By: *Robert P. Muckle*

Robert P. Muckle, Mayor
City of Louisville, Colorado

03020146
 10/27/2015 10:30 AM
 81 822338
 Boulder County Clerk, CO



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
 A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

LAND USE SUMMARY

BLOCK/TRACT	TOTAL AREA		OWNERSHIP	PRIMARY USES	MAINTENANCE ¹
	ACRES	SF			
TRACT A	0.45 AC	19,561 SF	LOUISVILLE PROPERTIES 1, INC.	OUTDOOR USES, PUBLIC AND PRIVATE UTILITY EASEMENTS, PUBLIC ACCESS AND PARKING	THE FOUNDRY RESIDENTIAL OWNERS ASSOCIATION T.F.F.O.A
TRACT B	0.22 AC	9,750 SF	LOUISVILLE PROPERTIES 1, INC.	PRIVATE PEDESTRIAN PLAZA, OUTDOOR USES PUBLIC AND PRIVATE UTILITY EASEMENTS, PUBLIC ACCESS	THE FOUNDRY RESIDENTIAL OWNERS ASSOCIATION T.F.F.O.A
TRACT C	1.03 AC	44,949 SF	STEEL STREET COMMERCIAL, INC.	OUTDOOR USES, PUBLIC AND PRIVATE UTILITY EASEMENTS, PUBLIC ACCESS AND PARKING	THE FOUNDRY COMMERCIAL OWNERS ASSOCIATION T.F.F.O.A
TRACT D	0.67 AC	29,207 SF	STEEL STREET COMMERCIAL, INC.	OUTDOOR USES, PUBLIC AND PRIVATE UTILITY EASEMENTS, PUBLIC ACCESS AND PARKING	THE FOUNDRY COMMERCIAL OWNERS ASSOCIATION T.F.F.O.A
TRACT E	0.34 AC	14,597 SF	STEEL STREET COMMERCIAL, INC.	OUTDOOR USES, PUBLIC AND PRIVATE UTILITY EASEMENTS, PUBLIC ACCESS AND PARKING	THE FOUNDRY COMMERCIAL OWNERS ASSOCIATION T.F.F.O.A
BLOCK 1	0.44 AC	18,958 SF	LOUISVILLE PROPERTIES 1, INC.	RESIDENTIAL USE	THE FOUNDRY RESIDENTIAL OWNERS ASSOCIATION T.F.F.O.A
BLOCK 2	0.56 AC	24,547 SF	LOUISVILLE PROPERTIES 1, INC.	RESIDENTIAL USE	THE FOUNDRY RESIDENTIAL OWNERS ASSOCIATION T.F.F.O.A
BLOCK 3	0.49 AC	21,164 SF	LOUISVILLE PROPERTIES 1, INC.	RESIDENTIAL USE	THE FOUNDRY RESIDENTIAL OWNERS ASSOCIATION T.F.F.O.A
BLOCK 4	0.54 AC	23,317 SF	LOUISVILLE PROPERTIES 1, INC.	RESIDENTIAL USE	THE FOUNDRY RESIDENTIAL OWNERS ASSOCIATION T.F.F.O.A
BLOCK 5	0.53 AC	23,145 SF	STEEL STREET COMMERCIAL, INC.	COMMERCIAL USE	THE FOUNDRY COMMERCIAL OWNERS ASSOCIATION T.F.F.O.A
BLOCK 6	0.50 AC	21,729 SF	STEEL STREET COMMERCIAL, INC.	COMMERCIAL USE	THE FOUNDRY COMMERCIAL OWNERS ASSOCIATION T.F.F.O.A
BLOCK 7	0.06 AC	2,627 SF	LOUISVILLE PROPERTIES 1, INC.	RESIDENTIAL USE	THE FOUNDRY RESIDENTIAL OWNERS ASSOCIATION T.F.F.O.A
TOTAL	5.82 AC	253,561 SF			

¹REFER TO SUBDIVISION AGREEMENT FOR DELINEATION OF MAINTENANCE RESPONSIBILITIES.

LEGAL DESCRIPTION:

A REPLAT OF LOT 1, BLOCK 9 AND TRACT 1 OF TAKODA SUBDIVISION WITH A RECEPTION NUMBER OF 02103584 AND LOT 2 OF SUMMIT VIEW SUBDIVISION WITH A RECEPTION NUMBER 2216330 AND BEING PART OF SOUTHEAST 1/4 OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH P.M., CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO.

AREA - 5.82 ACRES

NOTES

1. SURVEY BY ROCK CREEK SURVEYING, DATED AUGUST 23, 2013.
2. COMMERCIAL CONSTRUCTION IN PLANNING AREA 1 SHALL COMMENCE PRIOR TO OR CONCURRENT WITH ANY RESIDENTIAL BUILDING PERMITS.

PROJECT DESCRIPTION

GROSS PROPERTY AREA: ±5.82 AC
 CURRENT ZONING: PC2D-C: ±5.82 AC

PROPOSED ZONING: PLANNED COMMUNITY ZONED DISTRICT (CITY OF LOUISVILLE)
 PC2D-CR: ±5.82 AC

MAX. APPROVED FLOOR AREA (PER TAKODA GOP SRD AMENDMENT): 0.30 FAR
 MAX. APPROVED COMMERCIAL SF TOTAL WITHIN THIS FINAL DEVELOPMENT PLAN: 0.30 FAR (76,055 S.F.)

MAX. APPROVED NUMBER OF RESIDENTIAL UNITS IS 33 UNITS (75% OF THE RESIDENTIAL UNITS SHALL BE AGE RESTRICTED.)

ACCESS: HIGHWAY 42 (FULL MOVEMENT SIGNALIZED AT PASCHAL DRIVE)
 HIGHWAY 42 (1/2 MOVEMENT AT SUMMIT VIEW DRIVE)
 HIGHWAY 42 (RIGHT IN / RIGHT OUT MOVEMENT)
 PASCHAL DRIVE (RIGHT IN / RIGHT OUT MOVEMENT)
 SUMMIT VIEW DRIVE (FULL MOVEMENT)
 KAYLIX AVENUE (FULL MOVEMENT)

CITY COUNCIL SIGNATURE BLOCK

APPROVED THIS 14th DAY OF January 2016, BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO
 Mayor: *Justin McClell* City Clerk: *Michelle Walker*



PLANNING COMMISSION CERTIFICATION

RECOMMENDED APPROVAL THIS 16th DAY OF December 2015 BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO. RESOLUTION NO. 39 SERIES 2015

CLERK & RECORDER CERTIFICATE - COUNTY OF BOULDER, STATE OF COLORADO

I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT 9:00 A.M. THIS DAY OF June 2015 AND IS RECORDED IN PLAN FILE _____ FEE _____ FILM NO. _____ RECEPTION _____

RECORDED: _____ DEPUTY: _____

OWNERSHIP SIGNATURE BLOCK

BY SIGNING THIS FDP/PUD, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS FDP/PUD. WITNESS OUR HANDS AND SEALS THIS 14th DAY OF June 2015.

OWNER: LOUISVILLE PROPERTIES 1, INC., A COLORADO CORPORATION

BY: *Justin McClell* PRESIDENT
 NOTARY: *Alexander T. Carlson*

(NOTARY SEAL)



OWNERSHIP SIGNATURE BLOCK

BY SIGNING THIS FDP/PUD, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS FDP/PUD. WITNESS OUR HANDS AND SEALS THIS 14th DAY OF June 2015.

OWNER: STEEL STREET COMMERCIAL, INC., A COLORADO CORPORATION

BY: *Justin McClell* PRESIDENT
 NOTARY: *Alexander T. Carlson*

(NOTARY SEAL)



SHEET INDEX

- NUMBER SHEET TITLE
- 1 COVER SHEET
- 2 MASTER PLAN AND GENERAL NOTES
- 3 RESIDENTIAL AREA - PLAN, NOTES AND STANDARDS
- 4 COMMERCIAL AREA - PLAN, NOTES AND STANDARDS
- 5 SITE PHOTOMETRIC
- 6 LIGHTING DETAILS
- 7 MASTER LANDSCAPE PLAN
- 8 LANDSCAPE ENLARGEMENT
- 9 EMERGENCY VEHICLE ACCESS PLAN AND TURNING MOVEMENT DETAILS
- 10 HORIZONTAL PUBLIC IMPROVEMENT PLAN
- 11 HORIZONTAL CONTROL PLAN
- 12 UTILITY PLAN
- 13 GRADING PLAN
- 14 ELEVATIONS COMMERCIAL IN-LINE
- 15 ELEVATIONS COMMERCIAL IN-LINE
- 16 ELEVATIONS COMMERCIAL FLEX*
- 17 ELEVATIONS COMMERCIAL FLEX*
- 18 ELEVATIONS RESIDENTIAL BUILDING A
- 19 ELEVATIONS RESIDENTIAL BUILDING B
- 20 ELEVATIONS RESIDENTIAL GARAGES
- 21 SITE DETAILS
- 22 SITE SKETCHES

* NEW SHEETS IN SECOND SUBMITTAL.

VICINITY MAP



S. BOULDER RD.
 KEY MAP

OWNER'S REP
 RMCS, INC.
 21 SOUTH SUNSET STR.
 LONGMONT, CO
 80503
 P. 720.524.3620

PLANNING & LA
 PCS GROUP, INC.
 1001 16TH STR., 38-180
 DENVER, CO
 80205
 P. 303.531.4905
 WWW.PCSGROUP.CO.COM

ENGINEERING
 J3 ENGINEERING
 CONSULTANTS
 2011 CHERRY STREET
 SUITE 206
 LOUISVILLE, CO 80027
 P. 720.975.0177

ARCHITECTURE
 OZ ARCHITECTURE
 3003 LARIMER STREET
 DENVER, CO
 80205
 P. 303.861.5704
 WWW.OZARCH.COM

NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA
 Checked By: PMS
 Drawn By: KLM
 Project #: 03000701



**FINAL DEVELOPMENT PLAN
 PLANNED UNIT DEVELOPMENT**

SHEET 1 OF 22

COVER SHEET



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
 A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

BULK AND DIMENSION STANDARDS

	RESIDENTIAL	COMMERCIAL
MIN. LOT AREA	MF: NA	1,500 SF
MIN. LOT WIDTH	55'	30'
MAX. LOT COVERAGE	NA	NA
BUILDING SETBACKS**		
MIN. FRONT YARD SETBACK (PRINCIPAL USES)	5' (ALL CONDITIONS)	0'
MIN. SIDE YARD SETBACK* (PRINCIPAL USES)	0' (IN BLDG) 5' (ALL OTHER CONDITIONS)	0'
MIN. SIDE YARD SETBACK* (ACCESSORY USES)	0'	0'
MIN. REAR YARD SETBACK (PRINCIPAL USES)	5'	5'
MIN. REAR YARD SETBACK (ACCESSORY USES)	0'	0'
SETBACK FROM HWY 42 ROW	NA	PARKING: 20' BUILDING: 10'
SETBACK FROM COLLECTOR STREET ROW	PARKING: 10' BUILDING: 10'	PARKING: 10' BUILDING: 10'
SETBACK FROM LOCAL STREET ROW	PARKING: 5' BUILDING: 10'	NA
SETBACK FROM GREEN WAYS AND OPEN SPACE	PARKING: 0' BUILDING: 0'	PARKING: 0' BUILDING: 0'
MIN. BUILDING SEPARATION	10'	10'
MAX. BUILDING HEIGHT		
PRINCIPAL USES	MF: 35'	35'
ACCESSORY USES*	20'	20'

*IF FEE SIMPLE LOTS ARE CREATED WITHIN BUILDINGS, THERE IS NO SETBACK REQUIREMENT BETWEEN INTERNAL UNITS.
 **ACCESSORY USES TO INCLUDE GARAGES.
 *NO MAXIMUM BUILDING SETBACKS ARE REQUIRED
 *CORNER, CANOPY, EAVE, PATIO, FIRE PLACE, WING WALL OR SIMILAR ARCHITECTURAL FEATURE MAY EXTEND 3 FEET INTO A REQUIRED SETBACK.

DEVELOPMENT SUMMARY

GROSS DEVELOPABLE LAND AREA	MAXIMUM # OF UNITS	DENSITY	MAXIMUM FAR	MAXIMUM ALLOWABLE COMMERCIAL
5.82 ACRES	32	5.50 DU/AC	0.30	76,055 SF

GENERAL NOTES AND STANDARDS

- THE REQUIRED LAND DEDICATION SHALL BE MET VIA A CASH IN LIEU PAYMENT CONSISTENT WITH SECTION 16.16(B) OF THE CITY CODE.
- PROPOSED PEDESTRIAN AREAS ARE INTENDED TO ASSIST IN THE CONNECTION OF STEEL RANCH PARK, TO THE BULL HEAD GULCH OPEN SPACE TRAIL SYSTEM, AND TO DOWNTOWN LOUISVILLE, AS WELL AS, CONNECTING TO THE NEIGHBORING STEEL RANCH AND NORTHERN COMMUNITIES.
- THE DEVELOPER MAY CONDUCT ACTIVITIES (INCLUDING BUT NOT LIMITED TO GRADING) ON ALL DEDICATED LANDS FOR THE PURPOSE OF CONSTRUCTING PUBLIC AND PRIVATE IMPROVEMENTS.
- ENTRY MONUMENTS, PROJECT IDENTITY, ILLUSTRATIVE RENDERINGS AND WAY-FINDING SIGNAGE ARE CONCEPTUAL IN NATURE AT THE LOCATIONS SHOWN WITHIN THIS DEVELOPMENT PLAN. FINAL LOCATIONS SHALL BE DETERMINED DURING THE CONSTRUCTION DOCUMENTATION PROCESS, BUT SHALL CONFORM TO THE STANDARDS WITHIN THIS DEVELOPMENT PLAN.
- NO HISTORIC STRUCTURES EXIST ON THE PROPERTY.
- THE LIGHTING CONFIGURATION, DESIGN, FIXTURE TYPES, ETC. AS DEPICTED HEREIN ARE SUBJECT TO FURTHER ANALYSIS, DESIGN AND AVAILABILITY, AND AS SUCH, MAY VARY FROM THE FINAL PUD TO FINAL CONSTRUCTION DOCUMENTS. PROPOSED LIGHTING WILL INCLUDE DIRECTIONAL COVERS AND SHALL BE DIRECTED AWAY FROM THE RESIDENCES WITHIN THE FOUNDRY AND STEEL RANCH. THE ILLUSTRATIONS AND ANNOTATIONS WITHIN THIS FINAL DEVELOPMENT PLAN SUPERSEDE ALL PREVIOUS ENTITLEMENT DOCUMENTS IN THE EVENT OF A CONFLICT.
- TIMING OF THE DEVELOPMENT SHALL BE DEPENDENT ON MARKET CONDITIONS.
- DRAINAGE, CONVEYANCE WATER QUALITY AND DETENTION HAS ALREADY BEEN ACCOMMODATED FOR THIS SITE WITHIN THE STEEL RANCH COMMUNITY AND THROUGH REGIONAL POND X.
- THE SIDEWALK AND LANDSCAPE IMPROVEMENTS (WEST OF THE SIDEWALK) ALONG HIGHWAY 42 FROM PASCHAL DRIVE TO SUMMIT VIEW DRIVE WILL BE COMPLETED PRIOR TO ANY CONSTRUCTION OR OCCUPANCY ASSOCIATED WITHIN THE DEVELOPMENT ARE ISSUED.
- IMPROVEMENTS DEPICTED ADJACENT TO AND WITHIN THE HIGHWAY 42 ROW SHALL BE REFINED WITHIN THE CONSTRUCTION DOCUMENTS TO ENSURE HORIZONTAL AND VERTICAL LOCATIONS AND ASSOCIATED CONSTRUCTABILITY.

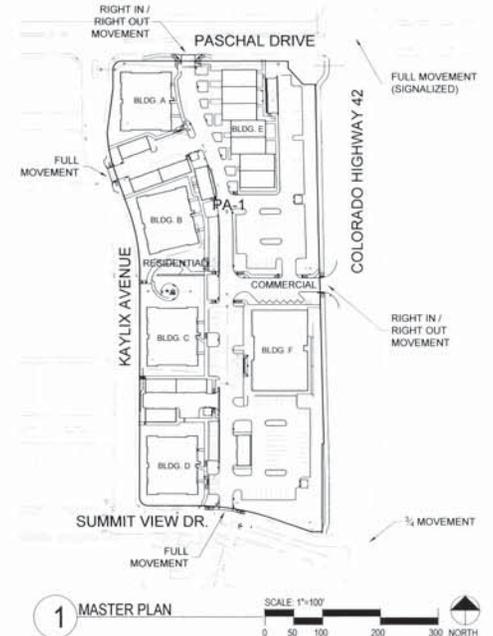
REQUESTED VARIANCE

VARIANCE	EXISTING CODE (CITY OF LOUISVILLE)	REQUEST	LOCATION
LANDSCAPE	PLANT STREET TREES 20' ON CENTER (COOSG)	PLANT AS NECESSARY TO AVOID EXISTING EASEMENTS AND SIGHT LINES.	HWY 42, PASCHAL DR, SUMMIT VIEW DR. AND KAYLIX AVE.

USE CHART

PRINCIPAL USE GROUP	USES	LOCATION
RESIDENTIAL	MULTI-UNIT FOR SALE DWELLINGS	BLOCK 1, 2, 3, 4 & 7
COMMERCIAL	PROFESSIONAL AND BUSINESS OFFICES, PERSONAL SERVICES, EATING AND DRINKING ESTABLISHMENTS, RETAIL & RETAIL CONVENIENCE GOODS	BLOCK 5 & 6

*ALL USES PER SECTION 17.72.006 ARE ALLOWED WHICH MAY FURTHER THE DIVERSITY AND RANGE OF USES WITHIN THE PROJECT.



1 MASTER PLAN SCALE: 1"=100' 0 50 100 200 300 NORTH



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 21 SOUTH SUNSET STR.
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 WWW.PCSGROUP.CO

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 J3 ENGINEERING CONSULTANTS
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 OZ ARCHITECTURE
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THE FOUNDRY

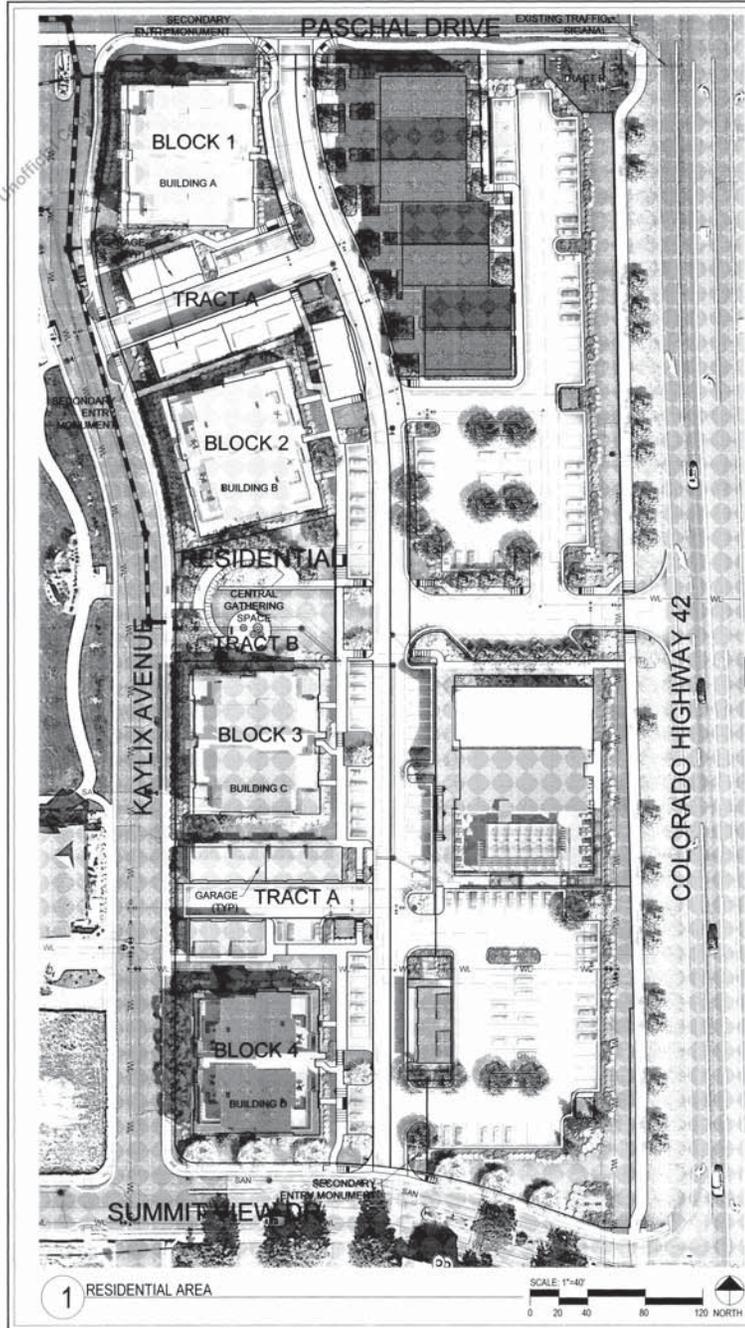
NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

**FINAL DEVELOPMENT PLAN
 PLANNED UNIT DEVELOPMENT**

SHEET 2 OF 22
 MASTER PLAN AND GENERAL NOTES

Designed By: SA
 Checked By: PMS

Drawn By: KLM
 Project #: 03000701



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
 A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

MULTI UNIT DWELLING SUMMARY

LAND USE FOR SALE MULTI DWELLING UNITS (PROPOSED)	PC2D-CR 32 D.U.
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PARKING SUMMARY

RESIDENTIAL CRITERIA	DWELLING UNITS	REQUIRED	PROVIDED	PARKING RATIO
2 BEDROOM & LARGER X 2 SP/ DU	32	64	PRIVATE GARAGE *32 OFF STREET 32	2.0
		64	64	

BULK AND DIMENSION STANDARDS

	RESIDENTIAL
MIN. LOT AREA	MF: NA
MIN. LOT WIDTH	55'
MAX. LOT COVERAGE	NA
BUILDING SETBACKS**	
MIN. FRONT YARD SETBACK (PRINCIPAL USES)	5' (ALL CONDITIONS)
MIN. SIDE YARD SETBACK* (PRINCIPAL USES)	0' (IN BLDG) 5' (ALL OTHER CONDITIONS)
MIN. SIDE YARD SETBACK* (ACCESSIBLE USES)	0'
MIN. REAR YARD SETBACK (PRINCIPAL USES)	5'
MIN. REAR YARD SETBACK (ACCESSORY USES)	0'
SETBACK FROM HWY 42 ROW	NA
SETBACK FROM COLLECTOR STREET ROW	PARKING: 10' BUILDING: 10'
SETBACK FROM LOCAL STREET ROW	PARKING: 5' BUILDING: 10'
SETBACK FROM GREEN WAYS AND OPEN SPACE	PARKING: 0' BUILDING: 0'
MIN. BUILDING SEPARATION	10'
MAX. BUILDING HEIGHT	
PRINCIPAL USES	MF: 35'
ACCESSORY USES*	20'

- GENERAL NOTES - RESIDENTIAL AREA
- INTENT: RESIDENTIAL AREA IS INTENDED TO BE INTEGRATED INTO THE NEIGHBORHOOD WITHIN STEEL RANCH. THE CONDOMINIUM PRODUCT TYPES LENDS ITSELF TO THE COMMUNITY BY ADDING TO THE DIVERSITY OF HOUSING WITH 75% OF UNITS BEING AGE RESTRICTED.
 - THE RESIDENTIAL AREA SHALL ACCOMMODATE UP TO 32 DWELLING UNITS, ASSOCIATED PARKING, AND GREENSPACE AREAS. THE UNIT COUNT MAY VARY DEPENDING ON FINAL HOUSING PRODUCT DESIGN, BUT IN NO EVENT SHALL THE TOTAL NUMBER OF DWELLING UNITS EXCEED 32.
 - THE RESIDENTIAL AREA WILL BE ORGANIZED BY A PRIMARY POINT OF ACCESS KNOWN AS SUMMIT VIEW DRIVE, AND THREE SECONDARY ACCESS POINTS OFF PASCHAL DRIVE, KAYLIX AVENUE AND HIGHWAY 42. THE SITE LAYOUT ENHANCES ACCESS TO COMMON OPEN AREAS, GREEN SPACES, AND MULTI-MODAL TRANSPORTATION OPPORTUNITIES.
 - REFER TO THE SUBDIVISION AGREEMENT FOR DELINEATION OF MAINTENANCE RESPONSIBILITIES.
 - BUILDINGS MAY BE BUILT AT ONE, TWO, OR THREE STORY HEIGHTS, OR COMBINATIONS THEREOF, SUBJECT TO THE MAXIMUM BUILDING HEIGHT REQUIREMENTS.
 - THE RESIDENTIAL AREA MAY HAVE ADDITIONAL CONDITIONS, COVENANTS AND RESTRICTIONS (CCRS) TO BE DEVELOPED BY THE FOUNDRY'S COMMERCIAL OWNERS ASSOCIATION (F.C.O.A.).
 - SITE SPECIFIC BULK AND DIMENSION STANDARDS ON THIS SHEET SHALL GOVERN DEVELOPMENT OF THE RESIDENTIAL AREA.
 - LANDSCAPE DEPICTED WITHIN THIS SHEET IS CONCEPTUAL IN NATURE AND MAY BE REVISED WITHIN THE CONSTRUCTION DOCUMENTS.
 - LOW IMPACT TO SCHOOLS AND PUBLIC SERVICES WILL ENSURE BECAUSE OF THE AGE TARGETED NATURE OF THE PROJECT AND PRODUCT TYPE PROPOSED.
 - THE REQUIRED LAND DEDICATION WITHIN THE TAKODA GDP 3RD AMENDMENT SHALL BE MET VIA A CASH IN LIEU PAYMENT CONSISTENT WITH SECTION 16.16.608 OF THE CITY CODE.
 - TRACTS DESIGNED AS AMENITY SPACES SHALL BE AVAILABLE FOR PUBLIC USE, AND WILL BE FUNDED AND MAINTAINED PRIVATELY BY THE FOUNDRY HOA.
 - TRASH ENCLOSURE LOCATIONS WILL BE FINALIZED AND COORDINATED DURING THE CONSTRUCTION DOCUMENT PHASE.
 - BLOCKS AND TRACTS DO NOT CONSTITUTE A FIRM DELINEATION OF PARKING AND SHALL BE SHARED BETWEEN RESIDENTIAL AND COMMERCIAL USES.

*IF FEE SIMPLE LOTS ARE CREATED WITHIN BUILDINGS, THERE IS NO SETBACK REQUIREMENT BETWEEN INTERNAL UNITS.
 **ACCESSORY USES TO INCLUDE GARAGES.
 NO MAXIMUM BUILDING SETBACKS ARE REQUIRED.
 *CORNICHE, CANOPY, LEAVE PATIO, FIRE PLACE, WING WALL OR SIMILAR ARCHITECTURAL FEATURE MAY EXTEND 3 FEET INTO A REQUIRED SETBACK.



**FINAL DEVELOPMENT PLAN
 PLANNED UNIT DEVELOPMENT**

SHEET 3 OF 22

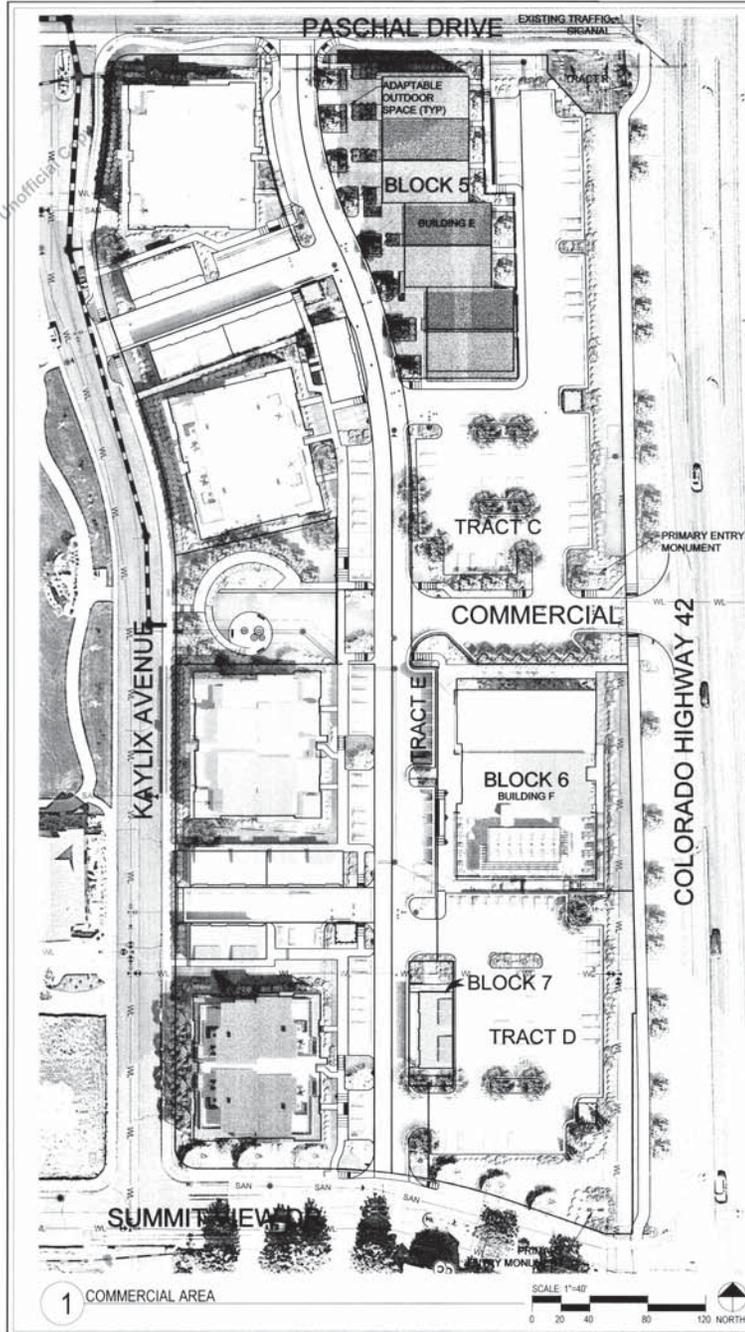
RESIDENTIAL AREA -
 PLAN, NOTES AND STANDARDS

OWNER'S REP RMCS, INC. 21 SOUTH SUNSET STR. LONGMONT, CO 80503 P. 720.524.3620	PLANNING & LA PCS GROUP, INC. 1001 16TH STR, 3B-180 DENVER, CO 80255 P. 303.531.4905 WWW.PCSGROUP.CO	ENGINEERING J3 ENGINEERING CONSULTANTS 2011 CHERRY STREET SUITE 206 LOUISVILLE, CO 80027 P. 720.975.0177	ARCHITECTURE OZ ARCHITECTURE 3003 LARIMER STREET DENVER, CO 80205 P. 303.861.5704 WWW.OZARCH.COM
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NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA Drawn By: KJM
 Checked By: PMS Project #: 03000701





FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT

A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

PARKING SUMMARY

COMMERCIAL CRITERIA	GROSS LEASABLE AREA (GLA) ¹ REQUIRED	PROVIDED	PARKING RATIO
(IN LINE COMMERCIAL) 1 SP/300 SF	X 2 STORES = 17,850 SF	60	OFF STREET=151
(FLEX COMMERCIAL) 1 SP/300 SF	14,110 SF	47	ADAPTABLE OUTDOOR=14
TOTAL	31,960 SF	107	165

¹GLA IS ASSUMED TO BE 85% OF TOTAL COMMERCIAL AREA PROPOSED (37,600 SF)

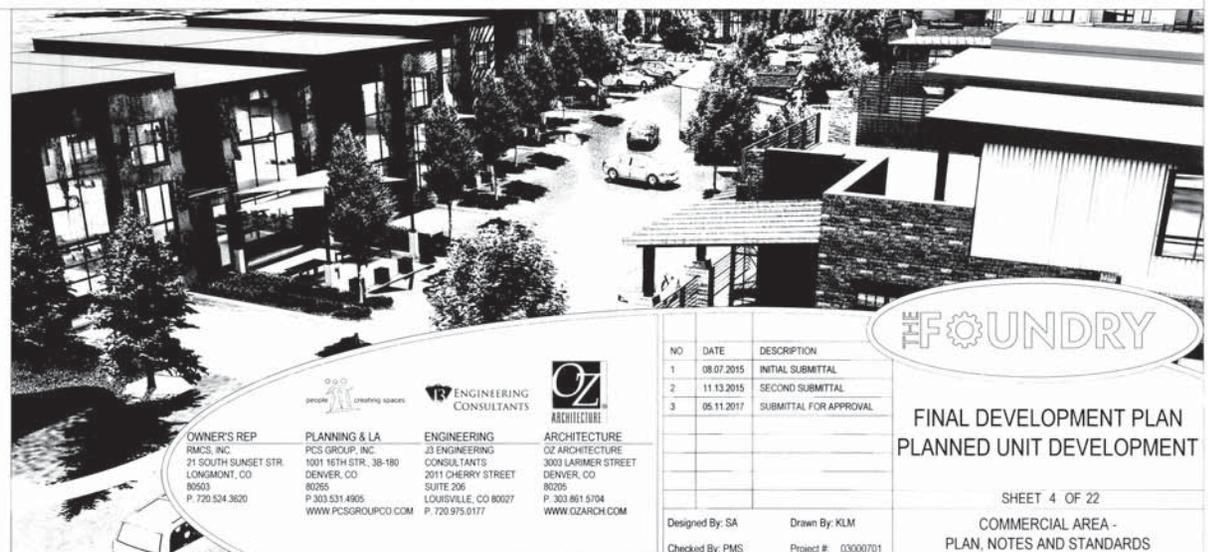
BULK AND DIMENSION STANDARDS

	COMMERCIAL
MIN. LOT AREA	1,500 SF
MIN. LOT WIDTH	30'
MAX. LOT COVERAGE	NA
BUILDING SETBACKS¹¹	
MIN. FRONT YARD SETBACK (PRINCIPAL USES)	0'
MIN. SIDE YARD SETBACK ¹ (PRINCIPAL USES)	0'
MIN. SIDE YARD SETBACK (ACCESSIBLE USES)	0'
MIN. REAR YARD SETBACK (PRINCIPAL USES)	5'
MIN. REAR YARD SETBACK (ACCESSORY USES)	0'
SETBACK FROM HWY 42 ROW	PARKING: 20' BUILDING: 10'
SETBACK FROM COLLECTOR STREET ROW	PARKING: 10' BUILDING: 10'
SETBACK FROM LOCAL STREET ROW	NA
SETBACK FROM GREEN WAYS AND OPEN SPACE	PARKING: 0' BUILDING: 0'
MIN. BUILDING SEPARATION	10'
MAX. BUILDING HEIGHT	
PRINCIPAL USES ¹	35'
ACCESSORY USES ¹	20'

¹¹ IF FEE SIMPLE LOTS ARE CREATED WITHIN BUILDINGS, THERE IS NO SETBACK REQUIREMENT BETWEEN INTERNAL UNITS.
¹ NO MAXIMUM BUILDING SETBACKS ARE REQUIRED.
¹ CORNICE, CANOPY, EAVE, PATIO, FIRE PLACE, WING WALL OR SIMILAR ARCHITECTURAL FEATURE MAY EXTEND 3 FEET INTO A REQUIRED SETBACK.

GENERAL NOTES - COMMERCIAL AREA

1. THE COMMERCIAL AREA IS INTENDED TO ACCOMMODATE COMMERCIAL / OFFICE USES, RETAIL AND RESTAURANT USES AND TO ALLOW RESIDENTS OPPORTUNITIES TO WALK SHORT DISTANCES TO WORK, SHOP, OR RECREATE AT THE NEIGHBORHOOD LEVEL ENJOYING THE USES ALLOWED WITH IN STEEL RANCH.
2. USES ALLOWED BY RIGHT: RESTAURANTS, COFFEE SHOPS, CHILD AND DAYCARE FACILITIES, GYMNASIUMS AND FITNESS CENTERS, RETAIL, SALES, PERSONAL SERVICE SHOPS, MEDICAL CLINICS, SMALL ANIMAL VETERINARY CLINICS AND OTHER USES COMPATIBLE WITH THE ABOVE. ALLOWED USES INCLUDE THOSE APPROVED PER THE GDP 3RD AMENDMENT. ALL USES PER CITY OF LOUISVILLE CODE 17.72.090 ARE ALLOWED, WITH THE EXCLUSION OF THE FOLLOWING USES: AUTO SALES, MAJOR AUTOMOTIVE REPAIRS, INCLUDING PAINTING AND AUTO BODY AND TRANSPORTATION TERMINALS.
3. MAXIMUM BUILDING HEIGHT IS 35'. BUILDING HEIGHT SHALL BE MEASURED FROM THE AVERAGE FINISH GRADE TO THE RIDGELINE OF THE ROOF PER CITY MUNICIPAL CODE.
4. REFER TO THE SUBDIVISION AGREEMENT FOR DELINEATION OF MAINTENANCE RESPONSIBILITIES.
5. BUILDINGS MAY BE BUILT AT ONE, TWO, OR THREE STORY HEIGHTS, OR COMBINATIONS THEREOF, SUBJECT TO THE MAXIMUM BUILDING HEIGHT REQUIREMENTS.
6. THE COMMERCIAL AREA MAY HAVE ADDITIONAL CONDITIONS, COVENANTS AND RESTRICTIONS (CCRS) TO BE DEVELOPED BY THE FOUNDRY'S COMMERCIAL OWNERS ASSOCIATION (T.F.C.O.A.).
7. REFER TO OWNERSHIP AND USE SUMMARY TABLE WITHIN THIS DOCUMENT FOR TRACT OWNERSHIP AND GENERAL MAINTENANCE INFORMATION.
8. LANDSCAPE ILLUSTRATIONS DEPICTED WITHIN THIS SHEET ARE CONCEPTUAL IN NATURE AND MAY BE REVISED WITHIN THE CONSTRUCTION DOCUMENTS AS LONG AS THE LANDSCAPE PLAN IS IN COMPLIANCE WITH THE COMMERCIAL DEVELOPMENT DESIGN STANDARDS AND GUIDELINES (CDDSG) AND THE VARIANCE HERE IN.
9. SITE SPECIFIC BULK AND DIMENSION STANDARDS ON THIS SHEET SHALL GOVERN DEVELOPMENT OF THE COMMERCIAL AREA.
10. TRASH ENCLOSURE LOCATIONS WILL BE FINALIZED AND COORDINATED DURING THE CONSTRUCTION DOCUMENT PHASE.
11. LIGHTING CONFIGURATION, DESIGN FIXTURES TYPES, ETC AS DEPICTED HEREIN IS SUBJECT TO FURTHER ANALYSIS DESIGN AND AVAILABILITY AND AS SUCH MAY VARY FROM THE FINAL PUD TO FINAL CONSTRUCTION DOCUMENTS.
12. ACCESSIBLE SIDEWALKS AND PEDESTRIAN WAYS SHALL BE PROVIDED THAT MEET ADA STANDARDS FOR RUNNING SLOPE AND CROSS SLOPE.
13. ENTRY MONUMENTS, PROJECT IDENTITY AND WAY FINDING SIGNAGE ARE CONCEPTUAL IN NATURE AT THE LOCATIONS DEPICTED WITHIN THIS DEVELOPMENT PLAN. FINAL LOCATIONS SHALL BE DETERMINED DURING THE CONSTRUCTION DOCUMENTATION PROCESS BUT SHALL CONFORM TO THE STANDARDS WITHIN THIS DEVELOPMENT PLAN.
14. BLOCKS AND TRACTS DO NOT CONSTITUTE A FIRM DELINEATION OF PARKING AND SHALL BE SHARED BETWEEN RESIDENTIAL AND COMMERCIAL USES.
15. THE ALIGNMENT OF THE SIDE WALK AT THE INTERSECTION OF HIGHWAY 42 AND PASCHAL DRIVE TO THE SUMMIT VIEW DRIVE INTERSECTION IS CONCEPTUAL DUE TO EXISTING UTILITIES AND WILL BE FURTHER REFINED WITHIN THE CONSTRUCTION DOCUMENTS.



**FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT**

SHEET 4 OF 22

COMMERCIAL AREA -
PLAN, NOTES AND STANDARDS

NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA
Checked By: PMS

Drawn By: KLM
Project #: 03000701

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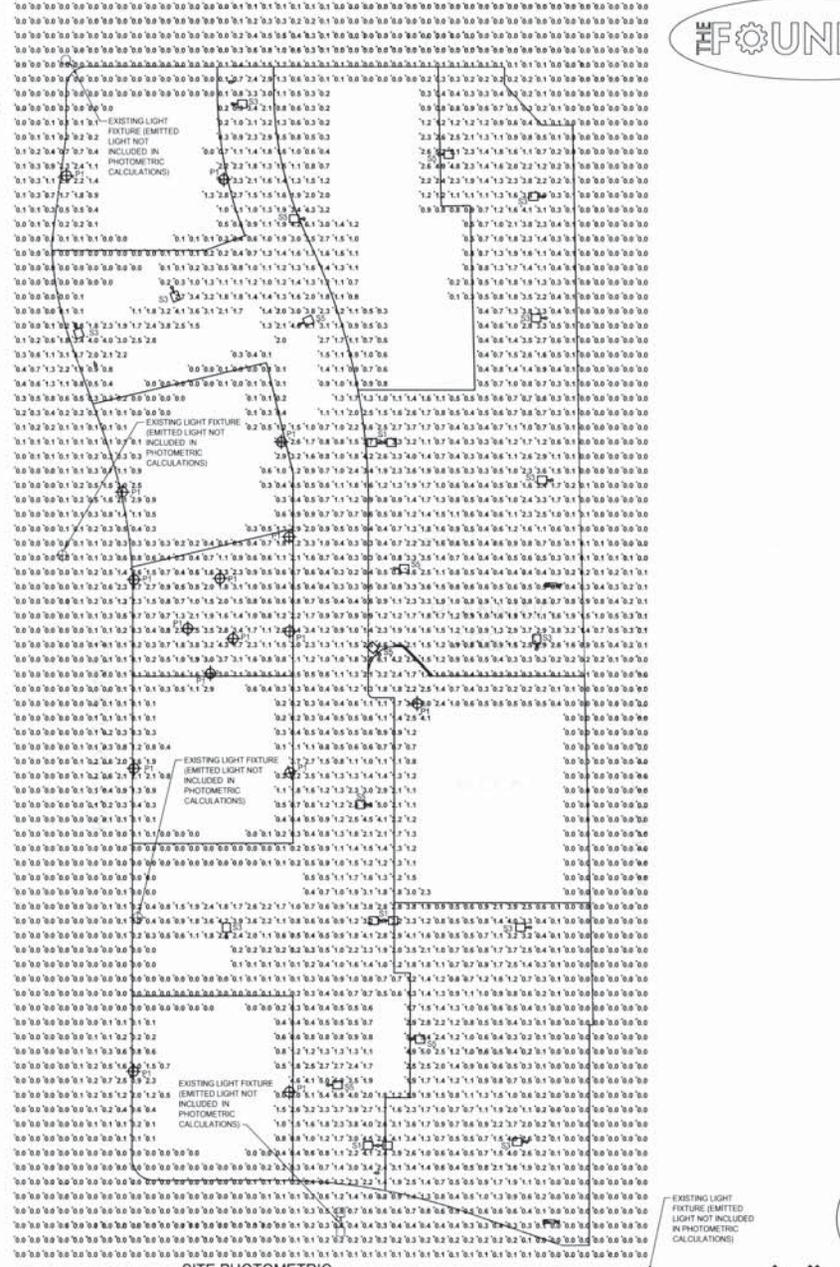
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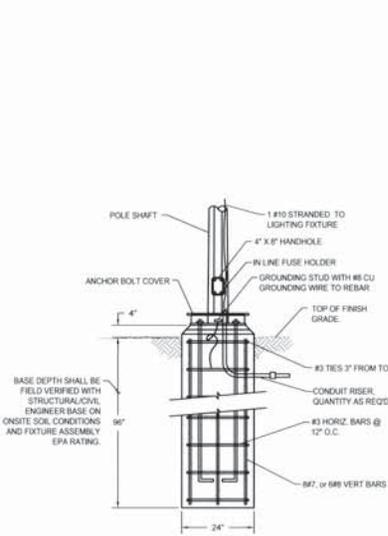


FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

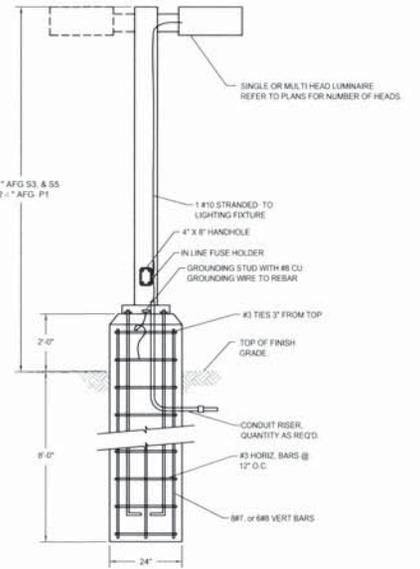
Unofficial Copy



1 SITE PHOTOMETRIC
SCALE: 1" = 40' 0"



1 POLE BASE DETAIL (LANDSCAPE)
NO SCALE



1 POLE BASE DETAIL (PARKING)
NO SCALE

LUMINAIRE SCHEDULE table with columns: KEY, DESCRIPTION, MFR. & CAT. NO., LAMP, VOLTS, MOUNTING INFORMATION, NOTE, MAX. WATT.

NOTES: 1. COORDINATE FINISH COLOR WITH ARCHITECT.

OWNER'S REP, PLANNING & LA, ENGINEERING CONSULTANTS, ARCHITECTURE logos and contact information.



FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 5 OF 22

Revision table with columns: NO, DATE, DESCRIPTION.

Designed By: SA, Checked By: PMS, Drawn By: KLM, Project #: 03000701

SITE PHOTOMETRIC

FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



SUGGESTED PLANT PALETTE

Table with 2 columns: BOTANICAL NAME and COMMON NAME. Lists various plant species such as Acer, Quercus, and Prunus.

LEGEND



GENERAL NOTES:

- 1. FINAL CONSTRUCTION DOCUMENTS SHALL CONFORM TO THE REQUIREMENTS OUTLINED WITHIN THIS PUD SUBMITTAL.
2. LANDSCAPE PLANS ARE SCHEMATIC IN NATURE AND SUBJECT TO MODIFICATION...
3. LOCATION OF LANDSCAPE PLANTINGS MAY BE ALTERED TO PROVIDE ADEQUATE CLEARANCE...
4. GRASS AREAS DESIGNATED AS IRRIGATED TURF SHALL BE SEEDING OR SOODED WITH A DROUGHT TOLERANT GRASS MIXTURE.
5. DECIDUOUS TREES WILL BE 2 1/2" CALIPER MINIMUM. EVERGREEN TREES WITH BE 6-8" HT. MINIMUM.
6. SHRUBS (DECIDUOUS AND EVERGREEN) WILL BE 5 GALLON MINIMUM.
7. MECHANICAL DEVICES AT GRADE SHALL BE SCREENED WITH LANDSCAPE MATERIAL.
8. FOR TREES PLANTED WITHIN 5 FEET OF PUBLIC STREETS AND WALKS ROOT BARRIER IS REQUIRED.
9. ALL LANDSCAPING WITHIN THE DIRECTLY ADJACENT STATE HIGHWAY 42, PASCHAL, SUMMIT VIEW, AND KAYLIX DRIVE ROW SHALL BE MAINTAINED BY THE HOA.
10. ALL SNOW REMOVAL FROM PUBLIC WALKS / TRAIL ADJACENT THE DEVELOPMENT SHALL BE PROVIDED BY THE HOA.

LANDSCAPE REQUIREMENTS

- 1. THE LANDSCAPE STANDARDS OUTLINED BELOW SUPERCEDE ANY LANDSCAPE REQUIREMENTS FROM PREVIOUS PUD SUBMITTALS AND ARE SPECIFIC TO THE FOUNDRY.
2. EACH PAD SITE SHOULD AVERAGE 10% LANDSCAPE AREA AND 10% FOR THE OVERALL PARCEL OF GROUND.
3. LANDSCAPED AREAS, INCLUDING AREAS WITHIN PRIVATE STREETS, SHALL BE PRIVATELY MAINTAINED BY THE FOUNDRY'S OWNERS ASSOCIATION.
4. WHEEL STOPS OR BUMPER CURBS WILL BE PROVIDED AS REQUIRED TO PROTECT SIDEWALKS, STRUCTURES AND PLANTERS FROM EXCESSIVE VEHICULAR DAMAGE.
5. INTERNAL LANDSCAPING - A MINIMUM OF FIVE PERCENT (5%) OF THE REQUIRED LANDSCAPE AREA SHOULD BE ALLOCATED TOWARDS BUILDING (OR FOUNDATION) LANDSCAPING. THIS SHOULD BE DRIP IRRIGATED AND XERIC IN NATURE. BUILDING LANDSCAPING PRIORITIES ARE THOSE THAT FACE THE PUBLIC RIGHT-OF-WAY.
6. LOCAL STREETS - ONE TREE PER 40 LINEAR FEET ADJACENT TO OFF STREET PARKING AREAS (WITH THE EXCEPTION OF EASEMENTS OR OTHER OBSTRUCTIONS).
7. COMMON AREAS - ONE TREE AND THREE SHRUBS PER 2,000 SQUARE FEET OF SOFTSCAPE OR A COMBINATION THEREOF AS AGREED UPON WITH THE PLANNING DEPARTMENT.

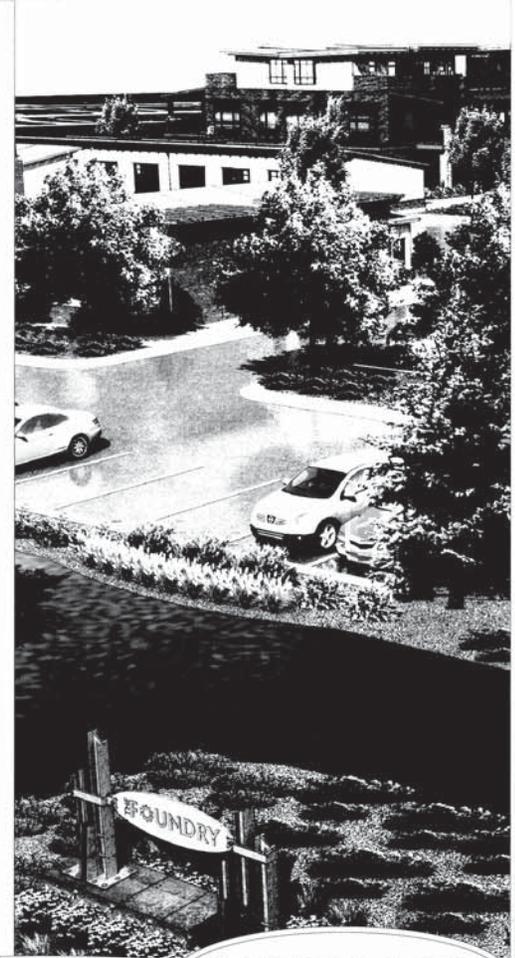
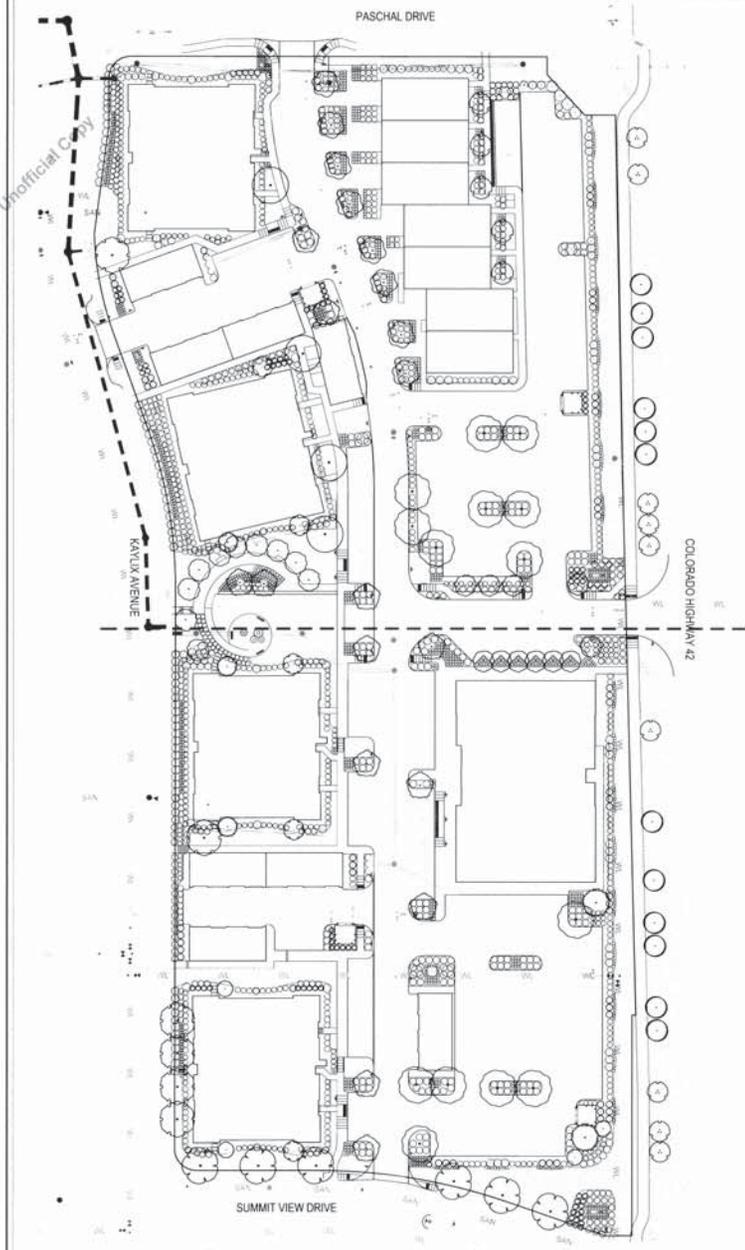


Table with 3 columns: NO, DATE, DESCRIPTION. Shows submission history: 1. 08.07.2015 INITIAL SUBMITTAL, 2. 11.13.2015 SECOND SUBMITTAL, 3. 05.11.2017 SUBMITTAL FOR APPROVAL.

FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 7 OF 22

MASTER LANDSCAPE PLAN

Designed By: SA
Checked By: PMS

Drawn By: KLM
Project #: 03000701

Logos for Planning & LA (PCS GROUP, INC.), Engineering Consultants (J3 ENGINEERING), and Architecture (OZ ARCHITECTURE).

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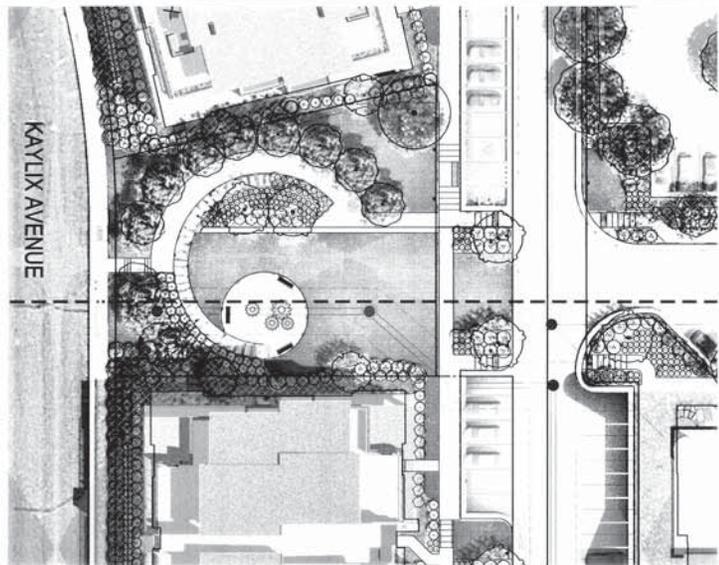
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1 MASTER LANDSCAPE PLAN





FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



- LEGEND
- EMPTY TREE
 - LANDSCAPED TREE
 - PERENNIAL PLANT
 - BIENNIAL PLANT
 - ANNUAL PLANT
 - ORNAMENTAL SHRUB
 - GRASS
 - SAND
 - MULCH
 - NATIVE BED

1 GREENWAY / PEDESTRIAN AMENITY



OWNER'S REP
RMC3, INC.
21 SOUTH SUNSET STR.
LONGMONT, CO
80503
P. 720.524.3620

PLANNING & LA
PCS GROUP, INC.
1001 16TH STR., 3B-180
DENVER, CO
80202
P. 303.531.4505
WWW.PCSGROUP.CO

ENGINEERING
J3 ENGINEERING
CONSULTANTS
2011 CHERRY STREET
SUITE 206
LOUISVILLE, CO 80027
P. 720.975.0177

ARCHITECTURE
OZ ARCHITECTURE
3003 LARIMER STREET
DENVER, CO
80205
P. 303.861.5704
WWW.OZARCH.COM

NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA Drawn By: KLM
Checked By: PMS Project #: 03000701



FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 8 OF 22

LANDSCAPE ENLARGEMENT

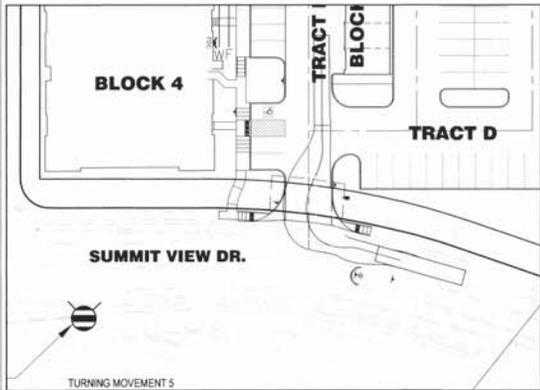
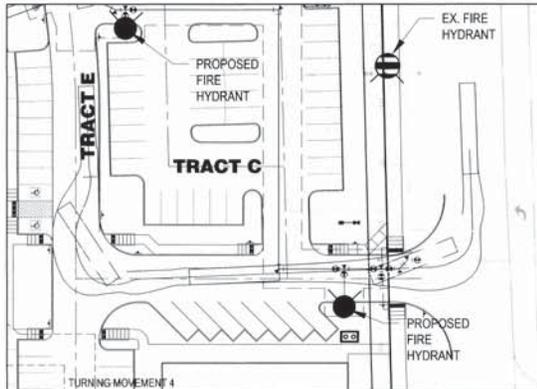
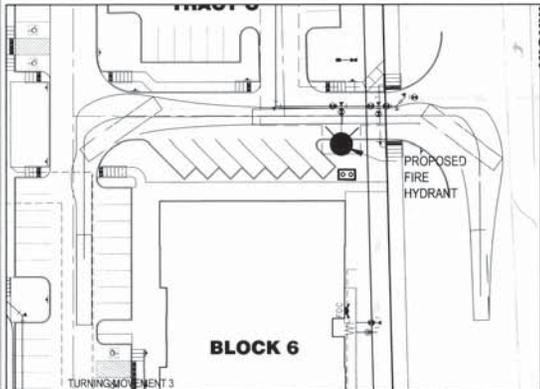
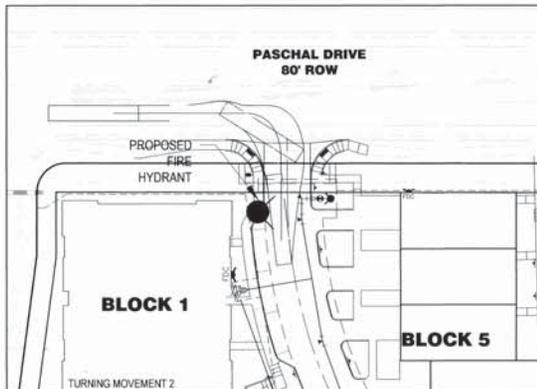
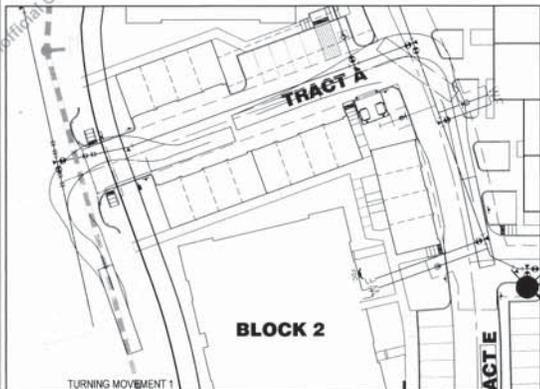


EMERGENCY VEHICLE TURNING MOVEMENTS
SCALE: 1" = 30'



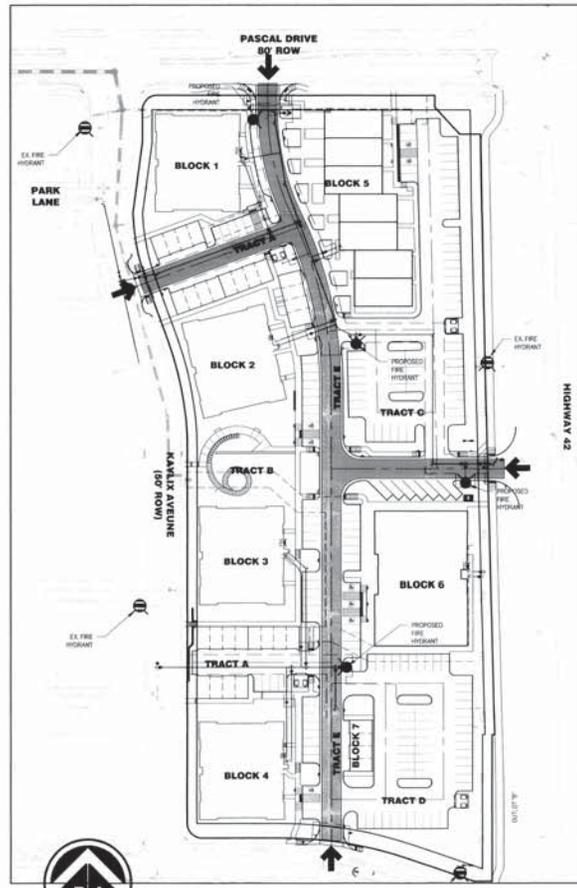
FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT

A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

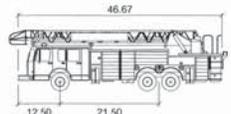


LEGEND

- 20' WIDTH EMERGENCY VEHICLE ACCESS ROUTE
- FIRE HYDRANT
- EXISTING FIRE HYDRANT
- ANTICIPATED, FUTURE ACCESS POINT



EMERGENCY VEHICLE ACCESS PLAN
SCALE: 1" = 60'



LOUISVILLE TRUCK 17

feet

Width : 8.00

Track : 8.00

Lock to Lock Time : 5.00

Steering Angle : 45.00

TURNING LEGEND

— VEHICLE LADDER/BUMPER OVERHANG

○ VEHICLE FRONT TIRES

○ VEHICLE REAR TIRES

- NOTES:**
1. THE CITY OF LOUISVILLE TRUCK WAS EVALUATED UTILIZING THE PROGRAM AUTOTURN FOR ALL ROADS DEPICTED IN GRAY. THOSE AREAS ACCOMMODATE THIS VEHICLE.
 2. FIRE HYDRANTS DEPICTED ARE TO PROVIDE THE REQUIRED COVERAGE TO THE SITE. FINAL LOCATION OF THE FIRE HYDRANTS SHALL BE DETERMINED WITHIN THE CONSTRUCTION DOCUMENTS.

<p>OWNER'S REP</p> <p>RMCs, INC.</p> <p>21 SOUTH SUNSET STR.</p> <p>LONGMONT, CO 80503</p> <p>P. 720.524.3620</p>	<p>PLANNING & LA</p> <p>PCS GROUP, INC.</p> <p>1001 16TH STR., 3B-180</p> <p>DENVER, CO 80265</p> <p>P. 303.531.4905</p> <p>WWW.PCSGROUP.CO</p>	<p>ENGINEERING</p> <p>J3 ENGINEERING CONSULTANTS</p> <p>2011 CHERRY STREET SUITE 206</p> <p>LOUISVILLE, CO 80027</p> <p>P. 720.975.0177</p>	<p>ARCHITECTURE</p> <p>OZ ARCHITECTURE</p> <p>3003 LARIMER STREET</p> <p>DENVER, CO 80205</p> <p>P. 303.861.5704</p> <p>WWW.OZARCH.COM</p>
--	--	--	---

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Designed By: SA Drawn By: KLM
Checked By: PMS Project #: 03000701



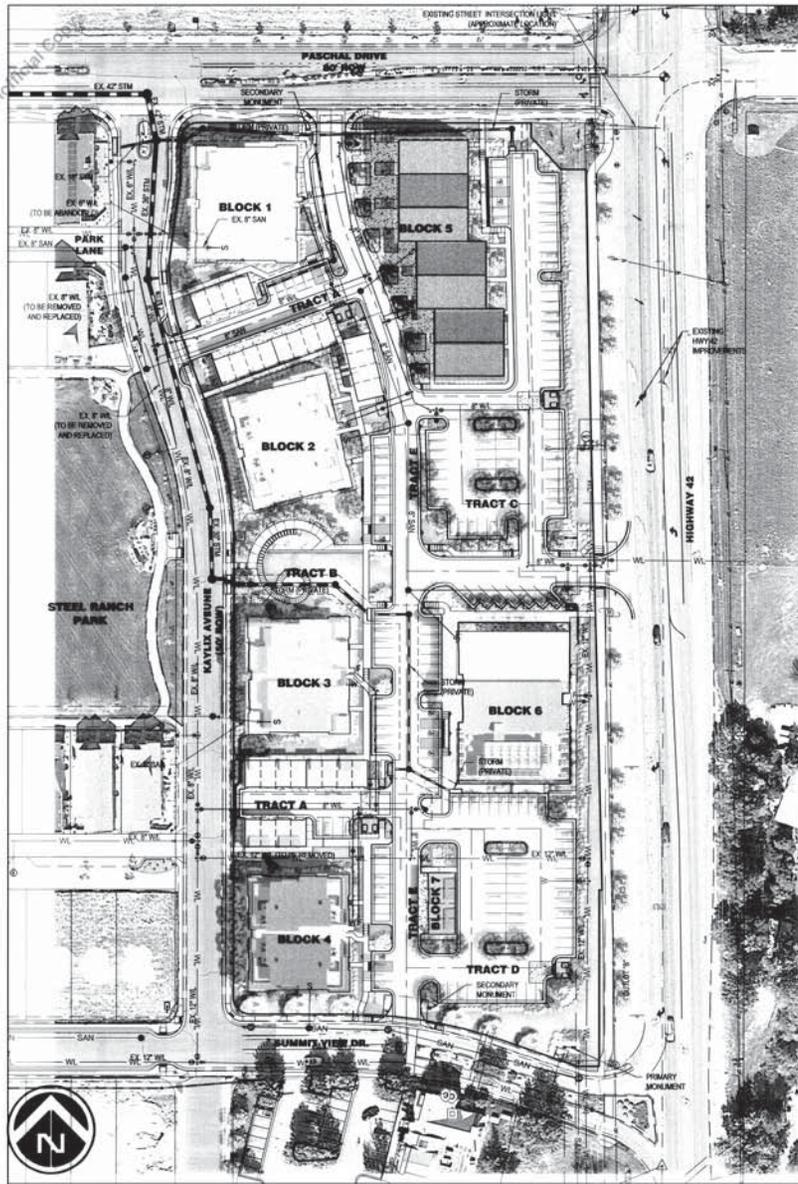
FINAL DEVELOPMENT PLAN PLANNED UNIT DEVELOPMENT

SHEET 9 OF 22

EMERGENCY VEHICLE ACCESS PLAN AND
TURNING MOVEMENT DETAILS



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



GENERAL HORIZONTAL PUBLIC IMPROVEMENT NOTES

PUBLIC HORIZONTAL IMPROVEMENTS FOR THE PROPERTY WILL BE CONSTRUCTED TO SERVICE THE PLANNING AREAS OF THE PROJECT. THIS PLAN IS NOT MEANT TO DICTATE THE PHASING ASSOCIATED WITH THIS PROJECT BEYOND WHAT IS STATED IN THESE NOTES. THE PUBLIC HORIZONTAL IMPROVEMENTS INCLUDE THE FOLLOWING:

- 1) PASCAL DRIVE ALONG THE NORTHERN BOUNDARY IS EXISTING AND ALL REQUIRED IMPROVEMENTS ON THIS ROAD WERE PREVIOUSLY MET WITH THE STEEL RANCH (TACCOA) DEVELOPMENT.
2) KAYLIX AVENUE ALONG THE WESTERN BOUNDARY IS EXISTING AND ALL REQUIRED IMPROVEMENTS ON THIS ROAD WERE PREVIOUSLY MET WITH THE STEEL RANCH (TACCOA) DEVELOPMENT.
3) COLORADO HIGHWAY 42 ALONG THE EASTERN BOUNDARY IS EXISTING AND WILL SERVICE THE DEVELOPMENT THROUGH A PROPOSED RIGHT IN/RIGHT OUT ACCESS POINT.
4) THE SANITARY SEWER LOCATED WITHIN KAYLIX AVENUE IS EXISTING AND SHALL SERVICE THE PROJECT.
5) THE WATER LINE LOCATED WITHIN KAYLIX AVENUE AND RUNNING EASTWEST WITHIN THE SOUTH THIRD PORTION OF THE DEVELOPMENT AND ADJACENT TO THE WEST HIGHWAY 42 ROW ARE EXISTING. THE EXISTING WATERLINE MAY BE REALIGNED WITH THE PHASED DEVELOPMENT. HOWEVER, PHASES MUST HAVE TWO POINTS OF CONNECTION AND MAY ROUTE THROUGH THE PROPERTY TO ALLOW FOR THE INDEPENDENT DEVELOPMENT OF PLANNING AREAS.
6) ON-SITE STORM SEWER IS REQUIRED TO SERVICE THE DEVELOPMENT. OFF-SITE, WATER QUALITY POND 1 AND REGIONAL DETENTION POND X ARE EXISTING AND WERE SIZED ANTICIPATING THE PROJECT DEVELOPMENT. ALL ON-SITE STORM SEWER FACILITIES SHOWN ARE PRIVATELY OWNED AND MAINTAINED.
7) PHASING OF THE DEVELOPMENT MAY OCCUR IN ANY ORDER AND IS NOT LIMITED EXCEPT AS REQUIRED TO PROVIDE A LOOPED WATER SERVICE, SANITARY SERVICE, TWO POINTS OF EMERGENCY ACCESS AND THAT NO 'DEAD END' LONGER THAN 500' WITHOUT A SECONDARY EMERGENCY ACCESS SHALL OCCUR.
8) NO OFF-SITE REGIONAL IMPROVEMENTS ARE ANTICIPATED TO BE REQUIRED WITH THIS DEVELOPMENT BEYOND THE PREVIOUSLY AGREED TO HIGHWAY 42 WEST SIDEWALK AND LANDSCAPING FROM PASCAL DRIVE TO SUMMIT VIEW DRIVE. AS CONCEPTUALLY DEPICTED WITHIN THIS PLAN, THESE IMPROVEMENTS ARE TO BE CONSTRUCTED IN CONCERT WITH PLANNING AREA 1.
9) RETAINING WALLS MAY BE REQUIRED TO ACCOMMODATE SITE GRADING AND SHALL BE ALLOWED.

HORIZONTAL IMPROVEMENT NOTES

- RESIDENTIAL AREA
1. THE EXISTING WATER, SANITARY SEWER AND STORM SEWER STUBS ADJACENT TO BLOCK 1 & BLOCK 3 SHALL BE EXTENDED TO SERVICE THE DEVELOPMENT.
2. THE STORM SEWER STUB ADJACENT TO TRACT B SHALL BE EXTENDED TO SERVICE THE DEVELOPMENT.
* NOTE: ON-SITE HORIZONTAL PUBLIC IMPROVEMENTS ARE CONCEPTUAL AND ARE SUBJECT TO CHANGE BASED ON THE APPROVED CONSTRUCTION PLANS.

HORIZONTAL IMPROVEMENT NOTES

- COMMERCIAL/RETAIL AREA
1. WATER AND FIRE SERVICES WILL BE VIA THE EXISTING 8" MAIN IN KAYLIX AVENUE OR VIA EXTENDING THE MAIN ON SITE. SHOULD A MAIN EXTENSION BE UTILIZED, THE LOCATIONS OF SERVICES AND MANNING ARE SUBJECT TO CITY REVIEW AND APPROVAL.
2. ONE STREET LIGHT AT THE PROPOSED HIGHWAY 42 ACCESS WILL BE CONSTRUCTED AT THE TIME OF THE ACCESS.
3. THE LANDSCAPING (WEST OF THE WALK) AND SIDEWALK IMPROVEMENTS ADJACENT TO HIGHWAY 42 WILL BE CONSTRUCTED CONCURRENTLY WITH THE BUILD-OUT OF PLANNING AREA 1.
4. A RIGHT IN / RIGHT OUT ACCESS ONTO HIGHWAY 42 IS PROPOSED WITHIN OR CONCURRENT WITH PLANNING AREA 1.
* NOTE: ON-SITE HORIZONTAL PUBLIC IMPROVEMENTS ARE CONCEPTUAL AND ARE SUBJECT TO CHANGE BASED ON THE APPROVED CONSTRUCTION PLANS.



HORIZONTAL PUBLIC IMPROVEMENT PLAN
SCALE: 1" = 50'

Logos for PCS GROUP, INC., ENGINEERING CONSULTANTS, and ARCHITECTURE.

Table with columns: NO, DATE, DESCRIPTION. Rows: 1 (08.07.2015) INITIAL SUBMITTAL, 2 (11.13.2015) SECOND SUBMITTAL, 3 (05.11.2017) SUBMITTAL FOR APPROVAL.



FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

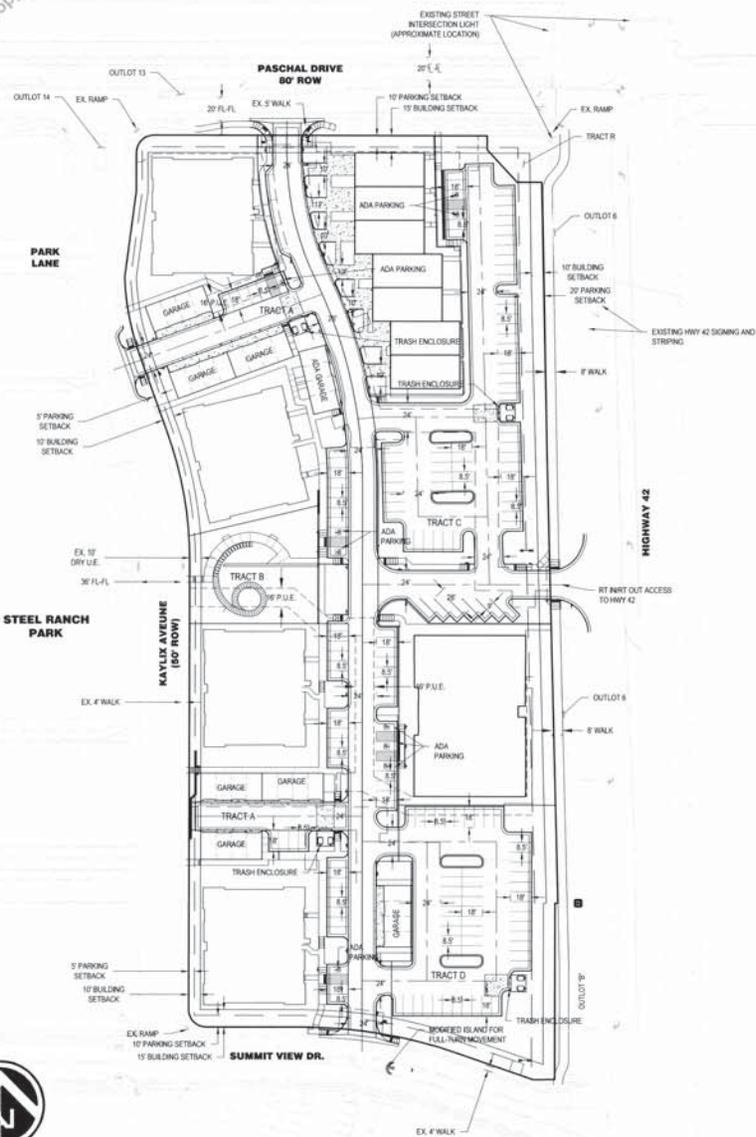
SHEET 10 OF 22

HORIZONTAL PUBLIC IMPROVEMENT PLAN

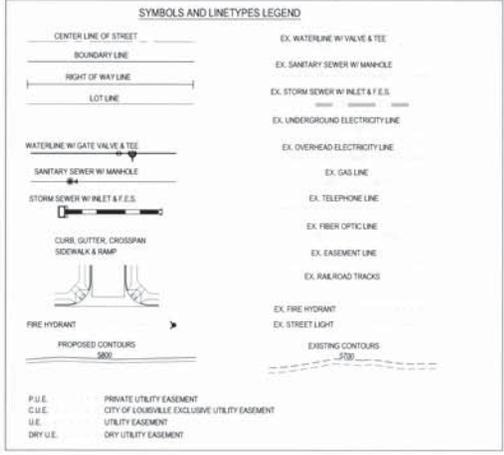


FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

Unofficial Copy



NOTES:
1. THE HORIZONTAL LAYOUT PRESENTED BY THIS PLAN IS A REPRESENTATION OF THE DESIGN INTENT FOR THE FOUNDARY. SPECIFIC ELEMENTS ARE SUBJECT TO REVISION IN ORDER TO ACCOMMODATE UNFORSEEN CIRCUMSTANCES AND CHANGING PROJECT CONDITIONS.



HORIZONTAL CONTROL PLAN
SCALE: 1" = 50'

OWNER'S REP: RMCS, INC. 21 SOUTH SUNSET STR. LONGMONT, CO 80503 P. 720.524.3620

PLANNING & LA: PCS GROUP, INC. 1001 16TH STR., 38-180 DENVER, CO 80202 P. 303.531.4805 WWW.PCSGROUPCO.COM

ENGINEERING: J3 ENGINEERING CONSULTANTS 2011 CHERRY STREET SUITE 206 LOUISVILLE, CO 80227 P. 720.975.0177

ARCHITECTURE: OZ ARCHITECTURE 3003 LARIMER STREET DENVER, CO 80205 P. 303.861.5704 WWW.OZARCH.COM

NO.	DATE	DESCRIPTION
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3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA
Checked By: PMS

Drawn By: KLM
Project #: 03000701



FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

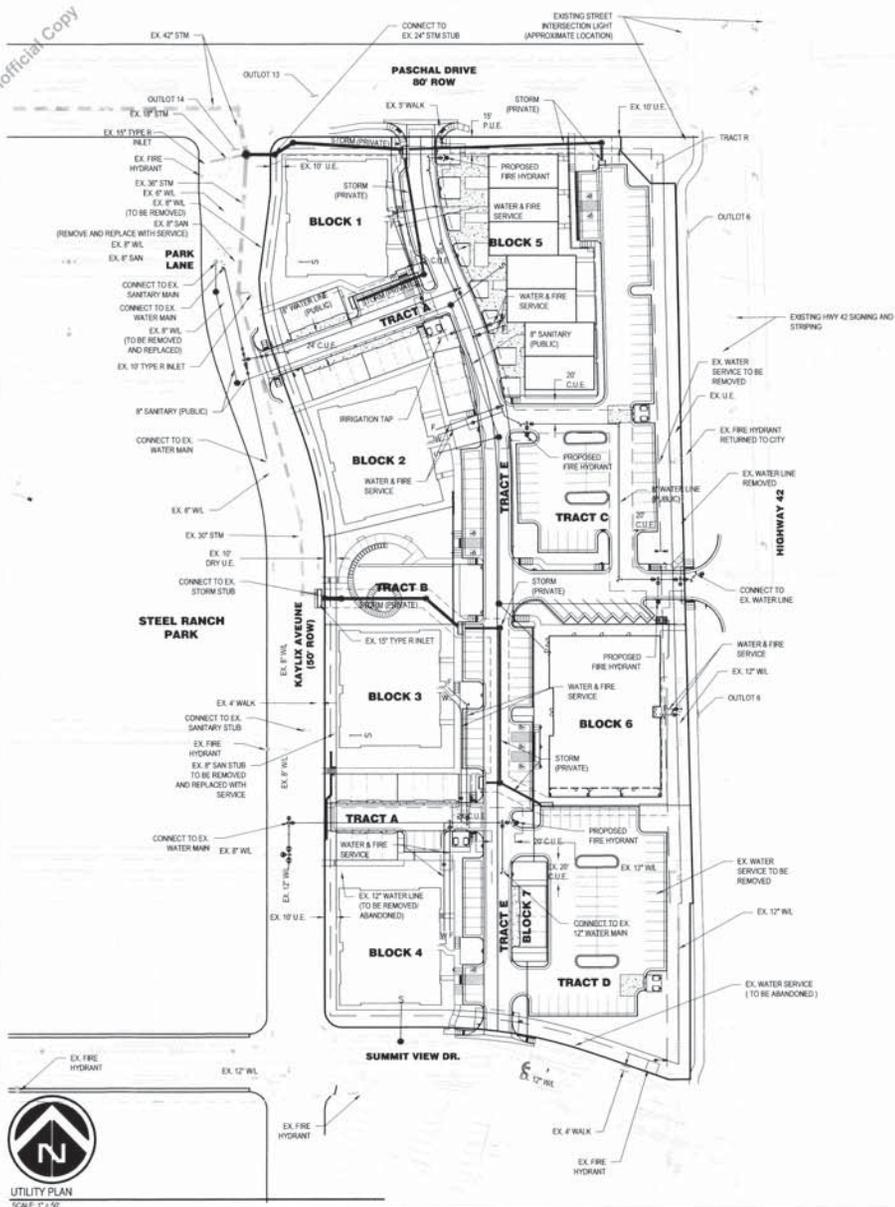
SHEET 11 OF 22

HORIZONTAL CONTROL PLAN



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

Unofficial Copy



- UTILITY NOTES:**
1. THE UTILITY LAYOUT PRESENTED IN THIS PLAN IS A REPRESENTATION OF THE DESIGN INTENT FOR THE FOUNDRY. SPECIFIC ELEMENTS ARE SUBJECT TO REVISION IN ORDER TO ACCOMMODATE UNFORSEEN CIRCUMSTANCES AND CHANGING PROJECT CONDITIONS.
 2. FOR THE PURPOSES OF CLARITY, CERTAIN EXISTING UTILITIES ARE NOT SHOWN ON THIS PLAN. THESE INCLUDE MOST DRY UTILITIES, UTILITIES BEING ABANDONED OR REMOVED AS A PART OF THIS PROJECT.

LEGEND

- PROPERTY BOUNDARY LINE
- RIGHT OF WAY LINE
- LOT LINE
- WATERLINE (PUBLIC)
- SANITARY SEWER (PUBLIC)
- PROPOSED STORM (PRIVATE)
- EX. WATERLINE W/ VALVE & TEE
- EX. SANITARY SEWER W/ MANHOLE
- EX. SANITARY SEWER FORCE MAIN
- EX. STORM
- EX. OVERHEAD ELECTRICITY LINE
- EX. DITCH LATERAL
- CURB, GUTTER, CROSSSPAN SIDEWALK & RAMP
- OVERHEAD STREET LIGHT
- REINFORCED CONCRETE PIPE
- WATER LINE
- SANITARY SEWER
- UTILITY EASEMENT
- CITY OF LOUISVILLE EXCLUSIVE U.E.
- PRIVATE UTILITY EASEMENT
- DRAINAGE UTILITY EASEMENT
- ACCESS & UTILITY EASEMENT
- SANITARY SEWER EASEMENT
- WATERLINE EASEMENT
- REP
- WL
- SAN
- U.E.
- C.U.E.
- P.U.E.
- D.U.E.
- A.U.E.
- S.S.E.
- W.E.



THE FOUNDRY

FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 12 OF 22

UTILITY PLAN

NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA Drawn By: KLM
Checked By: PMS Project #: 03000701

OWNER'S REP
RMC, INC.
21 SOUTH SUNSET STR.
LONGMONT, CO 80503
P. 720.524.3820

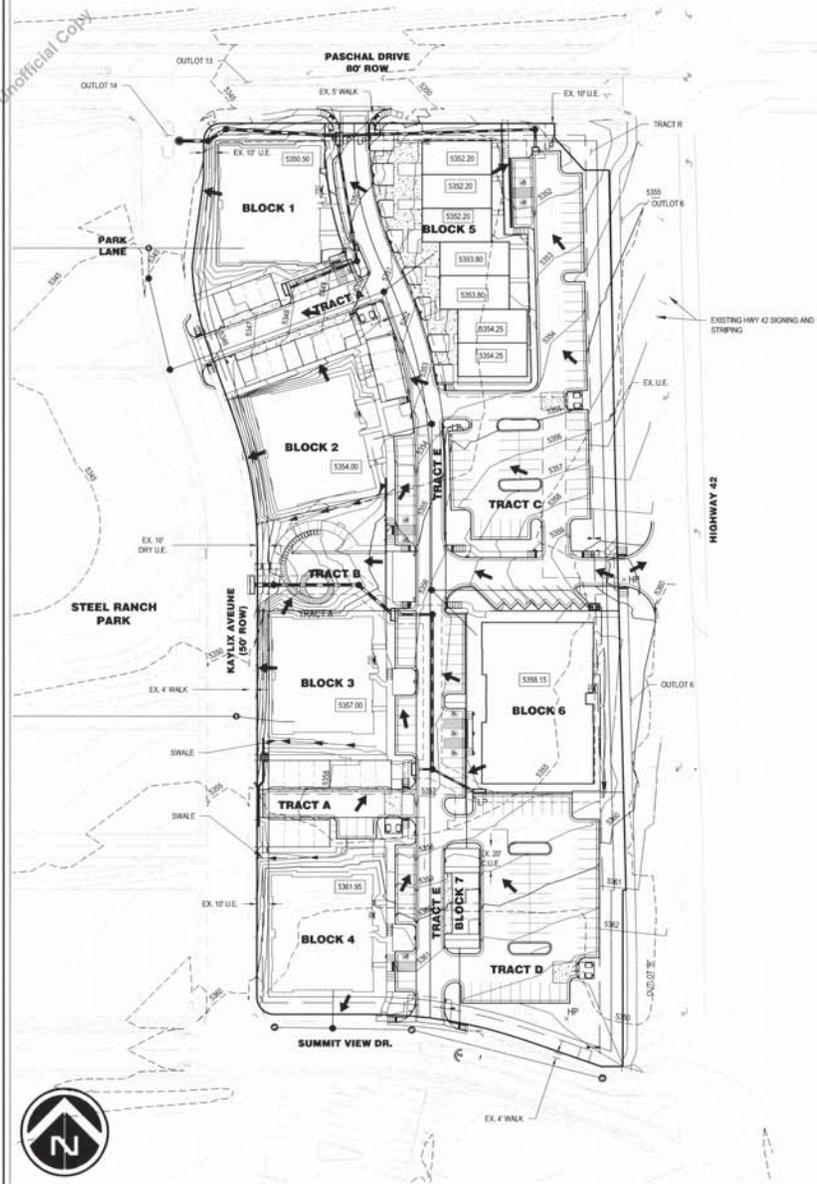
PLANNING & LA
PCS GROUP, INC.
1001 16TH STR., 3B-180
DENVER, CO 80202
P. 303.531.4905
WWW.PCSGROUP.CO

ENGINEERING CONSULTANTS
J3 ENGINEERING
2011 CHERRY STREET
SUITE 209
LOUISVILLE, CO 80027
P. 720.975.0177

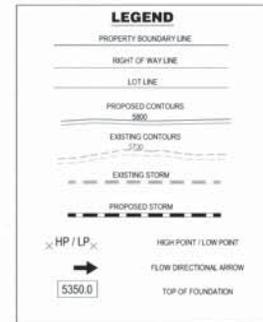
ARCHITECTURE
OZ ARCHITECTURE
3003 LARIMER STREET
DENVER, CO 80202
P. 303.861.5704
WWW.OZARCH.CO



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



- GRADING NOTES:**
1. THE GRADING PRESENTED IS CONCEPTUAL BUT SHALL BE CONSIDERED AS BASIS FOR SUBSEQUENT GRADING PLANS.
 2. TOP OF FOUNDATION ELEVATIONS ARE PRELIMINARY AND SUBJECT TO CHANGE WITHIN THE FINAL APPROVED CONSTRUCTION PLANS.



GRADING PLAN
SCALE: 1"=50'

OWNER'S REP
RMCS, INC.
21 SOUTH SUNSET STR.
LONGMONT, CO
80503
P. 720.524.3620

PLANNING & LA
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1001 16TH STR., 3B-190
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J3 ENGINEERING
CONSULTANTS
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LOUISVILLE, CO 80027
P. 720.975.0177

ARCHITECTURE
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Designed By: SA
Checked By: PMS

Drawn By: KLM
Project #: 03000701

THE FOUNDRY

**FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT**

SHEET 13 OF 22

GRADING PLAN



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT

A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM, CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

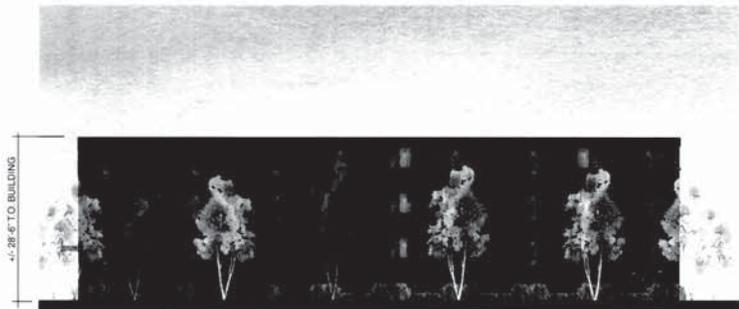
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COMMERCIAL IN-LINE FLEX BUILDING - WEST ELEVATION - 1" = 10'-0"



IN-LINE FLEX BUILDING PERSPECTIVE



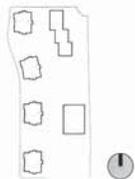
COMMERCIAL IN-LINE FLEX BUILDING - NORTH ELEVATION - 1" = 10'-0"



IN-LINE FLEX BUILDING PERSPECTIVE

NOTE: ANY MATERIALS & COLORS FROM THE FOUNDRY BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION PROCESS. MATERIAL AND COLOR ASSIGNMENT WILL BE FINALIZED IN SUBSEQUENT BUILDING CONSTRUCTION DOCUMENTS.

KEY LEGEND



MATERIAL LEGEND

BRICK 1 - RUNNING BOND	WOOD 1 - VERT OR HORIZ SIDING OR TRIM	METAL PANEL 1 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	WYNL CLAD WINDOWS
BRICK 2 - RUNNING BOND	WOOD 2 - VERT OR HORIZ SIDING OR TRIM	METAL PANEL 2 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	ALUM CLAD WOOD WINDOWS
BRICK 3 - RUNNING BOND	WOOD MANSCREEN - VERT OR HORIZ SIDING	METAL PANEL 3 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	ALUM STONE FRONT WINDOWS
BRICK 4 - RUNNING BOND	CEMENT LAP SIDING 1 OR STUCCO	METAL PANEL 4 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	POWDER COATED RAILING
STONE VENEER 1	CEMENT LAP SIDING 2 OR STUCCO	METAL PANEL 5 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	STANDING SEAM METAL SIDING ROOF
STONE VENEER 2	GAU BLOCK OR VENEER	METAL PANEL 6 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	INSULATED GARAGE DOOR

<p>OWNER'S REP RMCS, INC. 21 SOUTH SUNSET STR. LONGMONT, CO 80503 P. 720.524.3620</p>	<p>PLANNING & LA PCS GROUP, INC. 1301 16TH STR., 3B-180 DENVER, CO 80265 P. 303.531.4905 WWW.PCSGROUPCO.COM</p>	<p>ENGINEERING J3 ENGINEERING CONSULTANTS 2011 CHERRY STREET SUITE 206 LOUISVILLE, CO 80027 P. 720.975.0177</p>	<p>ARCHITECTURE OZ ARCHITECTURE 3303 LARIMER STREET DENVER, CO 80205 P. 303.861.5704 WWW.OZARCH.COM</p>
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Designed By: SA
Checked By: PMS

Drawn By: KLM
Project #: 03000701



FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 14 OF 22

ELEVATIONS COMMERCIAL IN-LINE



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
 A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

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COMMERCIAL IN-LINE FLEX BUILDING - EAST ELEVATION - 1" = 10'-0"



IN-LINE FLEX BUILDING PERSPECTIVE



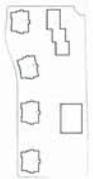
COMMERCIAL IN-LINE FLEX BUILDING - SOUTH ELEVATION - 1" = 10'-0"



IN-LINE FLEX BUILDING PERSPECTIVE

NOTE:
 ANY MATERIALS & COLORS FROM THE FOUNDRY BUILDING MATERIAL
 PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION
 PROCESS. MATERIAL AND COLOR ASSIGNMENT WILL BE FINALIZED IN
 SUBSEQUENT BUILDING CONSTRUCTION DOCUMENTS.

KEY LEGEND



MATERIAL LEGEND

BRICK 1, RUNNING BOND	WOOD 1 - VERT OR HORIZ SIDING OR TRIM	METAL PANEL 1 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	VINYL CLAD WINDOWS
BRICK 2, RUNNING BOND	WOOD 2 - VERT OR HORIZ SIDING OR TRIM	METAL PANEL 2 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	ALUM CLAD WOOD WINDOWS
BRICK 3, RUNNING BOND	WOOD RAINSCREEN - VERT OR HORIZ SIDING	METAL PANEL 3 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	ALUM STOREFRONT WINDOWS
BRICK 4, RUNNING BOND	CEMENT LAP SIDING 1 OR STUCCO	METAL PANEL 4 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	POWDER COATED RAILING
STONE VENEER 1	CEMENT LAP SIDING 2 OR STUCCO	METAL PANEL 5 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	STANDING SEAM METAL SIDING/ROOF
STONE VENEER 2	CMU BLOCK OR VENEER	METAL PANEL 6 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	INSULATED GARAGE DOOR

NO	DATE	DESCRIPTION
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 PCS GROUP, INC.
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**FINAL DEVELOPMENT PLAN
 PLANNED UNIT DEVELOPMENT**

SHEET 15 OF 22

ELEVATIONS COMMERCIAL IN-LINE

Designed By: SA

Checked By: PMS

Drawn By: KLM

Project #: 03000701



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
 A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

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COMMERCIAL FLEX BUILDING - WEST ELEVATION - 1" = 10'-0"



COMMERCIAL FLEX BUILDING - NORTH ELEVATION - 1" = 10'-0"



COMMERCIAL FLEX BUILDING PERSPECTIVE



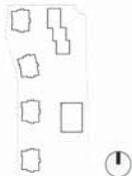
COMMERCIAL FLEX BUILDING PERSPECTIVE



COMMERCIAL FLEX BUILDING PERSPECTIVE

NOTE:
 ANY MATERIALS & COLORS FROM THE FOUNDRY BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION PROCESS. MATERIAL AND COLOR ASSIGNMENT WILL BE FINALIZED IN SUBSEQUENT BUILDING CONSTRUCTION DOCUMENTS.

KEY LEGEND



MATERIAL LEGEND

BRICK 1, FLARING END	WOOD 1 - VERT OR HORIZ. SIDING OR TRIM	METAL PANEL 1 - VERT OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	VINYL GLAZ WINDOWS
BRICK 2, FLARING END	WOOD 2 - VERT OR HORIZ. SIDING OR TRIM	METAL PANEL 2 - VERT OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	ALUM GLAZ WOOD WINDOWS
BRICK 3, FLARING END	WOOD RAINSCREEN - VERT OR HORIZ. SIDING	METAL PANEL 3 - VERT OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	ALUM STOREFRONT WINDOWS
BRICK 4, FLARING END	CEMENT LAP SIDING 1 OR STUCCO	METAL PANEL 4 - VERT OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	FIBROSKY COATED RAILING
STONE VENEER 1	CEMENT LAP SIDING 2 OR STUCCO	METAL PANEL 5 - VERT OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	STANDING SEAM METAL SIDING/ROOF
STONE VENEER 2	CMU BLOCK OR VENEER	METAL PANEL 6 - VERT OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	INSULATED GARAGE DOOR

OWNER'S REP RMCS, INC. 21 SOUTH SUNSET STR. LONGMONT, CO 80503 P. 720.524.3620	PLANNING & LA PCS GROUP, INC. 1011 16TH STR., 3B-180 DENVER, CO 80265 P. 303.531.4905 WWW.PCSGROUP.CO	ENGINEERING J3 ENGINEERING CONSULTANTS 2011 CHERRY STREET SUITE 206 LOUISVILLE, CO 80027 P. 720.975.0177	ARCHITECTURE OZ ARCHITECTURE CONSULTANTS 3003 LARIMER STREET DENVER, CO 80205 P. 303.861.5704 WWW.OZARCH.CO
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NO	DATE	DESCRIPTION
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA
 Checked By: PMS

Drawn By: KLM
 Project #: 03000701

THE FOUNDRY

**FINAL DEVELOPMENT PLAN
 PLANNED UNIT DEVELOPMENT**

SHEET 16 OF 22

ELEVATIONS COMMERCIAL FLEX



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
 A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

Unofficial Copy



COMMERCIAL FLEX BUILDING - EAST ELEVATION - 1" = 10'-0"



COMMERCIAL FLEX BUILDING PERSPECTIVE



COMMERCIAL FLEX BUILDING - SOUTH ELEVATION - 1" = 10'-0"



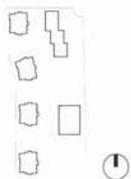
COMMERCIAL FLEX BUILDING PERSPECTIVE



COMMERCIAL FLEX BUILDING PERSPECTIVE

NOTE:
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 PROCESS. MATERIAL AND COLOR ASSIGNMENT WILL BE FINALIZED IN
 SUBSEQUENT BUILDING CONSTRUCTION DOCUMENTS.

KEY LEGEND



MATERIAL LEGEND

BRICK 1 RUNNING BOND	WOOD 1 - VERT OR HORIZ SOUND OR TRIM	METAL PANEL 1 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	VINYL CLAD SIDING
BRICK 2 RUNNING BOND	WOOD 2 - VERT OR HORIZ SOUND OR TRIM	METAL PANEL 2 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	ALUM CLAD WOOD WINDOWS
BRICK 3 RUNNING BOND	WOOD HANDSCREEN - VERT OR HORIZ SIDING	METAL PANEL 3 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	ALUM STOREFRONT WINDOWS
BRICK 4 RUNNING BOND	CEMENT LAP SIDING 1 OR STUCCO	METAL PANEL 4 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	POWDER COATED PAINTING
STONE VENEER 1	CEMENT LAP SIDING 2 OR STUCCO	METAL PANEL 5 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	STANDING SEAM METAL SIDING/ROOF
STONE VENEER 2	CMU BLOCK OR VENEER	METAL PANEL 6 - VERT OR HORIZ CORRUGATED METAL SIDING OR METAL PANEL	INSULATED GARAGE DOOR

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 80503
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 WWW.PCSGROUP.CO

ENGINEERING
 JJ ENGINEERING
 CONSULTANTS
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 P. 720.975.0177

ARCHITECTURE
 OZ ARCHITECTURE
 3003 LARIMER STREET
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 P. 303.861.5704
 WWW.OZARCH.COM

NO	DATE	DESCRIPTION
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA
 Checked By: PMS
 Drawn By: KLM
 Project #: 03000701



**FINAL DEVELOPMENT PLAN
 PLANNED UNIT DEVELOPMENT**

SHEET 17 OF 22

ELEVATIONS COMMERCIAL FLEX



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

Unofficial Copy



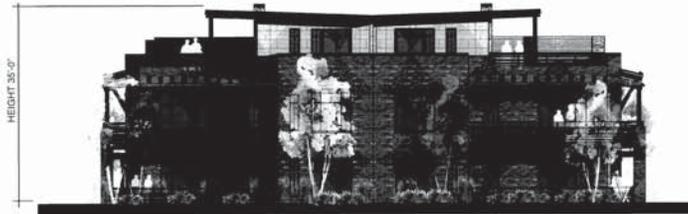
RESIDENTIAL BUILDING A - WEST ELEVATION - 1" = 10'-0"



RESIDENTIAL BUILDING A - SOUTH ELEVATION - 1" = 10'-0"



RESIDENTIAL BUILDING A - EAST ELEVATION - 1" = 10'-0"



RESIDENTIAL BUILDING A - NORTH ELEVATION - 1" = 10'-0"



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KEY LEGEND



MATERIAL LEGEND

Material legend table with color swatches and descriptions for various building materials like brick, wood, metal panels, vinyl clad windows, etc.

Professional seals and contact information for RMCS, INC., PCS GROUP, INC., J3 ENGINEERING CONSULTANTS, and OZ ARCHITECTURE.

Revision table with columns for NO, DATE, and DESCRIPTION, listing three revisions.



FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 18 OF 22
ELEVATIONS RESIDENTIAL BUILDING A



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
 A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

Unofficial Copy



RESIDENTIAL BUILDING B - WEST ELEVATION - 1" = 10'-0"



RESIDENTIAL BUILDING B - SOUTH ELEVATION - 1" = 10'-0"



RESIDENTIAL BUILDING B - EAST ELEVATION - 1" = 10'-0"

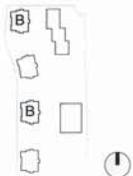


RESIDENTIAL BUILDING B - NORTH ELEVATION - 1" = 10'-0"



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KEY LEGEND



MATERIAL LEGEND

BRICK 1, RUNNING BOND	WOOD 1 - VERT. OR HORIZ. SIDING OR TRIM	METAL PANEL 1 - VERT. OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	VINYL CLAD WINDOWS
BRICK 2, RUNNING BOND	WOOD 2 - VERT. OR HORIZ. SIDING OR TRIM	METAL PANEL 2 - VERT. OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	ALUM. CLAD WOOD WINDOWS
BRICK 3, RUNNING BOND	WOOD HANDSCREEN - VERT. OR HORIZ. SIDING	METAL PANEL 3 - VERT. OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	ALUM. STOREFRONT WINDOWS
BRICK 4, RUNNING BOND	CEMENT LAP SIDING 1 OR STUCCO	METAL PANEL 4 - VERT. OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	POWDER COATED RAILING
STONE VENEER 1	CEMENT LAP SIDING 2 OR STUCCO	METAL PANEL 5 - VERT. OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	STANDING SEAM METAL SIDING/ROOF
STONE VENEER 2	CMU BLOCK OR VENEER	METAL PANEL 6 - VERT. OR HORIZ. CORRUGATED METAL SIDING OR METAL PANEL	INSULATED GARAGE DOOR



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ARCHITECTURE
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NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
2	11.13.2015	SECOND SUBMITTAL
3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA
 Checked By: PMS

Drawn By: KLM
 Project #: 03000701

FINAL DEVELOPMENT PLAN
 PLANNED UNIT DEVELOPMENT

SHEET 19 OF 22

ELEVATIONS RESIDENTIAL BUILDING B



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

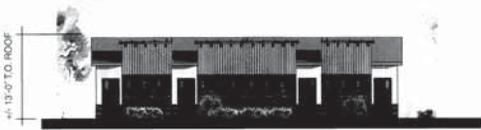
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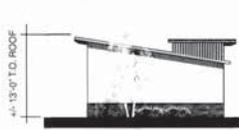
RESIDENTIAL GARAGES - WEST ELEVATION -
1' = 10'-0"



RESIDENTIAL GARAGES - NORTH ELEVATION -
1' = 10'-0"



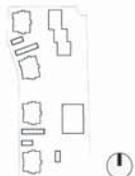
RESIDENTIAL GARAGES - EAST ELEVATION -
1' = 10'-0"



RESIDENTIAL GARAGES - SOUTH ELEVATION -
1' = 10'-0"

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KEY LEGEND



MATERIAL LEGEND

Material legend table with columns for Brick, Wood, Metal Panels, Vinyl Clad, Alum. Clad, Powder Coated, Standing Seam, and Insulated Garage Door.

Project team contact information including Owner's Rep (RMCS, Inc.), Planning & LA (PCS Group, Inc.), Engineering (J3 Engineering Consultants), and Architecture (OZ Architecture).

Revision table with columns: NO, DATE, DESCRIPTION. Includes entries for Initial Submittal, Second Submittal, and Submittal for Approval.



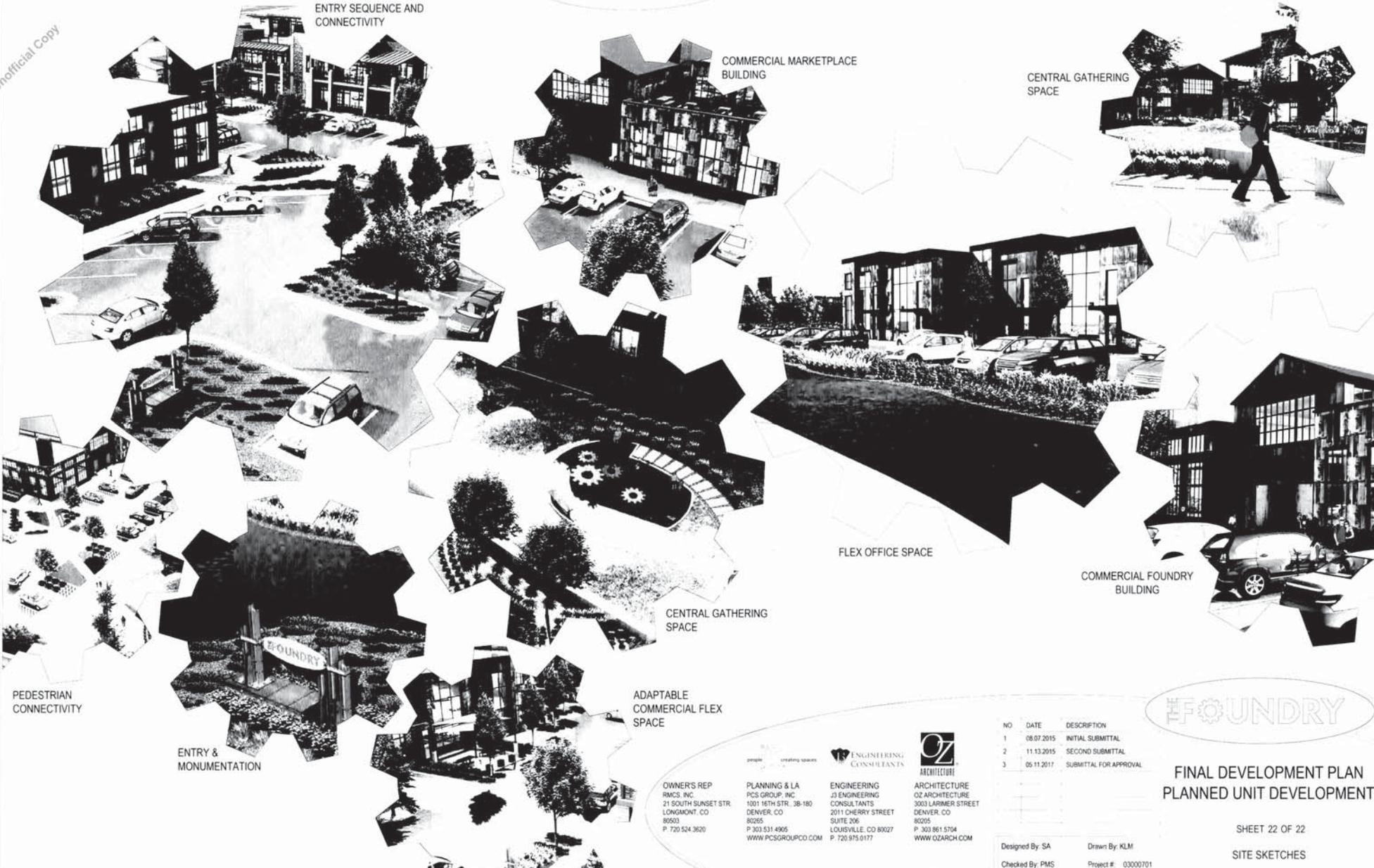
FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 20 OF 22
ELEVATIONS RESIDENTIAL GARAGES

Unofficial Copy



FINAL DEVELOPMENT PLAN / PLANNED UNIT DEVELOPMENT
A PART OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PM,
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



FINAL DEVELOPMENT PLAN
PLANNED UNIT DEVELOPMENT

SHEET 22 OF 22
SITE SKETCHES

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NO	DATE	DESCRIPTION
1	08.07.2015	INITIAL SUBMITTAL
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3	05.11.2017	SUBMITTAL FOR APPROVAL

Designed By: SA
Checked By: PMS
Drawn By: KLM
Project #: 03000701

Official Copy

THE FOUNDRY FINAL SUBDIVISION PLAT

A REPLAT OF LOT 1, BLOCK 9 AND TRACT T OF TAKODA SUBDIVISION, AND LOT 2 OF SUMMIT VIEW SUBDIVISION, AND BEING PART OF SOUTHEAST 1/4 OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PRINCIPAL MERIDIAN, BOULDER COUNTY, CITY OF LOUISVILLE, STATE OF COLORADO

ORIGINAL PLAT
 TAKODA SUBD. (REC. #03103584) AND SUMMIT VIEW SUBD. (REC. #2216330)

LEGAL DESCRIPTION

THE FOUNDRY:
 A REPLAT OF LOT 1, BLOCK 9 AND TRACT T OF TAKODA SUBDIVISION WITH A RECEPTION NUMBER OF 03103584, AND LOT 2 OF SUMMIT VIEW SUBDIVISION WITH A RECEPTION NUMBER 2216330 AND BEING PART OF SOUTHEAST 1/4 OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH P.M., CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO.
 AREA = 5.82 ACRES

DEDICATION

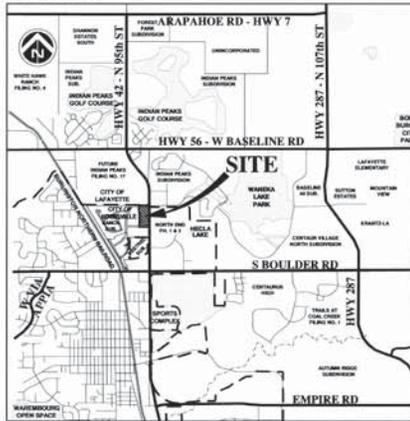
KNOW ALL MEN BY THESE PRESENTS, THAT THE UNDERSIGNED, BEING THE OWNERS OF THAT REAL PROPERTY SITUATED IN THE SOUTHEAST 1/4 OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PRINCIPAL MERIDIAN IN THE COUNTY OF BOULDER, STATE OF COLORADO DESCRIBED AS FOLLOWS:
 LOT 1, BLOCK 9 AND TRACT T OF TAKODA SUBDIVISION WITH A RECEPTION NUMBER OF 03103584, AND LOT 2 OF SUMMIT VIEW SUBDIVISION WITH A RECEPTION NUMBER 2216330.

HAS LAID OUT, SUBDIVIDED AND PLATTED SAID LAND AS PER DRAWING HEREON CONTAINED UNDER THE NAME AND STYLE OF "THE FOUNDRY", A SUBDIVISION OF A PART OF THE CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO, AND BY THESE PRESENTS DO HEREBY DEDICATE TO THE CITY OF LOUISVILLE AND THE PUBLIC ALL PUBLIC RIGHTS-OF-WAY, THE INGRESS-EGRESS AND FIRE LANE EASEMENTS FOR VEHICULAR, PEDESTRIAN AND EMERGENCY ACCESS AND THE PUBLIC USE THEREOF FOREVER AND DOES FURTHER DEDICATE TO THE USE OF THE CITY OF LOUISVILLE AND ALL MUNICIPALLY OWNED AND/OR FRANCHISED UTILITIES AND SERVICES THOSE PORTIONS OF SAID REAL PROPERTY WHICH ARE SO DESIGNATED AS EASEMENTS AND RIGHT-OF-WAYS FOR THE CONSTRUCTION, INSTALLATION, OPERATION, MAINTENANCE, REPAIR AND REPLACEMENT FOR ALL SERVICES, INCLUDING WITHOUT LIMITING THE GENERALITY OF THE FOREGOING, TELEPHONE AND ELECTRIC LINES, WORKS, POLES AND UNDERGROUND CABLES, GAS PIPELINES, WATER PIPELINES, SANITARY SEWER LINES, STREET LIGHTS, COLLECTS, HYDRANTS, DRAINAGE DITCHES AND DRAINS AND ALL APPURTENANCES THERETO, IT BEING EXPRESSLY UNDERSTOOD AND AGREED BY THE UNDERSIGNED THAT, EXCEPT AS EXPRESSLY PROVIDED BY WRITTEN AGREEMENT WITH THE CITY OF LOUISVILLE, ALL EXPENSES AND COSTS INVOLVED IN CONSTRUCTING AND INSTALLING SANITARY SEWER SYSTEM WORKS AND LINES, GAS SERVICE LINES, ELECTRICAL SERVICE WORKS AND LINES, STORM SEWERS AND DRAINS, STREET LIGHTING, GRADING AND LANDSCAPING, CURBS, GUTTERS, STREET PAVEMENT, SIDEWALKS AND OTHER SUCH UTILITIES AND SERVICES SHALL BE GUARANTEED AND PAID FOR BY THE SUBDIVIDER OR ARRANGEMENTS MADE BY THE SUBDIVIDER THEREOF WHICH ARE APPROVED BY THE CITY OF LOUISVILLE, COLORADO, AND SUCH SUMS SHALL NOT BE PAID BY THE CITY OF LOUISVILLE, COLORADO, AND THAT ANY ITEM SO CONSTRUCTED OR INSTALLED WHEN ACCEPTED BY THE CITY OF LOUISVILLE, COLORADO, SHALL BECOME THE SOLE PROPERTY OF SAID CITY OF LOUISVILLE, COLORADO, EXCEPT PRIVATE ROADWAY CURBS, GUTTER AND PAVEMENT AND ITEMS OWNED BY MUNICIPALLY FRANCHISED UTILITIES AND/OR CENTURYTEK, INC. WHICH WHEN CONSTRUCTED OR INSTALLED, SHALL REMAIN THE PROPERTY OF THE OWNER AND SHALL NOT BECOME THE PROPERTY OF THE CITY OF LOUISVILLE, COLORADO.

NOTES

- LAND TITLE GUARANTEE COMPANY, TITLE COMMITMENT NO. ABZ70529804, DATED FEBRUARY 22, 2017 AND OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY, TITLE COMMITMENT ORDER NO. ABZ70529804 DATED FEBRUARY 22, 2017 WERE ENTIRELY RELIED UPON FOR EASEMENTS OF RECORD AND TITLE INFORMATION. SEE SCHEDULE B FOR EXCEPTIONS.
- BEARINGS SHOWN HEREON ARE BASED ON THE ASSUMPTION THAT THE WESTERLY LINE OF THE SOUTHERLY PORTION OF OUTLOT 6, TAKODA SUBDIVISION BEARS N 01°20' 52" W, 380.11 FEET BETWEEN A FOUND 2 INCH ALUMINUM CAP MONUMENT STAMPED RMCS LS 28667 AT THE SOUTHWEST CORNER OF SAID OUTLOT 6 AND A FOUND 2 INCH ALUMINUM CAP MONUMENT STAMPED RMCS LS 28667 AT THE NORTHWEST CORNER OF THE SOUTHERLY PORTION OF SAID OUTLOT 6.
- ANY PERSON WHO KNOWINGLY REMOVES, ALTERS OR DEFEATS ANY PUBLIC LAND SURVEY MONUMENT OR LAND SURVEY MONUMENT OR ACCESSORY COMBATS A CLASS TWO (2) MISDEMEANOR PURSUANT TO STATE STATUTE 18-6-508, C.R.S.
- ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED ON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVERED SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF CERTIFICATION SHOWN HEREON. CRS 13-80-105.
- SURVEY INFORMATION SHOWN HEREON AS PROVIDED BY ROCK CREEK SURVEYING, LLC DATED AUGUST 25, 2013. ROCK CREEK SURVEYING, LLC IS NOT RESPONSIBLE FOR UTILITY INFORMATION PROVIDED BY OTHERS. ROCK CREEK SURVEYING, LLC RECOMMENDS THAT THE LOCATION OF THE UTILITIES BE FIELD VERIFIED PRIOR TO ANY DIGGING OR ADJACENT TO THE SUBJECT PROPERTY.
- THE CITY OF LOUISVILLE SHALL REVIEW AND APPROVE THE LOCATIONS OF UTILITY LINES, PEDESTALS AND CABINETS WITHIN PUBLIC PROPERTY AND WITHIN CITY OF LOUISVILLE EXCLUSIVE CITY EASEMENTS. ALL UTILITY EASEMENTS NOT SPECIFICALLY DESIGNATED FOR OTHERS (E.G. DRY UTILITY EASEMENTS) ARE EXCLUSIVELY FOR OF THE CITY OF LOUISVILLE MET UTILITIES.
- EXCLUSIVE CITY UTILITY EASEMENTS ARE RESERVED FOR CITY OF LOUISVILLE EXCLUSIVE USE FOR CITY WATER, SANITARY SEWER AND STORM SEWER FACILITIES. DRY UTILITIES COMPANIES AND/OR PRIVATE OWNERS OF STORM DRAINAGE AND IRRIGATION LINES MUST OBTAIN PRIOR WRITTEN APPROVAL FROM THE CITY FOR ANY PROPOSED CROSSING OF ANY CITY MET UTILITY EASEMENTS AND MUST EXECUTE AN AGREEMENT WHICH STIPULATES THE DRY UTILITIES, STORM DRAINAGE, AND/OR IRRIGATION LINES APPROVED TO CROSS CITY EASEMENTS ARE SUBJECT TO RELOCATION AT THE COMPANY'S OR OWNER'S EXPENSE AT THE DIRECTION OF THE CITY. DRY UTILITIES, STORM DRAINAGE, AND/OR IRRIGATION LINES THAT ARE APPROVED TO CROSS CITY EASEMENTS SHALL DO SO AT SUBSTANTIALLY RIGHT ANGLES. MET UTILITIES MAY TRAVERSE DRY UTILITY EASEMENTS WITHOUT REQUIREMENT FOR FURTHER PERMISSION. NO JOINT USE OF ANY CITY EXCLUSIVE UTILITY EASEMENTS IS PERMITTED WITHOUT THE EXPRESS WRITTEN APPROVAL OF THE CITY AND EXECUTION OF A JOINT USE AGREEMENT, WHICH SHALL BE AT THE CITY'S DISCRETION.
- PROPOSED VEHICULAR CONNECTIONS MAY BE REFINED, WITH FUTURE CONSTRUCTION PLAN SUBMITTALS.
- ACCESS IS GRANTED HEREMETH OVER AND ACROSS ALL PAVED AREAS FOR EMERGENCY, PUBLIC AND PRIVATE VEHICULAR ACCESS.
- THE CITY OF LOUISVILLE IS GRANTED A 4' MAINTENANCE ACCESS EASEMENT BEYOND KAYLIX AVE, PASCHAL DR AND SUMMIT VIEW OF ARBITING RIGHT OF WAY FOR THE MAINTENANCE OF CURB AND SIDEWALK, MINIMUM SURFACE DISTURBANCE IS ALLOWED WITHIN THE EASEMENT.
- THE MINIMUM SEPARATION BETWEEN WATER AND SANITARY SHALL BE 10' OUTSIDE OF PIPE TO OUTSIDE OF PIPE. SEPARATION BETWEEN ALL OTHER UTILITIES SHALL NOT BE LESS THAN 7'. TO THE EXTENT PRACTICAL, BASED ON CONDUCTABILITY, RIGHT-OF-WAY AND ASSOCIATED STREET WIDTHS, ETC. THE CITY WILL WORK WITH THE SUBDIVIDER TO GENERATE PRACTICAL SOLUTIONS FOR AREAS OF UTILITY SEPARATION CONCERNS.
- THE CITY OF LOUISVILLE IS GRANTED A 10' MAINTENANCE ACCESS EASEMENT BEYOND THE UTILITY EASEMENTS GRANTED TO THE CITY BY THIS PLAT. THE 10' MAINTENANCE ACCESS EASEMENT SHALL BE ALLOWED ON EITHER SIDE OF THE UTILITY EASEMENT, OR 5' ON EACH SIDE OF THE UTILITY EASEMENT, MINIMUM SURFACE DISTURBANCE IS ALLOWED WITHIN THIS EASEMENT.
- DISTANCES SHOWN HEREON ARE BASED UPON THE U.S. SURVEY FOOT.
- EXISTING ENCUMBRANCES AND USES ARE NOTED ON THE DRAWINGS AND ARE NOT SUPERCEDED BY THE LAND USE SUMMARY TABLE.
- PUBLIC MET UTILITIES LOCATED ON PRIVATE PROPERTY SHALL BE WITHIN A CITY EXCLUSIVE UTILITY EASEMENT (C.U.E.).
- THE PRIVATE UTILITY EASEMENTS (P.U.E.) SHALL BE CONVEYED TO THE FOUNDRY HOA AND SHALL ALLOW FOR THE ACCESS TO AND MAINTENANCE AND/OR REPLACEMENT OF THE PRIVATE UTILITIES CONTAINED THEREIN.
- EASEMENTS ON SOME LOTS, AS WELL AS OTHER EASEMENTS, ARE PROVIDED FOR IN THE DECLARATION OF COVENANTS, CONDITIONS AND RESTRICTIONS OF THE FOUNDRY. TO BE RECORDED IN THE CITY OF LOUISVILLE, COUNTY OF BOULDER, COLORADO.

VICINITY MAP
 Scale: 1" = 2000'



ACKNOWLEDGEMENT

STATE OF COLORADO)
) SS
 COUNTY OF BOULDER)
 THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS 15th DAY OF June 2017, by Justin McClure
 WITNESS MY HAND AND SEAL.
 MY COMMISSION EXPIRES 4-16-19
 NOTARY PUBLIC

SURVEYOR'S CERTIFICATE

I, ROBERT A. RICKARD, A DULY REGISTERED LAND SURVEYOR IN THE STATE OF COLORADO DO HEREBY CERTIFY THAT THE SUBDIVISION PLAT OF THE FOUNDRY TRULY REPRESENTS THE RESULTS OF A SURVEY MADE BY ME OR UNDER MY DIRECT SUPERVISION AND IN ACCORDANCE WITH APPLICABLE STATE OF COLORADO REQUIREMENTS AND ETHICAL STANDARDS OF SURVEYORS.

NOTICE: ACCORDING TO COLORADO LAW YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS SURVEY WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS SURVEY BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF CERTIFICATION SHOWN HEREON.

OWNERSHIP CERTIFICATE

OWNER: LOUISVILLE PROPERTIES 1, INC., A COLORADO CORPORATION
 BY: JUSTIN MCCLURE, PRESIDENT
 DATE: 6.14.17

OWNERSHIP CERTIFICATE

OWNER: STEEL STREET COMMERCIAL, INC., A COLORADO CORPORATION
 BY: JUSTIN MCCLURE, PRESIDENT
 DATE: 6.14.17

PLANNING COMMISSION CERTIFICATE

RECOMMENDED APPROVAL THIS 15th DAY OF Oct 2016 BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO.
 RESOLUTION NO. 23 SERIES 2016

CITY COUNCIL CERTIFICATE

APPROVED THIS 15th DAY OF Nov 2016 BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO.
 RESOLUTION NO. 58 SERIES 2016
 MAYOR: [Signature] CITY CLERK: [Signature]

CLERK AND RECORDER'S CERTIFICATE

STATE OF COLORADO)
) SS
 COUNTY OF BOULDER)
 I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT 10:33 O'CLOCK AT A.M. ON THIS 29th DAY OF June 2017, AND IS RECORDED IN PLAN FILE --- FEE 23.00
 PAD: 23.00 FILM NO. ---
 RECEPTION NO. 03600744
 RECORDER: Hillary Hall DEPUTY: [Signature]

SHEET INDEX

NUMBER	TITLE
1	COVER SHEET
2	FINAL PLAT

LAND USE SUMMARY

BLOCK/TRACT	TOTAL ACRES	OWNERSHIP	MAINTENANCE ¹
TRACT A	0.45	Louisville Properties 1, Inc	The FOUNDRY Residential Owner's Association T.F.R.O.A.
TRACT B	0.22	Louisville Properties 1, Inc	The FOUNDRY Residential Owner's Association T.F.R.O.A.
TRACT C	1.03	Steel Street Commercial, Inc	The FOUNDRY Commercial Owner's Association T.F.C.O.A.
TRACT D	0.67	Steel Street Commercial, Inc	The FOUNDRY Commercial Owner's Association T.F.C.O.A.
TRACT E	0.34	Steel Street Commercial, Inc	The FOUNDRY Commercial Owner's Association T.F.C.O.A.
BLOCK 1	0.44	Louisville Properties 1, Inc	The FOUNDRY Residential Owner's Association T.F.R.O.A.
BLOCK 2	0.96	Louisville Properties 1, Inc	The FOUNDRY Residential Owner's Association T.F.R.O.A.
BLOCK 3	0.49	Louisville Properties 1, Inc	The FOUNDRY Residential Owner's Association T.F.R.O.A.
BLOCK 4	0.54	Louisville Properties 1, Inc	The FOUNDRY Residential Owner's Association T.F.R.O.A.
BLOCK 5	0.53	Steel Street Commercial, Inc	The FOUNDRY Commercial Owner's Association T.F.C.O.A.
BLOCK 6	0.50	Steel Street Commercial, Inc	The FOUNDRY Commercial Owner's Association T.F.C.O.A.
BLOCK 7	0.06	Louisville Properties 1, Inc	The FOUNDRY Residential Owner's Association T.F.R.O.A.
TOTALS	5.82 AC.		

¹ Refer to Subdivision Agreement for delineation of maintenance responsibilities.



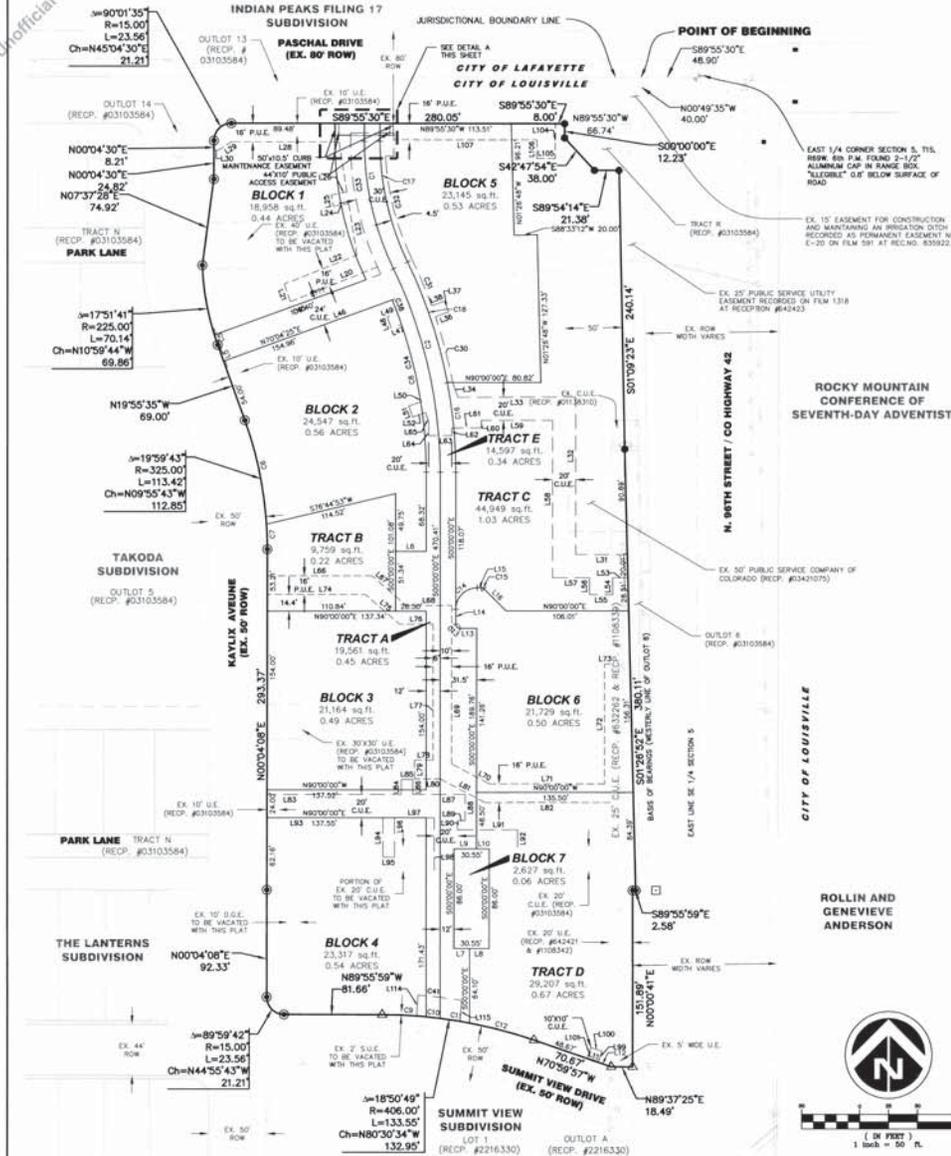
BENCHMARK
 BENCHMARK IS TOP OF FOUND 1-1/2" DIAMETER ALUMINUM CAP MONUMENT WITH ILLEGIBLE STAMPING AT THE SOUTHEAST CORNER OF OUTLOT 6, TAKODA SUBDIVISION, RECEPTION NO. 03103584 RECORDED 10/06/2010, WITH ELEVATION OF 5358.90, LOCAL VERTICAL DATUM.

ROCK CREEK SURVEYING, LLC
 3021 GARDENIA WAY
 SUPERIOR, COLORADO 80027
 (303) 221-2376

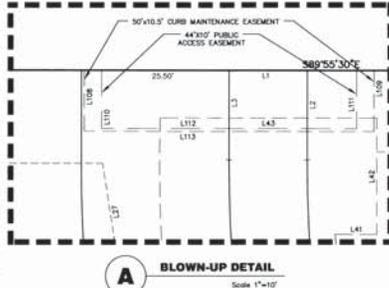
5/9/2017 SHEET 1 OF 2

THE FOUNDRY FINAL SUBDIVISION PLAT

A REPLAT OF LOT 1, BLOCK 9 AND TRACT T OF TAKODA SUBDIVISION, AND LOT 2 OF SUMMIT VIEW SUBDIVISION, AND BEING PART OF SOUTHEAST ¼ OF SECTION 5, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH PRINCIPAL MERIDIAN, BOULDER COUNTY, CITY OF LOUISVILLE, STATE OF COLORADO



PARCEL LINE TABLE		
LINE #	BEARING	DISTANCE
L1	N89°55'30"W	13.50'
L2	N00°00'00"E	15.38'
L3	S00°00'00"E	15.39'
L4	S19°55'35"E	15.00'
L5	N90°00'00"W	26.50'
L6	N90°00'00"W	13.50'
L7	N90°00'00"W	17.00'
L8	N90°00'00"W	19.50'
L9	N90°00'00"W	11.00'
L10	S70°59'57"E	12.00'
L11	S70°59'57"E	12.00'
L12	N90°00'00"W	14.49'
L13	N90°00'00"W	13.00'
L14	N90°00'00"W	3.60'
L15	S45°00'00"E	26.83'
L16	S45°00'00"E	26.83'



PARCEL CURVE TABLE					
CURVE #	ARC LENGTH	RADIUS	DELTA ANGLE	CHORD BEARING	CHORD LENGTH
C1	113.60'	250.00'	026°02'08"	S12°56'34"E	112.63'
C2	181.24'	400.00'	025°57'38"	S12°58'49"E	179.69'
C5	9.00'	225.00'	002°17'33"	S18°46'48"E	9.00'
C6	91.78'	325.00'	016°10'48"	N11°50'11"W	91.47'
C7	21.64'	325.00'	003°48'55"	S01°50'19"E	21.84'
C8	152.15'	388.00'	022°28'03"	S11°14'02"E	151.17'
C9	37.97'	406.00'	005°21'29"	S8°15'15"E	37.95'
C10	12.07'	406.00'	001°42'13"	S8°15'23"E	12.07'
C11	25.82'	406.00'	003°38'37"	S81°02'58"E	25.81'
C12	57.69'	406.00'	008°08'30"	S75°09'25"E	57.64'
C13	5.50'	3.51'	089°54'58"	N45°02'31"W	4.95'
C14	29.06'	18.50'	090°00'00"	N45°00'00"E	26.16'
C15	2.75'	3.50'	045°00'00"	S67°30'00"E	2.68'
C16	77.58'	413.50'	010°45'00"	N05°22'30"W	77.47'
C17	107.50'	236.50'	026°02'39"	N12°56'34"W	106.58'
C18	109.76'	413.50'	015°2'29"	S18°21'15"E	109.43'

U.E. LINE TABLE		
LINE #	BEARING	DISTANCE
L20	S88°21'23"W	80.67'
L21	N20°03'01"W	16.00'
L22	N68°20'58"E	67.50'
L23	N08°51'52"W	45.00'
L24	S80°19'25"W	13.67'
L25	N09°40'35"W	10.00'
L26	N80°19'25"E	13.67'
L27	N08°51'43"W	43.70'
L28	N89°55'30"W	92.87'
L29	S57°00'10"W	13.21'
L30	N89°55'30"W	4.25'
L31	N90°00'00"W	44.59'
L32	N00°00'00"E	135.70'
L33	N90°00'00"W	101.89'
L34	N19°55'35"W	13.26'
L36	N70°04'25"E	11.94'
L37	N19°55'35"W	10.00'
L38	S70°04'25"W	11.84'
L41	S89°56'12"E	7.19'
L42	N00°03'48"E	20.00'
L43	N89°56'12"W	37.44'
L45	S70°04'25"W	152.97'
L46	N70°04'25"E	154.96'
L47	S70°04'25"W	9.32'
L48	N19°55'35"W	10.00'
L49	N70°04'25"E	9.41'
L50	N02°21'54"W	2.50'
L51	N19°49'19"W	10.00'
L52	N70°10'41"E	11.55'
L53	N90°00'00"W	13.11'

U.E. LINE TABLE		
LINE #	BEARING	DISTANCE
L54	S00°00'00"E	14.50'
L55	N90°00'00"W	20.00'
L56	N00°00'00"E	14.50'
L57	N90°00'00"W	31.98'
L58	N00°00'00"E	135.70'
L59	N90°00'00"W	61.46'
L60	S00°00'00"E	6.50'
L61	N90°00'00"W	25.78'
L62	S00°00'00"E	6.73'
L63	N90°00'00"W	20.00'
L64	N00°00'00"E	12.19'
L65	N19°55'35"W	8.34'
L66	S89°55'30"E	90.57'
L67	S49°13'00"E	39.01'
L68	N89°48'15"E	39.20'
L69	S00°00'00"E	136.63'
L70	S62°11'25"E	36.89'
L71	N90°00'00"E	99.56'
L72	N00°00'00"E	103.34'
L73	N90°00'00"E	4.97'
L74	S89°55'30"E	84.86'
L75	S48°56'48"E	38.87'
L76	N89°48'15"E	29.13'
L77	N00°00'00"E	117.40'
L78	N90°00'00"W	16.17'
L79	S00°00'00"E	16.00'
L80	N90°00'00"E	22.19'
L81	S62°11'25"E	43.70'
L82	N90°00'00"E	108.22'
L83	N90°00'00"E	116.06'

U.E. LINE TABLE		
LINE #	BEARING	DISTANCE
L84	N00°00'00"E	13.00'
L85	N90°00'00"W	10.00'
L86	S00°00'00"E	13.00'
L87	N90°00'00"E	45.21'
L88	S00°00'00"E	22.50'
L89	N90°00'00"W	6.72'
L90	S00°00'00"E	7.81'
L91	S89°55'32"E	52.07'
L92	S00°04'08"W	15.00'
L93	N90°00'00"E	100.23'
L94	S00°01'24"W	34.00'
L95	N90°00'00"E	10.00'
L96	N00°01'24"E	34.00'
L97	N90°00'00"E	34.32'
L98	S00°00'00"E	45.29'
L99	N19°00'03"E	10.00'
L100	N70°59'57"W	10.00'
L101	S19°00'03"W	10.00'
L104	S00°04'30"W	30.01'
L105	N89°55'30"W	16.00'
L106	N00°04'30"E	16.01'
L107	N89°55'30"W	123.52'
L108	N00°04'30"E	10.50'
L109	S00°04'30"W	10.50'
L110	S00°04'30"W	10.00'
L111	N00°04'30"E	10.00'
L112	S89°55'30"E	44.00'
L113	N89°55'30"W	50.00'
L114	N04°14'45"E	18.00'
L115	S04°14'45"W	18.08'

U.E. CURVE TABLE					
CURVE #	ARC LENGTH	RADIUS	DELTA ANGLE	CHORD BEARING	CHORD LENGTH
C30	58.73'	418.00'	008°02'58"	N14°39'09"W	58.68'
C31	43.13'	418.00'	005°54'44"	N23°00'16"W	43.11'
C32	92.63'	232.00'	022°52'31"	N14°31'22"W	92.01'
C33	125.84'	262.00'	027°31'11"	S12°06'33"E	124.64'
C34	66.38'	378.60'	010°52'47"	N14°20'31"W	66.30'
C38	12.32'	388.00'	001°49'09"	N21°33'29"W	12.32'
C41	38.00'	424.00'	005°08'06"	S83°11'12"E	37.99'

- LEGEND**
- FOUND NO. 5 REBAR WITH 1-1/2" ALLUM. CAP STAMPED "TRMCS LS 28667"
 - SET NO. 5 REBAR WITH CAP STAMPED "ROCKY MOUNTAIN LS 28283"
 - △ FOUND NO. 5 REBAR WITH CAP STAMPED "SCOTT COX LS 24310"
 - FOUND 5/8" REBAR W/ ALLUM. CAP STAMPED "STABELE LS 26300"
 - P.U.E. --- PRIVATE UTILITY EASEMENT
 - C.U.E. --- CITY OF LOUISVILLE EXCLUSIVE UTILITY EASEMENT
 - U.E. --- UTILITY EASEMENT

ROCKY CREEK SURVEYING, LLC
3027 GARDENIA WAY
SUPERIOR, COLORADO 80027
(303) 521-7376

**City Council
Meeting Minutes**

**January 19, 2016
City Hall, Council Chambers
749 Main Street
7:00 PM**

Call to Order – Mayor Muckle called the meeting to order at 7:00 p.m.

Roll Call was taken and the following members were present:

City Council: *Mayor Muckle, Mayor Pro Tem Jeff Lipton;
City Council members: Dennis Maloney, Chris Leh,
Susan Loo, Jay Keany and Ashley Stolzmann*

Staff Present: *Malcolm Fleming, City Manager
Heather Balsler, Deputy City Manager
Kevin Watson, Finance Director
Kurt Kowar, Public Works Director
Troy Russ, Interim Planning & Building Safety Director
Sean McCartney, Principal Planner
Suzanne Janssen, Cultural Arts & Special Events
Nancy Varra, City Clerk*

Others Present: *Sam Light, City Attorney*

PLEDGE OF ALLEGIANCE

Mrs. Rachuinski's first grade class from Coal Creek Elementary led the pledge of allegiance.

APPROVAL OF AGENDA

Mayor Muckle called for changes to the agenda and hearing none, moved to approve the agenda as published, seconded by Council member Keany. All were in favor.

PUBLIC COMMENTS ON ITEMS NOT ON THE AGENDA

Jean Morgan, 1131 Spruce Street, Louisville, CO noted tomorrow, January 20, 2016 is the 80th anniversary of the Monarch Mine disaster. She asked Council to take a moment to think about the miners who made the town.

APPROVAL OF THE CONSENT AGENDA

Mayor Muckle called for changes to the consent agenda and hearing none, moved to approve the consent agenda, seconded by Council member Stolzmann. All were in favor.

- A. Approval of the Bills**
- B. Approval of Minutes; December 15, 2015 and January 5, 2016**
- C. Approval of Agreement with Resource Based International for 2016 Water Rights Administration**
- D. Approval of Resolution No. 5, Series 2016 – A Resolution Approving Agreements Between the City of Louisville and Dutko Worldwide, LLC D/B/A Grayling, and the City of Louisville and Boyagian Consulting LLC, to Furnish Lobbyist Services to the US 36 Mayors and Commissioners Coalition**
- E. Approval of Changes to the March 2016 City Council Meeting Schedule**

COUNCIL INFORMATIONAL COMMENTS ON PERTINENT ITEMS NOT ON THE AGENDA

No items to report.

CITY MANAGER'S REPORT

City Manager Fleming reported the Boulder Weekly recognized the Coal Creek Golf Course as the best golf course in Boulder County.

REGULAR BUSINESS

PROCLAMATION: ONE ACTION: ART + IMMIGRATION

Mayor Muckle requested a staff presentation.

Cultural Arts & Special Events Coordinator Janssen expressed her pleasure to accept the proclamation on behalf of the Boulder County One Action – Art + Immigration Steering Committee. This project is the first arts-based collaboration to take place in the County. The intent is to present programs that foster community conversation on historic and contemporary uses of immigration. Through the arts, personal expression and individual cultures will be shared throughout 2016. The hope is to be able to engage in meaningful discussion about ancestry and heritage and what everyone brings to the community. Extensive planning efforts began in early 2015. The One Action 2016 Project Kick-Off Celebration will be held at the Longmont Museum on Saturday, January 23, 2016 from 2-5 p.m. This event is free and open to the public. She invited and encouraged the public to attend the event.

In 2016 there will be programs and events throughout the County, which will bear the One Action Logo. In Louisville alone, 15 events are currently being planned. The programming will begin on February 19th at the Louisville Center for the Arts with Rock, Karma, Arrows; a 3-part film series with panel discussion addressing the early history and immigration of the Boulder County area.

She acknowledged the efforts of the Louisville Cultural Council, the Louisville Art Association, the Louisville Public Library and the Louisville Historical Museum, as well as Clay Art Pottery and individual artists, such as Dona Laurita, Dawn DeAno and Kat Fritz, all of whom are actively involved in One Action. She encouraged local artists, performers and organizations who are interested in participating in the project to contact her. The program information can be found on the City's Web Site.

She asked Mayor Muckle to share his contribution to the One-Action project. Mayor Muckle explained as Mayor he was asked to have his DNA tested. The reports documented his prominent Native American heritage and Basque ancestry. All of the Mayors in the County had their DNA tested as part of the program. He stated his understanding that artists will paint pictures of the Mayors based on their DNA.

Mayor Muckle read the proclamation, which proclaimed 2016 as One Action: Art + Immigration within Boulder County.

AWARD BID FOR 95TH STREET (COUNTY ROAD) BRIDGE REPLACEMENT

Mayor Muckle requested a staff presentation.

Public Works Director Kowar recommended Council award a contract to Hamilton Construction Co to rebuild the 95th Street Bridge over Coal Creek, which was destroyed in the 2013 flood. The contract amount is \$1,817,175.20, with a 10% contingency of \$180,000. Also under consideration is a contract extension with Michael Baker Jr. Inc., for additional design and construction management services for \$47,582.17. If approved, the staff can proceed with CDOT review and agreement to begin the construction of the bridge. It is anticipated the bridge construction will take six months after final CDOT approval. The construction anticipates a complete replacement of roadway from Bella Vista and south, past the Wecker property. There will be space beneath for a future trail. There will be aesthetic components, with a brick look and a three rail fence. The roadway will have 4' shoulders and 11' lanes in either direction.

Staff Recommendation: Staff recommended the City Council award the 95th Street Bridge Replacement Project to Hamilton Construction Co. per their bid of \$1,817,175.20, authorize a project contingency of \$181,717.52, and authorize the Mayor, Public Works Director and City Clerk to sign and execute contract documents on behalf of the City. Staff also recommended the City Council approve funds for additional design and construction management services for Michael Baker Jr. Inc., per their proposal fee of \$47,582.17.

COUNCIL COMMENT

Council member Maloney inquired whether the roadway would actually be open in six months or would the bridge just be replaced. Public Works Director explained the six months benchmark is when the project is complete and the roadway is open. He stressed the opening would be contingent upon the weather.

Council member Maloney noted Hamilton was the low bidder. He asked Public Works Director Kowar for his comfort level with this construction firm. Public Works Director Kowar stated he was very comfortable with the firm. Because it is a CDOT project, it came with more requirements. He noted any of the bidders would be qualified to complete the bridge project.

Council member Stolzmann explained this is a huge priority for the City Council and the Public Works Department. She felt there should be a City Council study session where Council could look at the results of the flood and the lessons learned. She stated the bridge will cost one million dollars less than expected, and she wondered if Council would have waited this long to have the bridge replaced had they known the actual cost.

MOTION: Mayor Muckle moved to award the bid for the 95th Street Bridge Replacement to Hamilton Construction Company in the amount of \$1,817,175.20, authorize a project contingency of \$181,717.52, and authorize the Mayor, Public Works Director and City Clerk to sign and execute contract documents on behalf of the City and approve funds for additional design and construction management services for Michael Baker Jr. Inc., per their proposal fee of \$47,582.17. The motion was seconded by Mayor Pro Tem Lipton. Roll call vote was taken. The motion carried by a vote of 7-0.

Mayor Muckle referenced the process and noted this is the last really big construction project resulting from the flood. He voiced his appreciation to the Public Works Department, City Manager's Department and all the Departments for their work on the flood recovery projects.

6TH AMENDMENT TO THE TAKODA GENERAL DEVELOPMENT PLAN (GDP) AND THE FOUNDRY PLANNED UNIT DEVELOPMENT (PUD) HIGHWAY 42 AND PASCHAL DRIVE

- 1. ORDINANCE No. 1712, SERIES 2016 – AN ORDINANCE APPROVING AN AMENDMENT TO THE TAKODA GENERAL DEVELOPMENT PLAN (GDP) TO REZONE THE PROPERTY FROM PCZD-C TO PCZD-C/R– SECOND READING - PUBLIC HEARING**
- 2. ORDINANCE No. 1713, SERIES 2016 – AN ORDINANCE APPROVING THE VACATION OF VARIOUS EASEMENTS ON LOT 1, BLOCK 9 AND TRACT T OF TAKODA SUBDIVISION, AND LOT 2 OF SUMMIT VIEW SUBDIVISION – SECOND READING - PUBLIC HEARING**

3. RESOLUTION No. 3, SERIES 2016 – A RESOLUTION APPROVING A FINAL PLAT AND FINAL PLANNED UNIT DEVELOPMENT (PUD) TO CONSTRUCT A MULTI-USE DEVELOPMENT CONSISTING OF 24 AGE RESTRICTED CONDOMINIUMS, 8 NON-RESTRICTED CONDOMINIUMS, AND 38,000 SF COMMERCIAL AND OFFICE LAND USES

Mayor Muckle requested a City Attorney introduction.

City Attorney Light introduced Ordinance Nos. 1712 and 1713, Series 2016 and Resolution No. 3, Series 2016. Members of the public may speak on any of the three agenda items.

Mayor Muckle opened the public hearing and requested a staff presentation.

Principal Planner McCartney explained several emails were received after the packet was assembled. Council member Stolzmann requested several informational items and staff's response to her requests were placed at the dais for the City Council to review.

The request before the City is for a rezoning, Final Plat and Final Planned Unit Development (PUD) to construct a multi-use development consisting of 24 age restricted condominiums, 8 non-restricted condominiums, and 38,000 SF commercial and office land uses. The subject property is located in north Louisville and zoned PCZD-C. The applicant is requesting PCZD-C/R zoning of 5.82 acres for a mixed-use development. The property is south of Indian Peaks, Filing 17.

Comp Plan: The 2013 Comp Plan identifies this area as an "Urban Corridor" with a focus on commercial, office, neighborhood retail and residential density allowance up to 25 units per acre. Principal NH-5 calls for a mix of housing types; multi-generational needs and empty nesters. The proposal is for 24 age restricted units for empty nesters.

Rezoning: The property is surrounded by PCZD-C/R and PCZD-R zoning and complies with the surrounding zoning. Public Land Dedication (PLD): 3% additional PLD for the residential portion of property. The commercial zoning has already been dedicated. The original site plan included 3 access points, no access to Kaylix Street, 48 residential units, 56,200 SF commercial (two story in-line commercial) two drive-thru's and two in-line commercial uses. Residents requested age restricted housing and no drive-thru's. The applicant then resubmitted the application.

Site Plan: This plan has four primary points: Highway 42 – right-in/out; Paschal Drive – right-in/out; Kaylix Street – full access and Summit View – full access. It includes 32 residential units (24 age restricted to 55 years); 37,500 SF commercial (2 story in-line 17,850 SF and flex commercial 14,110 SF); no drive-thru's and 229 parking spaces.

Bulk and Dimension Standards: Height complies with CDDSG; Setbacks comply with

GDP and the 2-3 stories are compliant with the Comp Plan. The commercial component includes office; neighborhood retail; flex artisan space; close proximity to the roadway and complies with the CDDSG and Comp Plan.

Original Architecture: Height - 30 feet; Architecture 2nd Submittal - Commercial – 28.5 feet in height; 2-story and 17,850 SF. Residential: 32 units (24 age-restricted, 55 years and older and 8 non-restricted units); 35 feet maximum height; buffer between commercial and existing residential. Boulder Valley School District estimates 8 unrestricted units will result in 1 student at LES, 0 students at LMS, and 1 student at Monarch High.

Residential Parking: 64 spaces (2 per unit) and enclosed garage spaces are compliant with the Louisville Municipal Code. Commercial Parking: 165 spaces. CDDSG requires 4.5 spaces per 1,000 SF – 5.16 spaces per 1,000 SF if measured at 85% GLA (31,960 SF), 4.4 spaces per 1,000 SF at 37,600 SF (6 spaces less than required). Waiver approved through LMC for multi-tenant reduction, public easement in excess of Public Land Dedication and exceptional design.

Landscaping: Waiver requested to reduce amount of street due to existing easements and powerlines. Staff believes alternatives can be achieved by speaking with easement owners. Applicant will continue to work with staff on final tree placement.

Staff recommendation: Staff recommended approval of Ordinance Nos. 1712 and 1713, Series 2016 and Resolution No. 3, Series 2016 with the following conditions:

1. The 24 age restricted condominiums shall be for ages 55 and older. The 55 years and older age restriction shall be placed on the deed of each age restricted unit and shall also be included in the subdivision agreement and a covenant agreement enforceable by the City of Louisville.
2. Staff recommends the wall signs of the In-line building, shown as vertical address numbers, be removed from the PUD and all wall signs must comply with Chapter 7 of the CDDSG and Chapter 17.24 of the LMC.
3. The applicant shall continue to work with the Parks Department on the type and location of additional trees along Highway 42, prior to recordation.
4. The applicant shall continue to work with the Public Works Department on the items listed in the October 25, 2015 memo. Each item shall be completed prior to recordation.
5. Residential and Commercial Development shall be constructed concurrently.

APPLICANT PRESENTATION

Justin McClure, RMCS, 2100 Sunset Drive, Longmont, CO presented the Foundry Development proposal. He stated in his mind Steel Ranch is an unfinished project. He wanted to complete the project in a quality way and is sensitive to the residents concern relative to more residential development. He explained to complete the project there is

property to develop south of Steel Ranch, which has a commercial/retail component.

He addressed the development of an adjacent project in the City of Lafayette and voiced his opinion it is not of the same quality of development found in Louisville. He felt Louisville could do better. He addressed the great recession and the economic meltdown with the elimination of big box stores. He noted the Lafayette property was zoned commercial and the developer, McStain, sold the property to get the cash. He did not want the property south of Steel Ranch to meet the same fate and that is the reason for bringing forth the Foundry development project. They hosted a community meeting at the Recreation Center to receive public input. With that input they resubmitted their proposal for the Foundry.

He noted most of the development in Louisville has been in Ward I with the North End Project; Steel Ranch and The Lanterns. He requested Council approval of the Foundry to complete the development. The Foundry contains 28 age-restricted units and 8 non-age restricted units and will be a vibrant development containing retail, boutique services and adaptable spaces for entrepreneurs. The adaptable spaces will include retail on the bottom floors and 2.5 stories for condominiums, which lends toward outdoor living. Every unit will have living space above and has elevator access. He presented site plans and artists renditions of the proposal.

COUNCIL COMMENT

Council member Maloney inquired about the metrics of the design and asked Mr. McClure how the new design differs from the original metrics design, which did not work. Mr. McClure explained the development of the condominiums will fund the speculative development on commercial property. He explained the retail viability is what the property can support.

Council member Maloney asked if the developer anticipates the same success as The Source has in Denver. Mr. McClure explained there are eight bays and not quite as many tenants as The Source. He explained currently it models with the potential rents for those spaces. The rents will be discounted upfront in order to get the right tenants and to meet the requirements of the lenders.

Council member Stolzmann explained she submitted a number of detailed questions to the staff earlier this afternoon. She asked whether Council wished to review staff's responses during a recess or whether the staff should respond to her questions at this time. Mayor Muckle requested the staff respond to Council member Stolzmann's questions on the record. Interim Planning and Building Safety Director Russ reviewed the responses to Council member Stolzmann's questions as follows:

- 1) The applicant and the Planning Commission (minutes) cite retail vacancies over and over again- what is the retail vacancy rate (percent) in a 1 mile radius of the site and what is to be expected during a reasonably strong economic period?

Staff's response: Utilizing the Xceligent database, of the 29 retail properties within one mile of the location, there is a total of 511,540 square feet of leasable area and as of Q4 2015, there is 25,991 sf available resulting in a vacancy rate of 5.1%. In Q4 2012, there was a vacancy rate of 14.6% for the same area. Vacancy rates above 10% for retail is viewed as an early sign that challenges exist for the market.

- 2) How many properties have been required or will be required to remove driveways from HWY42 as part of the HWY 42 Plan and what is our City Traffic Engineers opinion/recommendation of the driveway onto 42? Staff's response: 8 driveways will be removed; the plan was approved by the City Traffic Engineer.
- 3) Could you include the Fire Departments Referral Comments? Staff response: The Fire Marshal comment letter was submitted.
- 4) Can you make a table explaining the property tax structure on this property (including metro district) and how the mills change with the change in zoning- including a comparison showing one commercial property to the many broken up areas. Staff's response: Commercial property is taxed at 29% of market valuation, while residential is taxed at 7.96% of market valuation. According to the model, the proposed development would generate \$22,000 per year in property tax at buildout, with a 20 year cumulative total of \$408,000. The original GDP would have generated \$29,000 per year and \$517,000 cumulatively. A table was presented.

COUNCIL QUESTIONS

Council member Stolzmann addressed tracts A through D and Blocks 1 – 6 and asked if there were individual properties. Interim Planning and Building Safety Director Russ explained tracts are typically public property/public shared spaces (Parks and Plazas). Those tracts are not revenue generating properties.

Council member Stolzmann asked if either the City or the Metro District would receive any revenue from those tracts. City Attorney Light explained if tracts are owned by an association, they would not have their own separate tax ID and separate tax bill. Under the Common Interest Act, the value of the residential and commercial property is parceled out and assumed as part of the value of the private land. None of the entities would realize the benefit of the land on a tax bill specific to a common area.

Council member Stolzmann inquired how the benefit would be distributed to a parking area in a commercial area. Mr. McClure explained parking for the commercial uses would be valued for the commercial units and would be collected with the commercial units' tax bills. The driveways and parking spaces for the residential uses would be valued for condominium units and would be collected with the residential tax bills.

Council member Stolzmann explained this Metro District has a steep mill rate and she wanted to ensure each parcel was paying their fair share.

Questions No 4) a and 4) b:

- a. What is the zoning of the parking lots? Staff's response: PCZD-C/R, same as entire property.
- b. How does this compare to other commercially zoned properties which include parking lots in the area (say the Walgreens on SOBORO or the Union Jack). Another way of asking is, can you show the mill rates in a column added to the table on page 136 & show how that is a change from the existing land use and explain how the assessment works with regard to properties with and without improvements? Staff's response: Answered above.

5) How many residential units were in each phase of this GDP and how much commercial was in each phase?

Staff's response:

- a. Original GDP – Ord. 1536, Series 2008: Creation of Takoda GDP, 350 Units in 4 Planning Areas and 71,743 SF of commercial development in Planning Area #1.
- b. 1st Amendment – Ord. 1576, Series 2010: Unit swap between Planning Areas, (no change in density) and no change to commercial square footage in Planning Area #1.
- c. 2nd Amendment – Ord. 1601, Series 2011: Added Steel Ranch South; Increased density by 104 units (306 total) and no change to commercial square footage in Planning Area #1
- d. 3rd Amendment – Ord. 1656, Series 2014: Added the Lanterns – 24 Units and no change to commercial square footage in Planning Area #1
- e. 4th Amendment – Ord. 1680, Series 2015: zoned 245 North 96th Street PCZD-C/R: 231 Affordable housing units and 18,406 SF of additional commercial square footage.
- f. 5th Amendment – Ord. 1710, Series 2015: Expanded commercial from 18,406 SF to 64,468 SF of commercial square footage.
- g. 6th Amendment – Ord. 1712, Series 2016: The Foundry – adding 32 Units (24 age restricted), while reducing the allowed commercial development to 37,100 SF in Planning Area #1.

COUNCIL QUESTIONS

Council member Stolzmann asked Mr. McClure why he could not leverage the 478 units to fund the commercial component. She asked what was so special about the 32 units. Mr. McClure explained it was because of the global economic meltdown and the level of support it would take for speculative commercial, in order to collect rents. Council member Stolzmann asked if they have leases. Mr. McClure explained he is currently working on discussion of leases.

Council member Loo inquired about the 104 units and the 306 total. Principal Planner McCartney explained the 306 units were derived by adding 104 units to their allowable 202 units on North Main. Earlier amendments adjusted the numbers in Steel Ranch South, which added 104 additional units. Steel Ranch South has a total of 306 units.

- 6) Please provide the assumptions for the fiscal model in a table (income, retail \$/sqft, absorption year for retail, any modifications to capacity factors from the base, and so on). Staff's response: Attachment #1 (Foundry Fiscal Model Assumptions).

- 7) What is the impact to the general fund (revenue and expense) if the retail is occupied in year 3, 10 or never? Staff's response: Fiscal Model Attachments # 3 year, 10 year, 20 year (Cumulative Combined Funds Results – Fiscal Impact Model.)

COUNCIL COMMENTS

Council member Stolzmann stressed the importance of having the fiscal model for development projects. She stated her understanding the condo residents will spend approximately \$276 per week in Louisville. When the household income is above the median, there is an assumption goods can be bought in Louisville.

- 8) Does the applicant own or have some right to design and rezone the Summit View subdivision? Staff's response: Yes, they own the property.

- 9) The drawings do not clearly depict internal circulation on the site. Does the alleyish road that runs North South go through? Staff's response: The internal roadway shown on the PUD is a private drive and provides access north, south, east and west.
 - a. Is it a named street? Staff's response: No.
 - b. Who is responsible for maintenance? Staff's response: The Developer.

- 10) The staff report refers to condominiums, which implies to me that the units being built are individually owned however I do not see the properties segregated on the plat. Are these really apartments? Staff response: We have been told they are condominiums. The City of Louisville does not have a condo platting process. These are typically done through the County.

- 11) What guidance is there in the City Code regarding rezoning policy? Staff's response: This is a rezoning only in terms of modifying the General Development Plan (GDP) which is processed as a Planned Community Zone District (PCZD) as established in Section 17.72. This request is an amendment to an existing GDP. City Attorney Light commented on changing plans to address condos and noted it would be a legislative change to provide the regulatory authority on filing a condo plat, which would be a subdivision action. If the PUD is for apartments and there is a desire for

condos, there would have to be a separate condo plat to create a legal interest in the air space. There is still a compliance with the PUD.

COUNCIL QUESTIONS

Council member Stolzmann asked what enforcements or assurances does the neighborhood have. Interim Planning and Building Safety Director Russ explained it could be conditioned in the resolution. City Attorney Light stated there is probably language in the plan, but confirmed it could be conditioned in the resolution. Mr. McClure confirmed the Final Development Plan refers to the units as condominium units.

Mayor Muckle asked if the Final Plan assured park spaces have permanent public access easements. Interim Planning and Building Safety Director Russ confirmed it did.

Council member Keany asked if the applicant accepted the six conditions. Mr. McClure confirmed the applicant accepts all six conditions.

Council member Maloney noted there were several emails from the public and addressed the concern for the Paschal median and the light requirement. Interim Planning and Building Safety Director Russ explained there have been neighborhood requests for direct left turn access off Paschal into the development. The staff is working with applicant to make that entrance a right in/right out. He stressed a left turn access is not an appropriate movement with a future signal light coming to this location.

Council member Maloney inquired about reducing the lighting requirements along Kaylix sidewalks. Principal Planner McCartney stated staff can look at the lighting for traffic and pedestrian safety.

PUBLIC COMMENT

Gary Larson, 2189 Park Lane, Louisville, CO stated he will be looking at the condos from his back porch. He explained he is a member of the newly formed Steering Committee for the 95th Street Coalition. They want to ensure any residential development is compatible with the existing community and any commercial development is economically viable. At their first meeting, Mr. McClure presented the Foundry proposal. After the meeting, the applicant made changes to incorporate the public concerns. The Coalition feels this development is compatible with the community. They propose a do not block box in the eastbound lane. He addressed the street lights and noted the Steel Ranch patio homes are on timers. He noted at the Planning Commission meeting, they discussed bringing back the water tower.

Peter Wengert, 872 Meadow Lark Lane, Louisville, CO stated there is a very good positive feeling about this project. The residents feel it is a people friendly project.

There are approximately 1,000 walkers in Steel Ranch who are looking forward to walking to the Foundry. He felt this will be a beautiful entry way into the City and voiced his support for the project.

Dave Ireland, 2358 Park Lane, Louisville, CO stated he is an enthusiastic supporter of the Foundry project.

Sherry Sommers, 910 Palisade Court, Louisville, CO stated her understanding this project is in compliance with the Comprehensive Plan and has the support of the neighbors. She inquired about the role of the small area plan in this development. She noted last spring the City Council stated there would not be more rezoning and urbanization in this area until the impact of the development could be analyzed. She also addressed the project's height and stated her understanding the maximum height for most residential units is two stories. He noted these units will be 2-3 stories. She stated a lot of people worked hard on the small area plan and the plan should be considered.

Sandy Stewart, 649 August Drive, Louisville, CO voiced his support for the project.

Alex Bradley, 1385 Caledonia Circle, Louisville, CO inquired why all the units are not age-restrictive. She wanted confirmation all the age-restricted units will be universal in design. She voiced her concern over the Foundry commercial component and noted the square footage was too small. She voiced her concern over the school enrollment at Louisville Elementary. She reported on meeting a local resident, who sends her children to school in Broomfield, because LES is too large. She stressed the BVSD referrals are old and out dated and should be redone. She requested all the age-restricted units be universal in design and for an explanation on why all 32 units cannot be age-restricted to solve the school issue.

COUNCIL COMMENT

Council member Stolzmann was also interested in knowing why all the units could not be age restricted, the issue of the small area planning and how they are impactful.

Mr. McClure stated there is a need for condo units for adults who are not 55 and do not wish to do yardwork anymore. Condos are a product type, which can provide such for those individuals.

Interim Planning and Building Safety Director Russ explained the small area plan was not applicable to this application as the plan has not been adopted.

Mayor Pro Tem Lipton stated he struggled with balancing the enthusiasm for condos against some significant policy issues related to density, infill and the request for commercial property owners to stimulate their project, by including residential components, not included in the original zoning. He voiced his concern for other commercial property owners who may request equity on how they are treated. He

stressed the importance of being fair, equitable and consistent. He did not believe the Council has finished its planning for potential growth. He noted there is citizen concern for the added stress on City services as new population is added. He did not believe the Council has discussed the broad principles and policy issues associated with this request.

Council member Keany stated he understood Mayor Pro Tem Lipton's concern. He asked the City Attorney whether the City is creating precedence on the Council's decision making in looking at this project and whether Council is following the City's Code. City Attorney Light explained this is a timing question. A rezoning is evaluated in light of the objectives, purposes and provisions of the Comprehensive Plan. If the small area plan is not adopted, it is not a part of the Comprehensive Plan. There are legal methods to close the time gap, but they are not available at this time. Action on one application does not have any bearing on another application being adjudicated under its own process, based on the law in effect at the time. If Council desires to make future decisions after the additional Comprehensive Plan is completed there must be a mechanism to close the time gap.

Council member Keany addressed the quasi-judicial process before the Council. He asked whether the Council was required to approve or disapprove the application this evening. City Attorney Light stated it is a matter of judgment and criteria for rezoning under common law and in the Louisville Municipal Code. It is an evaluation of judgment of a broad criteria relating to the question of whether the request is consistent with the policies and goals of the City's Comprehensive Plan. With respect to rezoning, Council must consider whether the rezoning change is in the public interest. Another criterion is whether the rezoning would be to provide land for a community use.

Mayor Pro Tem Lipton asked if there was a criteria related to a community benefit. City Attorney Light explained it is by referencing the desires of the community expressed in the Comprehensive Plan.

Mayor Pro Tem Lipton voiced his concern for a consistent process. He expressed his frustrations the small area plans have not been adopted. He was concerned the development would begin before the small area plan is complete and there will not be any guidelines. He noted if the small area plans are not adopted, the Council will not be able to use those tools in their decision making.

Council member Loo stated she also struggled with this development, but after listening to the public input, she was convinced this is a great project. She liked the design and the quality of the development. She felt if the development is not approved today, the land may lay vacant. With respect to the school issue, she did not feel this would add students to local schools. She did not agree with the full movement entrance on Paschal Drive and stated the signage needs improvement. She stated she was pleasantly surprised with the positive fiscal analysis. She noted many Louisville seniors are looking for this type of housing.

Council member Maloney stated when he first looked at this proposal he was opposed because of the erosion of the commercial space. After staff's presentation and the public input, he believed it was a quality proposal. He agreed with Mayor Pro Tem Lipton with respect to being consistent and fair. He also was concerned over the erosion of the City's commercial base.

Council member Leh supported the project because it would be a quality development. He agreed it is unfortunate the small area plans have not been adopted to provide guidance, but congratulated everyone on the process. He felt this would be a good project because of the age-restricted units, which would have less impact on traffic and the schools. He was concerned about what may go into the property, if the proposal is denied.

Council member Stolzmann commented she initially felt the development was not compatible with the surrounding homes, but after the neighborhood support, she has changed her mind. She felt there should be some language added to ensure condominiums and not apartments are built. She felt all the units should be age-restricted to satisfy the school and traffic issue and would be a valid reason for the rezoning. She addressed the intersection at Paschal Drive and stressed the importance of not creating an unsafe intersection. She requested comments on age-restriction and condo language. She stated the fiscal impacts are consistent with the Comprehensive Plan. She noted the \$600,000 condo units will be well above the City's median income level and those residents will be spending their dollars in Louisville. She had no opinion on the water tower and confirmed it is still in the project.

Mayor Muckle stated he was impressed by the comments, both from the public and from the Council. He stated there are definitely reasons to deny the application based on the loss of commercial and the densification, but felt the reasons to approve far outweigh those concerns, especially when considering the age-restricted units. He agreed it will be the northern gateway to the City. He felt the fiscal outcomes are acceptable. He noted there is neighborhood support for the development. He did not feel a decision on one project influences any other, as each project is judged on its own merits. He supported the water tower and well-lit sidewalks for walkers.

Council member Keany supported adding language stipulating condos only. He was comfortable with the 24 age-restricted units and leaving the remaining 8 market rate. He also supported keeping in the water tower.

Council member Maloney asked if there were five or six conditions. City Attorney Light stated there are five conditions on the PUD ordinance and one condition for the zoning ordinance regarding use issue. There is also a sixth condition for the PUD Resolution.

PUBLIC COMMENT

Jeffrey Gass, 784 Meadow Lark Lane, Louisville, CO voiced his support for the project.

He stated the Steel Ranch community is in full support of the project. He supported adding to the tax base instead of leaving the land vacant. It will improve the north entrance into Louisville by adding unique steel buildings, which would be different from the south entrance into the City and seeing the empty Sam's Club.

Debbie Fahey, 1118 Enclave Circle, Louisville, CO supported the project because of the age-restricted units and was in favor of having all the units age-restricted.

Mayor Muckle called for public comment and hearing none, closed the public hearing.

COUNCIL COMMENTS

Mayor Pro Tem Lipton stated he would vote in favor of the application although he had concerns over the loss of commercial. He voiced his frustration with not having the tools in the small area plan. He voiced his hope guidelines could be accomplished after the Council Retreat.

City Attorney Light reviewed the City Council's requested revisions to Ordinance No. 1712, Series 2016: In the last WHEREAS: WHEREAS, the PCZD-C/R zoning classification for the Property as further set forth on the Takoda GDP 6th Amendment, subject to the conditions herein, is consistent with the City of Louisville 2013 Citywide Comprehensive Plan.

Section 1. The City Council of the City of Louisville hereby approves the Takoda GDP 6th Amendment (the "Takoda GDP 6th Amendment") for the property legally described in Exhibit A attached hereto (the "Property") and, pursuant to the zoning ordinances of the City, such Property is zoned Planned Community Zone District Commercial/Residential (PCZD-C/R) for the uses permitted in the Takoda GDP for the Property, a copy of which Takoda GDP 6th Amendment is attached hereto as Exhibit A, subject to Section 2 hereof and subject further to the condition that a note shall be added to the GDP stating that drive-thru restaurants and automobile service stations are a prohibited use within the GDP and that single family attached dwelling uses are limited to duplex, townhouse and condominium uses, with apartments prohibited.

ORDINANCE No. 1712, SERIES 2016

MOTION: Council member Keany moved to approve Ordinance No. 1712, Series 2016, as amended by the City Attorney, seconded by Mayor Muckle. Roll call vote was taken. The motion carried by a vote of 7-0.

ORDINANCE No. 1713, SERIES 2016

MOTION: Mayor Muckle moved to approve Ordinance No. 1713, Series 2016, seconded by Council member Keany. Roll call vote was taken. The motion carried by a vote of 7-0.

City Attorney Light reviewed the City Council's requested revision to Resolution No. 3, Series 2016, which adds Condition 6. Add a sentence to the PA-1B General Notes, item 1, stating "This PUD authorizes only condominium project type development." He asked Council for their preference in the number of age-restricted units.

Council Discussion: Mayor Muckle, Council member Loo, Leh, Keany and Maloney supported 24 age-restricted units. Council member Stolzmann supported all 30 units.

City Attorney Light added the following language to the revised condition: Further, revise the phrase "a potential amount of units" to state instead "24 units."

RESOLUTION No. 3, SERIES 2016

MOTION: Mayor Muckle moved to approve Resolution No. 3, Series 2016 with the six conditions as cited by the City Attorney, seconded by Council member Loo.

Council member Loo voiced her frustrations with signage and offered a friendly amendment to eliminate condition number 2. Mayor Muckle did not accept the amendment.

MOTION: Council member Loo moved to strike condition 2 from the resolution, seconded by Council member Keany.

Council member Stolzmann preferred to have public comment on the matter.

Council member Leh did not support the amendment. Council member Keany voiced his support for the amendment.

VOTE ON THE AMENDMENT: Roll call vote was taken. The motion failed by a vote of 5-2. Mayor Muckle, Mayor Pro Tem Lipton and Council members Maloney, Leh and Stolzmann voted no.

VOTE ON THE MAIN MOTION: All were in favor.

1125 PINE STREET MINOR REPLAT

- 1. ORDINANCE No. 1711, SERIES 2016 – AN ORDINANCE APPROVING A REZONING OF A PARCEL OF LAND LOCATED AT 1125 PINE STREET FROM CITY OF LOUISVILLE COMMERCIAL COMMUNITY (CC) TO MIXED-USE RESIDENTIAL (MU-R) AND RESIDENTIAL MEDIUM DENSITY (R-M) AND AMENDING THE BOUNDARIES OF THE OLD TOWN OVERLAY DISTRICT IN CONNECTION THEREWITH – 2ND READING – PUBLIC HEARING**

2. RESOLUTION No. 2, SERIES 2016 – A RESOLUTION APPROVING A REPLAT TO COMBINE THREE PARCELS AND SUBDIVIDE THE PROPERTY INTO TWO SEPARATE LOTS AT 1125 PINE STREET

Mayor Muckle requested a City Attorney introduction.

City Attorney Light introduced Ordinance No. 1711, Series 2016 and Resolution No. 2, Series 2016 and noted members of the public may speak on either of the agenda items.

Mayor Muckle opened the public hearing and requested a staff presentation.

Interim Planning and Building Safety Director Russ explained the request for rezoning, replat to combine three parcels to subdivide the property into two separate lots, rezoned mixed use residential (MU-R) and Residential Medium Density (RM). The subject property is located on the north side of Pine Street between the Burlington Northern Santa Fe Railroad and Highway 42. It is currently zoned Commercial Community Zone (CC) and part of the Highway 42 Revitalization area. The lot is 15,813 SF.

Section 16.16.050 (C) of the Louisville Municipal Code requires the maximum depth of all residential lots not to exceed 2 ½ times the width of the lot. For all other lots, the depth shall not exceed three times the width. The dimensions for the proposed Lot 2 are approximately 230' X 55' from the northernmost corner to the southernmost corner. The depth is 4.18 times the width. Lot 2 does not comply with the Code. Section 16.24.010 of the Louisville Municipal Code grants the City Council, upon advice of the Planning Commission, to authorize modifications from the requirements in cases where there is exceptional topographical conditions or other conditions peculiar to the site. Staff believed the site is a "peculiar" shape due to the abandoned railroad right-of-way and existing depth of the lot. The subdivider would not be able to provide two lots, which meet the depth to width ratio while providing the required lot frontage. Staff recommended the City Council authorize the modification.

Proposed Zoning: The required rezoning of this property must be consistent with the framework provided Land Use Exhibit A in the MUDDSG. Lot 2 – Residential Medium Density: 10,502 SF allows up to three residential units. Staff recommended the proposed Lot 2 be included with in the Old Town Overlay Zoning District. If authorized, the Old Town Overlay will be amended to include the proposed Lot 2, which does not require a PUD. Lot 1: Mixed Use – Residential: 4,703 SF must comply with the MUDDSG and requires a PUD. The existing single-family dwelling is considered a legal, non-conforming use and can continue with its use as a single-family home.

The Planning Commission held a public hearing on the application on December 10, 2015 and voted 6-0 to approve the replating as well as the rezoning and recommended City Council approval. Staff recommended City Council approval of Ordinance 1711, Series 2016 and Resolution No. 2, Series 2016.

APPLICANT PRESENTATION

Arn Rasker, 4782 Valhalla Drive, Boulder, CO explained he represents the owner, who lives out of state. He explained this project began when the City requested a right-of-way easement for the new drainage plan on the northern parcel. He explained nothing could be done with the property until it conformed to the new zoning overlay. Once the zoning is approved plans to develop the property can begin. He noted this project will add commercial space, which is currently under design.

Jean Morgan, 1131 Spruce Street, Louisville, CO explained in the packet, sometimes the street is referred to as Lee Street and other times it is referred to as Lee Avenue. She requested it be referred to as Lee Avenue. She addressed the Spruce side addition and asked if it would be compatible with the existing homes on Spruce Street. Interim Planning and Building Safety Director Russ confirmed it would be compatible.

Ms. Morgan addressed the 15% public land dedication and voiced her concern that parking for the units would impact the historic miner's cabins. She requested the 15% public land dedication be for land to separate the development from the miner's cabins. She asked for confirmation there will be approval for 3-units. Interim Planning and Building Safety Director Russ confirmed there could be up to 3-units. She felt preserving the historic cabins was important.

Interim Planning and Building Safety Director Russ confirmed the subject property is south of the miner's cabins. He explained there is a drainage easement between the cabins and the subject property, which is part of the Flood Plan Improvement project.

COUNCIL COMMENT

Council member Stolzmann addressed the notion of hardship and lot layout and inquired about the long range layout for the area. She referred to the lot lines and completed calculations on the depth of the lot. She calculated it would be 125.9' deep from the property's east property line. The applicant calculated 137.2' deep. She understood why it should not apply to the whole property, but did not feel it would create a hardship to apply from the street and back (south of Spruce Street). She felt the applicant was trying to maximize the lot depth of Lot 2.

Council member Keany asked for clarification it would add 12 feet to Lot 1 on Pine Street. Council member Stolzmann confirmed it would add 12 feet.

Mayor Muckle inquired how the angled portion of property would be used. Interim Planning and Building Safety Director Russ explained it is a land dedication for Spruce Street, which is not currently part of the City's right-of-way, but has access from Spruce.

Mayor Muckle asked if the public land dedication could be for a public park for the miner's cabins. Interim Planning and Building Safety Director Russ explained the staff worked with the Parks Division and Historic Preservation and this land is not in any

adopted plans and therefore, payment in lieu is recommended.

Mayor Muckle requested the measurement for public land dedication for the north lot.

Council member Leh left the meeting at 10:05 p.m.

Council member Maloney inquired about the zoning of adjacent lots. Planning and Building Safety Director Russ explained the property is currently in the Highway 42 Revitalization Plan, which extends to South Boulder. Any request requires a mandatory rezoning. A replat is an intent to redevelop the property and Council has the option to consider the waiver.

Council member Keany inquired whether the odd depth of the property line is located on the north side. Interim Planning and Building Safety Director Russ explained Council member Stolzmann is suggesting if the property line is moved 12' north, that portion of the site where the development would likely be would be more consistent with the Louisville Municipal Code.

Council member Keany asked if that would change the number of units allowed. The applicant, Mr. Rasker stated the recalculation would increase the square footage of the southern lot, which would increase the allowance for commercial and above residential. He felt the larger area on the back lot would be advantageous because it would minimize what is built and allow for parking. The recalculation would also reduce the number of units on the northern lot from 3 units to 2 units.

Council member Keany explained Council is asked to consider a waiver for this. Mr. Rasker noted the owner has provided the easement and the triangular piece to the City. He noted it is not a minor thing to replat the entire area.

Planning and Building Safety Director Russ stated, in response to Mayor Muckle's question about the measurement for public land dedication, a change in the calculation would reduce the square footage by approximately 6,000 square feet, which would reduce Lot 2 by 660 SF.

Council member Keany inquired why the triangular piece of property is not acceptable as cash in-lieu. Interim Planning and Building Safety Director Russ explained public land dedication is for public use. Easements and streets are not eligible for public land dedication.

Council member Keany asked if a two lot subdivision could be done without a PUD. Interim Planning and Building Safety Director Russ explained a PUD is not required for a minor subdivision.

Council member Keany asked what would prevent the applicant from subdividing the second lot. City Attorney Light explained if the applicant met the yard and bulk

requirements they could subdivide the lot, but would have to provide legal access to both lots and provide a new subdivision plat that meets and the requirements.

Interim Planning and Building Safety Director Russ recalculated the public land dedication to be 2,000 SF and the 15% requirement would be 1,575 SF.

Mr. Rasker explained the lot is not wide enough to subdivide, and there would not be any access.

Mayor Muckle inquired about the minimum lot in the RM zoning. Principal Planner McCartney stated it is 7,000 SF, but in the MUR zoning there is no minimum lot size.

Jean Morgan, 1131 Spruce Street, Louisville, CO noted the entire area will be developed eventually. She suggested running Spruce Street to the west to access this development. This would allow a border for the south side of the miner's cabins. She requested the Council provide a small park near the cabins.

Interim Planning and Building Safety Director Russ explained the minimum area per unit is 3,500 SF in the RM zone district. Three units will fit into the 10,500 SF, but 10,049 SF will not provide for the three units.

Mayor Muckle called for public comment and hearing none, closed the public hearing.

COUNCIL COMMENTS

Council member Stolzmann proposed the lot line be moved to the right to 125.9. This will take into account the odd angle of the lot and give the width to the applicant. This also ensures the neighborhood can allow the density for the width of the lot. She felt this would be reasonable and consistent with the Louisville Municipal Code.

Mayor Muckle voiced his support and suggested the land dedication be close to the miner's cabins to allow a pocket park. Council member Loo requested a map be drawn to reflect the recalculations.

Interim Planning and Building Safety Director Russ explained if the calculations are changed the applicant must be allowed to respond.

Council member Keany suggested continuing this matter to allow the applicant and staff time to discuss alternatives. There was Council consensus.

Mr. Rasker explained he could not move the lot line without the consent of the owner. He stated the land is private property and if the City wanted the northern portion for a park, they could discuss purchasing it from the owner. He explained the owner has already been delayed in developing his property when the City wanted it for a street. He would discuss moving the lot line with the owner and requested a continuance.

ORDINANCE No. 1711, SERIES 2016 AND RESOLUTION No. 2 SERIES 2016

MOTION: Mayor Muckle moved to continue Ordinance No. 1711, Series 2016, and Resolution No. 2, Series 2016 to February 2, 2016, seconded by Council member Keany. All were in favor.

633 CTC BOULEVARD PLANNED UNIT DEVELOPMENT

- 1. ORDINANCE No. 1714, SERIES 2016 – AN ORDINANCE APPROVING THE VACATION OF AN EASEMENT WITHIN LOT 5, COLORADO TECHNOLOGY CENTER FILING NO. 2 SUBDIVISION – PUBLIC HEARING**
- 2. RESOLUTION No. 4, SERIES 2016 – A RESOLUTION TO APPROVE A FINAL PLANNED UNIT DEVELOPMENT (PUD) PLAN TO CONSTRUCT A 153,018 SF SINGLE STORY INDUSTRIAL/FLEX BUILDING WITH ASSOCIATED SITE IMPROVEMENTS FOR LOT 1, BLOCK 4, THE BUSINESS CENTER AT CTC**

Mayor Muckle requested a City Attorney introduction.

City Attorney Light introduced Ordinance Nos. 1714, Series 2016 and Resolution No. 4, Series 2016. Members of the public may speak on either agenda items.

Mayor Muckle opened the public hearing requested a staff presentation.

Principal Planner McCartney explained Ordinance No. 1714, Series 2016 is an ordinance approving the vacation of an easement within Lot 5, Colorado Technology Center Filing No. 2 Subdivision. Resolution No. 4, Series 2016 is a request to approve a Final Planned Unit Development (PUD) to construct a 153,018 SF single story industrial/flex building with associated site improvements on Lots 3, 4, 5, and 16 of the CTC Filing 2 Subdivision. The subject property is located in CTC and zoned Industrial (I). It is required to follow the IDDSG. The proposal is for a 153,018 SF general flex space with 72% hardscape; 28% soft scape; 5 access points: two on CTC; two on Boxelder and one from East.

Parking: The “office without loading” amount of 3.7 spaces per 1,000 SF requires a waiver from the IDDSG. Staff believed the waiver request is acceptable and recommended approval.

Signs: Monuments Signs: IDDSG allows one freestanding sign for each access. The applicant is requesting 4 monument signs. Wall Signs Waiver: IDDSG allows 15 SF all signs, not to total more than 80 SF. The applicant is proposing 40 SF signs not to total more than 120 SF.

Staff recommended approval of Ordinance No. 1714, Series 2016 and Resolution

No. 4, Series 2016 with the following condition: 1.) The applicant must comply with the October 22, 2015 Public Works memo prior to recordation.

APPLICANT PRESENTATION

Jim Vasbinder, Etkin Johnson Group, 1512 Larimer Street, Suite 100, Denver, CO, explained this is a proposal for the largest building to be constructed at the Colorado Technology Center. They just broke ground of the property at 2000 Taylor and with Council consideration and approval of this proposal; the applicant will apply for a building permit within the next 30 days.

COUNCIL COMMENTS

Mayor Muckle addressed the requested sign waiver. Council member Stolzmann stated there is consistency as this request is similar to their last request relative to signage.

Mayor Muckle called for public comment and hearing none, closed the public hearing.

ORDINANCE No. 1714, SERIES 2016

MOTION: Council member Stolzmann moved to approve Ordinance No. 1714, Series 2016, seconded by Mayor Muckle. Roll call vote was taken. The motion carried by a vote of 6-0. Absent: Council member Leh.

RESOLUTION No. 4, SERIES 2016

MOTION: Mayor Muckle moved to approve Resolution No. 4, Series 2016, seconded by Council member Keany. The vote was 6-0. Absent: Council member Leh.

DISCUSSION/DIRECTION/ACTION – KESTREL HOUSING PROJECT DEVELOPMENT AGREEMENTS

Mayor Muckle requested a staff presentation.

Interim Planning and Building Safety Director Russ reported on the final Subdivision Plat and Planned Unit Development (PUD), for Kestrel, the Boulder County Housing Authority's (BCHA) affordable housing development located at 245 N.96th Street. BCHA has submitted building permits and construction plans for the required public improvements.

Traditionally, a draft subdivision agreement is not shown to City Council because the agreement follows established forms and protocols which staff can negotiate and the mayor can execute once City Council approves a resolution allowing the development. However, in some cases, applicants request non-standard solutions which require Council discussion, direction, and action. Such is the case for the Kestrel Development.

BCHA has four unique requests within the subdivision agreement requiring City Council direction:

1) Improvement guarantee: BCHA is requesting a hybrid improvement guarantee, which provides only a portion of the guarantee be in the form of a letter of credit to assure stabilization of site soils and construction of Hecla Drive and related underground utilities.

2) Traffic Signal Funding: BCHA, and its lenders, are requesting a modification to this requirement to establish at this time a cost for BCHA's share of the signal improvement. With Council approval, staff would negotiate and set in the subdivision agreement an amount and time for payment based on a City cost estimate and an inflation factor recognizing the new Paschal and Highway 42 signal warrant is anticipated to occur in 2018 (an estimated BCHA payment of \$214,000).

3) Impact fee deferral: BCHA is requesting their impact fee payment be deferred from the issuance of building permits, expected this month, to March/April when State of Colorado grant monies are available to pay these fees.

4) Estoppel agreement: City Attorney Light reviewed the request for an Estoppel Agreement. Regarding the funding of the affordable housing project, the BCHA's lender (Citibank N.A.) requests the City enter into a project-specific "estoppel agreement" intended to confirm certain obligations, such as the requirement to provide the warranty guarantee for completed public improvements, will remain with BCHA notwithstanding transfer of project land into the new, single-purpose entity that will own the property, build the improvements and operate the affordable housing project. This estoppel agreement will also include a subordination stating that the required affordability restrictions for BCHA's affordable housing development are subordinate to the lender's collateral interest under its loan. All of the other funding agencies are also being asked to subordinate, under their restrictive covenants, to the lender's collateral interest under its loan.

Staff Recommendation: Staff recommended the City Council adopt a motion to (1) approve the improvement guarantee, traffic signal funding and impact fee solutions as outlined above; (2) approve as to form the proposed estoppel and subordination agreement for the project; and (3) authorize the Mayor to execute the final versions of the estoppel and subordination agreement and other development agreements for the Kestrel development.

APPLICANT PRESENTATION

Norrie Boyd, Boulder County Housing Authority, 2525 13th Street, Boulder, CO explained this has been a lengthy process and requested Council consideration.

COUNCIL COMMENT

Council member Lipton addressed the “estoppel agreement” and asked what is backstopping this project, if it fails. City Attorney Light explained the only backstop is what has been approved on the property to date. In the event of foreclosure the lender does not have the right to develop whatever they choose. The property would still be subject to general zoning laws. There are cases in Colorado between public entities and foreclosing lenders on what exactly survives on foreclosure. In the interest of the City other land use provisions of the City would continue and the zoning would still be in place. The property is in PCZD zoning, which is a negotiated zoning.

Mayor Pro Tem Lipton voiced his concern over the probable worst case scenarios, which would be the loss of affordable housing restrictions. He explained he was always leery of real estate matters.

Mayor Muckle stated the worst case scenario would be the City would end up with a nice PUD and design that was not for affordable housing. He supported the conditions as proposed.

Mayor Pro Tem Lipton asked Interim Planning and Building Safety Director Russ to address the potential risk for the public improvements not being made. Interim Planning and Building Safety Director Russ explained the downside of this project not being complete is there would not be a financial guarantee to complete the public improvements. The improvement guarantee provides the land can get to a point of development at Council’s discretion.

City Attorney Light explained because it is not automatic, the City asks for letters of credit to have ready access to the funds to complete the public improvements. If the public improvements are not completed and there is not a financial guarantee, there is still a contract, which stipulates they will complete the improvements. The standard rule for letter of credits is 115% for all public improvements. To date, the City has asked for a cash guarantee for the Hecla exchange and drainage improvements.

MOTION: Mayor Muckle moved to (1) approve the improvement guarantee, traffic signal funding and impact fee solutions as outlined above; (2) approve as to form the proposed estoppel and subordination agreement for the project; and (3) authorize the Mayor to execute the final versions of the estoppel and subordination agreement and other development agreements for the Kestrel development. Council member Keany seconded the motion. The motion carried by a vote of 6-0. Absent: Council member Leh.

CITY ATTORNEY’S REPORT

No items to report.

COUNCIL COMMENTS, COMMITTEE REPORTS, AND IDENTIFICATION OF FUTURE AGENDA ITEMS

Council member Stolzmann reported the DRCOG's representatives received a packet of information for tomorrow night's meeting, which requests a legislation position on a number of bills. She will use the City's legislative policy to guide her decisions and look at the area of local controls. DRCOG staff members have asked for Board direction on these items.

City Manager Fleming noted this is Interim Planning and Building Safety Director Russ' last meeting with the City. He thanked Troy for his contributions to the City including the DDI, the 2013 Comprehensive Plan, and future items, including the South Street Underpass and a procedure issue - the electronic development review process.

Mayor Muckle also expressed his thanks to Interim Planning and Building Safety Director Russ on behalf of the City Council.

Planning and Building Safety Director Russ expressed his thanks to City Manager Fleming and the Mayor and City Council. He stated it was a pleasure to plan a City he lives in and the City he loves.

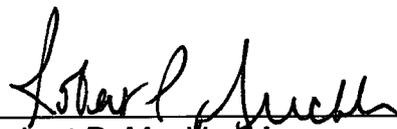
ADJOURN

MOTION: Mayor Muckle moved for adjournment, seconded by Council member Keany. All were in favor. The meeting was adjourned at 10:55 p.m. Absent: Council member Leh.



Janice Carra

Janice Carra, City Clerk



Robert P. Muckle, Mayor

Planning Commission

Meeting Minutes

December 10, 2015
City Hall, Council Chambers
749 Main Street
6:30 PM

Call to Order: Chairman Tengler called the meeting to order at 6:30 P.M.

Roll Call was taken and the following members were present:

Commission Members Present:	Cary Tengler, Vice Chairman Ann O'Connell, Secretary Steve Brauneis Jeff Moline Tom Rice Scott Russell
Commission Members Absent:	Chris Pritchard, Chairman
Staff Members Present:	Troy Russ, Interim Planning Director Sean McCartney, Principal Planner Lauren Trice, Planner I

Approval of Agenda:

Brauneis made motion and Russell seconded to approve the December 10, 2015 agenda. Motion passed by voice vote.

Approval of Minutes:

Russell made motion and Brauneis seconded to approve November 12, 2015 minutes. Motion passed by voice vote.

Public Comments: Items not on the Agenda

John Leary, 1116 Lafarge Avenue, Louisville, CO 80027

I would like to make comments on 824 South Street, Louisville, CO. I think the Planning Commission (PC) made the correct decision on 824 South Street for a lot of the right reasons but not all of the right reasons. Some of the things not considered, and some of the things I think should have been considered, could set a precedent that would not be in the interest of the City. One of the main discussion items that several people commented on was that the guidelines in the Design Handbook for Downtown were voluntary issues and voluntary recommendations, that they are not mandatory. That is not true. This issue was really discussed back in 2009 and the City Attorney issued an official opinion that said that some provisions of the Design Handbook for Downtown are mandatory and some are voluntary. He also made the point that some of them are pretty general and if you ever went to court, you wouldn't necessarily rely on them. He was very clear that there are mandatory provisions in the Design Handbook for Downtown.

Tengler asks which were mandatory and which were to be specifically followed?

Leary says if you look at the introduction to the Design Handbook for Downtown, there is a description of what the words mean. It starts out with the imperative. When the imperative “should” is used, those are mandatory. If it is a suggestion or the word “shall” is used, that would not be considered imperative. A second thing that I think is important is that there is a Downtown Framework Plan. There is a PUD requirement that any PUD has to be consistent with the Comprehensive Plan. The Downtown Framework Plan is incorporated into the Comp Plan and in the Resolution, there is no mention of that. These things come together in another provision in the statute that says you will use the strictest requirements. When you get into the Design Handbook for Downtown, very likely some of those “shoulds” are going to be much stricter than something else. Regarding the Downtown Framework Plan, there was one comment saying “I’m not too concerned about the height”. It is not a matter of whether you are concerned about the heights because the Comp Plan says in the transition zone, it will be two stories. Whether that will be waived or not, and I don’t know if it can, it would be by City Council. My only comment is to thank you, and mainly Mr. Russell, when you very firmly rejected the concept of doing quasi-judicial things, that there be any crony-ism. It was an important thing to say. I have two copies of the letter.

Tengler says that since that hearing on 824 South Street is closed, we probably can’t accept anything on the record relative to that hearing.

Russ says I am not sure about collecting. The City Attorney today made it very clear that the item is closed and it is the Planning Commission’s discretion.

Tengler says John Leary has made very good points and since we closed the hearing on the item last month, we probably will not enter it into the record as an after-the-fact submission.

Leary says my concern is that this applicant or other applicants coming in with the belief that the Design Handbook for Downtown is totally voluntary is an important issue. I don’t know if the PC can set precedent that the City Council (CC) would have to follow. My other comment is that I haven’t paid a lot of attention to Resolutions of Denial, but there seems to be a little bit of different style in this one. There is a list of the violations rather than a definition or explanation.

Brauneis asks if the PC can have Staff follow up on the clarification from the City Attorney back in 2009?

Russ says Staff supports what Mr. Leary said about the Design Handbook for Downtown. Staff will track down the letter for the PC records.

Regular Business – Public Hearing Items

- **A Resolution of Denial for 824 South Street Final PUD:** A resolution denying a Planned Unit Development (PUD) and Special Review Use (SRU) for the construction of a new mixed-use building with 6,800 sf of commercial space and one residential unit, the remodel of the existing house, and outdoor sales at 824 South Street.
 - Staff member: Scott Robinson, Planner II

Russ clarifies that the applicant has requested a continuance. Staff supports it. The hearing is closed. If the PC wishes to proceed with the Resolution of Denial, Staff has talked to the City Attorney and you have a right to proceed. PC can also choose not to proceed.

Rice asks about the purpose of the continuance. If the hearing is complete and the record is closed, why continue it?

Russ says the applicant wishes to be present. I want to point out, and the City Attorney asked that I make sure I point out to you, that the hearing is closed.

Rice asks about the ramifications, if any, of continuing it. We are being asked to take the action item and move out one month. Is the applicant doing to City Council?

Russ says yes, the applicant is asking for that. The applicant has not stated if they are going to City Council. If they choose to, it will delay it one month.

Tengler says he requests that the PC honor the applicant’s request for continuation. Motion made by Brauneis for denial continuance, seconded by Moline. Roll call vote.

Name	Vote
Chris Pritchard	N/A
Jeff Moline	Yes
Ann O’Connell	Yes
Cary Tengler	Yes
Steve Brauneis	Yes
Scott Russell	Yes
Tom Rice	Yes
Motion passed/failed:	Pass

Motion passes 6-0.

- **633 CTC Blvd Final PUD: Resolution 37, Series 2015.** A resolution recommending approval of a final Planned Unit Development (PUD) to construct a 153,018 sf single story industrial/flex building with associated site improvements on Lots 3, 4, 5, and 6, CTC Filing 2 subdivision.

- Applicant/Owner/Representative: Etkin Johnson
- Staff Member: Sean McCartney, Principal Planner

Conflict of Interest and Disclosure:

None.

Public Notice Certification:

Published in the Boulder Daily Camera on November 22, 2015. Posted in City Hall, Public Library, Recreation Center, the Courts and Police Building, and mailed to surrounding property owners on November 20, 2015.

Material board submittal: Motion made by Russell to enter material board into record, seconded by Rice. Motion passed by voice vote.

Staff Report of Facts and Issues:

McCartney presented from Power Point:

- Project located on southwest corner of Boxelder and CTC Blvd. To the west is the property discussed last month for the Louisville Corporate Campus. During the development of this property, there was an access constructed from Louisville Corporate Campus to CTC Blvd. The access is in this development.
- The property is zoned Industrial (I). It is required to follow the IDDSG.
- The building is a 153,018 sf building general flex space.
- IDDSG requires maximum coverage of 75% hardscape and 25% soft scape. This proposal is 74% hardscape and 26% soft scape which exceeds IDDSG requirement.
- There are five access points: two on CTC Blvd, two on Boxelder, one access from eastern project.
- **PARKING:**
 - The “warehouse with loading” requires 2 spaces per 1,000 sf (307 spaces) and “office without loading” requires 4 spaces per 1,000 sf (612 spaces). The applicant is proposing 2.73 spaces per 1,000 sf (421 spaces) and 3.7 spaces per 1,000 sf (558 spaces).
 - The “office without loading” amount of 3.7 spaces per 1,000 square feet requires a waiver from the IDDSG. Staff believes the waiver request is acceptable and recommends approval.
- **SIGNS:**
 - Monument Signs:

- IDDSG allows one freestanding sign for each access.
- Applicant has five accesses but is requesting 4 monument signs.
- Wall Signs - waiver:
 - IDDSG allows 15 sf wall signs, not to total more than 80 sf.
 - Applicant is proposing 40 sf signs not to total more than 120 sf.

Staff Recommendations:

Staff recommends approval of 633 CTC Blvd Final PUD: Resolution 37, Series 2015. A resolution recommending approval of a final Planned Unit Development (PUD) to construct a 153,018 sf single story industrial/flex building with associated site improvements on Lots 3, 4, 5, and 6, CTC Filing 2 subdivision, with the following condition:

1. The applicant must comply with the October 22, 2015 Public Works memo prior to recordation.

Commission Questions of Staff:

Brauneis asks about the parking spaces. Are we over on one and under on another?

McCartney says to get the overage, you look at the rear of the property. When you take out the loading area, the overage of the parking occurs.

Rice says when he read the discussion about parking spaces, there is an indication for allowance for another 134 spots. Is that what you just described? If they do not use the loading area, does this take them over?

McCartney says yes. It does not take them over it as it is still just under at 3.7. Four spaces would be needed for all office and they would be at 3.7 spaces/1000 sf. They have 558 spaces total without the loading area. Staff feels this is adequate.

Brauneis says there have been a number of buildings coming before PC. Some signage proposals have been accepted and some were not. In your view, is this sign waiver request okay because it is not hugely different?

McCartney says the 15 sf is a small sign in regard to a building measuring 153,000 sf in size. Almost every project in the CTC has requested a sign modification. They are not asking for a change of the type. They are allowed 2' signs which are standard. They want more sign area to cover more of the building.

Applicant Presentation:

Jim Vasbinder, Etkin Johnson Group, 1512 Larimer Street, Suite 325, Denver, CO 80202
Etkin Johnson Group now owns this property. We sold this property back in 2006 and just recently repurchased it last month. Regarding parking, we more than adequately satisfy the IDDSG which is 2 spaces/1000 sf. We always want to have the flexibility regarding parking since this is a spec building and we do not have a tenant presently. We want to provide some flexibility on additional parking if we do get office. We have slightly over 1,000,000 sf in the CTC and do not have any buildings that are 100% office. We have buildings with a substantial amount of R&D space or laboratory space, and very little warehouse. We do not use the doors and in most cases, we take the doors out and put windows in. We have not experienced any issues with the flexibility that the City has granted us to date.

Commission Questions of Applicant:

Tengler asks relative to the docks, my assumption is that if the space is that flexible so you can install windows or doors, I assume they are not loading bays with a ramp?

Vasbinder says there is a combination. There are locations with ramps but the balance of the building between the ramps is traditional loading docks. We have installed glass, store front entrances, stairs, and mechanical equipment chases. We have a lot of flexibility. There is also a service area which will be walled enclosures. If a tenant had specialized equipment like cooling towers, this would provide a secure area as well as a visibility break for screening.

Public Comment:

None.

Summary and request by Staff and Applicant:

Staff recommends Planning Commission move to approve 633 CTC Blvd Final PUD: Resolution 37, Series 2015. A resolution recommending approval of a final Planned Unit Development (PUD) to construct a 153,018 sf single story industrial/flex building with associated site improvements on Lots 3, 4, 5, and 6, CTC Filing 2 subdivision, with the following condition:

1. The applicant must comply with the October 22, 2015 Public Works memo prior to recordation.

Closed Public Hearing and discussion by Commission:

Rice says that the PC has seen a brisk pace of development in the CTC with lots of commercial space being developed. I think it is great and I am pleased to see it.

Tengler is in support. I suggest that Staff put the signage issue on the agenda for a first quarter meeting of 2016 since it comes up frequently.

McCartney says that the February agenda looks light so it may be presented then.

Motion made by O’Connell to approve **633 CTC Blvd Final PUD: Resolution 37, Series 2015**. A resolution recommending approval of a final Planned Unit Development (PUD) to construct a 153,018 sf single story industrial/flex building with associated site improvements on Lots 3, 4, 5, and 6, CTC Filing 2 subdivision, with the following condition:

1. The applicant must comply with the October 22, 2015 Public Works memo prior to recordation.

Seconded by Brauneis. Roll call vote.

Name	Vote
Chris Pritchard	N/A
Jeff Moline	Yes
Ann O’Connell	Yes
Cary Tengler	Yes
Steve Brauneis	Yes
Scott Russell	Yes
Tom Rice	Yes
Motion passed/failed:	Pass

Motion passes 6-0.

- **The Foundry Final Plat/PUD: Resolution 39, Series 2015.** A resolution recommending approval of a rezoning, final plat and final Planned Unit Development (PUD) to construct a multi-use development consisting of 24 age-restricted condominiums, and 38,000 sf commercial/office.
 - Applicant /Representative: RMCS LLC
 - Owner: Takoda Properties/Summit View Properties LLC
 - Staff member: Sean McCartney, Principal Planner

Conflict of Interest and Disclosure:

None.

Public Notice Certification:

Published in the Boulder Daily Camera on November 22, 2015. Posted in City Hall, Public Library, Recreation Center, the Courts and Police Building, and mailed to surrounding property owners on November 20, 2015.

Emails entered into record: Motion made by Moline, seconded by Brauneis, passed by voice vote. Fiscal model memo also entered into record. Motion made by Moline, seconded by Brauneis, passed by voice vote.

Staff Report of Facts and Issues:

McCartney presented from Power Point:

- Previously, this property came before PC in 2013 and was known as Steel Ranch Marketplace. It was a 12,000 to 14,000 sf theater for the Art Underground. It was a single, stand-alone building and had the option for additional commercial. The user pulled and the building was never constructed; it made it through a PUD which expired.
- Located on southwest corner of Paschal and Highway 42 in north Louisville.
- Zoned PCZD-C. Requesting rezoning to PCZD-C/R.
- 5.82 acres and requesting Mixed-Use.
- South of Indian Peaks, Filing 17.

REZONING: The 2013 Comp Plan identifies this area as an “Urban Corridor” with focus on:

- commercial
- office
- neighborhood retail
- residential density allowance up to 25 units per acre

Principal NH-5

- Mix of housing types
- Multi-generational needs
- Empty nesters
 - Proposing 24 age-restricted units for ages 55+ empty nesters

Surrounded by PZCD-C/R and PZCD-R

- Complies with surrounding zoning

FISCAL IMPACT:

Russ presents. The City has updated its fiscal model. The City did that through the Finance Committee as part of City Council (CC) in reviewing a city-wide marginal cost model. Upon approval of CC on the city-wide marginal cost model, our consultant took a hybrid for a development specific review model. We have two models: city-wide marginal cost model and hybrid average cost model. Many of our developments are small and the marginal cost model doesn't work well for smaller developments. The actual impact on the City through the hybrid average cost is more reflective. The fiscal model is based on our budget. It is based on the point forward. Looking at development based on our annual approved budget, it looks at development and its impact over 20 years point forward. It does not look at the residential mix of the city. It assumes a balance because our budget has been approved. Looking at the numbers before you, it is a 20 year forecast of how this project affects the City going forward.

It is a sophisticated model that can play a number of scenarios. It looks at the number of units, where those units are located in the City, at the value of the home, and the income of the owner. If a residential development were to be proposed on the Phillips 66 property, everyone would acknowledge that the Broomfield retail is more convenient to those residents, so the City of Louisville would have a lower capture of those disposal dollars. It is geographically significant of where development goes, and on what percent of disposal income comes into the City. We ask every applicant to provide some base information so we can calibrate the model specific to the development request, such as construction costs and proposed values of homes. We equate that and evaluate that against what our base model assumptions are.

In the memo in front of you, we have two scenarios. The item on the left is showing the applicant's numbers. It is the same for construction costs, incomes, and cross points. They have differences in traffic trip generation rates. The City's development and review model takes national averages for mixed use trip assignments. We are following a national trend within the

model. The applicant provides a more specific Louisville characteristic that is supported by a traffic engineer, so they are proposing a different persons/household than what our model assumes for that type of housing structure which is based on a national ITE. They are showing it is 1.8 persons/household where the adopted model is 1.4 persons/household. They have more residents within a unit than ours. With those base assumptions, we do a 20 year forecast based on the different funds within the budget.

	Adopted Model Numbers	Developer Numbers
RESIDENTIAL		
Persons per household	1.4	1.8
Vehicle Trips	Lower Generation	Higher Generation
MU Trip Adjustment	50% (ITE)	25%
COMMERCIAL		
MU Trip Adj. (retail)	28% (ITE)	25%
MU Trip Adj. (office)	50% (ITE)	25%

Fiscal Impact Model

Revenue by Fund	SC		
	Developer Numbers	%	Mod Numb
General Fund	\$2,891	62%	\$1
Urban Revitalization District Fund	\$0	0%	
Open Spaces & Parks Fund	\$373	8%	
Lottery Fund	\$0	0%	
Historic Preservation Fund	\$132	3%	
Capital Projects Fund	\$1,256	27%	\$1
TOTAL REVENUE	\$4,653	100%	\$4
Expenditures by Fund			
General Fund	\$1,519	39%	
Urban Revitalization District Fund	\$0	0%	
Open Spaces & Parks Fund	\$409	10%	
Lottery Fund	\$0	0%	
Historic Preservation Fund	\$132	3%	
Capital Projects Fund	\$1,852	47%	
TOTAL EXPENDITURES	\$3,913	100%	\$1

For comparison purposes, staff also provided a fiscal analysis using the City's established vehicle trip generation rates and adjustment factors as documented by the Institute of Transportation Engineers (ITS). This scenario yields a net positive fiscal impact of **+\$2,327,000** over the same 20-year period, or **+\$116,350** per year. The following table summarizes the model's output for all both scenarios and the approved GDP.

According to the new model, the previously approved GDP would yield a net positive fiscal impact of **+\$2,670,000** over a 20-year period, or **+\$138,000** per year. The proposed rezoning, using the applicant's numbers, would yield a net positive fiscal impact of **+\$739,000** on the City over the same 20-year period, or a positive **+\$36,900** per year.

It is important to note that we do not have a single criterion in the Comp Plan or in the LMC that says there is fiscal performance as the sole determinate of anything. It is information. The

Comp Plan does identify this as an urban corridor. The Comp Plan says any development should produce a positive fiscal return to the City. That is as descriptive as it gets. When you look at the Comp Plan, we look at character, housing, parks and recreation, and transportation. We look at the Comp Plan in its totality. This is just one element of the Comp Plan. All rezoning needs to be consistent. Staff believes, based on this fiscal model, that it is consistent with the Comp Plan.

We can also determine when retail is occupied or leased in this model. The numbers before you show that retail would be leased the first year in all three scenarios, the GDP, Model Number, and the Developer's Number. If the market for some reason can't produce that retail square footage until year 10, you do see a negative fiscal return from the Developer's Number and very minor positive returns from the other two.

Questions from Planning Commission regarding Fiscal Model:

Russell asks about "leased in the first year" means Day 365, and if the commercial is leased in the first year or by the end of the first year.

Russ says we assume it is occupied and sales tax is being produced by the end of the first year.

Russell points out Scenario 1, Developer Number, the input for market units says 18 persons/unit. I am looking at the hard copy. Is that a typo in the report? If that is inaccurate data, it is translating into the numbers.

Russ clarifies it is the Back-Up Tables. It is an Excel spreadsheet and it hasn't been edited. I will put in 1.8 instead of 18 persons.

Moline asks about the Net Fiscal Result. Why are there such big differences between the developer numbers, the model numbers, and the original GDP?

Russ says in the City Budget, there are different funds within the budget. They each have revenues and expenditures. The development influences all of those. We have sales tax revenues that fund a number of these and the persons/household have disposable income. That disposable income influences sales tax which goes into the different funds. This reflects the adopted budget. Revenues such as property tax, sales tax, and other forms the city gains equate to the revenue. The expenditures within those funds are what the level of service is, for example, a trail. We have a certain linear feet of trail that is a minimum expectation based on population. Based on this population growth, we need so many linear feet of trail. Those come back to the expenditures such as police service, library service, City Manager service, and planning department service. We have it broken out by each department type within each of these funds. The combination of the two under the Net Fiscal Result is the revenues and expenditures and the difference based on the adopted budget. That is why it is a point forward. Regarding the big differences between the developer numbers, the model numbers, and the original GDP is Commissioner Russell's catch, the difference between 1.8 and 18. The 18 is going to generate a higher expenditure on the City, but it will increase the revenues as well. It is based on households so it may not be as dramatic on the revenue side whereas it will be dramatic on the expenditure side.

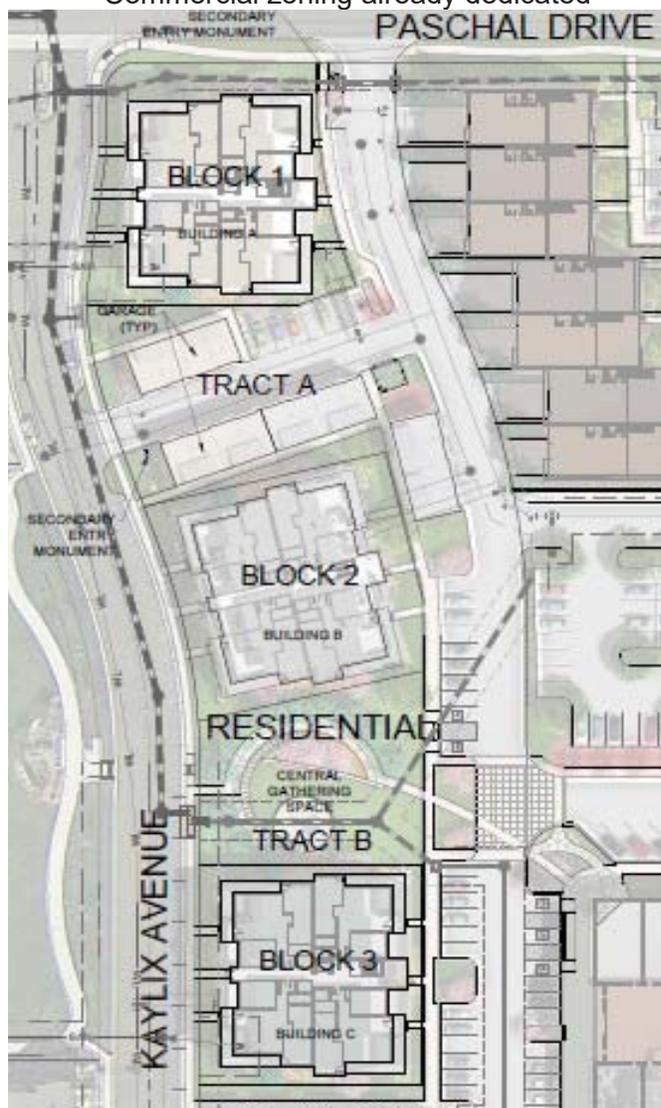
McCartney continues presentation. This application is for a replat to an existing plat but we are combining two plats. We are combining the Takoda subdivision as well as the Summit View subdivision. It is broken up into Tracts A, B, C, and D and Blocks 1, 2, 3, 4, 5, and 6.

	Area	Ownership	Use
Tract A	1.6 acres	Takoda Properties Inc.	Access/Access Drive/Parking
Tract B	.22 acres	Takoda Properties Inc.	Public plaza, parking
Tract C	1.03 acres	Takoda Properties Inc.	Parking/Highway 42 Access
Tract D	.67 acres	Takoda Properties Inc.	Parking

Block 1	.33 acres	Takoda Properties Inc.	Residential
Block 2	.32 acres	Takoda Properties Inc.	Residential
Block 3	.30 acres	Takoda Properties Inc.	Residential
Block 4	.32 acres	Takoda Properties Inc.	Residential
Block 5	.53 acres	Takoda Properties Inc.	Commercial (Lots 1-7)
Block 6	.5 acres	Takoda Properties Inc.	Commercial (Foundry)

Public Land Dedication (PLD)

- 3% additional PLD for residential portion of property
- Commercial zoning already dedicated



ORIGINAL SITE PLAN

- Three access points
- No access to Kaylix St.
- 48 residential units in four buildings
- 56,200 sf commercial
 - Two story in-line commercial
 - Two drive-thru's
 - Two inline commercial uses

- Received communication from residents requesting age-restricted housing, no drive-thru's, and consider access to Kaylix
- Applicant resubmitted

RESUBMITTED SITE PLAN

- Access – 4 primary points
 - Highway 42 – right-in/out
 - Paschal Dr. – right-in/out
 - Kaylix St. – full
 - Summit View – full
- 32 residential units
 - 24 age-restricted to 55 years
- 37,600 SF commercial
 - 2 story in-line 17,850 sf
 - Flex commercial 14,110 sf
- No drive-thru's
- 229 parking spaces

BULK AND DIMENSION STANDARDS

Different than any commercial development because a typical commercial development follows the CDDSG for height, bulk, and setback. This project follows the General Development Plan (GDP) such as Takoda. The height complies with CDDSG and setbacks comply with GDP. Two to three stories complies with Comprehensive Plan.

BULK AND DIMENSION STANDARDS		
	RESIDENTIAL	COMMERCIAL
MIN. LOT AREA	MF: NA	1,500 SF
MIN. LOT WIDTH	55'	30'
MAX. LOT COVERAGE	NA	NA
BUILDING SETBACKS¹⁴		
MIN. FRONT YARD SETBACK (PRINCIPAL USES)	5' (ALL CONDITIONS)	0'
MIN. SIDE YARD SETBACK ¹ (PRINCIPAL USES)	0' (IN BLDG) 5' (ALL OTHER CONDITIONS)	0'
MIN. SIDE YARD SETBACK ² (ACCESSORY USES)	0'	0'
MIN. REAR YARD SETBACK (PRINCIPAL USES)	5'	5'
MIN. REAR YARD SETBACK (ACCESSORY USES)	0'	0'
SETBACK FROM HWY 42 ROW	NA	PARKING: 20' BUILDING: 10'
SETBACK FROM COLLECTOR STREET ROW	PARKING: 10' BUILDING: 10'	PARKING: 10' BUILDING: 10'
SETBACK FROM LOCAL STREET ROW	PARKING: 5' BUILDING: 10'	NA
SETBACK FROM GREEN WAYS AND OPEN SPACE	PARKING: 0' BUILDING: 0'	PARKING: 0' BUILDING: 0'
MIN. BUILDING SEPARATION	10'	10'
MAX. BUILDING HEIGHT		
PRINCIPAL USES	MF: 35'	35'
ACCESSORY USES ³	20'	20'

COMMERCIAL:

Includes office, neighborhood retail, flex artisan space with is commercial, close proximity to the roadway, and complies with CDDSG and Comp Plan.

ARCHITECTURE:

Second submittal, commercial. Foundry building broken into three components (south, center, north) with rooftop patios and a center atrium. Design elements and use similar to The Source in downtown Denver. Has high center atrium with several units coming off. Applicant anticipates restaurants. It is 35 feet in height, 14,110 sf, and has flex artisan space. North and south components are 28.5 feet in height and two stories. Reduced overall glazing but included material to coexist with Foundry. There are corrugated steel, metal frame windows, and step backs and setbacks from entrance.

RESIDENTIAL:

Second submittal 32 total units.

- 24 age-restricted, 55 years and older.
- 8 non-restricted units.
- 35 feet maximum height.
- Good buffer between commercial and existing residential.
- BVSD says 8 unrestricted units will result in 1 student at Louisville Elementary School, 0 students at Louisville Middle School, and 1 student at Monarch High School.
- Residential broken into ground plane, middle plane, and top plane, each having a purpose.
 - Ground plane – more pedestrian-oriented, facing the roadways, active with sidewalks nearby.
 - Second plane – patio area for users.
 - Top plane – compatibility with use and architecture and stepped back. Architectural treatments provide shading and articulation and step back. Compatible with same Steel Ranch type of architecture in residential units and apartments.

PARKING:

Residential

- In LMC, 2 spaces required per unit.
- 32 units require 64 spaces.
- Enclosed garage spaces.

Commercial

- 165 spaces.
- CDDSG requires 4.5 spaces per 1,000 sf for retail commercial.
- 5.16 spaces per 1,000 sf if measured at 85% gross leasable area (GLA) of 31,960 sf.
- 4.4 spaces per 1,000 sf at 37,600 sf (6 spaces less than required).
- Waiver approved through LMC multi-tenant reduction, public easements in excess of public land dedication, and exceptional design.

LANDSCAPING:

- Waiver request to reduce amount of street trees.
- Requested because of existing easements and powerlines. Referral letter from Xcel requesting they approve landscaping before planted.
- Staff believes alternatives can be achieved in speaking with easement owners.
- Applicant shall continue to work with staff on final tree placement.

Staff Recommendations:

Staff recommends approval of Resolution No. 39, Series 2015, with following conditions:

1. The 24 deed-restricted condominiums shall be for ages 55 and older. The 55 years and older age restriction shall be placed on the deed of each age-restricted unit and shall also be included in the subdivision agreement.

2. Staff recommends the wall signs of the In-line building, shown as vertical address numbers, be removed from the PUD and all wall signs must comply with Chapter 7 of the CDDSG and Chapter 17.24 of the LMC.
3. The applicant shall remove the water tower element from the PUD package prior to recordation.
4. The applicant shall continue to work with the Parks Department on the type and location of additional trees along Highway 42, prior to recordation.
5. The applicant shall continue to work with the Public Works Department on the items listed in the September 25, 2015 memo. Each item shall be completed prior to recordation.
6. Residential and commercial development shall be constructed concurrently.

Commission Questions of Staff:

Moline asks Russ about “stuff” that was left off the PUD?

Russ says there were notes on the PUD stating that the commercial would be built concurrently with the residential. The applicant can verify this. They were removed during the referral process without clear understanding from the planning department based on the public works request. We understand their request and staff can live with this PUD without the terms on it by simply having this condition than we can perform in the development agreement to make sure we time the building permits and the CO's together.

Moline asks about the age restriction. What is the origin of this?

McCartney says when staff talked about age restriction, the applicant had wanted to include residential on this development. We know that additional residential has an impact on the schools. Staff asked if you can do age restriction which typically does not come with an impact on the schools, we would work it out. The first condition is we need to have it located somewhere, that these are going to be age-restricted units that we carry forward with this project.

Rice asks about the zoning issue. It becomes a bit of an alphabet soup when we start talking about designations. The way this property is currently zoned is for this to be developed commercially. What we are being asked is to change that designation and turn it into essentially half commercial and half residential. One of the concerns I have when I read this, and it is expressed in a number of the submissions received from the public, is that if we go backwards in time and when this overall development was first conceived, I'm sure there was discussion about a balance between commercial and residential. That balance was reached and the proposal was approved, and the residential got built, but none of the commercial got built. So the commercial lots remained empty. The Lanterns project which is currently being constructed was commercial property as well. We rezoned that into residential.

Russ says a nuance to that is they expanded the Takoda GDP to include the office Summit. The original discussion of the residential-commercial balance of the market place was at the time, the portion of the property that was related to the Lanterns was not a part of that conversation. They expanded it to include it.

Rice says that essentially what we see going on, and again this is expressed in a number of submissions from the public, is that we have these developments that will have a balance between commercial and residential, but what we end up with is more and more residential. That is a concern of mine and a concern of many people. The overall question is why should we do them?

McCartney says the applicant can request anything and it is staff's job to take the request and apply it to the documents that staff uses for review (primarily technical review). We went through the steps of how we look at it. We apply it to the Comp Plan and surrounding zoning. We now have the fiscal analysis to see if this change will impact the overall services and finances of the City.

Rice says this seems like a planning issue and trying to strike a balance between how much residential we build and how much commercial space we have in the City. Ultimately, that has a

lot of impact fiscally and economically. We have made a plan and then over time, we have slowly changed the plan to end up with a lot of residential.

McCartney says if you look back at the 1989 GDP which was the north Louisville plan, they actually do call for commercial mixed-use in this area. I remember nine years ago when we looked at one of the original amendments to create the Takoda area. We had a different lay-out for the commercial, extending further into this development, and then we turned it more linear. This is a request from the applicant to provide more residential. It does comply with the 2013 Comp Plan as far as overall uses and the request for different types of housing mix.

Russ says planning documents are not exact documents. This is an important note for the community to understand. The Comp Plan is deliberately vague and is supposed to convey a character and a core set of principles for the public to determine what that means. CC and PC determine what this conceptual document means. It is not a zoning document because the State doesn't allow it to be. It is meant to be a character and a "feel" and CC's and PC's ultimate comfort. It gives PC some room to determine that deliberately. Staff simply evaluates it based on the principals and framework. An applicant can submit a very exact PUD and Staff uses every tool at the time to say, is it consistent with the Comp Plan. This new request, when compared to the character vision document, it meets the principals of that document. PC has the discretion to determine if that is the case or not.

Brauneis asks about evaluating different sites throughout the City that have proposed to move out of commercial use. We have identified areas that appear to be suboptimal locations for retail. This location seems to be perhaps the only undeveloped spot left within Louisville that has retail potential. From a planning perspective, wouldn't it make sense to push it further towards commercial-retail than residential?

Russ says in looking at the uses and total square footage allowed, half of the allowed commercial square footage would be retail. We are not trading, in my opinion, retail for residential. You are trading office for residential because the second floor will never perform as retail. Looking at the total square footage that is allowed in the market place, we are getting retail on the ground floor. We are getting flex office space that is somewhat gray. We certainly don't have, or anyone has, the true market potential to determine if that retail will be leased. We know with this condition that a built building has a better chance of being leased than a vacant lot. I don't look at this as residential for retail; I look at it as residential for office. The retail component is essentially the same size as the retail component of what was originally approved.

O'Connell says, in looking at page 3 in the packet and how the Indian Peaks filing in Lafayette is directly to the north of this, there are two spaces that are labeled commercial in yellow in Indian Peaks. Along the lines of retail in general, is the City aware of any moves to put in commercial in those areas?

McCartney says Lafayette just recently received a pre-submittal from WW Reynolds for 11 acres commercial that had a 59,000 sf box, and some associated uses. There was a neighborhood meeting that was listed in the paper. No Staff attended the meeting. The reception to the plan, from my reading of the article, was not positive. What they referenced was that the City of Lafayette immediate residents would like what is being proposed on the Foundry, perhaps primarily for the architectural design. They were not specific but they said they would like to see more of what is proposed at the Foundry in the WW Reynolds submittal. Since then, the City of Lafayette has requested a copy of the Foundry submittal and so has WW Reynolds. They both have copies of this submittal.

O'Connell asks if this development will be further along on a time frame?

Russell asks how long has this property been zoned commercial and available for the market?

McCartney says at least nine years.

Russell asks how much commercial square footage is on that lot today?

McCartney says none.

Russell asks how much, if approved tonight, would there be?

McCartney says 38,000 sf.

Russell says we are not trading anything. You can't lose what you don't have. There is potential that has been there a very long time. Secondly, we are age-restricting this as a tool to manage demand in BVSD. We are now building age-restricted residential development in our city to manage the demand on BVSD.

McCartney says yes and partly a mix of housing types as requested by the Comp Plan. I think the underlying theme is to try and alleviate the impact on the school district.

Russell says what happens if you can't lease age-restricted units? Is it as simple as coming back to PC and asking for an amendment? Finally, what do you have against water towers?

McCartney says we called it architecturally confusing.

Tengler says the previous PUD had 48 residential units, is that correct?

McCartney says the original submittal of this Foundry had 48 residential units.

Moline asks if BVSD had a chance to comment on what would happen if this was not an age restricted project?

McCartney says BVSD might have. When we get the original submittal, we sent it to them. I can check to see if staff has those numbers. We did consult with BVSD during this process and we asked them how they look at 55 years and older as far having an impact. They use the numbers found in HUD for senior housing which states 55 years and older. It is their assumption is that 55 years and older would have zero impact on schools.

Russ adds from a senior prospective that the Comp Plan has broad reaching goals and the diversity of housing stock in serving our seniors is certainly very clearly stated in the Comp Plan. Yes, schools are a motivation but this residential development with required senior housing is more consistent with the Comp Plan than without.

Brauneis asks about traffic. How would this proposal compare to alternatives?

Russ says it would be less. Office and residential development are significantly higher trip generators than residential.

Tengler asks about net fiscal impacts. It looks like we are talking about an annual differential between developer numbers, the model numbers, and the original GDP of literally \$10,000 year and \$20,000 a year.

Russ says the numbers are very close. There are variables here. The original GDP produces about \$400,000 additional revenue over 20 years than what is being proposed.

McCartney says the BVSD numbers for the original submittal of 48 units were 3 for LES, 1 for LMS, and 5 for Monarch HS.

Applicant Presentation:

Justin McClure, RMCS LLC, 21 South Sunset Street, Longmont, CO 80501.

I would like to begin by answering some questions. Commissioner Rice, McCartney is accurate. In 2006 was when the original GDP was approved. I was 26 years old, about a decade ago. What was reality then and what is reality now is different and we try to be as accurate as we possibly can when we come forward with comprehensive land development. I am personally very passionate about it. We have tried so many different ways to activate commercial space on that parcel through cooperation with 501(c)3 for which received final PUD approval. We spent money on construction documents that were unutilized. We are talking of hundreds of thousands of dollars of investment to try to get it off the ground. If you read the market analysis included in your packets, this goes back to 2006, listing the property with Becky Gamble. We couldn't ever make anything happen of substance. What we didn't want to do in the middle of the meltdown was fire-sale the property. To the north of us in Indian Peaks South, nothing disparaging against McStain and Indian Peaks South, but that property was sold at \$1.11/sf for the 11 acres. I can assure this PC that it will be very difficult to get a high quality user at that purchase price on land. That is troublesome. For me personally as an investor and creator in Steel Ranch, I have a significant vested interest in making sure that that property develops as quality as it possibly can. I think it is indicative of the challenges that my company has faced with bringing an entirely commercial product to market. In the original GDP, we generated a 0.3 FAR, 72,000 sf, of commercial space. More realistically in complying with CDDSG, complying

with parking requirements, the maximum yield is 55,000 sf of space. Today, with the reduction of residential densities, elimination of drive-thru pads, we still are proposing 38,000 sf of office space which I find to be significant. We always said office in the past because it was so scary to bring retail to market in this environment. We don't want to represent retail and mismanage municipal expectations. The buildings we propose in this site plan are geared toward retail and have an emphasis on retail, and they are unique. They cater towards local entrepreneurs and local investors, not credit tenants. If we could have had a credit tenant on this parcel, it would have been done by now and we would be collecting rents. Instead, we have a nonperforming asset and we have an unfinished community. I drive by it every day and it is unfinished. We have a signal as Paschal. Steel Ranch is a wonderfully designed community and is a significant contribution to the quality of the city of Louisville, and in particular, northeast Louisville.

Presentation: There are significant adjustments to the original site plan. The planning department and the City of Louisville deserve substantial credit with pushing back in the front round of referral comments about overall quality and height impact to the community. We have proven to this PC and City Staff that we are really good listeners and if we have an opportunity to comply, we will do that. We reached out and had neighborhood meetings. It is not required by Code but I hope the residents of Steel Ranch and Indian Peaks South will communicate to this PC and CC that I have taken a tremendous amount of personal time to make sure I had time for each and every resident and all of their concerns. In addition to holding an incredible positive neighborhood meeting with the residents of Steel Ranch, I don't recall any individual being opposed to the application in front of you tonight. They were profuse in their praise and support. Some residents present tonight still have remaining concerns because nothing is ever going to be perfect. We are trying to address all concerns. We have eliminated drive-thrus and the staggering of units.

In getting into the history, we talked about the Lanterns. It was a split zoning in the original 1989 GDP. It is a pertinent distinction because it was PCZD-C/R. What we heard from the residents when we requested 24 ranch-style duplex units, that this would be a preferred use over large commercial buildings. Moving forward, the Lanterns are now under construction and I think it is a positive addition to the Steel Ranch community. They are empty nester friendly housing and while not age-restricted, they are zero step entries and Boulder Creek who is our building partner on that project, has done a fantastic job.

The Foundry will constitute the final piece that will complete Steel Ranch. From a plan view, we are providing a nice break from the transition on Kaylix Avenue and Steel Ranch Park, residential facing residential. We have multifamily product which is far more appropriate land transition when you talk about residential uses to a commercial concept than a single family detached patio home. I think the residents would support this concept and break and transition in land use.

The Foundry is my favorite part. I know Staff doesn't like the water tower, and I believe Director Russ called the water tower a cigarette butt. I want to give some background on it. There is a condition on the resolution of approval that says we will remove it. At the end of my presentation, I have a slide that shows it removed. We have been in the business of buying concrete batch plants for an extended period of time. DELO Phase I under construction now was an old concrete batch plant. We saved the silos and try to repurpose them in projects as we move forward. We also purchased over 20 acres in Longmont from Aggregate Industries, an old concrete batch plant. We have these big beautiful silos that we thought would be architecturally interesting and would be used for signage and continue to differentiate this product in Louisville. To go back to credit tenants and unique architecture and how do we make this special, we have to focus on entrepreneurs. We are trying to get a building and design. To Director Russ's comments, based on spec, this is a concept of the residential. The residential component allows

us to build a commercial building in spec. We originally had annotations and notes on every sheet of the original submittal that commercial and residential product, building permit, would be pulled simultaneously. That is a commitment on behalf of my company to make sure that we are not going to go out there and build 32 residential units and the commercial continues to stagnate. It is my firm commitment.

Entering from Highway 42, you can see the proposed age-restricted condominiums that sit in the background. You will notice that these buildings are 2.5 story buildings at 35'. All buildings have elevators so it is zero step access and zero step entries. There are senior friendly floorplans in terms of office and master bedrooms being located right next to each other. The junior master is actually a guest suite which sits on the top floor. If any of you have had an opportunity to go out to the site and look at existing grade, it had commanding views. Steel Ranch in general has a significant amount of open space and parks and trees, but it has a beautiful backdrop of Indian Peaks and the Flatirons. We want to be able to take advantage of that view for future residents. You will notice our commitment to open space as staff has directed. We feel this is a good public amenity. From a municipal perspective, it is enjoyed by the public but maintained privately. We have been through conversations with Parks and Rec Department and City Staff over long term maintenance obligations. We propose public spaces and things that will a benefit to the entirety of Steel Ranch without asking for any municipal maintenance.

We have an additional one acre under contract from the Summit View Group for \$11.00/sf. That is not a realistic market price but I am interested in comprehensively developing all of Steel Ranch and finishing it out. If we don't control that last acre, I don't have the ability to do that. A one acre parcel without access to drainage or off-site improvements that Steel Ranch has brought to the market presents a problem to the city of Louisville. Versus \$1.11/sf in Lafayette from WW Reynolds versus \$11.00/sf that my company is willing to pay, I want this PC and the City of Louisville knows how committed we are to quality development for the sake of the community. We also get a better project out of it and hopefully, we create better profits as a result. In theory, it should be a win-win.

Looking at the adaptable space, there is the Foundry Building. It would fantastic to have landscape improvements within the Highway 42 corridor. It has been problematic for an extended period of time for logistical reasons. There is an Xcel gas pipeline that they have done eminent domain over, so we will work with them to make sure we can landscape and park on it appropriately. It is indicative of one of the many challenges in developing a parcel like this. Irrespective of commercial and residential uses, this is an inherent complex process and there are impediments throughout the process. In the adaptable space, we have unique architecture. It could be a restaurant or yoga studio or architect space. I got the concept from PCS who does a lot of the work in our entitlement packages. They office out of a building like this in Denver with 1800 sf on the ground level and 1200 sf of loft or mezzanine space. It makes for very flexible space with large garage doors that roll up in the back. We are not going to get a credit tenant. It will be a local entrepreneur and how do we create space and a sense of uniqueness that attracts local Boulder County entrepreneurs.

In looking at the condominiums, you can see the interface between a large garage roll up doors and the parking areas in the back of the adaptable space, as well as the 2.5 story condominiums. We have significant setbacks on the lower units to provide amenity space through landscaping.

The location of the connectivity between Cowboy Park to Steel Ranch Park to the center amenity to the Foundry to the residential purposes out to Highway 42 and future trail

connectivity is positive. The location of it, the overall ability to finish it out and turn it into a public amenity with no maintenance expenses on behalf of the City of Louisville, is positive.

The Foundry building is shown with the water tower, and a second rendering shows the building without the water tower. We adjusted per Staff direction the symmetry of the building and adjusted the brick work. I would like to make it clear that it is an attempt on RMCS behalf to always be a good listener and cooperate to the best of our abilities.

Commission Questions of Applicant:

Brauneis asks, other than the water tower, how do you feel about the conditions?

McClure is fine with all conditions as stated by Staff. We have no problems with the conditions. The street tree locations will be a challenge. We have a fantastic design team. I am concerned about site lines. I want to make sure we have healthy visible CDDSG compliant landscaping adjacent to Highway 42.

Moline asks about the age restriction and any thoughts about it?

McClure says there are impacts on level of service. I try to ask anybody I interact with about how they feel about Steel Ranch. I can represent in a public forum that the vast majority of people I talk to will tell me they like what is going on in Louisville. I'd like the market to be as flexible as possible. If age restriction is what the City of Louisville feels is most appropriate for the Foundry, then I am happy to comply. It serves an important segment in the market place.

Rice says I do appreciate you speaking to my concerns and those that have been expressed by many others. It's all about balance. There are no absolutes in any of this and we all know that. I think your comments are well taken and you have attempted to address the balance.

Russell says regardless of age restriction, are you designing this for 55+? If we remove that, you would design it that way regardless?

McClure says it is designed for 55+. If it was removed, we would cater towards different demographic sets.

Russell asks if you feel people walked away from the neighborhood meeting with the belief that this was going to be a 55+ property.

McClure says yes, I represented it in the neighborhood meeting.

Public Comment:

Gary Larson, 2189 Park Lane, Louisville, CO 80027

Out of the 68 patio homes in Steel Ranch, there are two homes that have young children in elementary school and three homes with high school children. We know it because we keep a community map of who lives where and we all know each other. We have parties once a month in the summer. We have a community email list and have used it to get support for RMCS position on this proposal. Justin reached out to us at the first stage of the project. We got feedback to the community which was very positive and very certain that we didn't want drive thru, which have gone away. There is a lot of support for this project as there was for the Lanterns. Many of us spoke at PC as well as CC meetings. The demographic is there. We are older people living in the patio homes because it lends itself to that. I lived in Lafayette for nine years, I sold my 4,000 sf house on the fifth hole, and moved over the patio homes three years ago, and it has worked out great. We are very happy with the development there. Since I do get a lot of feedback from more than 20 houses in the patio homes, everybody is in favor of this project. I like the silo (water tower) and I don't see it as a cigarette butt. I highly encourage the PC to approve this project. We have gone through it with RMCS on two occasions. We used the same email list to get together for the WW Reynolds meeting regarding Indian Peaks South. There were over 150 people present, one-third was Steel Ranch residents. We are concerned about that because we see this project as very desirable, walking out to have dinner with great views. What is proposed just north in Lafayette is a big box store and two drive-thrus and a gas station. We are in the process of coalescing five different HOAs between Louisville and Lafayette and probably a sixth to get out the word to oppose the Lafayette development. At the

same time, you will find no negative comments from anybody who lives in Steel Ranch, maybe elsewhere in Louisville, but in Steel Ranch. We are in favor of the age restriction. If it weren't there, it would still be that way, just like the patio homes are. It is empty nesters and who know the demographic. The impact on the schools has already been mentioned, 2 children at elementary and 1 at high school. The cash flow is positive even though McCartney punted it off to Russ, we have all heard the cash flow is good. The Takoda Metro Tax District is the largest single item in our property tax bill in Steel Ranch. It won't cut it in half but is going to help mitigate the debt burden in Takoda Tax District. I have two things I'd like to ask the City to consider. We would like to see some entrance off of Paschal and a modification of the median strip so that traffic can come in and turn into the complex rather than coming down and pulling a U-turn. I understand the City has a concern about stacking traffic back up onto Highway 42. My drawing shows a do-not-block box at Pine and Highway 42 going into Mountain High Appliance strip mall. If that works there, it could work here the same way. Traffic doesn't clog up the access into the site so that traffic can get in off of Paschal and not back up onto Highway 42. The lighting along Kaylix calls for seven lights. We are fine with the three street lights there and we'd like to see less light pollution.

Dave Ireland, 2388 Park Lane, Louisville, CO 80027

I moved to Louisville in 1981 and I live in the first house on the north part of the horseshoe that forms the patio homes in Steel Ranch. I think this is a great plan. It is a wonderful transition between the single family homes and the retail and commercial. I think it provides a great entrance into the City of Louisville, something we can all be proud of. I think this enhances the community rather than detracts from it. I urge you to approve it.

Rick Miller, 2974 Shoshone Trail, Lafayette, CO 80206

I live in Indian Peaks on the west side. I have been there for 11 years and I moved there from the Highland neighborhood in Denver. I was in the Highlands neighborhood before it did what it did. There was retail everywhere and retail space that was boarded up. Since then, look what has happened to that neighborhood. It's not just the historic retail that exists in the neighborhood but all the enhancements with Elitch's and Central Avenue and Boulder Avenue. So 11 years in Indian Peaks, we have all been screaming for something just like this across the street from us. We have all rejected the idea of a big box retail store (I have no idea who they think they will get going in across the street from us) and it was pretty evident the other night, last week, at the Lafayette Commission meeting. I can tell you that the Indian Peaks residents absolutely support this. The retail is exactly what we need. We all want walk to and bike to retail. The design of it looks great. As far as the condo piece, if they build 48 condos, that would be about 25% of what was built in the entire metro area this year. I heard someone say that what if it doesn't lease to 55+. I don't know why, other than the schools, you want to age restrict it? I'm 53 years old and by the time my kids get out of the house, I'll be looking for something like this. We desperately need condos. I would support most condo projects out there. I encourage you to approve this project the way it is, except to lift the 55 age restriction.

Sherry Sommer, 910 S. Palisade Court, Louisville, CO 80027

I understand this is compliant with the Comp Plan, the surrounding zoning, and the Urban Corridor Directives. I haven't heard anything about the South Boulder Small Area Plan. As I understood, CC gave a directive that no more residential housing would be approved in the South Boulder Small Area Plan. Does this fall within that?

Russ says the study area does fall within that but that plan has not been adopted by CC.

Sommer says it hasn't been adopted but they very strongly gave a directive that we would wait. We already have much residential in this area that has not been developed. We should wait and see what the impact will be before we develop more. This was originally planned as a PCZD-C. Is that a whole plan for an area when that was adopted? When this plan was originally adopted, was that North Main and Steel Ranch? What was included in that?

Russ says in 1989, the North Louisville Small Area Plan was adopted by CC that included this area as well as North End. PCZD-C was the first official zoning from the small area plan that was done in 2006. That was a Takoda GDP and that incorporates largely what we know as Steel Ranch, not North End.

Sommer says not as North Main.

Russ says that came as an additional phase, South Steel Ranch came in as a GDP amendment at a later date.

Sommer asks how much bargaining power does a developer have when they propose commercial initially? Now we are asking for a change in zoning to residential which has less of a positive fiscal impact. I think there is a fiscal impact and I would like to see the numbers on the original plan because now we have the current fiscal impact which seems positive, but it is positive relative to what? City planners talk a lot of vibrancy or vitality and this mixed-use having the commercial. I think we are lacking something in that area and, as Commissioner Rice was saying, it is being eroded. When you look at North Main, it has nothing to do with a main street, it's just residential. I think that is a loss for our community, not just fiscally but as a community as a whole. There is no place that I would go there. I have a question about the age restriction. Does that mean no children can live there? Is there a rule about that? I am 52 and I have a middle schooler so there are many older parents in this community. Would there be a rule that says children cannot live in those apartments, or does it mean that the adults have to be 55 and older? I have a question about the artisan space. Is that residential space potentially or is it commercial space? What is that? This is a quasi-judicial board and I need clarification on what that means. I have heard a lot of people saying, "Well, I like this, this would be good, my opinion is that it would be good". Is that part of the quasi-judicial restrictions or are we looking at the zoning and history of this plot?

Moline asks Sommer, when you are referring to North Main, I am not sure I understand what you are referring to.

Sommer says the big apartment buildings on South Boulder Road that are by Christopher Village and before Alfalfa's between there. It's called North Main.

Russ says Steel Ranch South subdivision or the North Main.

Sommer says when I saw that, I was thinking, North Main. That must mean it's a main street where you can go and get a cup of coffee or have a cute little store or do something that is like a Main Street. But there is none of that. If that was the original plan, I like that plan better.

Michael Menaker, 1827 W Choke Cherry Drive, Louisville, CO 80027

I want to come back to Commissioner Rice's original question which is why would we do this change? The short answer is honestly, we're smarter now. At the time, this was driven by an assumption, an oversight, and some confusion. The assumption was that under the old fiscal model, that every resident costs the city money. Therefore, if you accept that premise, the idea was then that commercial, and ideally retail, would be required on the site to offset the perceived cost of that residential development. Our new fiscal models are better. There has been a lot of discussion lately that we've learned in the last nine years that infill is not the same thing as rebuilds, and that assumption was basically incorrect. But more importantly, we have also learned that if we only flew up a few feet above the surface, instead of looking at this in a silo, there were vast areas of commercial and retail space almost immediately adjacent to this and North End which will bring similar request to you soon. That is the shopping center where King Sooper's, ARC, the old Blockbuster video, and that big shopping center. There was commercial space and at one time retail space directly to the south where the old Trek Bicycle Store was and now is a Cross Fit Studio and a Yoga studio where retail actually went out. We didn't understand at the time that we have actually lost hundreds of thousands of square feet along the US 36 corridor of retail space, and we have hundreds of thousands of more square feet that are standing vacant today. The fallacy was that a bigger pie pan made bigger pies. If you simply increase the number of commercial and retail square feet, it will all get filled. What

we've learned is that it makes us thinner, runnier pies that satisfy no one. In fact, the standard of retail performance of dollars per square foot, not numbers of square feet. With a more sophisticated look at the models and a better understanding of the world in which we live, it's probably pretty appropriate to make this change. That is how we got here. That said, we also learned when we studied Alfalfa's, the question was often asked of the CEO of Alfalfa's, "Well, can't you just build the darn grocery store without those wrecked apartments?" The answer is absolutely not. At every public and private meeting, there were three here and two private meetings, in which they said over and over again, the store is not possible without the vitalization of the area from the adjacent apartments. You have a similar situation here. You can't give this land away over nine years at any price. There are a lot of guts in this project. To go ahead and commit to building the commercial concomitant with the residential is a real risk on their part but I think the bet is that the completion of the project area of Steel Ranch and vitalization and vibrancy that comes from the residential community will give them a fighting chance. Finally, I am really interested in your comments, Commissioner Russell. I too have nothing against water towers or silos and absolutely, there is no question that this restriction is a response to concerns over enrollment, especially at LES, whether that is justified or not.

Lisa Zucker, 798 Meadowlark Lane, Louisville, CO 80027

I speak for the one or two kids as I do have a second grader. I live in the patio homes. Just very quickly, I do want to give a plug for the 55+ component of this. I have heard opposition to the Foundry and the only opposition I have heard is from families at LES who are very concerned about enrollment. This is a legitimate concern. There is some buzz about how BVSD is coming up with their numbers that feed into the schools. There seems to be some concern that they are low-balling the numbers. That school is busting at the seams and even if you have a couple of children from each one of these little communities being built, it really does have an impact. I know that community is not really represented here. I do want to say that I do feel this is a legitimate concern. Everyone in Steel Ranch I know loves this plan. It is beautiful and it's exactly what I think many of the communities around want to see. Those opposed to the Foundry are appeased by the 55+ component of it.

Picture entered into record: Motion made by Brauneis, seconded by Russell. Passed by voice vote.

Questions to the Staff and Applicant:

Russell asks McClure about the lighting issue.

McClure says I have spoken with Mr. Larson about lighting. I followed up with my photometric consultants as well. The proposed lighting is based upon set criteria and set standards set by not only the City of Louisville but essentially national code standards. To be succinct, I chased everything down that I could.

Brauneis asks about the left turn in proposal and the legality of the U-turn.

Russ says regarding a left hand turn at Paschal, there are several concerns that we have from a best transportation planning principle and traffic engineering. There are two moves that we would be concerned with: One is the left turn in and what delays it may have stacking up onto Highway 42 as well as the left turn out of Paschal and the availability to find the gap, and secondly, the whole role and purpose of Kaylix. Kaylix is the parallel road. We appreciate the design of the residential fronting residential which is good urban design. From a traffic planning perspective, Kaylix has a bigger life and it has a role of supporting Highway 42. Planning Staff who looks at transportation looks at it 30%. Public Works takes it to 100% design and is not comfortable with proposing a median break in between. The applicant's original proposal had no connection to Kaylix. We don't think U-turns are an issue. This submittal does have connections to Kaylix. Some grade has prevented the second driveway to the south from connecting to Kaylix, but the first driveway to the north does indeed connect to Kaylix. From traffic planning, we acknowledge that Pine Street is "what it is". That was approved at a time when traffic

engineers didn't understand traffic dynamics. That was a stop gap. We recognize that it is a solution if that is the direction from CC. But Staff, both engineering and planning, do not accept that.

Brauneis also asks about how these deed restrictions work for age? Is it enforced by the HOA and is it restricted to ownership or occupancy?

Russ says we want it tied to the plat, the specifics of it are tied to the deed of the house itself. It is not an HOA issue, it's an ownership issue, with the ability to sell the house. The 55+ is the HUD standard. If we choose a definite date, the City is at risk of lawsuits of reasonable accommodation and discrimination. It exposes the City and the owners to a nonstandard which is why the age 55 was chosen.

Brauneis asks how that impacts the potential for children to live in the unit.

Russ says it doesn't, it is restricting the ownership. We are still a kid friendly town, and the intent of the age restriction is statistically there is less of a chance of having kids.

Moline asks about Paschal. Is there any reason to extend the median west? Could it prevent the U-turn?

Russ says there is left turn storage if you notice at Kaylix for the southbound left from Paschal. There is a left turn bay. There is opposite left turn bay to turn northbound off of Paschal to Highway 42. The left turn is accommodated and we would not extend it. That has been sized with the original commercial development program of this parcel. We need left hand turns to go to Kaylix. The only true enforcement with the geometrics is the truck may have done it but he may have done several turns, but a smaller vehicle could easily do it. We could put a No U-Turn Sign on there but from a geometric perspective, there is no real way to prohibit the U-turn from occurring other than enforcement.

Brauneis asks about confirmation regarding occupancy of the proposed flex art space. That is a commercial entity, correct?

Russ says yes, that is a commercial building. Residential would not be allowed.

Closed Public Hearing and discussion by Commission:

Rice says this issue about turning all commercial space into residential space is a legitimate concern. I asked the question tonight because I think it is something we have to constantly be thinking about. Of course, when I ask questions, I am usually looking for answers and I think Justin has provided a very good defense to the proposal being made. To me, it is all about balance and so, what happens is you look at space at the time it is being asked to be developed and you say, are we compromising the commercial aspect to such an extent that it makes it undesirable or are we balancing it. I am convinced that great care has gone into this in terms of trying to meet all of the competing demands. I am in support of the proposal. The other thing I will say is that this is another shining example and what we should be very proud of, is the interactive process that occurs between our planning department and applicants. The first proposal that we see, and we didn't discuss it in any detail, is I don't think we would have such great support for that one as we do for the second one before us. The reason it is before us is because Staff has done such a good job of looking out for the interests of the citizens of Louisville to make sure this is high quality, well balanced project.

Moline says I am in agreement with Tom and I think this is a good project. I agree that I am happy to see the applicant work with Staff. One of the features I like about this is the way they have it laid out. I agree with the buffering concept of having these larger buildings on Kaylix that block some of the traffic noise from Highway 42 as it would go further west into the residential parts of the development. I think it is a thoughtful design. I am impressed with the design of the buildings themselves. I am in support of it. I am not exactly thrilled about the age restriction. I think there has been enough discussion about it amongst the residents and Staff here, so I am not oppose that condition, but I don't know that is the way to solve the school crowding issue by restricting age on this. I think 55 year old people are going to buy this anyway. I don't know about the age limitation.

Brauneis says I also find myself at this point in favor of the project. So many questions have had quality answers in many ways. I am not opposed to the water tank and I would like to hear other Commissioners' thoughts on it. It is currently proposed as a requirement especially given the history of it. Life gives you lemons, put the water tank up there, it's kind of funky and I like it. **O'Connell** says I think I am in agreement with the comments of the other Commissioners about the balance being achieved between the commercial and the residential. I am more concerned about seeing more retail than I am more residential, especially considering that there are spots allotted to the north and Indian Peaks. It is a big question mark as to what is going in there. I hope, given the restriction and the demand from the citizens of Louisville, that there be more retail, and that you get this done quickly and get it in before Lafayette. Set the example and hopefully, there will be a push for a higher quality development to the north and not the big box that we hear about. If it were up to me, I would be in more favor of residential, but I get there is a demand and desire for the retail. I just hope it fills up. I am not a big fan of the age restriction on the units being built. I see it as being a little bit of a hindrance to the overall attractiveness of the condos. As someone who is farther away from that age restriction, I would actually be really interested in purchasing a condo like this. I think they are great ideas and I think even with a small child, it would be an attractive thing. I don't know if I want to push this hard. If we are going to reach an agreement, I am in favor of keeping the restriction. It sounds like the developer is making this work, but I want to throw out that it is not my choice to see that as a restriction. As for water tank is concerned, I can take it or leave it. I don't have enough information about what it looks like but I appreciate there is a nod to history and some effort to reuse things that have been removed from previous sites.

Russell says first of all, I am adamantly and strongly in favor of the water tower. I move that we remove that condition. I fully respect your perspective but I have been told frequently that we do not have design guidelines and design review in this community. I think that anybody who has been with me on this PC and I should note it in advance of my comments, that this is my last meeting, so I have to go out on a high note but with a little bit of a bang. I never let a good fight go unpicked. I do not take my direction from the CC outside of formally adopted policy that is regulatory. In fact, as a citizen, they take it from me just like we take it from you. I want to be absolutely clear, in my opinion, what distinguishes this PC is that this is a place where rational dialogue and rational planning carries the day usually, not always, but usually. It doesn't mean we always make the decision that everyone wants us to make, but it is not a place for politics and not a place for pandering. I will say for the record that CC punted on its opportunity to tell us what to do here when it cross-hatched the Comp Plan. They just said, we don't want to get into it. So here we are doing this and I think we are going to make a good decision. I want to make it very clear that I am a citizen of this community and they take direction from me and they take direction from all of you as well. We don't all agree but we should voice our opinions. With that out of my system, I will tell you that first of all, I love the retail approach here. I think you are doing something frankly that I don't think anybody has done up here, which is create this really flexible interesting scalable space. I am a huge fan of The Source. If you haven't been to The Source, you should go down there and check it out. It is interesting and vibrant and it is not big enough and there is not enough of it, but it is really, really interesting. I think if you can come even close to that, I think you are making a real contribution and you are actually creating retail space that will be used. Who cares if you create it if nobody ever uses it? I think this is a space that will be used. I don't want to tinker with the transportation. Designing transportation infrastructure on the fly in a PC is a terrible idea. I think inserting this access between Kaylix and Highway 42 has the making of a total disaster. I know it is not ideal for users, but from a transportation perspective, it would be a complete cluster. Finally, on the senior housing question, I think the developer has made a commitment to a key constituency, his community. These are people who will live with this. To remove that would drive fundamental redesign of the facility. I think it would probably change some of the demand that gets generated there. I will separate these issues. I think we need to stick with the 55+ housing. I am doing the arithmetic that about the time my youngest kid is out of the house, I will be eligible. I will not admit my age

but I'm getting close. I think as an issue specific to this project, I think can't mess with that. It is too fundamental and it is a major component of this project. I would be reluctant to unravel that. This is an issue my fellow Planning Commissioners will deal with in the future, 55+ housing is a terrible tool to manage public school demand. I think it is a terrible approach to it. It puts on us and developers this responsibility to fix a problem that we, as a community need to fix well beyond the realm of the built environment. I can think of some worse ways to manage school demand but it is a terrible way to approach it. I hope that we as a community can get around this issue and deal with it in the future. In summary, I like the project. It's a great one and I'm going to support it.

Tengler says I am also in support of this. I do appreciate Commissioner Rice's commentary about what is really a bit of a slippery slope. At what point does this conversion of commercial or retail into residential become very problematic? I fall back on the notion that businesses and communities vote with their dollars. There are too many instances of vacant retail space and vacant commercial space and undeveloped commercial space that I think we need to find a balance. We can't just be hidebound and suggest that after nine years, it should just be a flip of a switch where they can go out and find commercial renters or commercial purchasers. I think we need to be cognizant of the fact that again, the economic conditions in the immediate area tend to dictate what will work. We also had a project come up just before this where we are seeing 150,000 sf of commercial development out in the CTC and we have seen a number of those developments over the last couple of years. There is a demand for it but it is not necessarily in the North End or in Takoda or in Steel Ranch. We have got to be flexible as a PC and a community to say, "What is working and how do we make the best of this?" This is another example of where RCMS has worked brilliantly with Staff and come up a great project. I am very much in support. Before I ask for a motion, I would like to ask the PC if you are interested in removing Condition #3 on the water tower element?

Motion made by Russell to approve **The Foundry Final Plat/PUD: Resolution 39, Series 2015**. A resolution recommending approval of a rezoning, final plat and final Planned Unit Development (PUD) to construct a multi-use development consisting of 24 age restricted condominiums, and 38,000 sf commercial/office.

1. The 24 deed-restricted condominiums shall be for ages 55 and older. The 55 years and older age restriction shall be placed on the deed of each age restricted unit and shall also be included in the subdivision agreement.
2. Staff recommends the wall signs of the In-line building, shown as vertical address numbers, be removed from the PUD and all wall signs must comply with Chapter 7 of the CDDSG and Chapter 17.24 of the LMC.
3. **The applicant shall remove the water tower element from the PUD package prior to recordation. (to be removed)**
4. The applicant shall continue to work with the Parks Department on the type and location of additional trees along Highway 42, prior to recordation.
5. The applicant shall continue to work with the Public Works Department on the items listed in the September 25, 2015 memo. Each item shall be completed prior to recordation.
6. Residential and commercial development shall be constructed concurrently.
- 7.

Seconded by O'Connell. Roll call vote.

Name	Vote
Chris Pritchard	N/A
Jeff Moline	Yes
Ann O'Connell	Yes
Cary Tengler	Yes
Steve Brauneis	Yes
Scott Russell	Yes
Tom Rice	Yes
Motion passed/failed:	Pass

Motion passes 6-0.

- **1125 Pine Street Final Plat: Resolution 38, Series 2015.** A resolution recommending approval of a replat to combine three parcels and subdivide the property into two separate lots, rezoned Mixed Use Residential (MU-R) and Residential Medium Density (RM), located at 1125 Pine Street.
 - Applicant/Owner/Representative: Arn Rasker
 - Staff member: Lauren Trice, Planner I

Conflict of Interest and Disclosure:

None.

Public Notice Certification:

Published in the Boulder Daily Camera on November 22, 2015. Posted in City Hall, Public Library, Recreation Center, the Courts and Police Building and mailed to surrounding property owners and property posted on November 20, 2015.

Staff Report of Facts and Issues:

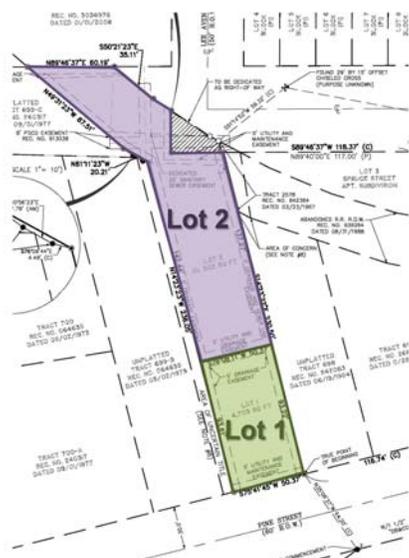
Trice presented from Power Point:

- North side of Pine Street between BNSF Railroad & Highway 42.
- Currently zoned Commercial Community Zone District (CC) & part of Highway 42 Revitalization area.
- 15,813 sf.
- One property with two legal descriptions, and three parcels.
- There is a 1060 sf home built in 1930, a tool shed, and a chicken coop.





- Proposal is to take the three parcels, combine them, and re-subdivide them into Lot 1 and Lot 2.
- Lot 1 will be 4,703 sf and Lot 2 will be 10,502 sf.
- Eligible for minor subdivision review.
- Complies with all design criteria except:
 - 16.16.050(C)
- Staff recommends the public land dedication of 15% come in the form of cash-in-lieu.

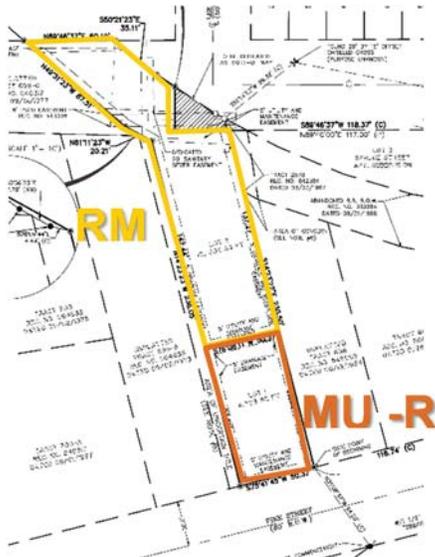
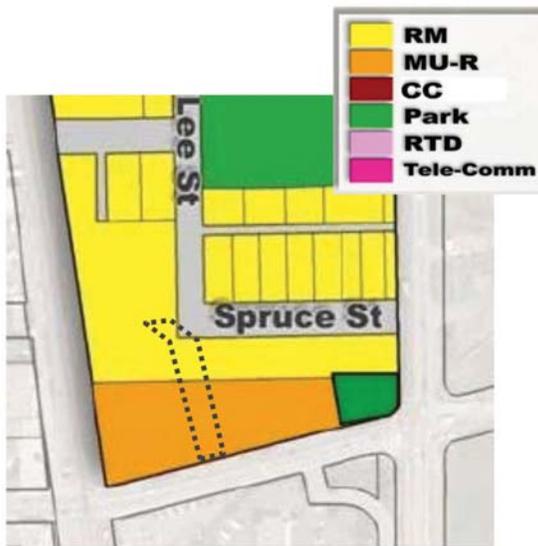


- 16.16.050 (C) deals with the dimensions of the lot so the proportion of depth to width. This subdivision does not comply with it. Lot 1 does but Lot 2 does not. Even if you look at the angle of Lot 2 but taking those as two separate lots with the street frontage on the corner, even the southern part of Lot 2 does not comply with the 2.5x width.
- Staff has looked at:

16.24.010

“The city council, upon advice of the planning commission, may **authorize modifications** from these regulations in cases where, due to exceptional topographical conditions or other conditions peculiar to the site, an **unnecessary hardship would be placed on the subdivider**. Such modifications shall not be granted if it would be detrimental to the public good or impair the basic intent and purposes of this title. Any modification granted shall be in **keeping with the intent of the comprehensive development plan** of the city.”

- Staff believes the site is a “peculiar” shape due to the abandoned railroad right-of-way and existing depth of the lot. The subdivider would be unable to provide two lots which meet the depth to width ratio while providing the required lot frontage. Staff recommends Planning Commission authorize this modification.
- This subdivision is triggering the rezoning consistent with Highway 42 Plan.



Lot 2: Residential Medium Density

- 10,502 sf
- Up to three residential units

- Staff recommends proposed Lot 2 would be included within the Old Town Overlay Zoning District
- If approved, the Old Town Overlay will be amended to include the proposed Lot 2
- Does not require a PUD

Lot 1: Mixed Use – Residential

- 4,703 sf
- Development needs to comply with MUDDSG
- Requires a PUD
- Existing single-family dwelling would be considered a legal, non-conforming use

Staff Recommendations:

Staff recommends Planning Commission approve of Resolution No. 38, Series 2015, a resolution recommending approval of a replat to combine three parcels and subdivide the property into two separate lots, rezoned Mixed Use Residential and Residential Medium Density, located at 1125 Pine Street.

Commission Questions of Staff:

Russell asks what is the difference between a property and a lot?

Trice says this is all triggered on Boulder County as one property that comes up under one address at 1125 Pine Street. It has the two legal descriptions on Boulder County so it is recorded in two separate incidences but when it goes to the actual plat that the surveyor was working with, it comes up as three different parcels.

Moline asks what would the current residential zoning allow? Is it meaningless to ask how many residences could be developed on the property now?

Trice says any development would trigger the rezoning based on the Highway 42 plan.

Russ says there is a required rezoning.

Brauneis says you undoubtedly uncovered some curious stories adjacent to this. I trust that what you are proposing at this point would be fit with what might happen to other lots nearby going forward?

Trice says it is something that has been a concern of Staff as this area continues to redevelop and how it will all work. This application does fit.

Rice says this is all a quirk of history, the way this land is shaped and how it came together. Unless we get creative here, there is not much you can do with this property, is that a fair statement? So that's why staff is proposing we get creative in terms of interpretation of the rules?

Trice says yes. The railroad spur is the real problem. If you want someone to blame, it is them.

Tengler asks if Lot 2 in the reconfiguration would be eligible for three dwellings?

Trice says based on the minimum square footage per dwelling unit, which is 3,500 sf in residential medium zone district, you could have three units. The applicant has discussed it and it would be tricky to fit the three units with parking and access.

Brauneis asks about the public land dedication and cash-in-lieu. What is the formula for that?

Russ says that will come in the description for CC that comes at issuance of building permit. We would require an appraisal. There were a number of appraisals done for this particular property and the City would be satisfied. It would not be an additional burden on the applicant. Based on the appraisal, it is 15% of the value for the cash-in-lieu or total land area. In reviewing this with the Parks Department, they did not see it as an appropriate land dedication. This is the property the City attempted to acquire as part of the extension of Lee Street, which CC directed to remove from the Highway 42 plan. We believe there are current appraisals that we can work out with the applicant.

Applicant Presentation:

Arn Rasker, 4782 Valhalla Drive, Boulder, CO 80301

I represent the owner. This was triggered because the City came to the owner asking for an

easement in the little triangular area for an underground drainage addition which would take the drainage from the west side of the railroad track over into the Spruce Street area underground. In the process of applying the new zoning overlay to Lot 1, it actually adds the commercial component to that. Right now, it is a residence and it is grandfathered in as a residence. It cannot be used as a commercial property although it has been in the past. Any redevelopment on Lot 1 would imply a mandatory commercial component.

Russ says this is the rezoning. The applicant is correct. They would be required to have the ground floor of the building to be commercial.

Commission Questions of Applicant:

None.

Public Comment:

Randy Caranci, 441 Elk Trail, Lafayette, CO 80026

This is a tough property because I hate to see it go. Is it currently zoned CC? It is right on Pine Street and it is hard to access. I hate to see us continually give up more and more commercial. We need that tax base and we want that sales tax base. I am not opposed to this at all or anything like that. I think there is a little bit of creep. In the construction business, we call it scope creep. I hope we can be aware of that in moving forward with other projects. I agree with Troy regarding traffic and the stacking of Highway 42 because I drive it frequently. I want to make a point about the last one because of the U-turn situation. Up there at Steel Ranch going in off of South Boulder Road eastbound, I think we should put a No U-Turn sign up there. I get almost hit continually and it's a bad situation. The traffic and the stacking all pertains to what we do and how we do it.

Summary and request by Staff and Applicant:

Staff supports it.

Closed Public Hearing and discussion by Commission:

No PC comments.

Motion made by Russell to approve **1125 Pine Street Final Plat: Resolution 38, Series 2015**. A resolution recommending approval of a replat to combine three parcels and subdivide the property into two separate lots, rezoned Mixed Use Residential (MU-R) and Residential Medium Density (RM), located at 1125 Pine Street, seconded by Brauneis. Roll call vote.

Name	Vote
Chris Pritchard	N/A
Jeff Moline	Yes
Ann O'Connell	Yes
Cary Tengler	Yes
Steve Brauneis	Yes
Scott Russell	Yes
Tom Rice	Yes
Motion passed/failed:	Pass

Motion passes 6-0.

- **Comprehensive Plan Review Time–Code Amendment, Resolution 40, Series 2015:**
A resolution recommending approval of an ordinance amending Section 17.64.050 of the Louisville Municipal Code to modify the minimum review schedule for review and updating of the citywide Comprehensive Plan.
 - Staff member: Troy Russ, Interim Planning Director

Over the next four months, we are cleaning up the LMC while we have extra help in implementing our new building software. The current municipal code 17.64.050 requires that the Comp Plan be updated every four years. During the Comp Plan adoption of 2013, CC made it very clear that they wished it were longer from a requirement. This is an extension of the minimum review of the Comp Plan, extending it from four years to ten years. It does not preclude PC from recommending from recommending or CC from initiating an earlier review. If CC chooses to do an earlier review, this simply says that at a minimum, you are going to do it ten years from the adoption of the plan. The next one will be required to be 2023; they could certainly do it anytime earlier. That is responding to comments made during the Comp Plan and since, and trying to put breathing time as a minimum between it.

Motion made by O’Connell to approve **Comprehensive Plan Review Time–Code Amendment, Resolution 40, Series 2015**: A resolution recommending approval of an ordinance amending Section 17.64.050 of the Louisville Municipal Code to modify the minimum review schedule for review and updating of the citywide Comprehensive Plan. Seconded by Brauneis, roll call vote.

Name	Vote
Chris Pritchard	N/A
Jeff Moline	Yes
Ann O’Connell	Yes
Cary Tengler	Yes
Steve Brauneis	Yes
Scott Russell	Yes
Tom Rice	Yes
Motion passed/failed:	Pass

Staff Comments: None.

Planning Commission Comments:

Brauneis asks Russell how many years he served on the PC. All Commissioners thank Russell for his service. Russell thanks the PC for their continued service. Russ says that Staff thanks Russell who has brought a level of expertise to the Board that will be missed.

Items Tentatively Scheduled for the regular meeting January 14, 2016:

- **1104 Garfield Minor Subdivision:** a request for the development of a 5.82 acre land assemblage located in the Takoda Subdivision (aka Steel Ranch). The project will join two properties and consist of condominiums, retail and drive through land uses. Case #15-030- FS/FP/ZN
 - Applicant/Owner: Cyla Simon Realty LLC
 - Representative: Joni Fournier
 - Staff member: Sean McCartney, Principal Planner
- **Centennial Peaks PUD Amendment:** A request for a rezoning from Commercial Community (CC) to Mixed-Use Residential (MU-R), and minor subdivision for the creation of two lots. Case #15-029-FS
 - Applicant: Boulder Associates, Inc.
 - Owner: Avista Adventist Hospital Representative: Universal Health Services, Inc.
 - Staff member: Lauren Trice, Planner I
- **North End Market PUD/GDP Amendment:** A request for a final Planned Unit Development (PUD) to construct a 153,018 square feet single story industrial/flex building with associated site improvements. Case #15-035-FP
 - Applicant/Owner/Representative: Markel Homes
 - Staff member: Scott Robinson, Planner II

Adjourn: Brauneis made motion to adjourn, seconded by O’Connell. Tengler adjourned meeting at 9:12 pm.

Rob Zuccaro

From: Gary Larson <GaryLarson@Q.com>
Sent: Saturday, February 2, 2019 5:47 PM
To: Rob Zuccaro
Cc: justin@foundrybuilders.com
Subject: Foundry Phasing Plan Amendment

Louisville City Council and Planning Commission,

In late 2015, I was instrumental in putting together a coalition of 11 HOAs comprised of over 1,700 homes along the 95th Street corridor, between South Boulder Road and Arapahoe. Two of our key objectives were to force changes in the proposed Indian Peaks Marketplace in Lafayette (WW Reynolds), and gain approval for the more “upscale” Foundry in Louisville. We were successful in both efforts.

The Foundry Phasing Plan Amendment, coming before Planning and Council during February 2019, revives the Foundry concept, including the age restriction for 75% of the condominiums, as well as *“commencing with one of the commercial buildings first”*, per the Foundry Builders’ letter to the Louisville Planning and Building Safety Director, dated January 18, 2019.

During the City Council meeting of January 19, 2016 when the Foundry was approved, Council members mentioned “Good community involvement” and were “Convinced by the neighborhood’s overwhelming support”. Mayor Muckle also mentioned that he was “Impressed with the public input” and that we “Couldn’t get a better project”.

Having been very involved in the community support for the Foundry in 2015/2016, I am very confident in pointing out that the *“Foundry building consisting of retail and restaurant space (including rooftop deck)”* per the Foundry Builders’ letter of January 18, 2019 was a vital and differentiating feature to this PUD, which earned this impressive community support, and the City’s approval.

I support the Foundry Builders’ PUD amendment with the condition that the promised “Foundry building consisting of retail and restaurant space (including rooftop deck)” be specified as the first commercial building to be included with the residential component.

Sincerely, Gary Larson

Gary Larson
2189 Park Lane, Louisville, CO 80027
M: 303.717.5555
E: GaryLarson@Q.com

ITEM:	2018 Development Review Audit
PLANNER:	Rob Zuccaro, AICP, Director of Planning & Building Safety Lisa Ritchie, AICP, Senior Planner
REQUEST:	Planning Commission discussion on results of the 2018 Development Review Audit, conducted on September 26, 2018

SUMMARY:

On September 26, 2018, Planning Commissioners Brauneis, Rice, Hsu, Hoefner, Williams, Howe, and Moline attended the 2018 Development Review Audit Work Session facilitated by Planning Staff. This Work Session was duly noticed in accordance with the City's public notice policies. Commissioners visited the following sites:

- 168 Centennial Parkway
- 633 CTC Blvd
- Delo Phase 2
- 945 Front Street

Each site was scored based on the criteria prepared by staff.

BACKGROUND:

Over the past few years, multiple projects have been approved by the City and constructed to completion by applicants. Staff and Planning Commission, beginning in 2017, have conducted audits of some completed projects. These audits serve the following purposes:

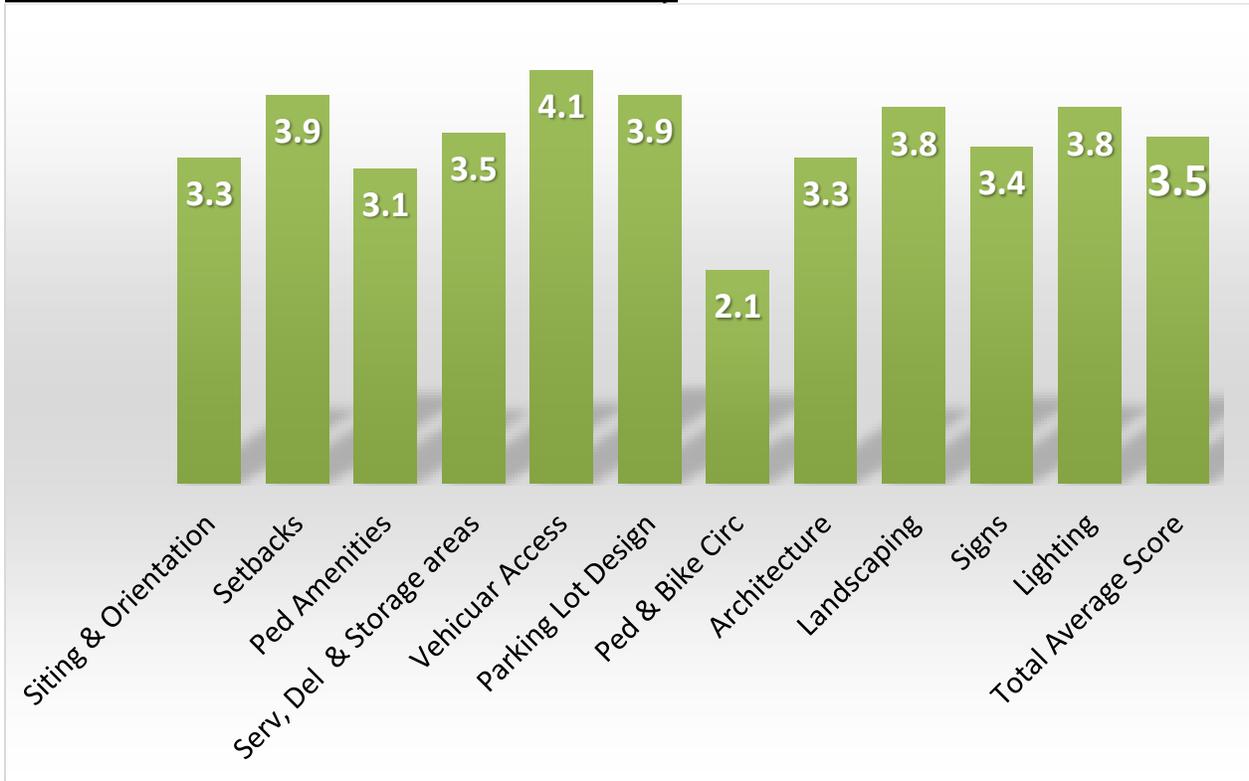
- Inform changes to the revised Design Guidelines, currently underway
- Evaluate outcomes of projects relative to the existing design guidelines' goals to understand where projects are meeting or exceeding standards, and where they are falling short
- Improve staff and Planning Commission's understanding of the results from the application of the Design Guidelines requirements

DISCUSSION:

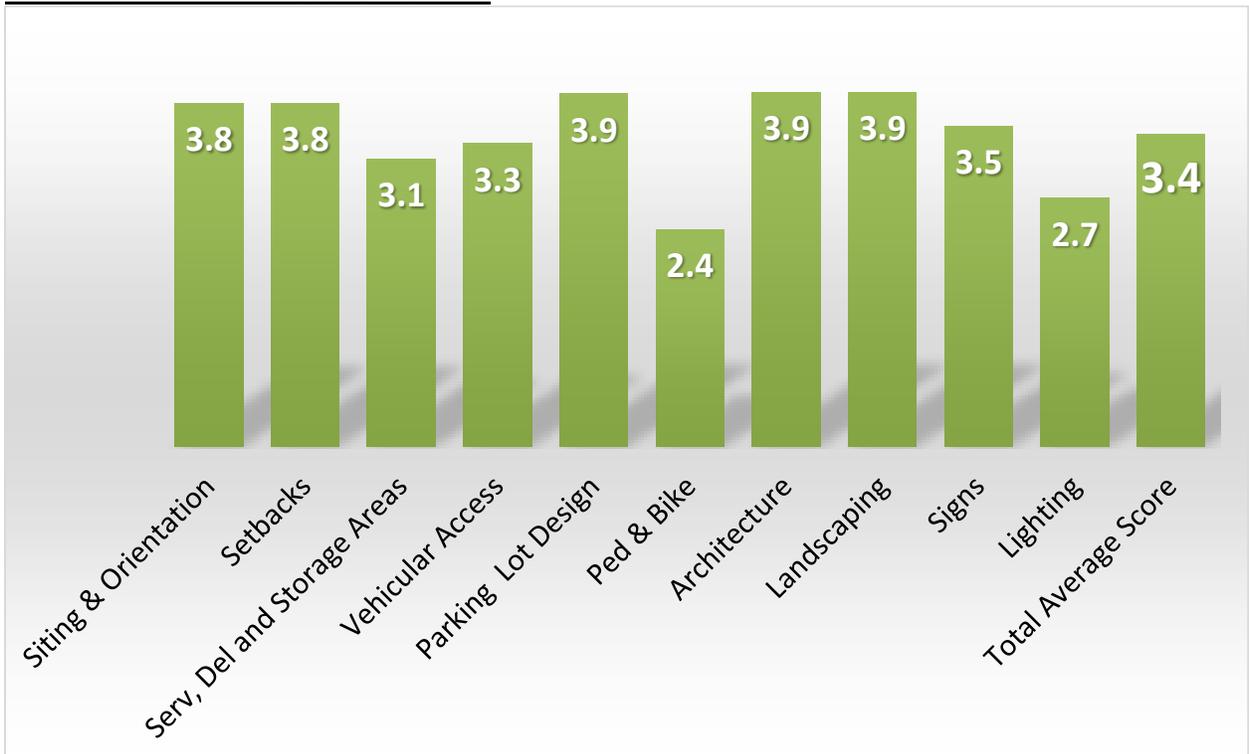
The tables below contain the compiled results of the audit for each property, with the following scoring system:

- 1 = Does not meet the standard and has a negative effect on the property
- 2 = Does not meet the standard
- 3 = Meets the standard
- 4 = Slightly exceeds the standards
- 5 = Greatly exceeds the standards

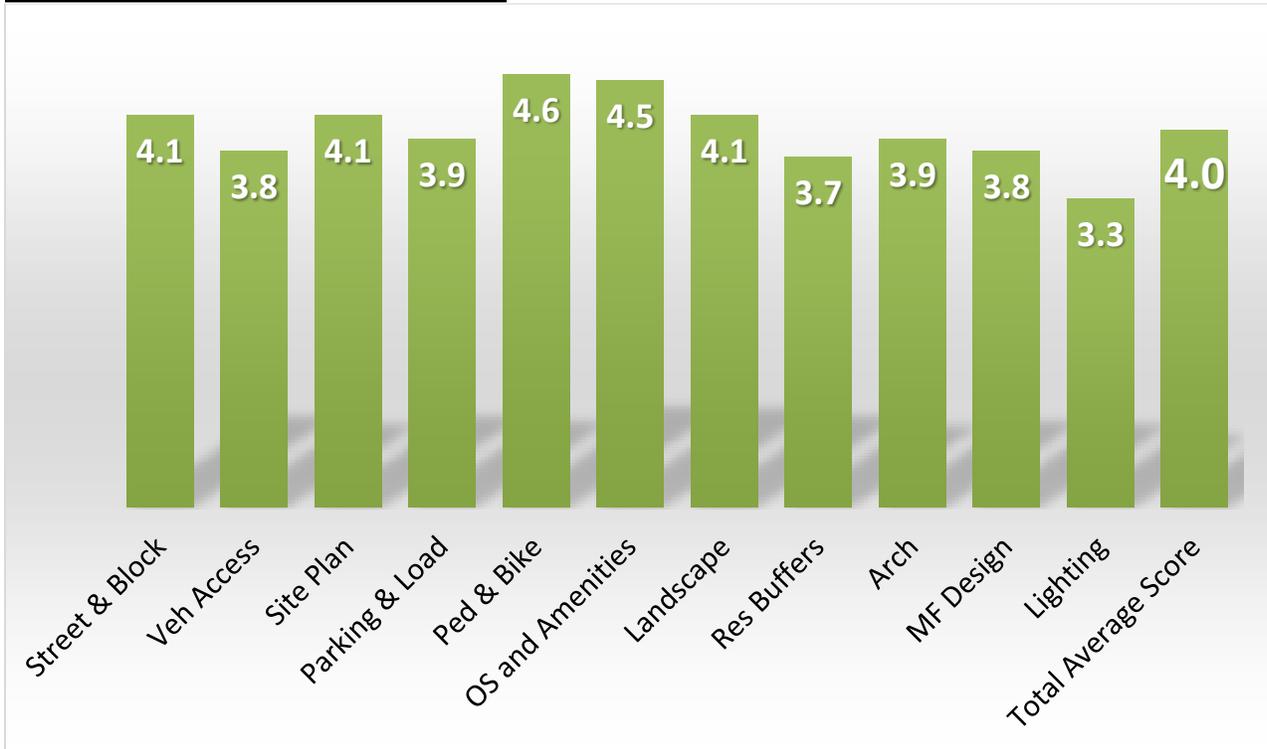
Commercial Review at 168 Centennial Parkway



Industrial Review at 633 CTC Blvd

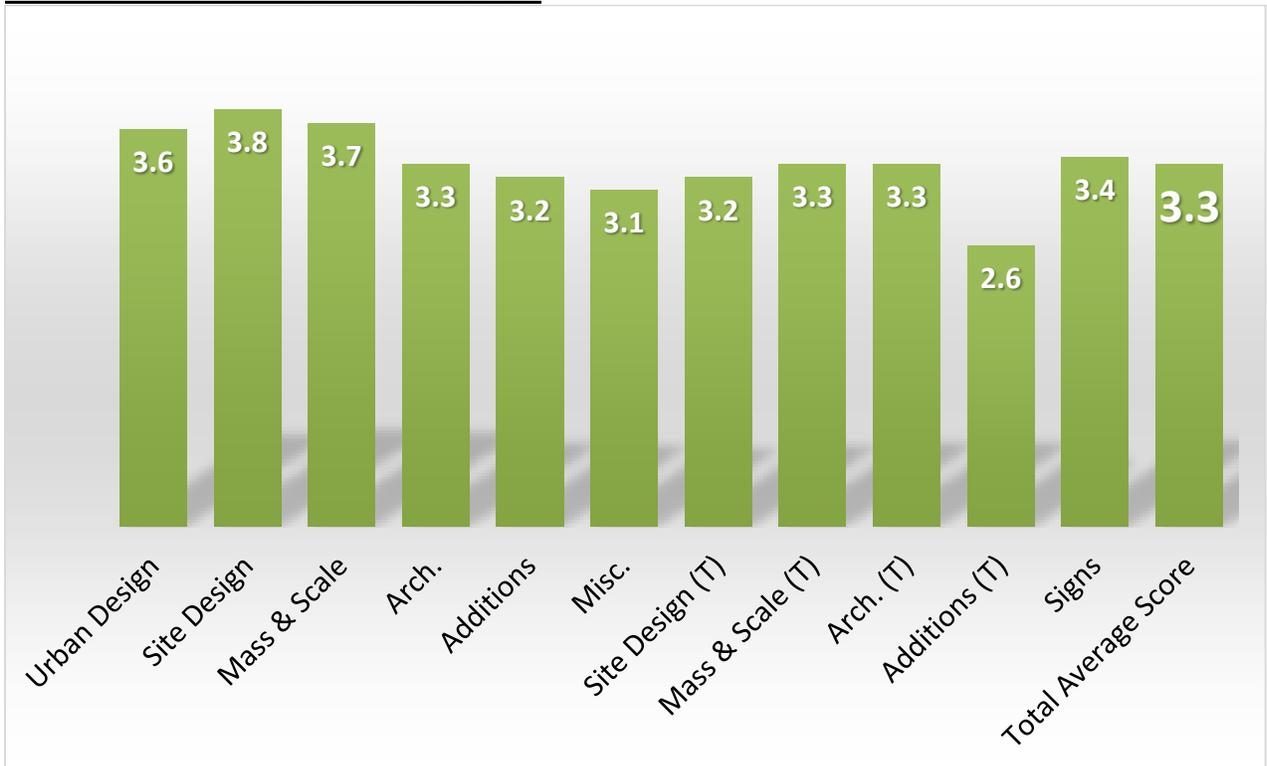


Mixed-Use Review at Delo Phase 2



*MF = Multi-family

Downtown Review at 945 Front Street



*(T) = Transition area goal/policy

Category	Average	Highest	Lowest
Commercial	3.4	Parking Lot Design, Arch. & Landscaping – 3.9	Ped & Bike – 2.4 Exterior Lighting – 2.7
Industrial	3.5	Vehicular Access – 4.1 Setbacks – 3.9 Parking Lot Design – 3.9	Ped & Bike – 2.1 Ped Amenities – 3.1
Mixed-Use	4.0	Ped & Bike – 4.6 OS & Amenities – 4.5	Exterior Lighting – 3.3 Residential Buffers – 3.7
Downtown	3.3	Site Design – 3.8 Mass & Scale – 3.7	Addition – 2.6 Color (Misc) – 3.1

Staff requests Planning Commission discuss the results, particularly in the following areas:

- Results continue to reflect lower scores for bicycle and pedestrian access and amenities in Commercial and Industrial areas. What other trends does Planning Commission observe?
- Most items meet or exceed standards
 - While they meet the design guidelines, are these elements what we desire?
 - What is important to change in the new Design Guidelines?
 - What is important to emphasize and enhance in the new Design Guidelines?
- What are other tools that could assist with meeting the goals and policies?
 - Development review checklist
 - Weighted or incentivized standards

ATTACHMENTS:

1. Scoring Sheets from 2018 Audit
2. 2017 Development Review Audit results
3. 168 Centennial Pkwy PUD
4. 633 CTC Blvd PUD
5. Delo Phase 2 PUD
6. 945 Front PUD

2018 Commercial Development Audit

Category	Summary of Goals and Policies	168 Centennial Pkwy
Building Siting and Orientation	Buildings should be sited so that the character or existing land forms and site features is enhanced; the relationships between buildings are strengthened; and pedestrian and vehicular circulation is facilitated.	
Building and Parking Setbacks	Setback all buildings and parking areas a sufficient distance to create a distinct landscape zone between buildings, parking and adjacent roadways in order to promote a comfortable walking environment.	
Pedestrian Amenities	Plazas, courtyards or similar pedestrian amenities should be incorporated into both overall and individual site development plans and should be easily accessible and comfortable for a substantial part of the year.	
Service, Delivery, and Storage Areas	Minimize the visual impact of these areas especially from public ways and along designated view corridors.	
Vehicular Access	Promote safety and mobility of through traffic by minimizing the number of access points to private property and from public streets. Design vehicle entrances as gateways.	
Parking Lot Design	Vehicle parking should meet the location and quantity requirements of specific uses without undermining the function of other modes of transportation or detracting from the creation of attractive pedestrian environments.	
Pedestrian & Bicycle Circulation	Pedestrian spaces and routes should be designed to invite walking throughout and around each commercial development. Routes should be integrated to form a comprehensive circulation system providing convenient, safe and visually attractive access to all destinations on site.	
Architectural Design	Architectural design should seek to blend in rather than stand out from adjacent structures. All elements including the scale and mass of buildings, materials, colors, roof styles, door and window openings, and details should be responsive to existing architectural design. New buildings should add to community character without rigid uniformity of design.	
	Building masses should respond to "human scale" with materials and details that are proportionate to human height and provide visual interest at the street and sidewalk level.	
Landscaping	Landscaping for commercial areas is provided within each building site to: 1) enhance the aesthetics of commercial developments; 2) create a pedestrian friendly environment; 3) break up the mass of buildings; 4) soften architectural materials; 5) provide screening of service structures; 6) enhance the streetscape/parking environment; 7) define building and parking area entrances; 8) provide shade and climate control; 9) control airborne particulates; and 10) provide buffers between incompatible land uses or site areas.	
Signs	Signs should be consistent with project and overall development design but should be subordinate to architectural and landscape elements. Each commercial building or group of commercial buildings should have a consistent and comprehensive sign program from project identification at the street through individual tenant suite identity.	

Exterior Site Lighting	Exterior lighting should be used to provide illumination for the security and safety of entry drives, parking, service and loading areas, pathways, courtyards and plazas, without intruding on adjacent properties. Site lighting shall be architecturally compatible and consistent in design between sites.	
Total		
Comments		

2018 Downtown Development Audit

Category	Summary of Goals and Policies	945 Front
	<i>General Standards for all Projects</i>	
Urban Design	Respect the traditional context of Downtown and the block; Design for the pedestrian at a human scale; Encourage walking and bicycling; Provide visual interest from the streets, alleys, and pedestrian ways; Use varied building setbacks, changes in materials, and step backs at the alley edge; Site furniture and lighting should be compatible with <i>City improvements in the ROW</i>	
Site Design	Maintain traditional patterns of building orientation; Include a clearly defined primary entrance; Lighting should be subdued, simple, reflect lighting used traditionally, and illuminate entrances and walkways; Parking should be accessed from the alley, be screened from the street and subordinate to other site features; Minimize visual impact of trash enclosures; Minimize the visual impact of utilities and mechanical equipment.	
Building Mass, Scale, and Form	New construction should appear similar in mass and scale to structures found traditionally; Rectangular forms are encouraged.	
Architectural Elements & Details	Respect the time and place in all projects; New interpretations of traditional building styles are encouraged; The exact copying or replication of historic styles is discouraged; Maintain the existing range of exterior wall materials found Downtown (horizontal and vertical siding, shingles, brick) and apply them in a similar manner to those used traditionally; New materials may be considered if they are similar in character to traditionally used materials and are durable; Windows should be of a traditional size and ratio and relate to the pedestrian scale; Upper stories should be less transparent than the first floor; Maintain the traditional pattern of doors along the street.	
Additions to Buildings	Design additions to not diminish the character of building traditions in Downtown; Additions should be compatible in size and scale with the main building.	
Miscellaneous	Use color schemes that compliment other buildings nearby; Use color to coordinate façade elements in an overall	
	<i>Transition Area Standards</i>	
Site Design	Maintain the general alignment of building fronts and side yards; Use porches to define entrances and to provide a sense of scale to building fronts.	
Building Mass, Scale, and Form	New construction should be similar in mass and scale to the established context (height, width, depth); Maintain the average perceived scale of one-story residential buildings; Maintain the traditional scale of buildings along the alley; Rectangular forms are encouraged; Use roof forms similar in scale and style to those used traditionally; Roof materials should be similar to those used on traditional residential buildings.	
Architectural Elements & Details	Use porches, balconies, bay windows, decks and stoops which are similar in form and scale to those found traditionally, to provide visual interest and human scale; Maintain the simple character of the area through building details and repeat patterns by similar shapes and sizes of traditional residential building features.	

	<i>Historic Buildings</i>	
Alterations & Additions	Design additions to be as inconspicuous as possible; Additions should be visually subordinate; Set an addition back from the primary façade to allow the original proportions, form, and overall character of the main building to remain prominent; consider setting an addition back from the sides of buildings as well; Additions at visible locations require greater sensitivity; Designs for ADA compliance should be compatible with the building and its setting.	
	<i>Signs</i>	
Signs	Signs should be located in a master sign plan for the entire building; Signs should be subordinate to the building design; Signs should not obscure historic building details; Flush-mounted signs should fit within architectural features; Locate projecting signs along the first floor level of the facade, not above; Where multiple businesses share a building, coordinate the signs; Sign materials should be similar to those used historically but utilize high quality materials; Lighting shall be indirect; Neon is acceptable if used in limited quantities and is appropriate to the context.	
Total		
Comments		

2018 Industrial Development Audit

Category	Summary of Goals and Policies	633 CTC Blvd	
Building Siting and Orientation	Locate buildings to maximize the presentation of streetscaping and primary building entries to major roadways, to provide clear orientation and access for both vehicles and pedestrian circulation. Place structures in consideration of the existing built context, the location of adjoining uses, and the location of major roads. Create pedestrian court-yards and common employee gathering areas.		
Building and Parking Setbacks	Provide a well-landscaped image along major streets, which promotes a formal streetscape appearance. All buildings and parking should be set back from perimeter and interior streets a sufficient distance to create a distinct landscape zone between buildings, parking and adjacent roadways.		
Service, Delivery, and Storage Areas	Minimize the visual impact of these areas especially from public ways and along designated view corridors.		
Vehicular Circulation & Parking	The parking/access/circulation system should provide for the safe, efficient, convenient, and functional movement of multiple modes of transportation both on and off the site where pedestrian/bicycle/vehicule conflicts are minimized.		
Parking Lot Design	Vehicle parking should meet the location and quantity requirements of specific uses without undermining the function of other modes of transportation or detracting from the creation of attractive pedestrian environments.		
Pedestrian & Bicycle Circulation	Pedestrian and bicycle systems should be designed to be safe and invite walking and bicycling throughout and around the project. Individual parcels and sites should be integrated with adjacent properties designed to form a comprehensive system and to provide convenient access to transit stops and to regional trail systems.		
Architectural Design	Buildings or portions of buildings within the "public zone" should be oriented on a site to create a strong relationship to adjacent structures, providing visual continuity, and compatibility within the overall development. Any wall within a public zone shall incorporate significant architectural features and treatments to diminish the building mass. All buildings should be energy efficient to conserve natural resources. Exterior materials and colors should be aesthetically pleasing, of high quality and compatible with materials and colors of nearby structures.		
Landscaping	Landscaping for industrial areas is provided within each building site to: 1) enhance the aesthetics of industrial developments; 2) create a pedestrian friendly environment; 3) break up the mass of buildings; 4) soften architectural materials; 5) provide screening of service structures and loading areas; 6) enhance the streetscape/parking environment; 7) define building and parking area entrances; 8) provide shade and climate control; 9) control airborne particulates; 10) provide buffers between incompatible land uses or site areas; and 11) filter drainage and stormwater runoff from parking areas and streets.		
Signs	Signs should be consistent with project and overall development design but should be subordinate to architectural and landscape elements. Each industrial building or group of industrial buildings should have a consistent and comprehensive sign program from project identification at the street through individual tenant suite identity.		

Category	Summary of Goals and Policies	633 CTC Blvd	
Exterior Site Lighting	Exterior lighting should be used to provide illumination for the security and safety of entry drives, parking, service and loading areas, pathways, courtyards and plazas, without intruding on adjacent properties. Site lighting shall be architecturally compatible and consistent in design between sites.		
Total			
Comments			

2018 Mixed-Use Development Audit

Category	Summary of Goals and Policies	Delo Phase 2
Street & Block Design Standards	Clear pedestrian and automobile links and safe pedestrian movements within the districts and between the districts and Downtown Louisville are essential; Internal public street systems should disperse and distribute project-generated traffic to multiple access points rather than funneling such traffic through a single primary access way; The street and block standards purposely model the existing street grid and block system in Downtown Louisville to encourage the MU-R and CC Districts to mature as an extension of Downtown, and to enable present and future vehicle, pedestrian, and visual access between the areas.	
Vehicle Access, Circulation, and Connections	Minimize the number of curb cuts; maximize the amount of on-street parking available for residents, employees, and visitors to the district; encourage the use of shared access ways to off-street parking areas (including the use of alleys for this purpose).	
Site Planning	These building orientation and siting standards are intended to accommodate and invite pedestrians to walk to and between destinations within the MU-R, and CC Districts, to feel safe and comfortable doing so, and to support the use and security of the commuter rail line and transit station located in the Highway 42 plan area.	
Off-Street Parking and Loading	Provide an adequate supply of off-street parking; Surface parking lots should be sited, designed and screened to be as unobtrusive as possible. Screen and break up parking lots with landscaping.	
Pedestrian & Bicycle Circulation	Provide continuous connections with off-site destinations with the following: well defined and differentiated bicycle and pedestrian access from the development site; connect to existing and designated public bike paths or greenways located on or adjacent to the development site; provide connections to provide direct pedestrian and bicycle travel from within the development to residential areas and to major pedestrian destinations located within the adjacent neighborhood(s), including, but not limited to adjacent parks, schools, and the Louisville Downtown area; separate pedestrian and vehicle movement to the extent practical with the use of landscaping, barriers or other appropriate design solutions recommended in Section 3.3 of the CDDSG. Provide well defined pedestrian connections to primary building entrances, transit stations, parks, on-site amenities, and parking lots.	
Open Space and On-site Amenities	Create on-site amenities and features, such as outdoor plazas, parks, and public art to provide desirable open space, create an inviting image for customers, visitors, and employees, enhance the pedestrian environment and streetscape in a zone district, offer attractive spaces for people to gather, interact, rest, shop, and eat, and contribute to the character of the city.	
Landscaping & Buffers	<u>Parking Lot Landscaping</u> : Encourage landscaped surface parking lots, including the planting of trees, that will improve the aesthetics of a development site by breaking up expanses of paved areas; will reduce the significant solar heat gain (“urban heat island effect”) from parked automobiles and paved parking areas; and will provide a more pedestrian-friendly environment. <u>Building Site Landscaping</u> : Provide respites from the overall higher densities and intensities of development encouraged in the MU-R and CC Zone Districts, and to provide public and community gathering places. Utilize policies of the CDDSG to promote xeriscaping and water conservation.	

Category	Summary of Goals and Policies	
Residential Protection & Transitional Standards	Ensure that site planning and building design of new development in the MU-R and the CC Zone Districts mitigate to the maximum extent possible any potential adverse visual or operational impacts on adjacent residentially zoned properties in adjacent neighborhoods.	
Architectural and Building Design	Promote high-quality building, streetscape, and open area design and construction that will give the MU-R and CC Zone Districts an identifiable character and unique physical image. Create the appearance that development has occurred over a period of time. Architectural features of new developments, including rooflines, materials, colors, door and window patterns, and decorative elements, should vary in form and style. Especially important is the ground-floor design of buildings and its interaction with adjacent public streets, sidewalks, and open spaces. The ground-floor is the portion of a building that, if designed well, can create high-quality visual interest and a human scale that pedestrians find comforting, inviting, and safe. Key elements that contribute to such a pedestrian environment include first-floor openings (doors and windows), emphasized customer/user entrances, materials, targeted landscaping, and continuity of the front building line along a block (to heighten the sense of “enclosure”). New development should evaluate increased opportunities to implement resource conservation and sustainable building practices. Local climate conditions afford the opportunity to incorporate passive and/or active solar energy applications. Buildings should be designed and sited to maximize the use of solar gain.	
Multi-Family Residential Site & Building Design	These siting standards for accessory parking lots and structures (private parking garages or carports) are intended to reduce the visual prominence of the garage and open parking lots along multi-family streetscapes and common areas; to encourage a more varied multifamily streetscape through the use of a variety of garage orientations and types; and to emphasize the prominence of primary pedestrian and vehicular entrances, open spaces, and residential units along multi-family streetscapes and common areas. Ensure that individual groupings of multi-family buildings within a larger development exhibit a distinct variation in size and mass that allows them to be easily distinguished from surrounding building groupings. The standards are specifically intended to avoid the bleak, “barracks-type” appearance associated with large concentrations of identical or very similar structures.	
Exterior Site Lighting	Exterior lighting should be used to provide illumination for the security and safety of entry drives, parking, service and loading areas, pathways, courtyards and plazas, without intruding on adjacent properties. Site lighting shall be architecturally compatible and consistent in design between sites.	
Total		
Comments		

**Planning Commission Development Audit Summary
Staff Memo**

RE: 2017 Development Project Audit
Date: September 14, 2017
Planner: Kristin Dean, Principal Planner

On June 22, 2017 staff and four Planning Commissioners conducted a Development Project Audit of the following five completed projects:

1. 2000 Taylor
2. 305 S. Arthur
3. McCaslin Marketplace
4. Center Court
5. DeLo Plaza

Scoring sheets were provided to the Planning Commissioners and staff to evaluate each project based on the applicable design criteria and development standards. The scoring criteria were based on the following:

- 1 = Does not meet the standards and has a negative effect on the project
- 2 = Does not meet the standards
- 3 = Meets the standards
- 4 = Slightly exceeds the standards
- 5 = Greatly exceeds the standards

The results of the scoresheets collected at the end of the audit on June 22, 2017 have been tallied and analyzed. Charts summarizing and comparing the results are provided herein. At the September 14 meeting, Staff will present the results of the audit and would like to discuss the following considerations:

1. For each category and development, what are the Commission's thoughts on the results? Does each individual project meet the intent of the Design Guidelines?
2. What are your thoughts on how the City currently implements the applicable design guidelines?
3. What design guidelines should we consider changing through the update to the CDDSG and IDDSG?
4. Currently, for each development application presented to the Commission, staff provides the development plans and a staff memo analyzing the project based on the applicable criteria, along with any other supporting documents. Would the Commission prefer any other materials in the packets which would lend to informing your decisions?

Table A: Summary of Commercial Development Audit Results

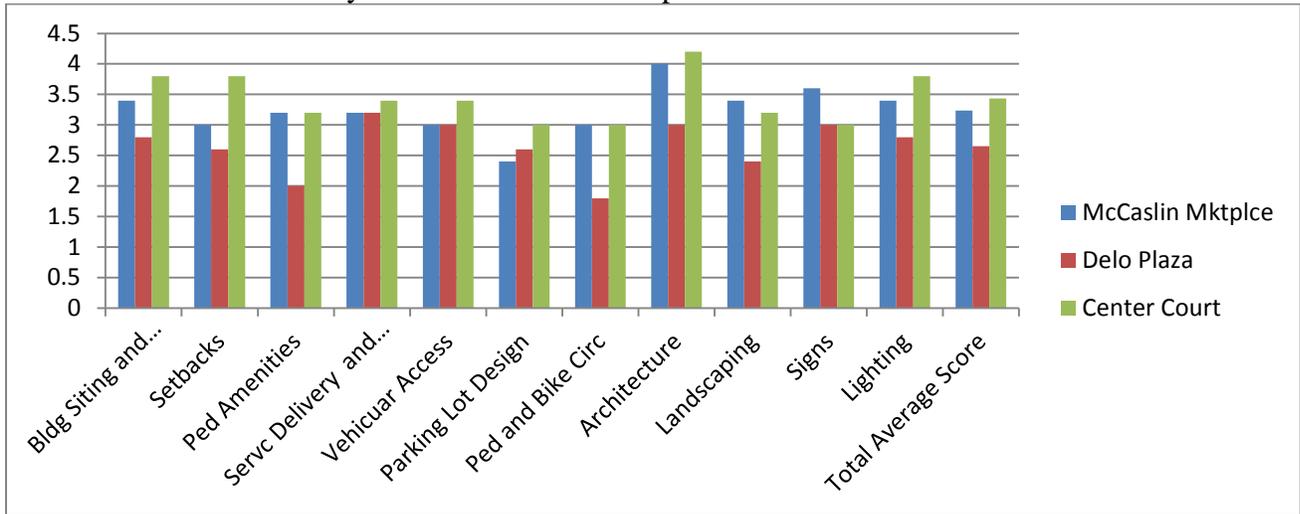


Table B: Summary of Industrial Development Audit Results

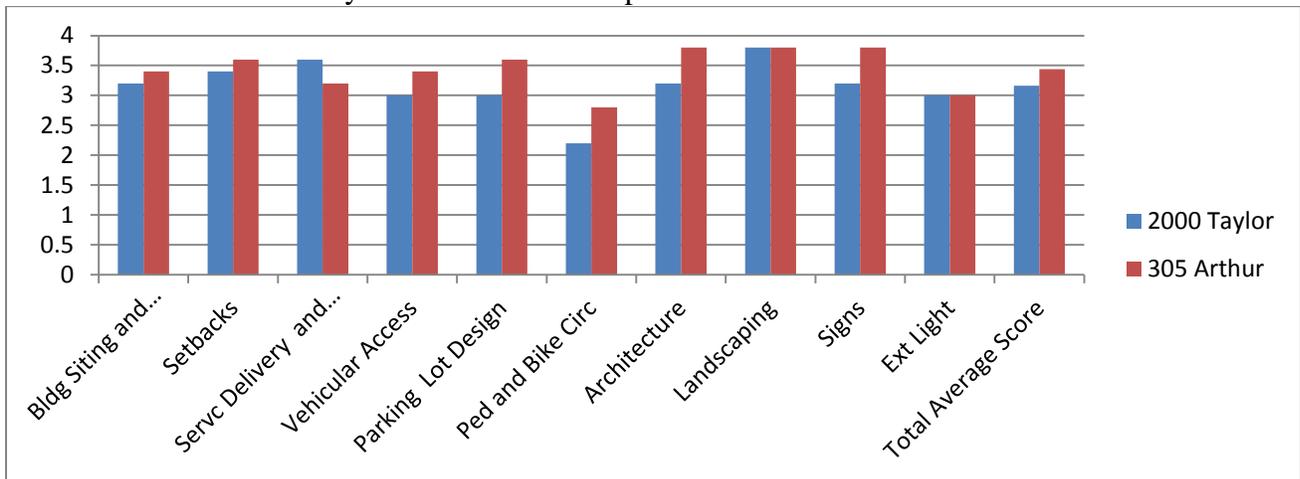
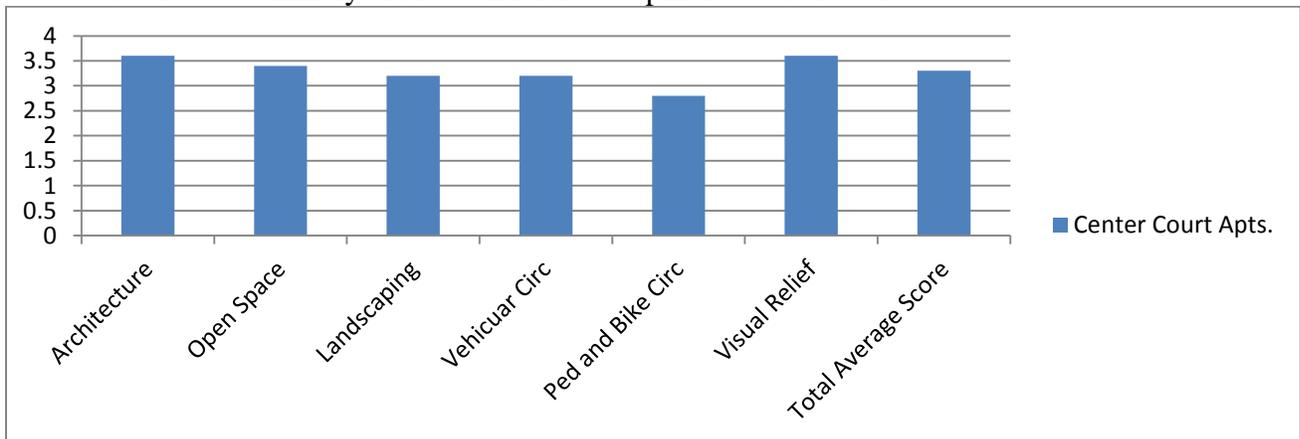


Table C: Summary of Residential Development Audit Results



**CENTENNIAL VALLEY BUSINESS PARK
LOTS 3 AND 4, BLOCK 3, FILING NO.1
PLANNED UNIT DEVELOPMENT**
LOCATED IN THE NORTHEAST QUARTER OF SECTION 13,
TOWNSHIP 1 SOUTH, RANGE 70 WEST OF THE 6TH P.M.
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO.

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12-3-15 SUBMITTAL #1
2-12-16 SUBMITTAL #2
10-14-16 MYLAR SET

LEGAL DESCRIPTION:

LOTS 3 AND 4, BLOCK 3, CENTENNIAL VALLEY BUSINESS PARK, FILING NO. ONE, COUNTY OF BOULDER, STATE OF COLORADO.

GENERAL NOTES

1. THE PROPERTY IS ZONED PC-PLANNED COMMERCIAL
2. ALL SETBACKS AND LAND USE REQUIREMENTS SHALL CONFORM TO THE CITY OF LOUISVILLE, COLORADO ZONING AND LAND USE REGULATIONS IN EFFECT AS OF THE DATE OF APPROVAL OF THIS PLANNED UNIT DEVELOPMENT BY THE CITY OF LOUISVILLE, COLORADO.
3. EXCEPT AS AMENDED BY THIS FINAL PLANNED UNIT DEVELOPMENT, ALL SIGNS SHALL CONFORM TO THE COMMERCIAL DEVELOPMENT DESIGN STANDARDS AND GUIDELINES.
4. THE CITY OF LOUISVILLE IS NOT RESPONSIBLE FOR DAMAGE TO OR REPAIR OF MONUMENT SIGNS DUE TO UTILITY MAINTENANCE.
5. THE CITY OF LOUISVILLE IS NOT RESPONSIBLE FOR DAMAGE TO PAVEMENT SURFACES OR LANDSCAPING CAUSED DURING REPAIR OR MAINTENANCE ACTIVITIES OF UTILITIES LOCATED WITHIN PUBLIC UTILITY EASEMENTS.
6. ON-STREET PARKING WILL NOT BE UTILIZED TO MEET THE PARKING REQUIREMENTS OF THE PROJECT.
7. ALL ROOF-MOUNTED MECHANICAL, ELECTRICAL, OPTICAL AND ELECTRONIC EQUIPMENT SHALL BE SET A MINIMUM OF 20' FROM THE BUILDING PARAPET, AND IF VISIBLE FROM THE PUBLIC STREET ADJACENT TO THE PROPERTY, SHALL BE PAINTED TO MATCH THE DOMINANT COLOR OF THE BUILDING.

WAIVER REQUESTS

1. AS REQUIRED BY THE CITY OF LOUISVILLE DESIGN GUIDELINES, A FORMAL WAIVER REQUEST HAS BEEN MADE TO ACCEPT REQUIRED PARKING CALCULATIONS BASED ON THE BOMA STANDARD OF USEABLE BUILDING SQUARE FOOTAGE IN LIEU OF THE GROSS BUILDING SQUARE FOOTAGE.



VICINITY MAP (NTS)

SHEET INDEX

- 1 OF 11 COVER SHEET
- 2 OF 11 LAND / IMPROVEMENT SURVEY
- 3 OF 11 SITE PLAN
- 4 OF 11 UTILITY PLAN
- 5 OF 11 GRADING PLAN
- 6 OF 11 LANDSCAPE PLAN
- 7 OF 11 LANDSCAPE NOTES & DETAILS
- 8 OF 11 EXTERIOR ELEVATIONS
- 9 OF 11 SITE DETAILS
- 10 OF 11 PHOTOMETRIC SITE PLAN
- 11 OF 11 PHOTOMETRIC DETAILS

STATISTICAL INFORMATION

Zone District: PC - Planned Commercial		
General Zone Lot Information		
Block 3 Lot 3 and 4	Square Feet	Acres
Zone Lot Size (Gross Project Area)	180,725 SF	4.15 Acres
Landscape - Includes: Parking & Drive Aisles	60,442 SF (33.44%)	
Gross Building Area	59,629 SF (33%)	
Landscape Area	60,554 SF (33.56%)	30% Minimum Required
TOTAL	180,725 SF (100%)	
Primary Street Designation: Centennial Parkway		
Proposed Uses: Office		
Occupancy Classification: B		
Construction Type: VB - Fully Sprinklered		
Design Elements		
Building Height, Feet	Max 35'-0"	Provided 35'-0"
Parking		
Building Useable Floor Area	57,700 SF	Provided
Standard Spaces - Office (4/1,000 sf)	(57,700 sf/1,000)4= 231 spaces	
Total Spaces Required	231	231
Accessible	7	8
Bicycle Parking - 1 Per Every 10 Vehicular Spaces	20 Max Required	20

OWNER	DEVELOPER
CENTENNIAL VALLEY PROPERTIES VIII 5281 E YALE AVENUE DENVER, COLORADO 80222 PH: (303) 758-3500	KOELBEL AND COMPANY JEFF SHEETS 5291 E YALE AVENUE DENVER, COLORADO 80222 PH: (303) 300-8850 CELL: (303) 748-8083
ARCHITECT	CIVIL ENGINEER
WARE MALCOMB MHC MIRANDA (PW) 1600 CHAMPA STREET SUITE 300 DENVER, COLORADO 80202 PH: (303) 688-1363	JANSEN STRAWN CONSULTING ENGINEERS, INC. MATHEW A. ADAMS 45 WEST 2ND AVENUE DENVER, COLORADO 80223 PH: (303) 561-3333
ELECTRICAL ENGINEER	LANDSCAPE ARCHITECT
AE ENGINEER DESIGN GROUP ERIC MEDIAN 1900 WAZEE STREET SUITE 150 DENVER, COLORADO 80202 PH: (303) 296-3034	MEURAN DESIGN GROUP KERRY SMETZNER 700 COLORADO BLVD. SUITE 130 DENVER, COLORADO 80206 PH: (303) 512-0549

OWNERSHIP SIGNATURE BLOCK:

BY SIGNING THIS FINAL PUD, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH IN THIS FINAL PUD. WITNESS MY HAND AND SEAL THIS 30 DAY OF October, 2016.

OWNER:

CENTENNIAL VALLEY PROPERTIES VIII, LLC, A COLORADO LIMITED LIABILITY COMPANY.

BY: Koelbel and Company, Its Manager

BY: [Signature]

ITS: PRESIDENT

PLANNING COMMISSION CERTIFICATE:

APPROVED THIS 10th DAY OF March, 2016, BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO.

RESOLUTION NO. 7, SERIES 2016

CITY COUNCIL CERTIFICATE:

APPROVED THIS 19th DAY OF April, 2016, BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO.

RESOLUTION NO. 18, SERIES 2016

BY: [Signature]
ROBERT P. MUCKLE, MAYOR

BY: [Signature]
NANCY VARGA, CLERK
Meredith Mann

CITY SEAL:



CLERK AND RECORDERS CERTIFICATE:

I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT 9 O'CLOCK, M. THIS 9th DAY OF November, 2016 AND IS RECORDED IN PLAN FILE _____

FEE _____ PAID _____ FILM NO. 03555932, RECEPTION.

CLERK & RECORDER _____

DEPUTY _____

CENTENNIAL VALLEY BUSINESS PARK
LOTS 3 AND 4, BLOCK 3, FILING NO.1
PLANNED UNIT DEVELOPMENT
 LOCATED IN THE NORTHEAST QUARTER OF SECTION 13,
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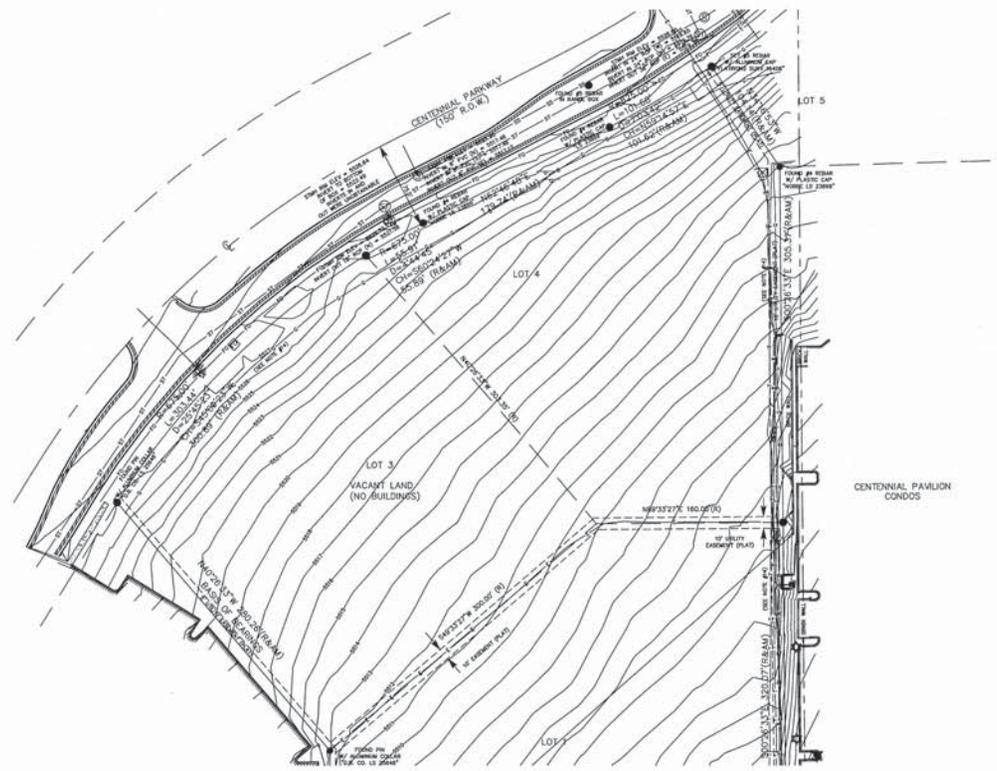
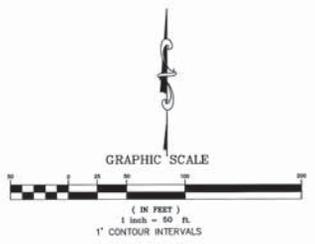
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 P.303.561.3333
 F.303.561.3339

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12-3-15 SUBMITTAL #1
 2-12-16 SUBMITTAL #2
 10-14-16 MYLAR SET

LEGEND

- FOUND MONUMENT (AS DESCRIBED HEREON)
- SET #0 REBAR w/ ALUMINUM CAP "LATRONS SURV LS #16406"
- ☆ LIGHT POST
- PVC PIPE
- RCP PIPE
- STORM SEWER MANHOLE
- SANITARY MANHOLE
- WATER VALVE
- FIBROPTIC RISER
- TELEPHONE RISER
- CABLE RISER
- ELECTRICAL TRANSFORMER
- CURB INLET
- DRAIN INLET
- FIRE HYDRANT
- HANDICAP RAMP
- SIGN
- GAS LINE
- ELECTRICAL LINE
- FIBROPTIC LINE
- WATER LINE
- STORM SEWER LINE
- SANITARY SEWER LINE
- ASPHALT
- CONCRETE
- CONTOUR LINE
- 93 PER RECORD PLAT
- 040 AS MEASURED

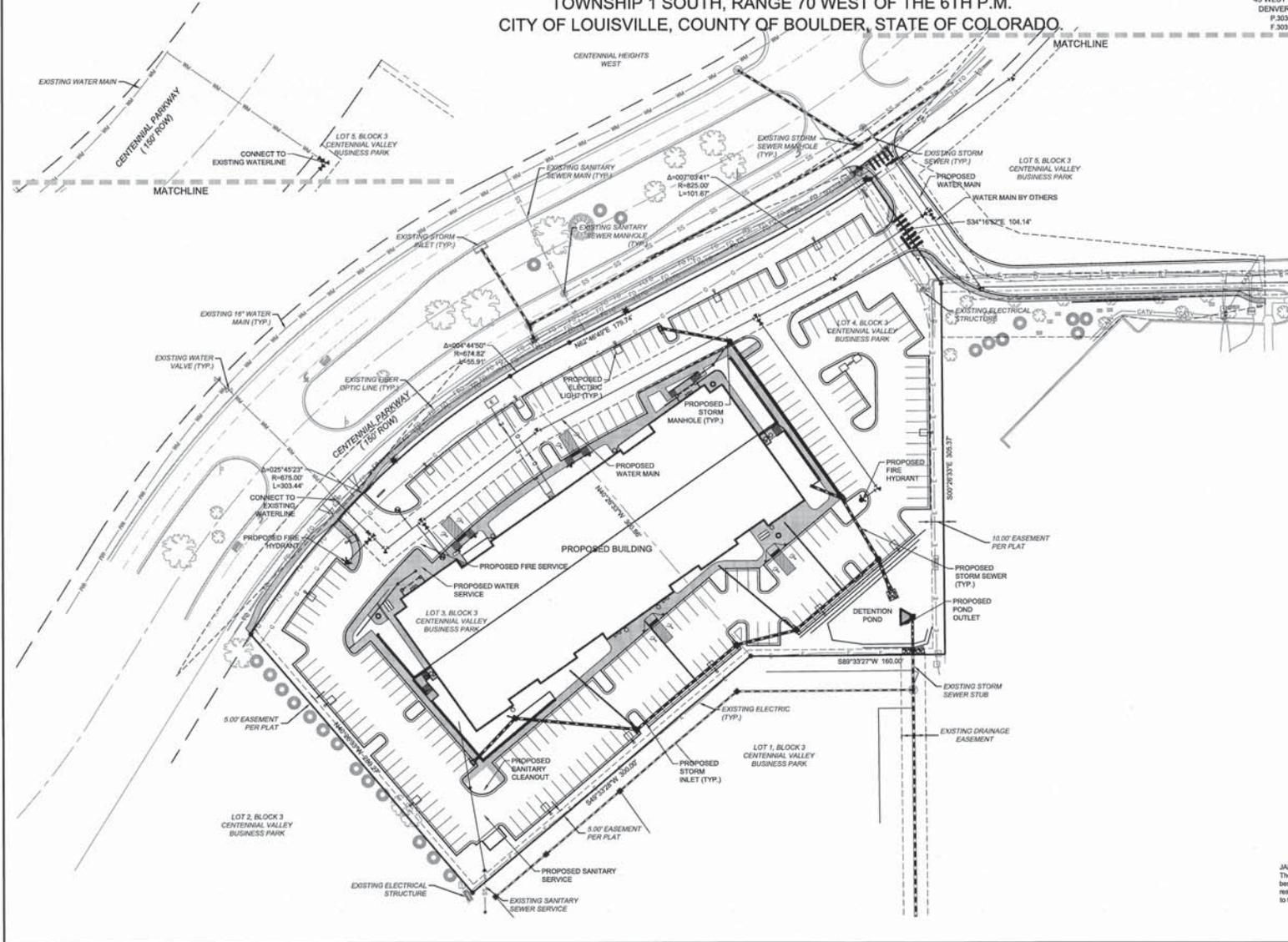


**CENTENNIAL VALLEY BUSINESS PARK
 LOTS 3 AND 4, BLOCK 3, FILING NO.1
 PLANNED UNIT DEVELOPMENT**
 LOCATED IN THE NORTHEAST QUARTER OF SECTION 13,
 TOWNSHIP 1 SOUTH, RANGE 70 WEST OF THE 6TH P.M.
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO.

JANSEN STRAWN
 CONSULTING ENGINEERS
 45 WEST 2ND AVENUE
 DENVER, CO 80223
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**12-3-15 SUBMITTAL #1
 2-12-16 SUBMITTAL #2
 10-14-16 MYLAR SET**



LEGEND:

	PROPERTY BOUNDARY
	EXISTING CURB & GUTTER
	PROPOSED CURB & GUTTER
	UTILITY CROSSING
	PROPOSED STORM LINE
	EXISTING STORM LINE
	PROPOSED STORM INLET
	EXISTING STORM INLET
	PROPOSED SANITARY SEWER W/ MANHOLE
	EXISTING SANITARY SEWER W/ MANHOLE
	PROPOSED WATERLINE & VALVE
	PROPOSED FIRE HYDRANT ASSEMBLY
	PROPOSED WATER METER
	EXISTING WATERLINE & VALVE
	EXISTING FIRE HYDRANT
	EXISTING GAS LINE
	EXISTING TELEPHONE LINE
	EXISTING ELECTRIC LINE
	EXISTING FIBER OPTIC LINE
	RIGHT OF WAY LINE
	EXISTING INTERIOR LOT LINE



JANSEN STRAWN assumes no responsibility for utility locations. The utilities shown on this drawing have been plotted from the best available information. It is, however, the contractor's responsibility to field verify the location of all utilities prior to the commencement of any construction.

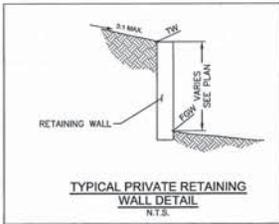
**CENTENNIAL VALLEY BUSINESS PARK
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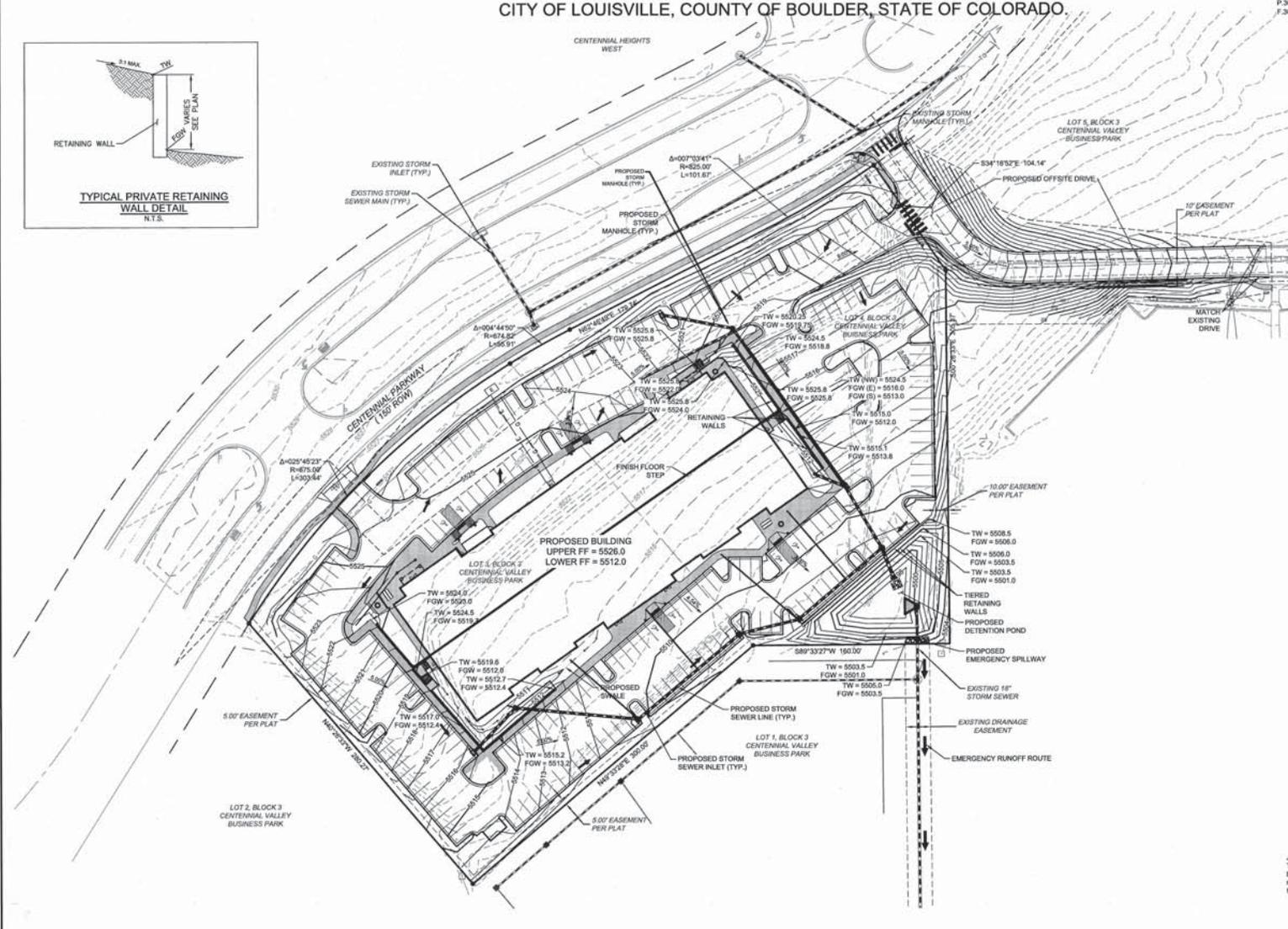
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**12-3-15 SUBMITTAL #1
 2-12-16 SUBMITTAL #2
 10-14-16 MYLAR SET**



LEGEND:

	PROPERTY LINE
	EXISTING CURB & GUTTER
	PROPOSED CURB & GUTTER
	5820 PROPOSED 5' CONTOUR
	5821 PROPOSED 1' CONTOUR
	5820 EXISTING 5' CONTOUR
	5821 EXISTING 1' CONTOUR
	PROPOSED STORM LINE
	EXISTING STORM LINE
	PROPOSED STORM INLET
	EXISTING STORM INLET
	PROPOSED RETAINING WALL
	PROPOSED FLOW ARROW
	RIGHT OF WAY LINE
	EXISTING INTERIOR LOT LINE

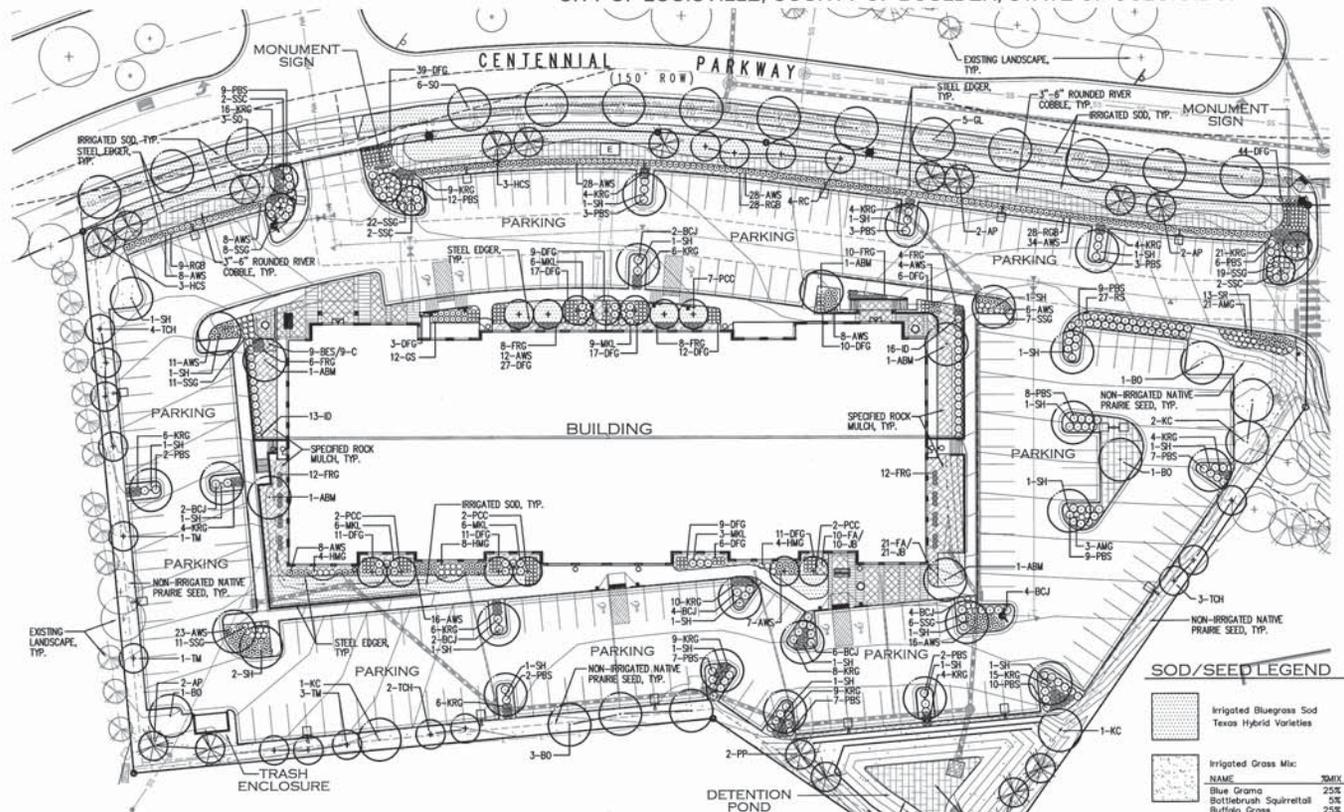


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CENTENNIAL VALLEY BUSINESS PARK
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12-3-15 SUBMITTAL #1
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 10-14-16 MYLAR SET



LANDSCAPE CALCULATIONS

Required by COSSC Section 5.3, Landscape Design-Note: All quantities below comply with the COSSC

SITE AREA	180,725 S.F.	PERIMETER LANDSCAPE AREA (SIMILAR USE)	
TOTAL LANDSCAPE AREA (34%)	60,664 S.F.	EAST PERIMETER (410 L.F.)	TREES REQUIRED: 10 TREES PROVIDED: 10
PARKWAY LANDSCAPE AREA		SOUTH PERIMETER (450 L.F.)	TREES REQUIRED: 12 TREES PROVIDED: 12
CENTENNIAL PARKWAY (609 L.F.)		WEST PERIMETER (280 L.F.)	TREES REQUIRED: 7 TREES PROVIDED: 7
TREES REQUIRED:	30	PER CODE SECTION 5.3.1.2	
SHRUBS REQUIRED:	180		
TREES PROVIDED:	32		
SHRUBS PROVIDED:	191		

IRRIGATED AREAS

IRRIGATED SOD (Includes R.O.W.)	13,991 S.F.
IRRIGATED SEED:	0 S.F.
IRRIGATED SHRUB BEDS:	9,283 S.F.
TOTAL:	23,274 S.F.

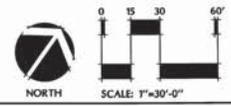
REFER TO SHEET 7 FOR LANDSCAPE NOTES & DETAILS

PLANT LIST

SYM.	COMMON / BOTANIC NAME	SIZE	COMMENTS
DECIDUOUS SHADE TREES			
KC	Kentucky Coffee Tree <i>Gymnocladia dioica</i>	2" cal.	Specimen quality, full crown, BAB, staked
SH	Shademaster Honeylocust <i>Gleditsia triacanthos 'Shademaster'</i>	2" cal.	Specimen quality, full crown, BAB, staked
EO	English Oak <i>Quercus ruber</i>	2" cal.	Specimen quality, full crown, BAB, staked
AM	Aurum Blaze Maple <i>Acer freemanii</i>	2" cal.	Specimen quality, full crown, BAB, staked
SL	Shumard Oak <i>Quercus shumardii</i>	2" cal.	Specimen quality, full crown, BAB, staked
OL	Greenleaved Linden <i>Tilia 'Greenlee'</i>	2" cal.	Specimen quality, full crown, BAB, staked
BO	Bur Oak <i>Quercus macrocarpa</i>	2" cal.	Specimen quality, full crown, BAB, staked
ORNAMENTAL TREES			
TM	Tatarian Maple <i>Acer tataricum</i>	1-1/2" cal.	Specimen quality, clump form, BAB, staked
TDH	Thornless Cockspur Hawthorn <i>Crataegus crus-galli var. hiemalis</i>	1-1/2" cal.	Specimen quality, clump form, BAB, staked
PCC	Chanticleer Pear <i>Pyrus calleryana 'Chanticleer'</i>	1-1/2" cal.	Specimen quality, full crown, BAB, staked
RC	Redstart <i>Crataegus mollis 'Redstart'</i>	1-1/2" cal.	Specimen quality, full crown, BAB, staked
SSC	Spring Snow Crabapple <i>Malus sp. 'Spring Snow'</i>	1-1/2" cal.	Specimen quality, full crown, BAB, staked
EVERGREEN TREES			
AP	Australian Pine <i>Pinus nigra</i>	6"-8" h.t.	Specimen quality, full form, BAB, puyed
HCS	Hopewell Colorado Spruce <i>Picea purpurea 'Hopewell'</i>	6"-8" h.t.	Specimen quality, full form, BAB, puyed
PP	Pinon Pine <i>Pinus ponderosa</i>	6"-8" h.t.	Specimen quality, full form, BAB, puyed
DECIDUOUS SHRUBS			
RGB	Rose Blue Barbary <i>Barbette thunbergii 'Rose Glow'</i>	5 gal.	container, 5 cones min. 4" h.t., plant 3" o.c.
AW	Anthony Waterer Spirea <i>Spiraea 'Anthony Waterer'</i>	5 gal.	container, 5 cones min. 3" h.t., plant 3" o.c.
SR	Shrub Rose <i>Rosa 'Ourmet Popcorn'</i>	5 gal.	container, 5 cones min. 3" h.t., plant 3" o.c.
GS	Goldflame Spirea <i>Spiraea 'Goldflame'</i>	5 gal.	container, 5 cones min. 4" h.t., plant 3" o.c.
ML	Miss Kim Lilo <i>Spiraea palmata 'Miss Kim'</i>	5 gal.	container, 5 cones min. 4" h.t., plant 3" o.c.
RS	Rustic Rose <i>Rosa rugosa 'Rustic Rose'</i>	5 gal.	container, 5 cones min. 5" h.t., plant 4" o.c.
FS	Fragrant Sumac <i>Rhus aromatica</i>	5 gal.	container, 5 cones min. 5" h.t., plant 4" o.c.
LD	Lantana Dogwood <i>Cornus sericea 'Lantana'</i>	5 gal.	container, 5 cones min. 5" h.t., plant 4" o.c.
PS	Prunella Butterfly <i>Prunella 'Prunella Butterfly'</i>	5 gal.	container, 5 cones min. 5" h.t., plant 4" o.c.
EVERGREEN SHRUBS			
BCJ	Blue Chip Juniper <i>Juniperus horizontalis 'Blue Chip'</i>	5 gal.	container, 18"-24" spread plant 4" o.c.
PERENNIALS/ORNAMENTAL GRASSES			
FA	Fall Aster <i>Aster obsoletus 'Alert'</i>	1 gal.	container, plant 18" o.c. red w/ yellow eye
BES	Block-Eyed Susan <i>Rudbeckia hirta 'Becky Mix'</i>	1 gal.	container, plant 18" o.c.
JB	April's Breeze <i>Centranthus ruber</i>	1 gal.	container, plant 18" o.c.
C	Coltsfoot <i>Nepeta 'Wilde's Low'</i>	1 gal.	container, plant 18" o.c.
DFG	Dwarf Fountain Grass <i>Pennisetum alopecuroides 'Hansen'</i>	1 gal.	container, plant 24" o.c.
KRG	Korean Reed Grass <i>Calamagrostis brachytricha</i>	1 gal.	container, plant 30" o.c.
FRG	Feather Reed Grass <i>Calamagrostis, cont. 'Yari Foerster'</i>	1 gal.	container, plant 30" o.c.
SSG	Shenandoah Switch Grass <i>Panicum virgatum 'Shenandoah'</i>	1 gal.	container, plant 36" o.c.
AMG	Adagio Maiden Grass <i>Miscanthus sinensis 'Adagio'</i>	1 gal.	container, plant 36" o.c.
HMG	Heavy Metal Switch Grass <i>Panicum virgatum 'Heavy Metal'</i>	1 gal.	container, plant 36" o.c.

SOD/SEED LEGEND

	Irrigated Bluegrass Sod
	Texas Hybrid Varieties
	Irrigated Grass Mix:
	Blue Grama 25%
	Bottlebrush Squarretail 5%
	Buffalo Grass 25%
	Green Needlegrass 5%
	Prairie June Grass 5%
	Sand Drop Seed 5%
	Side Oats Grass 20%
	Western Wheatgrass 10%
	Non-irrigated Native Prairie Mix:
	Blue Grama 23%
	Buffalo Grass 10%
	Green Needlegrass 20%
	Side Oats Grass 20%
	Western Wheatgrass 25%
	Sand Dropped 2%
	Specified 3/4" Crushed Granite Rock Mulch



LANDSCAPE PLAN
SHEET 6 OF 11

**CENTENNIAL VALLEY BUSINESS PARK
LOTS 3 AND 4, BLOCK 3, FILING NO.1
PLANNED UNIT DEVELOPMENT**

LOCATED IN THE NORTHEAST QUARTER OF SECTION 13,
TOWNSHIP 1 SOUTH, RANGE 70 WEST OF THE 6TH P.M.
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO.



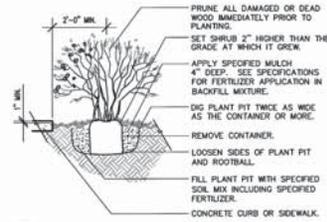
12-3-15 SUBMITTAL #1
2-12-16 SUBMITTAL #2
10-14-16 MYLAR SET



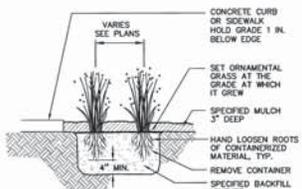
1 DECIDUOUS TREE PLANTING
N.T.S.



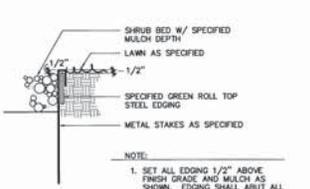
2 EVERGREEN TREE PLANTING
N.T.S.



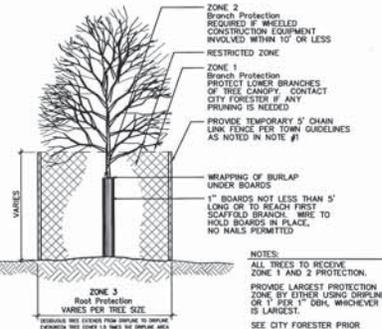
3 SHRUB PLANTING DETAIL
N.T.S.



4 ORNAMENTAL GRASS DETAIL
N.T.S.



5 STEEL EDGER DETAIL
N.T.S.



6 TREE PROTECTION DETAIL
N.T.S.

LANDSCAPE NOTES

- THIS LANDSCAPE ARCHITECTURAL SITE PLAN IS TO BE USED IN CON-JUNCTION WITH THE EXISTING CIVIL, MECHANICAL, ELECTRICAL, ARCHITECTURAL, AND IRRIGATION AS-BUILT SITE PLANS TO FORM COMPLETE INFORMATION REGARDING THIS SITE.
- ALL NEW LANDSCAPE AREAS SHALL BE ROTOTILLED WITH CLASS 1 ORGANIC COMPOST AT A RATE OF 4 CUBIC YARDS PER 1,000 SQUARE FEET. THIS PREPARATION SHALL BE THOROUGHLY INCORPORATED INTO THE TOP 6" OF SOIL.
- ALL DECIDUOUS AND EVERGREEN TREES ARE TO BE APPROVED BY THE OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION. ANY PLANT NOT MEETING THE LANDSCAPE ARCHITECT'S APPROVAL WILL BE REJECTED AT ANY TIME PRIOR TO FINAL ACCEPTANCE.
- ALL TREES TO BE STAKED OR GUYED AS PER PLANT LIST AND DETAILS.
- ALL TREE AND SHRUB BED LOCATIONS ARE TO BE STAKED OUT ON SITE FOR APPROVAL BY THE OWNER'S REPRESENTATIVE PRIOR TO INSTALLATION.
- ALL SHRUB/ORNAMENTAL GRASS BEDS SHALL BE MULCHED WITH 4" DEPTH 3/4" CRUSHED GRANITE ROCK MULCH OVER SPECIFIED FILTER FABRIC.
- ALL MULCH AREAS ADJACENT TO THE BUILDING SHALL BE MULCHED WITH 4" DEPTH 3/4" CRUSHED GRANITE ROCK MULCH OVER SPECIFIED FILTER FABRIC.
- ALL PERENNIAL BEDS SHALL BE MULCHED WITH 4" DEPTH WESTERN RED CEDAR WOOD MULCH. DO NOT PLACE FILTER FABRIC UNDER WOOD MULCH.
- ALL SHRUB AND PERENNIAL BEDS IN THE THERAPY GARDEN SHALL BE MULCHED WITH 4" DEPTH WESTERN RED CEDAR WOOD MULCH. DO NOT PLACE FILTER FABRIC UNDER WOOD MULCH.
- ALL NEW SHRUB BEDS AND MULCH AREAS ARE TO BE CONTAINED WITH SPECIFIED ROLL TOP STEEL EDGER (NOT REQUIRED AT CURB, WALKS OR BUILDING). PLACE STEEL EDGER BETWEEN ALL ROCK AND WOOD MULCH BEDS.
- ALL NEW LANDSCAPE AREAS, INCLUDING THE IRRIGATED SEED MIX, WILL BE WATERED WITH AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM. ALL TURF AREAS LESS THAN 15' IN WIDTH SHALL HAVE LOW-ANGLE SPRAY NOZZLES DESIGNED FOR THE SPECIFIED MOWTH. ALL TURF AREAS GREATER THAN 15' SHALL BE IRRIGATED WITH GEAR-DRIVEN ROTATORS WITH LOW ANGLE NOZZLES ON SEPARATE STATIONS. ALL SPRAY HEADS SHALL BE PRESSURE REDUCING DESIGNED TO PREVENT LOW HEAD DRAINAGE. ALL IRRIGATED SOO AND SEED AREAS SHALL BE SEPARATELY ZONED. ALL SHRUB BEDS SHALL BE IRRIGATED WITH A SEPARATELY ZONED DRIP SYSTEM PROVIDING FULL COVERAGE TO EACH PLANT. A RAIN SENSOR SYSTEM SHALL BE ADDED TO THE NEW IRRIGATION SYSTEM. THE LANDSCAPE CONTRACTOR SHALL SUPPLY AN IRRIGATION SYSTEM DESIGN AND SUBMITTALS FOR APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- IRRIGATION SYSTEM DESIGN SHALL NOT OVERSPRAY ONTO PAVED SURFACES CENTENNIAL PARKWAY. PUBLIC WORKS TO APPROVE SPRINKLER SPRAY PATTERNS IN RIGHT-OF-WAY PRIOR TO CONSTRUCTION ACCEPTANCE.
- ALL EXISTING TREES ADJACENT TO THE EAST PROPERTY LINE SHALL BE PRUNED AND/OR LIMBED UP ON THIS PROPERTY TO AVOID ANY LONG TERM DAMAGE DUE TO CONSTRUCTION.
- RIGHT OF WAY AND ASSOCIATED IMPROVEMENTS SHALL BE PRIVATELY MAINTAINED AND SHALL INCLUDE LANDSCAPE MAINTENANCE AND SNOW REMOVAL.
- EVERGREEN TREES SHALL NOT BE PLANTED WITHIN 10' OF PUBLIC WALK/ CURB OR CITY UTILITY. DECIDUOUS TREES SHALL NOT BE PLANTED WITHIN 5' OF PUBLIC WALK/CURB OR CITY UTILITY.

TREE PRESERVATION AND REMOVAL NOTES:

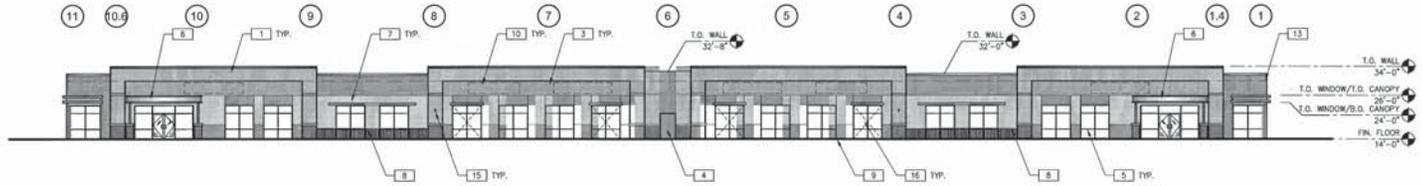
- BARRICADES SHALL HAVE A 1-FOOT RADIUS PER INCH OF TRUNK DIAMETER, WITH A WOOD CHIP MULCH 6 TO 8 INCHES DEEP EXTENDING BEYOND THE BARRIER TO THE DRIFLINE, IF NEEDED. IF LOW BRANCHES WILL BE KEPT, PLACE THE FENCE OUTSIDE THE DRIFLINE. EXAMINE TREES AND BARRICADES AT LEAST ONCE A WEEK DURING CONSTRUCTION. WITHIN THE DRIP LINE OF PROTECTED EXISTING TREES, THERE SHALL BE NO CUT OR FILL UNLESS THE CONSULTING ARBORIST HAS EVALUATED AND APPROVED THE DISTURBANCE. TREE ROOTS ON TREES IN NATIVE AREAS SUCH AS THIS CAN BE FOUND AT THE VERY SURFACE OF THE SOIL, AND AS MANY OF THESE ARE FEEDER ROOTS, NO DISTURBANCE OF ANY KIND SHALL BE PERMITTED UNLESS APPROVED BY THE CONSULTING ARBORIST.
- ALL EXISTING PRESERVED SPRUCE TREES SHALL BE TREATED FOR LPA BEETLES TWICE PER YEAR (MARCH AND JULY).
- ALL TRENCHING, BORING, AND PUSHING OF UTILITIES THROUGH THE ROOT ZONES OF EXISTING SHALL BE AVOIDED.
- IF DAMAGE OCCURS TO ROOTS OF EXISTING TREES, THE ROOTS SHALL BE PRUNED AND COVERED WITH SOIL OR MOST BURLAP WITHIN TWO HOURS.
- THE CONSULTING ARBORIST MUST BE CONTACTED PRIOR TO ANY NECESSARY CUT AND FILL EARTHWORK WITHIN THE DRIFLINE OF AN EXISTING PRESERVED TREE.
- EARTHWORK SHALL NOT BE PLACED ADJACENT TO TREE TRUNKS.
- ALL CONSTRUCTION ACTIVITY SHALL BE PROHIBITED WITHIN THE FENCED ROOT/DRIP LINE IN ORDER TO PREVENT SOIL COMPACTION.
- NO CONSTRUCTION RELATED WASH-OUT (E.G. LIME, ADD, CONCRETE) SHOULD OCCUR WITHIN 30 FEET OF ANY TREE TRUNK.
- A CERTIFIED ARBORIST SHALL PRUNE ALL EXISTING PRESERVED TREES.
- SUPPLEMENTAL WATERING IS NOT RECOMMENDED UNLESS APPROVED BY THE CONSULTING ARBORIST.

**LANDSCAPE NOTES
& DETAILS
SHEET 7 OF 11**

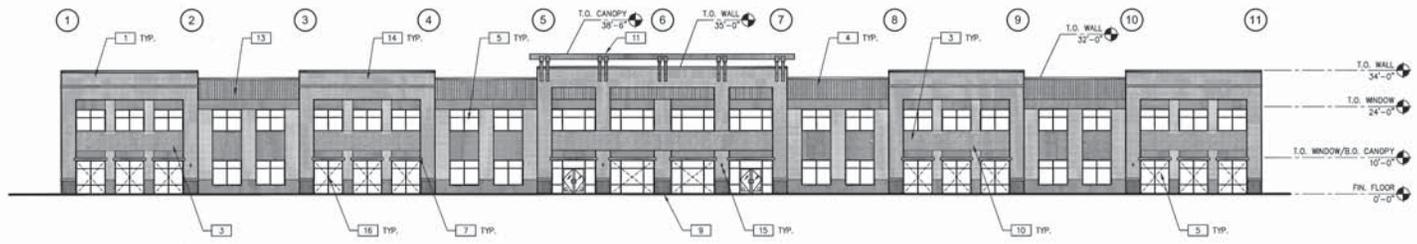
CENTENNIAL VALLEY BUSINESS PARK
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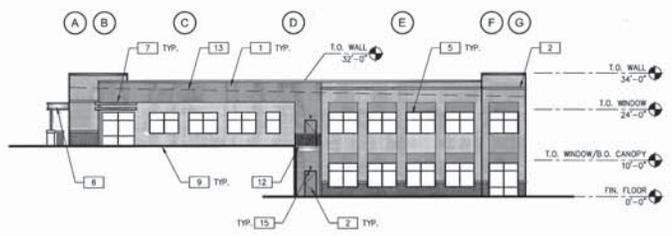
12-3-15 SUBMITTAL #1
 2-12-16 SUBMITTAL #2
 10-14-16 MYLAR SET



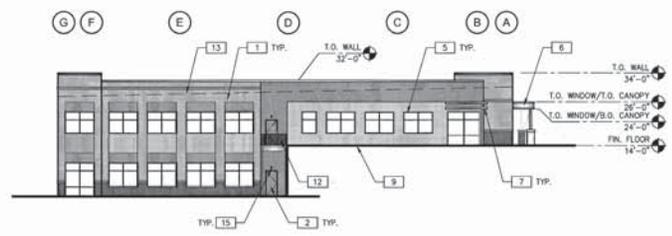
NORTH ELEVATION
 SCALE: 1/16" = 1'-0" (A)



SOUTH ELEVATION
 SCALE: 1/16" = 1'-0" (B)



WEST ELEVATION
 SCALE: 1/16" = 1'-0" (D)



EAST ELEVATION
 SCALE: 1/16" = 1'-0" (C)

COLOR LEGEND

	BASALITE MASONRY: PRECISION #101R		BERRIDGE METAL WALL PANEL: CHAMPAGNE
	BASALITE MASONRY: PRECISION #120R		ACCENT PAINT: "VIRTUAL TAUPE" SW7039
	BASALITE MASONRY: PRECISION #100R		FIELD PAINT: "REQUISITE GRAY" SW7023
	BASALITE MASONRY: GROUND FACE #605R		
	BASALITE MASONRY: SPLOT FACE #638R		

ELEVATION NOTES

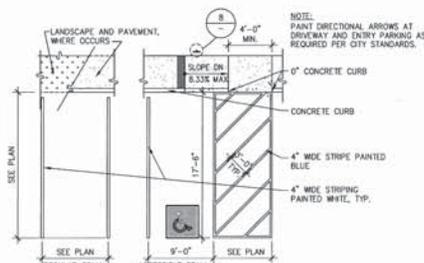
1 INTEGRALLY COLORED CONCRETE BLOCK	6 COMPOSITE METAL WRAPPED STEEL ENTRY CANOPY WITH MASONRY PLASTERS	11 STANDING SEAM METAL ROOF PANELS ON TUBE STEEL FRAME
2 HOLLOW METAL MAN DOORS, PAINTED	7 PAINTED METAL TUBE STEEL CANOPY	12 STEEL LANDING AND GUARDRAIL
3 PAINTED STUCCO	8 DECORATIVE METAL RAILING	13 ROOFLINE BEYOND
4 EXTERIOR METAL WALL PANEL	9 FINISH GRADE, SEE CIVIL DRAWINGS	14 FUTURE ROOFTOP MECHANICAL UNITS
5 CLEAR ANODIZED ALUMINUM STOREFRONT SYSTEM WITH 1" INSULATED GRAY GLAZING	10 POTENTIAL/FUTURE TENANT SIGNAGE LOCATION	15 WALL MOUNTED LIGHT FIXTURE, RE. ELEC.
		16 POTENTIAL FUTURE TENANT ENTRY

SHEET TITLE: EXTERIOR ELEVATIONS
SHEET 8 OF 11

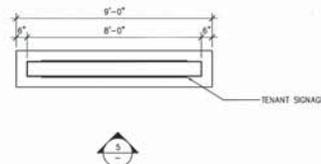
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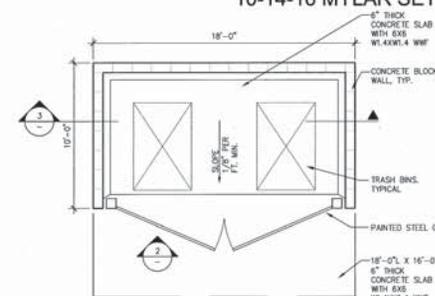
12-3-15 SUBMITTAL #1
2-12-16 SUBMITTAL #2
10-14-16 MYLAR SET



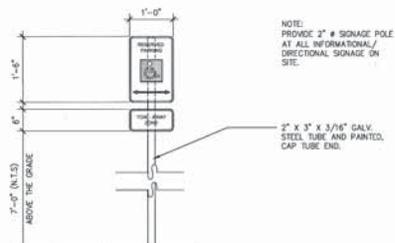
TYPICAL PARKING STALLS 7
 SCALE: 1/8" = 1'-0"
 ESRH-Parking_stalls-01



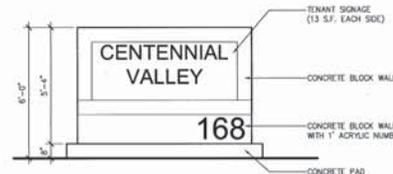
MONUMENT SIGN PLAN 4
 SCALE: N.T.S.



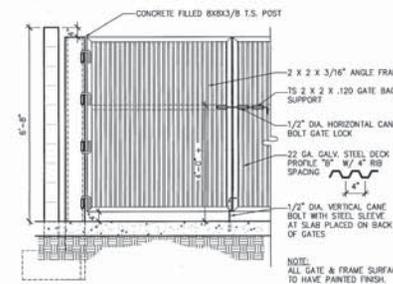
TRASH ENCLOSURE PLAN 1
 SCALE: 1/4" = 1'-0"
 ESRH-Trash_enclosure_plan-01



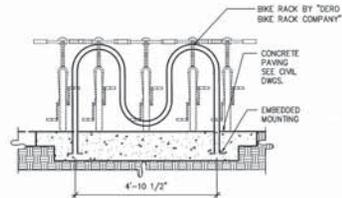
ACCESSIBLE PARKING SIGNAGE 8
 SCALE: 1 1/2" = 1'-0"
 ESRH-Sign_access_park-01



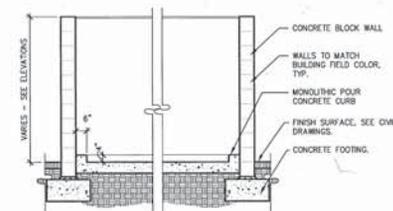
MONUMENT SIGN ELEVATION 5
 SCALE: N.T.S.



TRASH ENCLOSURE GATE 2
 SCALE: 1/2" = 1'-0"
 ESRH-Trash_enclosure_gate-02



BIKE RACK 6
 SCALE: 1/2" = 1'-0"
 ESRH-Bike_rack-01



TRASH ENCLOSURE SECTION 3
 SCALE: 3/8" = 1'-0"
 ESRH-Trash_enclosure_sect-01

SHEET TITLE: SITE DETAILS
SHEET 9 OF 11

CENTENNIAL VALLEY BUSINESS PARK
LOTS 3 AND 4, BLOCK 3, FILING NO.1
PLANNED UNIT DEVELOPMENT
 LOCATED IN THE NORTHEAST QUARTER OF SECTION 13,
 TOWNSHIP 1 SOUTH, RANGE 70 WEST OF THE 6TH P.M.
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO.



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12-3-15 SUBMITTAL #1
 2-12-16 SUBMITTAL #2
 10-14-16 MYLAR SET

POINT ILLUMINANCE SUMMARY:

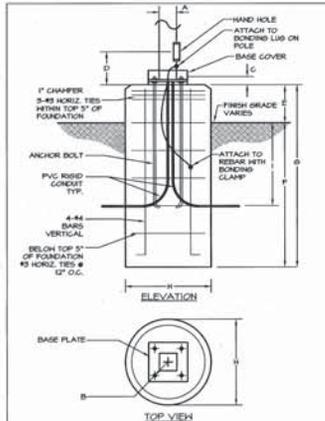
AREA	AVERAGE (FC)	MAX (FC)	MIN (FC)	MAX/MIN	AVG/MIN
OVERALL	0.5	0.2	0.0	N/A	N/A
BUILDING ENTRANCES	6.5	6.2	1.1	8.4:1	3.1:1
PARKING AREAS	2.0	4.8	0.2	25.0:1	10.0:1
WEST ROADWAY	2.2	3.5	0.8	4.3:1	2.75:1
PROPERTY LINE	0.2	2.1	0.0	N/A	N/A

EAI, EA2, EA3, EA4

EDIEM

EWI

- GENERAL NOTES**
- ALL FIXTURES ARE OF THE "FULL CUTOFF" VARIETY.
 - ILLUMINANCE VALUES SHOWN ARE AT GRADE AND SHOULD BE CONSIDERED INITIAL USING A LIGHT LOSS FACTOR OF 1.0.
 - ILLUMINANCE VALUES SHOWN HERE REPRESENT LIGHTING FROM LUMINAIRES SHOWN EXPLICITLY ON THIS DRAWING.



POLE	OVERALL HEIGHT	A	ANCHOR BOLT DATA	B	C	D	E	F	G	H	I
EAI	24'0"	4"	PER MANUFACTURER	2'0"	8'0"	10'0"	24"	36"			
EA2	24'0"	4"	PER MANUFACTURER	2'0"	8'0"	10'0"	24"	36"			
EA3	24'0"	4"	PER MANUFACTURER	2'0"	8'0"	10'0"	24"	36"			
EA4	24'0"	4"	PER MANUFACTURER	2'0"	8'0"	10'0"	24"	36"			

1 | POLE BASE DETAIL

SCALE: NONE

CSX1 LED
 LED Area Luminaire

Specifications

Size	12" H x 12" W
Length	12" H
Width	12" W
Height	12" H
Weight	37 lbs

Example: CSX1 LED 400 1000 80K TMAP 3000 SPA COB

Item	Qty	Unit	Description	Notes
100000	100	EA	CSX1 LED 400 1000 80K TMAP 3000 SPA COB	

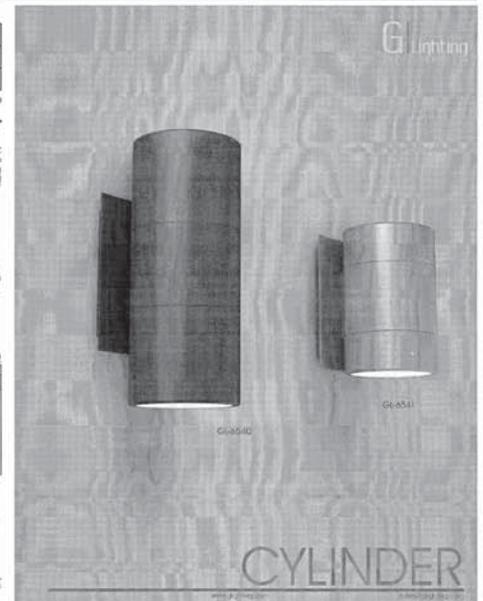
LITHONIA LIGHTING

REABILITY

FEATURES & SPECIFICATIONS

Example: REABILITY 1000 1000 80K TMAP 3000 SPA COB

Item	Qty	Unit	Description	Notes
100000	100	EA	REABILITY 1000 1000 80K TMAP 3000 SPA COB	



LIGHTING FIXTURE SCHEDULE

TYPE	DESCRIPTION	MANUFACTURER	CATALOG NUMBER	VOLTAGE	LAMP QTY	LAMP TYPE	HANG HIGHT	HOUSING LOCATION	INFO/NOTES
EAI	POLE MOUNTED AREA LUMINAIRE, TYPE IV DISTRIBUTION, HOUSE SIDE SHIELD	LITHONIA	CSX1 LED 400 1000 80K 14H HVOLT 1H	UNV	1	84 TV	154	POLE GROUND	24'-0" OPH
EA2	POLE MOUNTED AREA LUMINAIRE, TYPE II DISTRIBUTION, HOUSE SIDE SHIELD	LITHONIA	CSX1 LED 400 1000 80K 15H HVOLT 1H	UNV	1	84 II	154	POLE GROUND	24'-0" OPH
EA3	POLE MOUNTED AREA LUMINAIRE, TYPE III DISTRIBUTION, HOUSE SIDE SHIELD	LITHONIA	CSX1 LED 400 1000 80K 17PH HVOLT 1H	UNV	1	84 III	154	POLE GROUND	24'-0" OPH
EA4	POLE MOUNTED AREA LUMINAIRE, TYPE II DISTRIBUTION	LITHONIA	CSX1 LED 400 1000 80K 12H HVOLT 1H	UNV	1	84 II	154	POLE GROUND	24'-0" OPH
EDIEM	6" LED RECESSED DOWNLIGHT, W/ EMERGENCY BATTERY	LITHONIA	REALOG DRPH ELS 1000 35K AS5C XXX ELR	120/277V	1	14	14	RECESSED CEILING	10'-0" BOF
EWI	DR LUMEN LED CYLINDER SCORCE DOWNLIGHT ONLY	G-LIGHTING	GL-654I-B-CG-RS-TEL-UNV	120/277V	1	12	11	SURFACE HALL	8'-0" BOF

ADMINISTRATIVE: TOP - BOTTOM OF FIXTURE, BFD - RECESSED FIXTURE DEPTH OPH - OVERALL FIXTURE HEIGHT

GENERAL NOTES:

- ALL FLUORESCENT LAMPS TO BE 3000K COLOR TEMPERATURE AND A MINIMUM OF 80CRI, UNLESS NOTED OTHERWISE.
- ALL REFLECTOR LAMPS TO BE PROVIDED AS PER FLOOD DISTRIBUTION UNLESS NOTED OTHERWISE.
- ALL FLUORESCENT LUMINAIRES THAT UTILIZE DOUBLE-ENDED LAMPS AND CONTAIN BALLASTS THAT CAN BE SERVICED IN PLACE SHALL HAVE A DISCONNECTING MEANS AS DESCRIBED IN SEC. 402.0005.
- PROVIDE LUMINAIRES SHOWN AS SHARED WITH EMERGENCY BATTERY BALLASTS. EMERGENCY LUMINAIRES SHALL SENSE IN-SWITCHED POWER TO THE SPACE AND OPERATE AUTOMATICALLY ON LOSS OF NORMAL POWER. ALL SHARED LUMINAIRES WITH 277V AND 8FT LAMPS SHALL HAVE ONE (1) 40 MINUTE RATED, 150 LAMP, 1400 LUMEN EMERGENCY BALLAST. ALL SHARED LUMINAIRES WITH COMPACT FLUORESCENT LAMPS SHALL HAVE A FACTORY INSTALLED 90 MINUTE EMERGENCY BALLAST. ALL EMERGENCY LUMINAIRES SHALL HAVE REDUOTE TEST SWITCHES AND VISIBLE INDICATING LIGHTS. CONNECT THE EMERGENCY BATTERY BALLAST TO THE UN-SWITCHED LEGS OF THE LIGHTING CIRCUIT AS NOTED.

SPECIFIC NOTES:

- VERIFY MOUNTING HEIGHT AND FINISH WITH ARCHITECT/OWNER.

PHOTOMETRIC DETAILS
 SHEET 11 OF 11

COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
 RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN. CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



PROJECT SUMMARY

TOTAL LAND AREA: 531,012 SQUARE FEET (12.19 ACRES)
 BUILDING AREA: 153,018 SQUARE FEET
 FAR: 0.29

BUILDING HEIGHT:
 ALLOWABLE: 40.0 FEET
 PROPOSED: 37.0 FEET

SITE COVERAGE REQUIREMENT:
 BUILDING, PARKING, AND DRIVEWAY COVERAGE:
 ALLOWABLE: 75%
 PROVIDED: 74%
 LANDSCAPE COVERAGE:
 MINIMUM: 25%
 PROVIDED: 28%

BUILDING SETBACKS:
 FRONT YARD AT A LOCAL PUBLIC STREET
 ALLOWABLE: 30 FEET
 PROVIDED: OVER 170 FEET
 SIDE YARD FROM LOCAL PUBLIC STREET
 ALLOWABLE: 30 FEET
 PROVIDED: OVER 80 FEET
 REAR YARD ABUTTING SIMILAR ZONE DISTRICT
 ALLOWABLE: 15 FEET
 PROVIDED: OVER 120 FEET

PARKING SETBACKS:
 FRONT OR SIDE YARD AT A LOCAL PUBLIC STREET
 ALLOWABLE: 20 FEET
 PROVIDED: 20 FEET

PARKING
 REQUIRED: 307 SPACES @ 2 SPACES PER 1,000 SQUARE FEET
 PROVIDED:
 STANDARD: 416 SPACES
 HANDICAP: 9 SPACES
 TOTAL WITHOUT TRUCK COURT: 425 SPACES (2.78 SPACES PER 1,000 SQUARE FEET)
 TOTAL WITH TRUCK COURT: 553 SPACES (3.81 SPACES PER 1,000 SQUARE FEET)

GENERAL NOTES

- SITE ZONED I - INDUSTRIAL
- ALL SETBACKS AND LAND USE REQUIREMENTS SHALL CONFORM TO THE CITY OF LOUISVILLE, COLORADO ZONING AND LAND USE REGULATIONS IN EFFECT AS OF THE DATE OF APPROVAL OF THIS PLANNED UNIT DEVELOPMENT BY THE CITY OF LOUISVILLE, COLORADO.
- EXCEPT AS AMENDED BY THIS FINAL PLANNED UNIT DEVELOPMENT, ALL SIGNS SHALL CONFORM TO THE INDUSTRIAL DEVELOPMENT DESIGN STANDARDS AND GUIDELINES. THESE AMENDMENTS ARE:
 - FOUR FREESTANDING, GROUND MOUNTED DOUBLE FACED SIGNS LOCATED PER THE LANDSCAPE PLAN BE PERMITTED. THE DESIGNS FOR THESE SIGNS SHALL BE PER THE DETAIL ON SHEET 9.
 - SURFACE MOUNTED BUILDING SIGNS SHALL NOT EXCEED 2 FEET IN HEIGHT BY 20 FEET IN LENGTH EACH WITH A MAXIMUM OF FIVE SURFACE MOUNTED SIGNS. THREE OF THE BUILDINGS SIGNS SHALL BE PERMITTED ON THE EAST BUILDING ELEVATION (FACING CTC- BLVD.) MAXIMUM SURFACE MOUNTED BUILDING SIGNAGE AREA OF 120 SQUARE FEET IN THE AGGREGATE.
 - FOR BOTH MULTI TENANT AND SINGLE TENANT OCCUPANCY, THE SURFACE MOUNTED BUILDING SIGN SHALL NOT EXCEED 24 INCHES IN HEIGHT.
- THE CITY OF LOUISVILLE IS NOT RESPONSIBLE FOR DAMAGE TO OR REPAIR OF MONUMENT SIGNS DUE TO UTILITY MAINTENANCE.
- THE CITY OF LOUISVILLE IS NOT RESPONSIBLE FOR DAMAGE TO PAVEMENT SURFACES OR LANDSCAPING CAUSED DURING REPAIR OR MAINTENANCE ACTIVITIES OF UTILITIES LOCATED WITHIN PUBLIC UTILITY EASEMENTS.
- ON STREET PARKING WILL NOT BE UTILIZED TO MEET THE PARKING REQUIREMENTS OF THE PROJECT.
- ALL ROOF-MOUNTED MECHANICAL, ELECTRICAL, OPTICAL AND ELECTRONIC EQUIPMENT SHALL BE SET A MINIMUM OF 20' FROM THE BUILDING PARAPET, AND IF VISIBLE FROM THE PUBLIC STREET ADJACENT TO THE PROPERTY, SHALL BE PAINTED TO MATCH THE DOMINANT COLOR OF THE BUILDING.
- OWNER WILL ADD ADDITIONAL ADA PARKING SPACES TO THE PARKING IF THE BUILDING IS LEASED PRIMARILY AS OFFICE SPACE.
- NO FENCING SHALL BE PLACED WITHIN THE 40 FOOT DRAINAGE & UTILITY EASEMENT AT THE SOUTH SIDE OF THE SITE.



VICINITY MAP
 SCALE: NOT TO SCALE

LEGAL DESCRIPTION

COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5, & 16

DRAWING INDEX

SHEET 1 OF 15	COVER SHEET
SHEET 2 OF 15	UTILITY PLAN
SHEET 3 OF 15	GRADING PLAN
SHEET 4 OF 15	DEVELOPMENT PLAN
SHEET 5 OF 15	FLOOR PLAN
SHEET 6 OF 15	NORTH AND SOUTH ELEVATIONS
SHEET 7 OF 15	EAST ELEVATION
SHEET 8 OF 15	WEST ELEVATION
SHEET 9 OF 15	ARCHITECTURAL SITE DETAILS
SHEET 10 OF 15	LANDSCAPE PLAN
SHEET 11 OF 15	LANDSCAPE PLAN
SHEET 12 OF 15	LANDSCAPE PLAN
SHEET 13 OF 15	LANDSCAPE DETAILS
SHEET 14 OF 15	SITE PHOTOMETRIC PLAN
SHEET 15 OF 15	PHOTOMETRIC DETAILS

OWNERSHIP SIGNATURE BLOCK

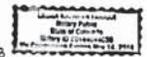
By signing this Final PUD, the owner acknowledges and accepts all the requirements and intent set forth in this Final PUD. Witness my hand and seal this 28 day of March, 2016.

Owner: EJ 633 CTC LLC, A COLORADO LIMITED LIABILITY COMPANY

Bruce H. Etkin
 BRUCE H. ETKIN, MANAGER
 3-23-16
 (Date)

Laura Farrar
 Notary Name(print)
 3-23-16
 (Date)

Laura Farrar
 Notary Signature
 Witness my hand and official seal



My commission expires November 14, 2018

PLANNING COMMISSION CERTIFICATE

Approved this 10 day of December, 2015 by the Planning Commission of the City of Louisville, Colorado.
 Resolution No. 37, Series 2015.

CITY COUNCIL CERTIFICATE

Approved this 18 day of January, 2016 by the City Council of the City of Louisville, Colorado.
 Resolution No. 4, Series 2016.

BY: Robert P. Muckie
 Robert P. Muckie, Mayor

BY: Carol Hanson
 Nancy-Ann, City Clerk
 Carol Hanson, Acting



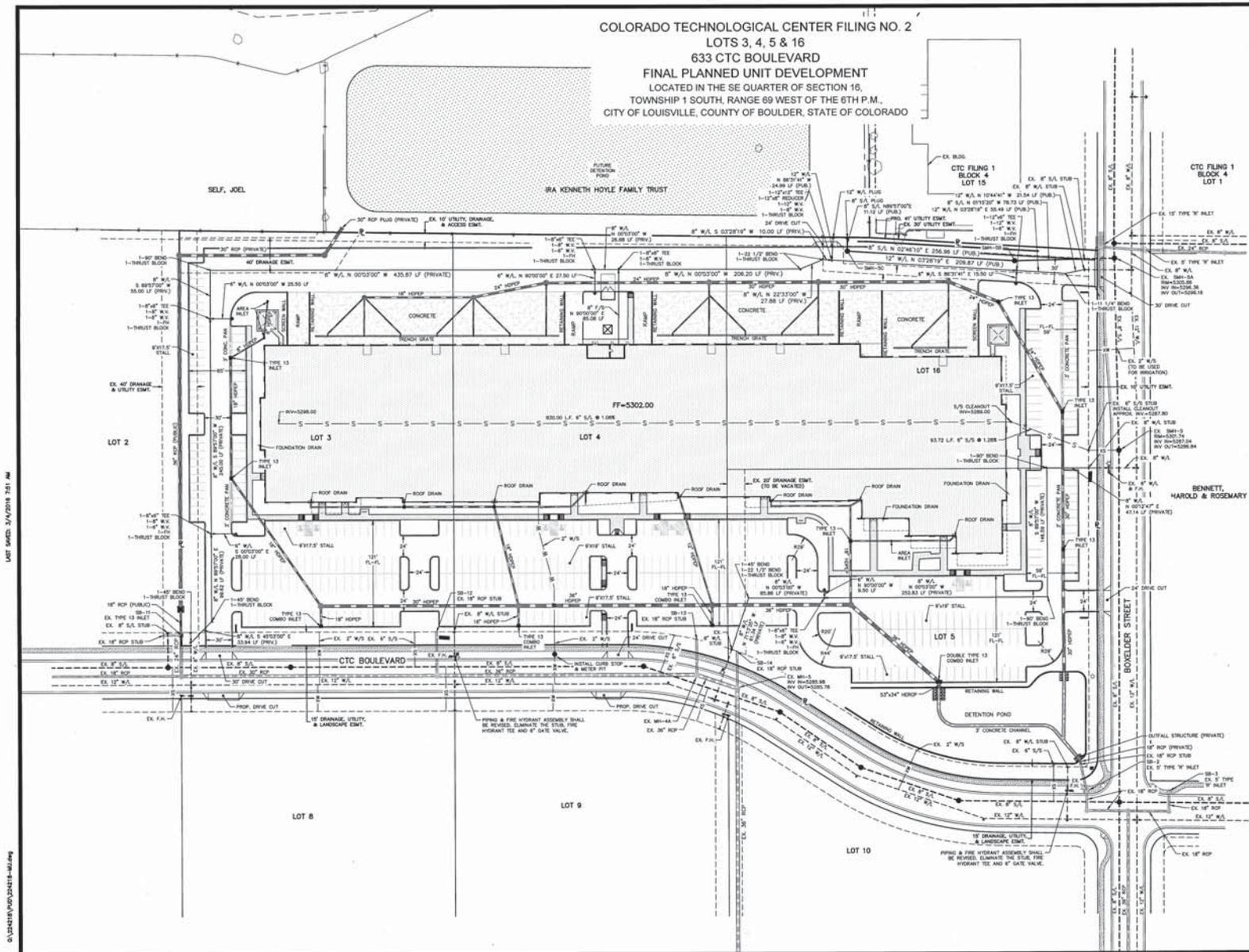
CLERK & RECORDER CERTIFICATE

I hereby certify that this instrument was filed in my office at 9:00 a.m. this 28 day of March, 2016, and is recorded in Plan File Fee paid. Reception.

Clerk & Recorder Issue Date
 Deputy

COVER SHEET

03/04/2016
 SHEET 1 OF 15



COLORADO TECHNOLOGICAL CENTER FILING NO. 2
 LOTS 3, 4, 5 & 16
 633 CTC BOULEVARD
 FINAL PLANNED UNIT DEVELOPMENT
 LOCATED IN THE SE QUARTER OF SECTION 16,
 TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH P.M.,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

LEGEND

- Proposed Waterline
- Existing Waterline
- Proposed Sewerline
- Existing Sewerline
- Stormline
- Water Service
- Existing Water Service
- Sewer Service
- Existing Sewer Service
- Fire Service
- Existing Gas Line
- Ex. Underground Electric
- Ex. Overhead Electric
- Existing Fiber Optic
- Existing Telephone Line
- Existing Cable TV Line
- Thrust Block
- Water Valve
- Fire Hydrant
- Plug
- Manhole
- Type 'W' Inlet
- Existing Contours
- Proposed Contours
- Street Light
- Install Handicap Ramp
- Public
- Private

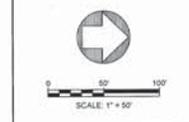
SCALE VERIFICATION

DATE: 03/04/16
 DRAWN BY: [Name]
 CHECKED BY: [Name]
 APPROVED BY: [Name]



HURST
 CIVIL ENGINEERING
 SURVEYING
 HURST & ASSOCIATES, INC.
 2500 E. 10th Ave., Suite B
 Boulder, CO 80504
 303.440.5100

- NOTES:**
- UNLESS NOTED OTHERWISE, THE PROPOSED WATER MAINS & APPURTENANCES SHALL BE PRIVATELY OWNED & MAINTAINED.
 - THE EXISTING WATER AND SEWER SERVICE STUBS THAT WILL NOT BE USED SHALL BE ABANDONED AT THE MAIN AS DIRECTED BY THE CITY.
 - ABANDON EXISTING UNUSED STORM SEWER PIPE STUBS AT STORM MANHOLES AND AT RIGHT-OF-WAY BY PLUGS AS DIRECTED BY THE CITY.
 - NO FENCING SHALL BE PLACED WITHIN THE 40 FOOT DRAINAGE & UTILITY EASEMENT AT THE SOUTH SIDE OF THE SITE.
 - THE PROPOSED HOPEF STORM PIPES, THE PROPOSED TYPE 13 INLETS, AND THE PROPOSED TYPE 13 COMBINATION INLETS SHALL BE PRIVATELY OWNED AND MAINTAINED.
 - THE PROPOSED ON-SITE 8" WATER MAINS, 6" APPURTENANCES, 6" WATER LINES, 4" APPURTENANCES, AND FIRE HYDRANTS SHALL BE PRIVATELY OWNED AND MAINTAINED.
 - THE 2" W/S SHALL BE ROUTED IN THE BUILDING JOIST SPACE FROM THE ENTRANCE INTO THE BUILDING TO THE FIRE RISER ROOM. THE 2" W/S LINE SHALL BE INSULATED AND MARKED "CITY LINE UNDER PRESSURE - NO TAP PERMITTED - NON-POTABLE WATER" ON FOUR FOOT SPACING TO THE 2" REDUCED PRESSURE BACKFLOW ASSEMBLY.



CTC FILING NO. 2, LOTS 3-5 & 16
 633 CTC BOULEVARD
 PLANNED UNIT DEVELOPMENT
 UTILITY PLAN

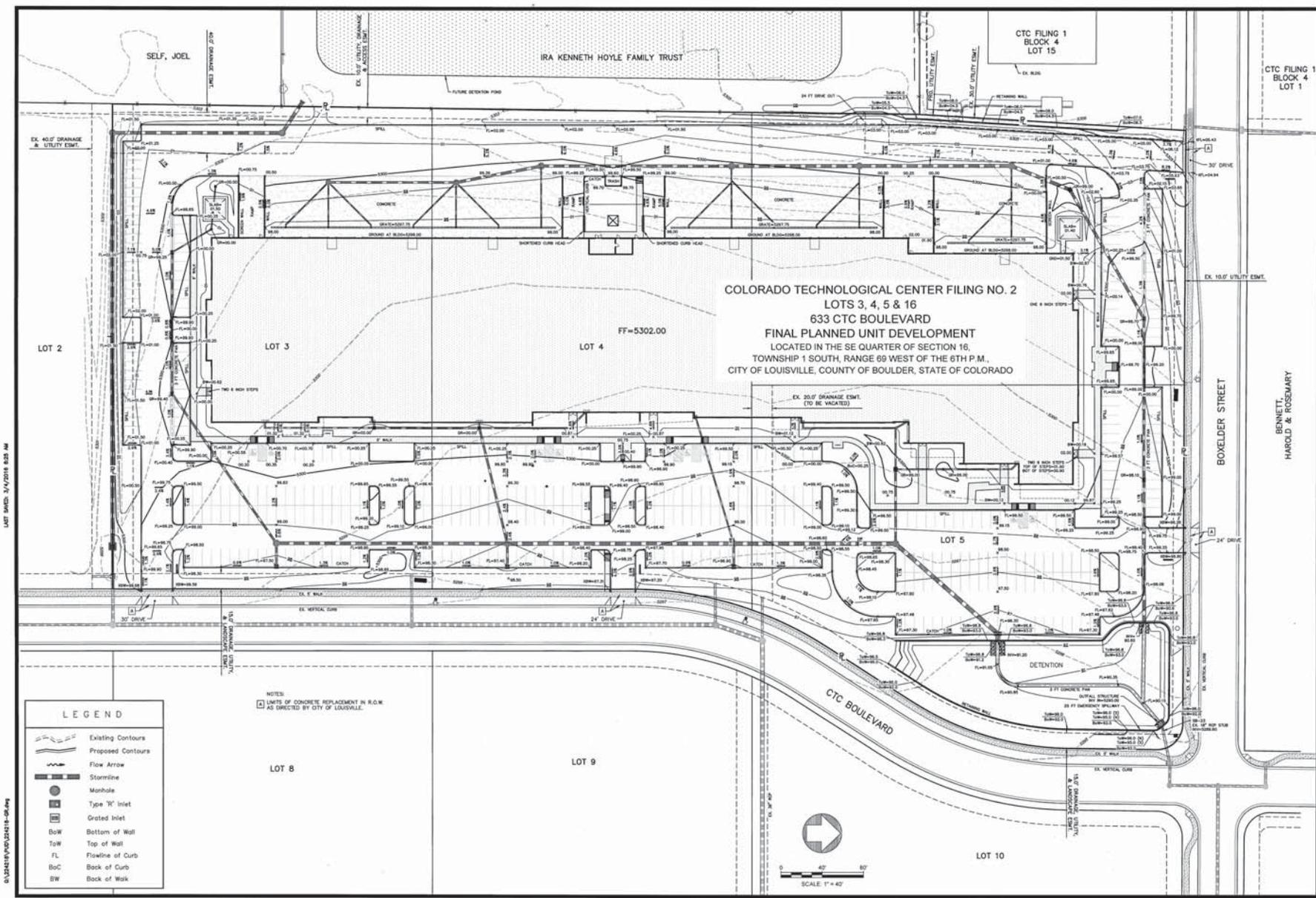
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 JOB NUMBER: 2242-18
 SCALE: 1"=50'
 SHEET NO: 2

CTC FILING NO. 2 FINAL PUD
 LOTS 3, 4, 5 & 16
 633 CTC BLVD. 3 of 15

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER



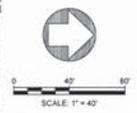
COLORADO TECHNOLOGICAL CENTER FILING NO. 2
LOTS 3, 4, 5 & 16
633 CTC BOULEVARD
FINAL PLANNED UNIT DEVELOPMENT
 LOCATED IN THE SE QUARTER OF SECTION 16,
 TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH P.M.,
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

FF=5302.00

LEGEND

	Existing Contours
	Proposed Contours
	Flow Arrow
	Stormline
	Manhole
	Type 'Y' Inlet
	Grated Inlet
	Bottom of Wall
	Top of Wall
	Flowline of Curb
	Back of Curb
	Back of Walk

NOTES:
 [Symbol] LIMITS OF CONCRETE REPLACEMENT IN R.O.M.
 AS DIRECTED BY CITY OF LOUISVILLE.



SCALE VERIFICATION

DATE: 03/04/16

BY: [Signature]

DESCRIPTION: [Text]

REVISIONS

NO.	DATE	DESCRIPTION



HURST & ASSOCIATES, INC.
 2000 Broadway, Suite B
 Boulder, CO 80508
 303.440.4300

HURST
 CIVIL ENGINEERING
 PLANNING
 SURVEYING

CTC FILING NO. 2, LOTS 3-5 & 16
 633 CTC BOULEVARD
 PLANNED UNIT DEVELOPMENT
 GRADING PLAN

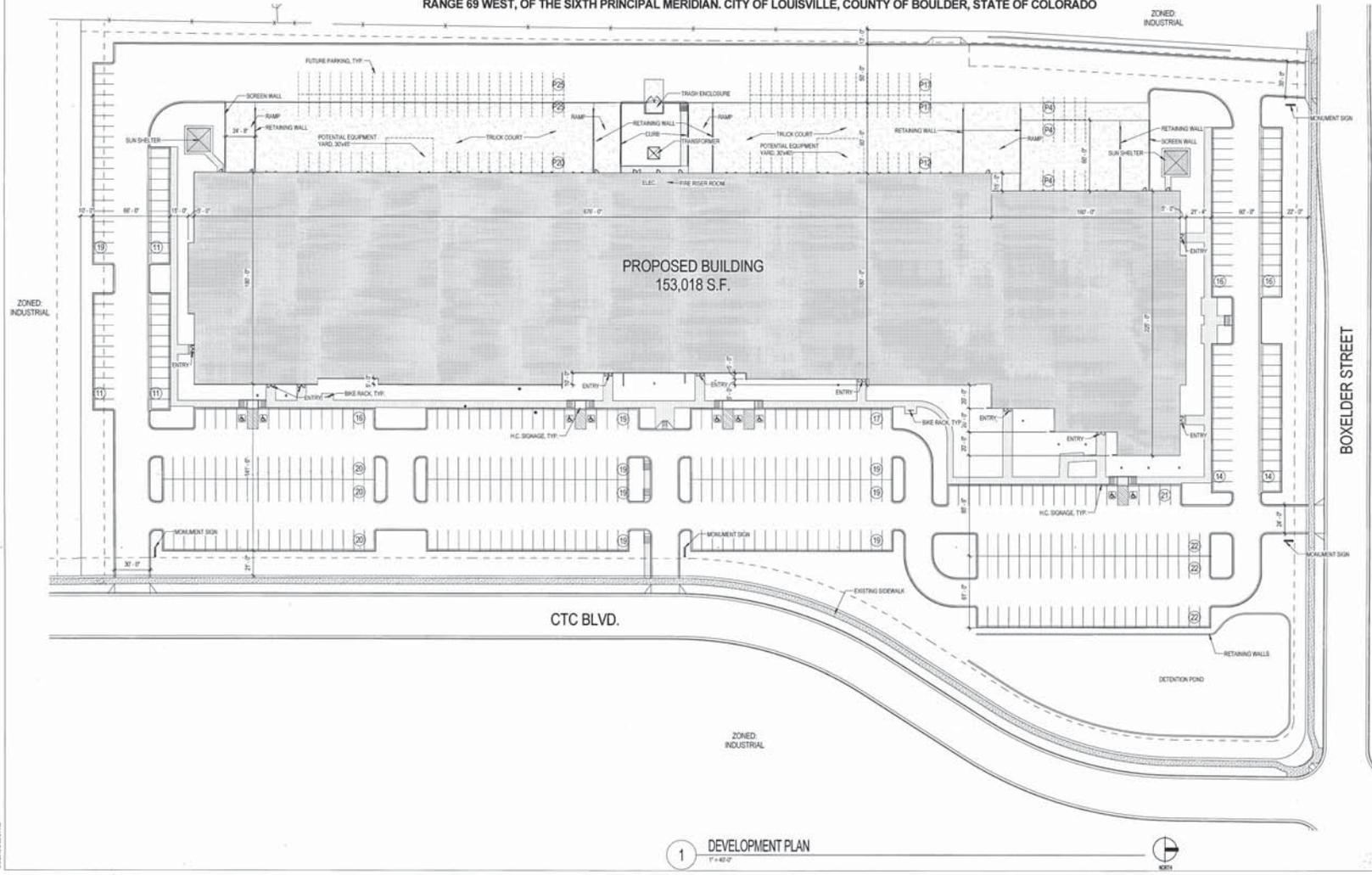
DATE	03/04/16
BY	[Signature]
SCALE	1"=50'
SHEET NO.	3



MOA ARCHITECTURE
801 17TH STREET, SUITE 400
DENVER, COLORADO 80202
P 303.338.1100 F 303.338.1107
WWW.MOAARCHITECT.COM

COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN. CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



1 DEVELOPMENT PLAN
1" = 40'-0"

DEVELOPMENT PLAN
03/04/2016
SHEET 4 OF 15

633 CTC Blvd - PUD

CTC FILING NO. 2 FINAL PUD
LANS 3, 4, 5 & 16
633 CTC Blvd 4 of 15

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER

CTC FILING No. 2 FINAL PUD
 Lots 3, 4, 5 & 16
 633 CTC Blvd. 5 of 15

DRAWING NUMBER

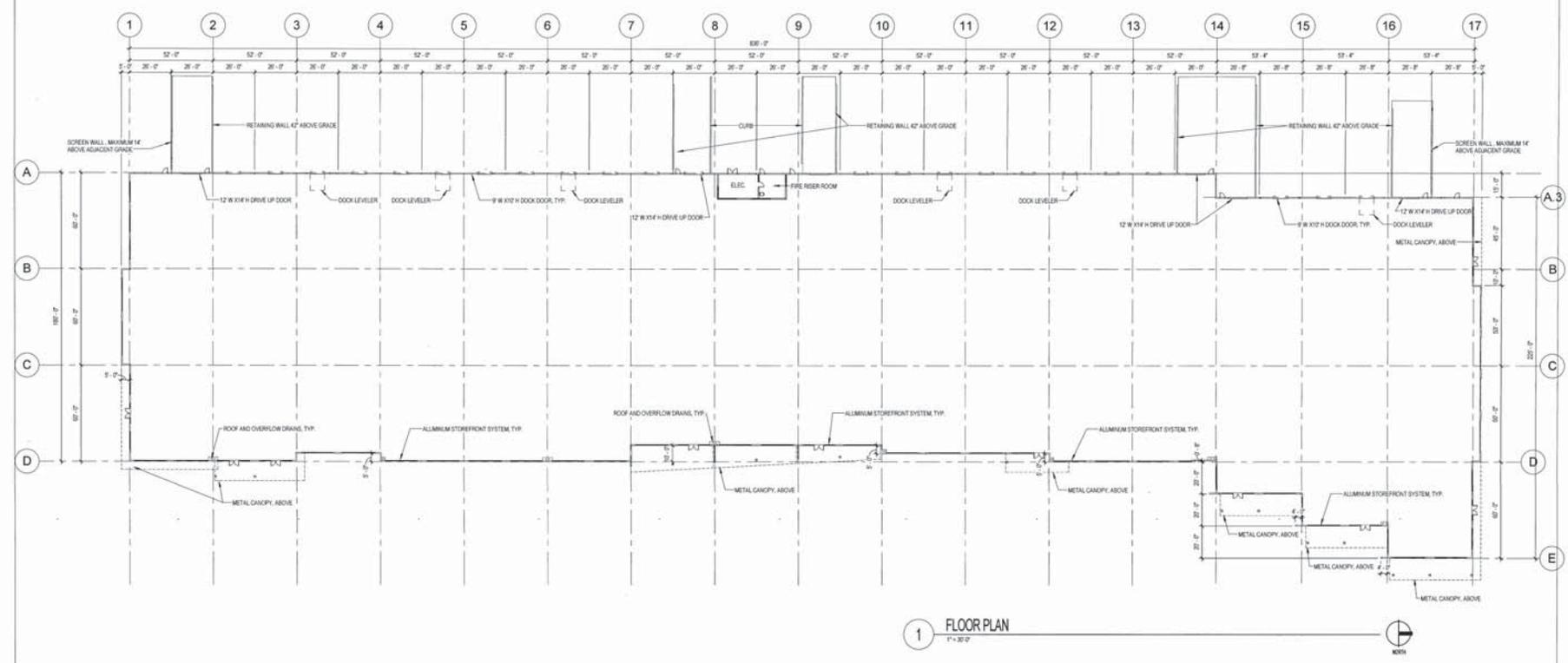
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DRAWING NUMBER

COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT



LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
 RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN, CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



1 FLOOR PLAN
 1" = 30'



FLOOR PLAN
 03/04/2016
 SHEET 5 OF 15

633 CTC Blvd - PUD

CTC FILING NO. 2 FINAL PUD
 LOTS 3, 4, 5, & 16
 633 CTC BLVD 6 of 15

DRAWING NUMBER

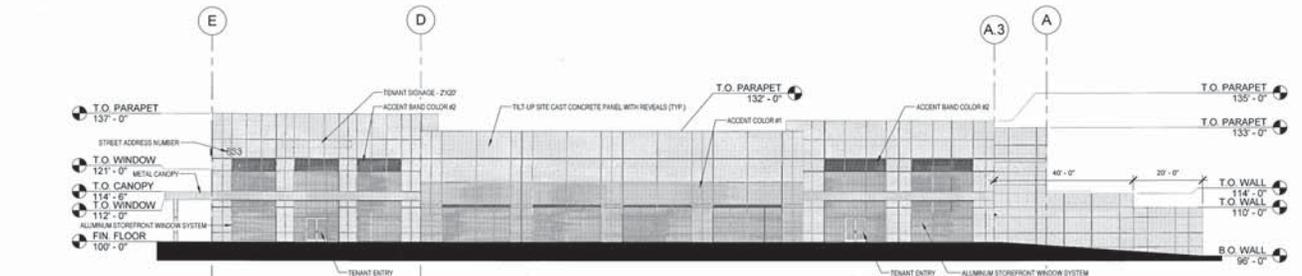
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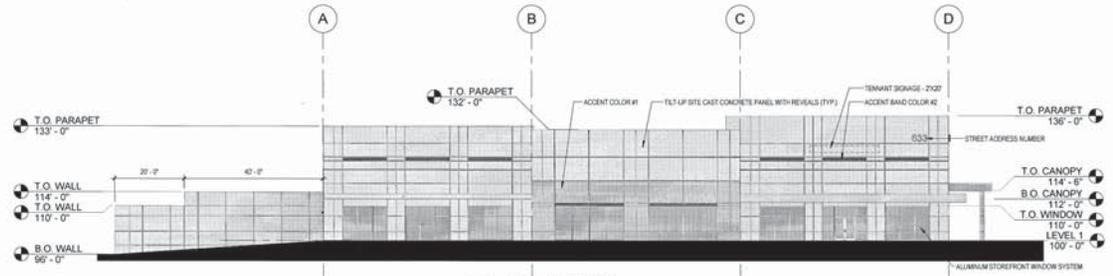


**COLORADO TECHNOLOGICAL CENTER
 FILING NO. 2, LOTS 3,4,5,& 16.
 633 CTC BLVD.
 FINAL PLANNED UNIT DEVELOPMENT**

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
 RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN. CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



1 NORTH ELEVATION
 1/8" = 1'-0"



2 SOUTH ELEVATION
 1/8" = 1'-0"

- FIELD COLOR - SHERWIN WILLIAMS - SW 6147 PANDA WHITE
- ACCENT COLOR 1 - SHERWIN WILLIAMS - SW 6150 UNIVERSAL PINK
- ACCENT COLOR 2 - SHERWIN WILLIAMS - SW 6017 REGATA
- METAL PANEL

**NORTH & SOUTH
 ELEVATIONS
 03/04/2016
 SHEET 6 OF 15**

633 CTC BLVD - PUD

CTC FILING NO. 2 FINAL PUD
 LOTS 3, 4, 5 & 16
 633 CTC BLVD. 7 of 15

DRAWING NUMBER

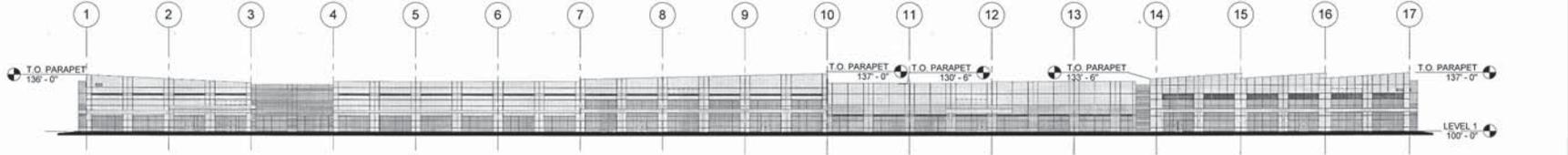
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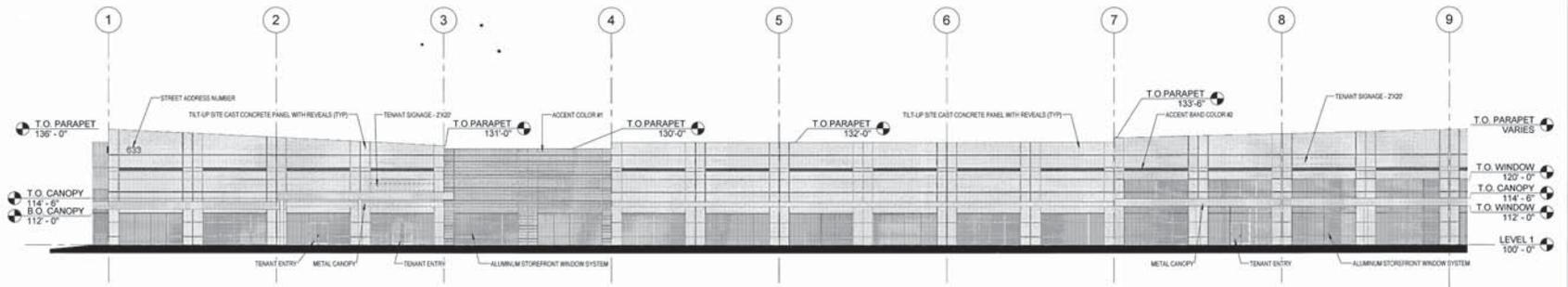
COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT



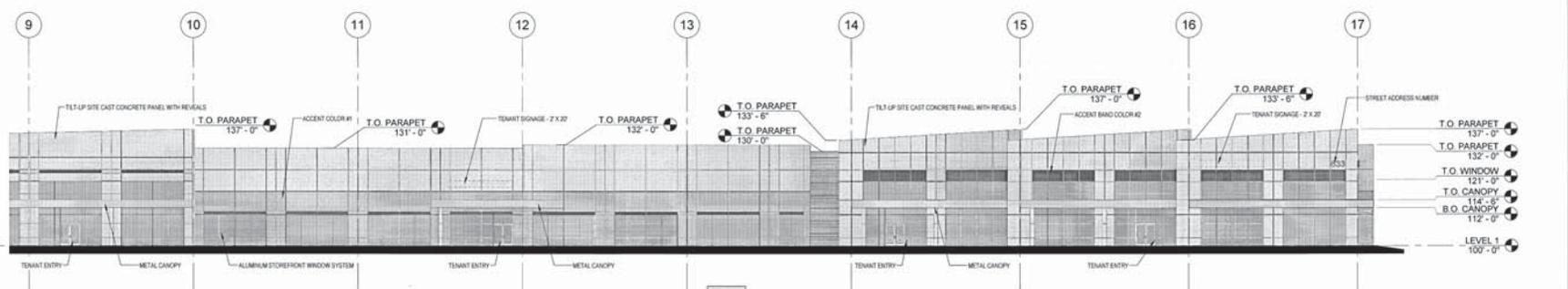
LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
 RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN, CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



1 EAST ELEVATION
 1/32" = 1'-0"



2 PARTIAL ENLARGED EAST ELEVATION
 1/16" = 1'-0"



3 PARTIAL ENLARGED EAST ELEVATION
 1/16" = 1'-0"

- FIELD COLOR - SHERWIN WILLIAMS - SW 9147 PANDA WHITE
- ACCENT COLOR 1 - SHERWIN WILLIAMS - SW 6703 UNUSUAL KUMAY
- ACCENT COLOR 2 - SHERWIN WILLIAMS - SW 10371 ROGATTA
- METAL PANEL

EAST ELEVATION

03/04/2016
 SHEET 7 OF 15

633 CTC BLVD - PUD

CTC FILING NO. 2 FINAL PUD
 LOTS 3, 4, 5 & 16
 633 CTC BLVD. 8 OF 15
 PHOTOGRAPHY BY MICHAEL UNIVERSITY

DRAWING NUMBER

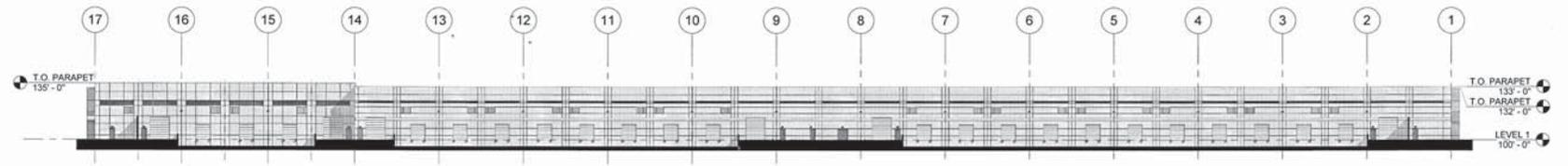
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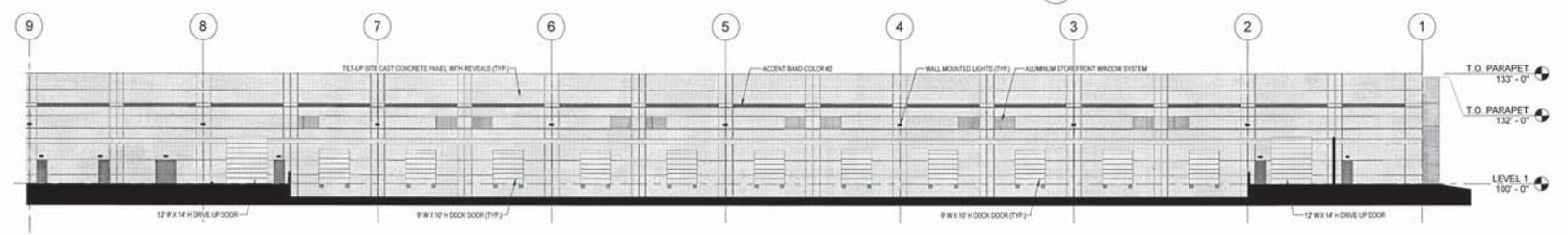
COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT



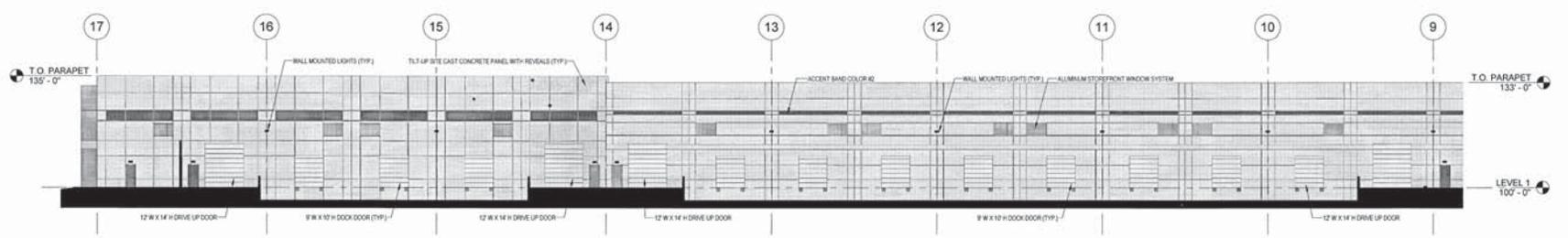
LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
 RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN, CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



3 OVERALL WEST ELEVATION
 1/32" = 1'-0"



2 PARTIAL ENLARGED WEST ELEVATION
 1/16" = 1'-0"



1 PARTIAL ENLARGED WEST ELEVATION
 1/16" = 1'-0"

- FIELD COLOR - SHERWIN WILLIAMS - SW 6147 PANDA WHITE
- ACCENT COLOR 1 - SHERWIN WILLIAMS - SW 6150 UNIVERSAL KINGS
- ACCENT COLOR 2 - SHERWIN WILLIAMS - SW 6151 BELLETTA
- METAL PANEL

WEST ELEVATION

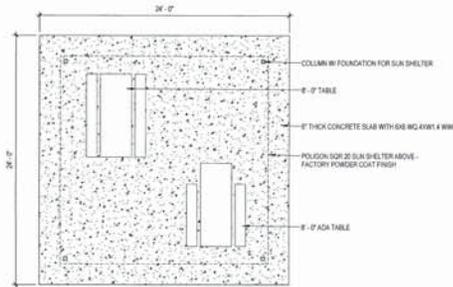
03/04/2016
 SHEET 8 OF 15

633 CTC BLVD - PUD

MOA 10/10/2016 AM

COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT

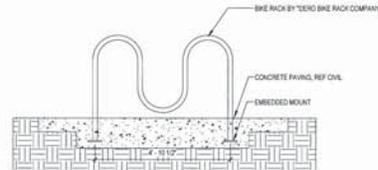
LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
 RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN. CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



9 EMPLOYEE BREAK AREA
 3/16" x 1'-0"



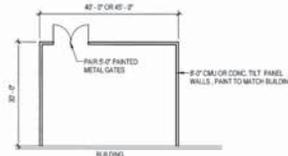
8 EMPLOYEE SUN SHELTER
 1/8" x 1'-0"



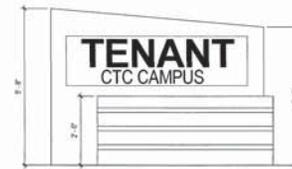
7 BIKE RACK
 1/2" x 1'-0"



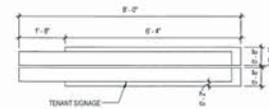
6 TYPICAL PARKING STALLS
 1/8" x 1'-0"



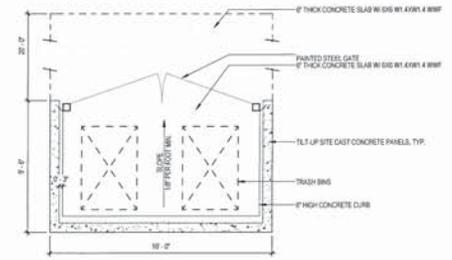
5 ALT. EQUIP. STORAGE YARD PLAN
 1/16" x 1'-0"



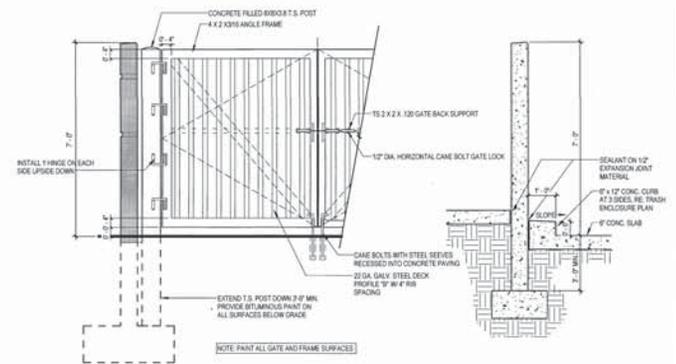
4 MONUMENT SIGN ELEVATION
 1/2" x 1'-0"



3 MONUMENT SIGN PLAN
 1/2" x 1'-0"



2 TRASH ENCLOSURE PLAN
 1/4" x 1'-0"



1 TRASH ENCLOSURE GATE ELEVATION & SECTION
 1/2" x 1'-0"

ARCHITECTURAL
 SITE DETAILS
 03/04/2016
 SHEET 9 OF 15

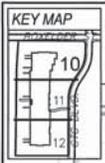
633 CTC BLVD - PUD

CTC FILING No. 2 FINAL PUD
 Lots 3, 4, 5 & 16
 633 CTC Blvd 10 of 15

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER



COLORADO TECHNOLOGY CENTER FILING NO. 2

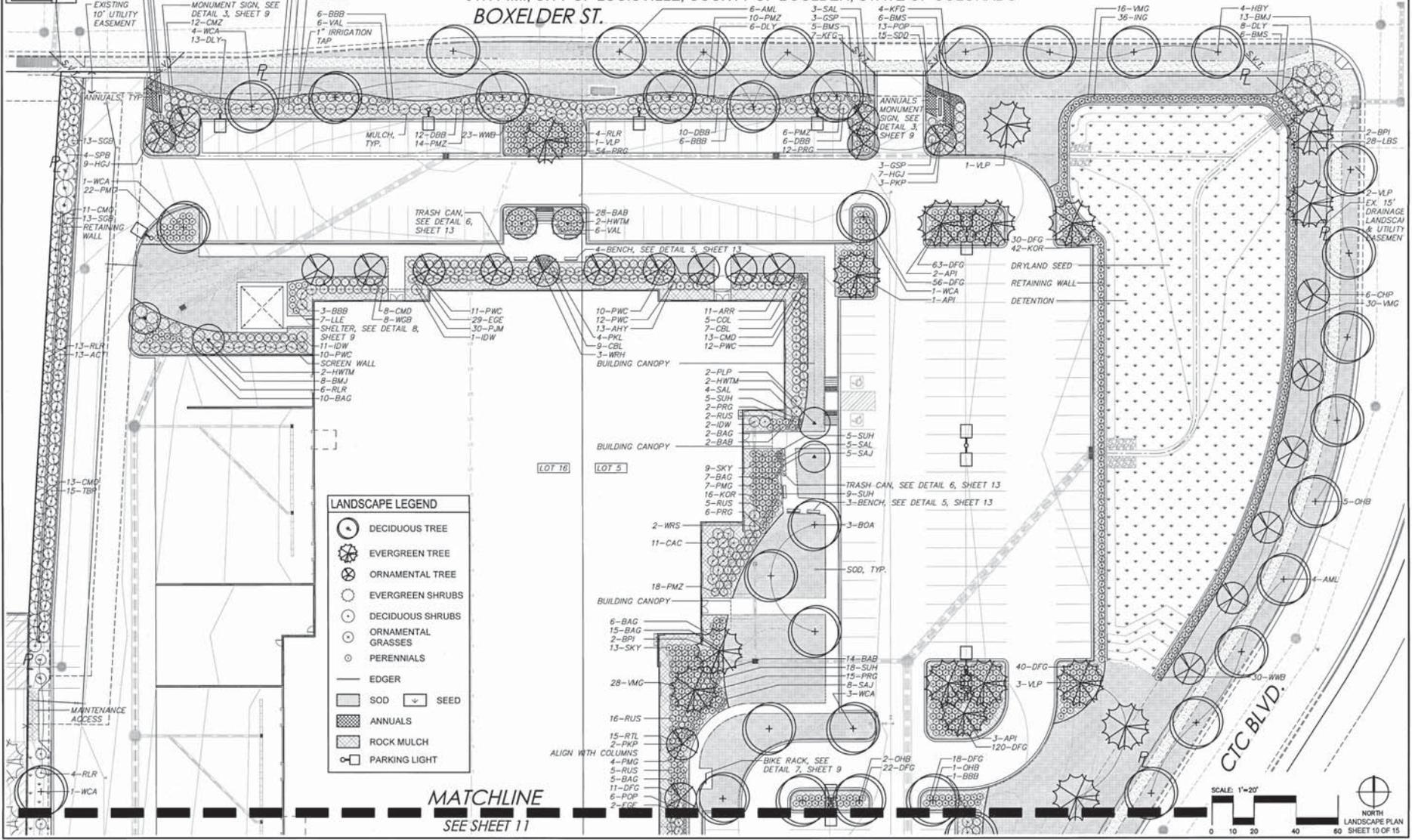
LOTS 3, 4, 5 & 16 - 633 CTC BLVD.

FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SE QUARTER OF SECTION 16, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE
 6TH P.M., CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO
BOXELDER ST.

COLORADO TECHNOLOGICAL CENTER
 633 CTC BLVD.
 LANDSCAPE PLAN

SCALE: 1"=20'
 2500 Broadway Suite B
 Broomfield, CO 80024
 303.449.9105
 www.ctc-engineering.com
 CIVIL ENGINEERING
 PLANNING
 SURVEYING



LANDSCAPE LEGEND	
	DECIDUOUS TREE
	EVERGREEN TREE
	ORNAMENTAL TREE
	EVERGREEN SHRUBS
	DECIDUOUS SHRUBS
	ORNAMENTAL GRASSES
	PERENNIALS
	EDGER
	SOD
	SEED
	ANNUALS
	ROCK MULCH
	PARKING LIGHT

MATCHLINE
 SEE SHEET 11

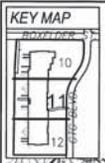


CTC FILING NO. 2 FINAL PUD
 Lots 3, 4, 5 & 16
 633 CTC Blvd 11 of 15

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER



COLORADO TECHNOLOGY CENTER FILING NO. 2

LOTS 3, 4, 5 & 16 - 633 CTC BLVD.

FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SE QUARTER OF SECTION 16, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH P.M., CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

COLORADO TECHNOLOGICAL CENTER
 633 CTC BLVD.
 LANDSCAPE PLAN

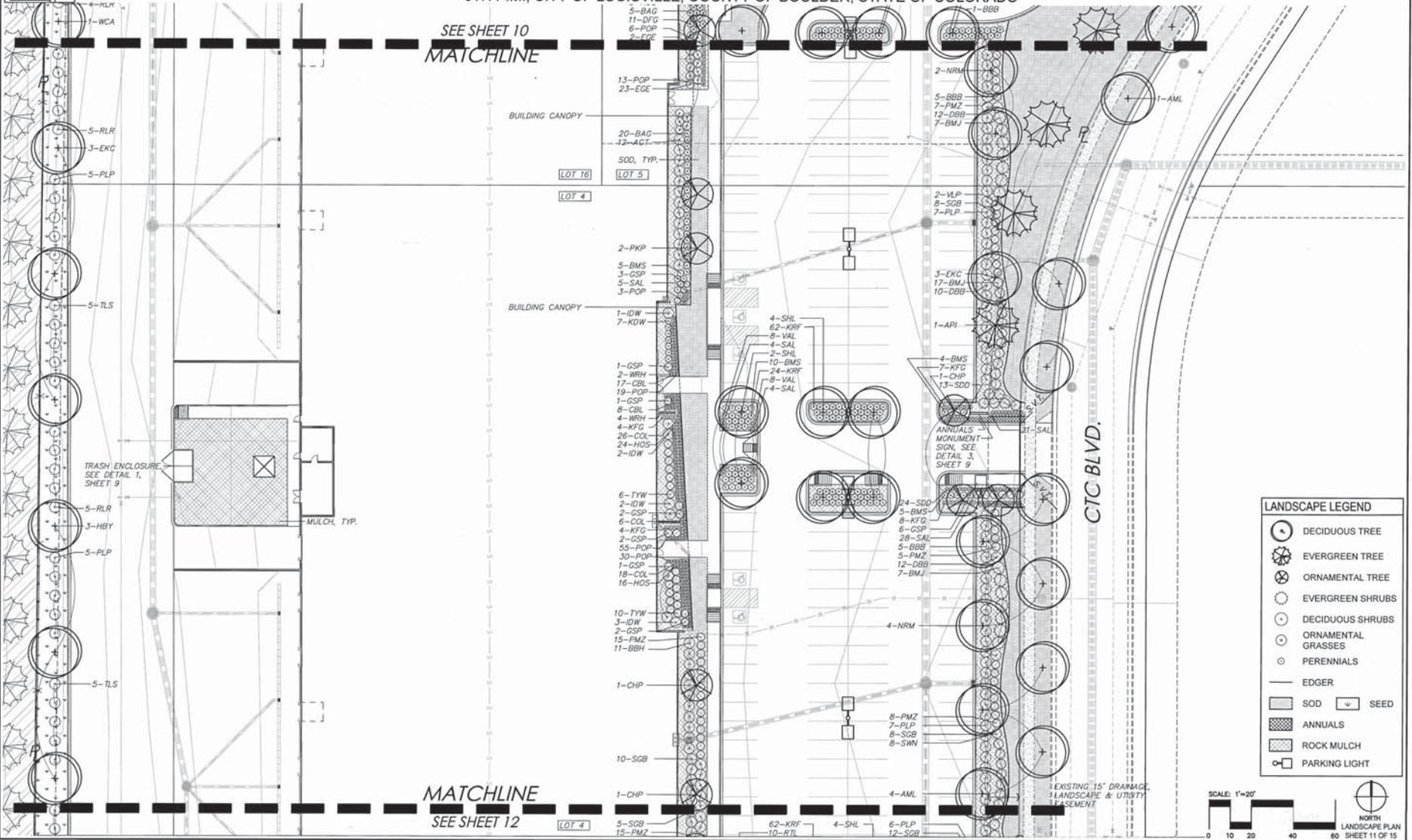
SCALE: 1"=20'
 SHEET 11 OF 15

DATE: 03/20/2010
 PROJECT: 03/20/2010

DESIGNER: **LOUISI**
 2500 Broadway Suite 8
 Boulder, CO 80304
 303.448.8900
 www.louislandscape.com

DATE: 03/20/2010
 PROJECT: 03/20/2010

DATE: 03/20/2010
 PROJECT: 03/20/2010



LANDSCAPE LEGEND

- DECIDUOUS TREE
- EVERGREEN TREE
- ORNAMENTAL TREE
- EVERGREEN SHRUBS
- DECIDUOUS SHRUBS
- ORNAMENTAL GRASSES
- PERENNIALS
- EDGER
- SOD
- SEED
- ANNUALS
- ROCK MULCH
- PARKING LIGHT



COLORADO TECHNOLOGICAL CENTER
633 CTC BLVD.
LANDSCAPE PLAN

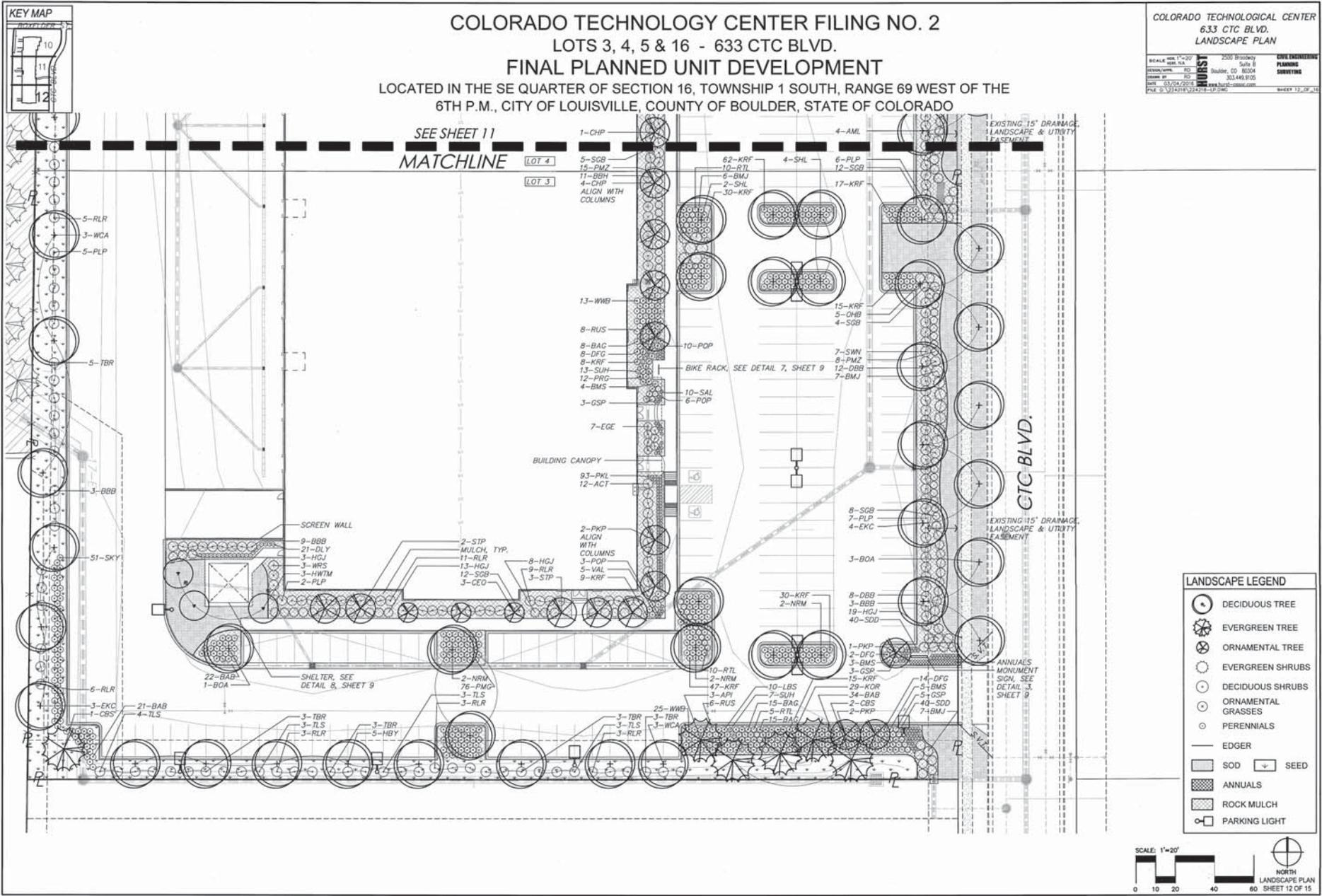
SCALE: 1"=20'
DATE: 11/11/10
PROJECT: 633 CTC BLVD. LOT 3, 4, 5 & 16
SHEET 13 OF 15

COLORADO TECHNOLOGY CENTER FILING NO. 2

LOTS 3, 4, 5 & 16 - 633 CTC BLVD.

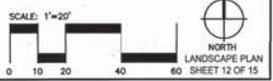
FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SE QUARTER OF SECTION 16, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE
6TH P.M., CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



LANDSCAPE LEGEND

- DECIDUOUS TREE
- EVERGREEN TREE
- ORNAMENTAL TREE
- EVERGREEN SHRUBS
- DECIDUOUS SHRUBS
- ORNAMENTAL GRASSES
- PERENNIALS
- EDGER
- SOD
- SEED
- ANNUALS
- ROCK MULCH
- PARKING LIGHT



CTC FILING NO. 2 FINAL PUD
Lots 3, 4, 5 & 16
633 CTC Blvd 12 of 15

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER

CTC FILING NO. 2 FINAL PUD
 LOTS 3, 4, 5 & 16
 633 CTC BLVD 13 of 15

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER

COLORADO TECHNOLOGICAL CENTER FILING NO. 2

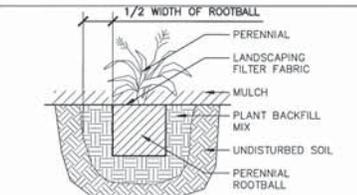
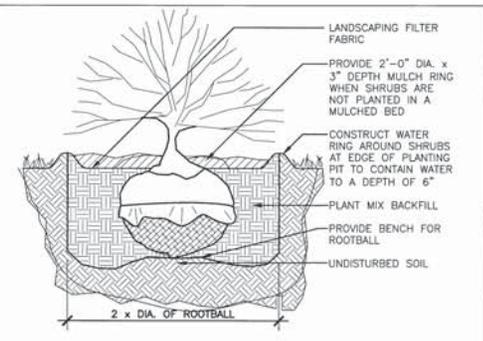
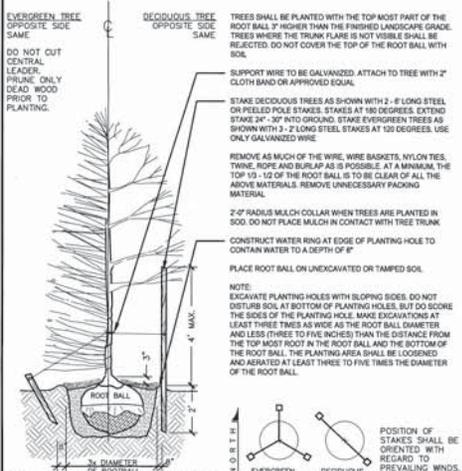
LOTS 3, 4, 5 & 16 - 633 CTC BLVD.

FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SE QUARTER OF SECTION 16, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE
 6TH P.M., CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

COLORADO TECHNOLOGICAL CENTER
 6.33 CTC BLVD
 LANDSCAPE DETAILS

SCALE: 1" = 2'-0" 2000 Broadway Suite 8 BOULDER, CO 80504
 PHONE: 303.449.9100
 FAX: 303.449.9100
 SHEET 14 OF 15



1 TREE PLANTING DETAIL

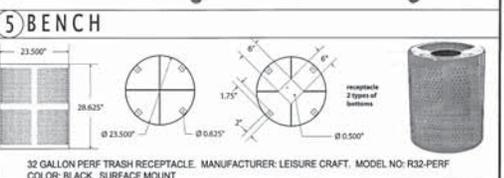
- ALL PLANT MATERIALS SHALL BE IN ACCORDANCE WITH AAN (American Association of Nurserymen) SPECIFICATIONS FOR NUMBER ONE GRADE.
- ALL TURF AREAS SHALL BE IRRIGATED WITH AN AUTOMATIC POP-UP IRRIGATION SYSTEM. ALL SHRUB BEDS AND TREES TO BE IRRIGATED WITH AN AUTOMATIC DRIP (FRICKLE) IRRIGATION SYSTEM, OR ACCEPTABLE ALTERNATIVE. THE IRRIGATION SYSTEM IS TO BE ADJUSTED TO MEET THE WATER REQUIREMENTS OF THE INDIVIDUAL PLANT MATERIAL. IN PARKWAY AREAS, IRRIGATION SPRAY HEADS SHALL BE SPACED AND ADJUSTED TO PREVENT IRRIGATION WATER FROM SPRAYING ON PUBLIC WALKS OR CURBS. STAFF WILL REQUEST A WALK THROUGH OF THE IRRIGATION SYSTEM WITHIN THE R.O.W. PRIOR TO ISSUANCE OF CONSTRUCTION ACCEPTANCE OR A CERTIFICATE OF OCCUPANCY. IRRIGATION PLANS TO BE SUBMITTED TO THE CITY FOR REVIEW AND APPROVAL PRIOR TO INSTALLATION OF THE SYSTEM. IRRIGATION DESIGN WILL BE DESIGN/BUILD BY OTHERS.
- ALL TREES TO BE BALLED & BURLAPPED, CONTAINERIZED, OR ROOT CONTROL BAGS.
- ALL SHRUB BEDS TO BE MULCHED WITH ROCK MULCH (3\"/>

2 SHRUB PLANTING DETAIL

DECIDUOUS TREES		
SYM	QTY COMMON NAME	BOTANICAL NAME
AHL	15 UNDEK, AMERICAN	<i>Ilex americana</i>
BOA	7 OAK BUR	<i>Quercus macrocarpa</i>
EKC	13 COFFEE TREE, ESPRESSO KENTUCKY	<i>Gymnocladia dioica</i> 'Espresso'
HYB	11 HACKBERRY	<i>Celtis occidentalis</i>
NRM	12 MAPLE, NORTHWOOD RED	<i>Acer rubrum</i> 'Northwood'
CHB	13 BUCKEYE, OHIO	<i>Aesculus glabra</i>
SLH	12 HONEYLOCUST, SHADEMASTER	<i>Gleditsia inornata</i> 'Shademaster'
WCA	16 CATALPA, WESTERN	<i>Catalpa speciosa</i>

4 EDGER

DECIDUOUS SHRUBS		
SYM	QTY COMMON NAME	BOTANICAL NAME
ACT	37 CURRANT, ALPINE	<i>Ribes alpinum</i>
AHY	13 HYDRANGEA, ANNABELLE	<i>Hydrangea arborescens</i> 'Annabelle'
B88	53 BUTTERFLY BUSH, BLACK KNIGHT	<i>Buddleia davidii</i> 'Black Knight'
B8H	22 BUTTERFLY BUSH, HARLEQUIN	<i>Buddleia davidii</i> 'Harlequin'
BMS	40 SPIREA, BLUE MIST	<i>Corylopsis x cladonensis</i> 'Blue Mist'
CAC	11 CRANBERRYBUSH, COMPACT AMERICAN	<i>Viburnum trilobum</i> 'Baller Compact'
CAD	25 DAPHNE, CAROL HACKIE	<i>Daphne x burkwoodii</i> 'Carol Hackie'
CMO	24 MOCKORANGE, CHEYENNE	<i>Philadelphus lewisii</i> 'Cheyenne'
D88	92 BURNING BUSH, DWARF	<i>Euonymus alatus</i> 'Compacta'
IDW	22 DOGWOOD, GANZI	<i>Cornus sericea</i> 'Ivanti'
KDW	7 DOGWOOD, KEILEY	<i>Cornus sericea</i> 'Kelsey'
PJM	30 RHODODENDRON, PJM	<i>Rhododendron</i> 'PJM'
PLP	38 PLUM, PURPLE LEAF	<i>Prunus cistena</i>
RPI	69 ROSE, RED LEAFED	<i>Rosa glauca</i>
RUS	33 SAGE, RUSSIAN	<i>Perovskia atriplicifolia</i>
SGB	93 BROOM, SPANISH GOLD	<i>Cytisus pungens</i> 'Spanish Gold'
SPB	4 BUTTERFLY BUSH, SPRING BLOOMING	<i>Buddleia alternifolia</i>
SWN	15 NINEBARK, SUMMER WINE	<i>Physocarpus opulifolius</i> 'Summer Wine'
TBR	32 RABBITBRUSH, TALL BLUE	<i>Cyniophthalmus nauseosus</i> 'abcauls'
TLS	23 SUMAC, THREE LEAF	<i>Rhus trilobata</i>
WRS	5 ROSE, WOODS	<i>Rosa woodii</i>



6 TRASH CAN

PERENNIALS		
SYM	QTY COMMON NAME	BOTANICAL NAME
CBL	51 CORAL BELLS	<i>Heuchera sanguinea</i>
COL	75 COLUMBINE, ROCKY MOUNTAIN	<i>Aquilegia scopulorum</i>
DLY	51 DAYLILY, SPP.	<i>Heimerocallis</i> spp.
HOS	40 HOSTA, SPP.	<i>Hosta</i> spp.
PKJ	97 PINKWINKLE	<i>Vinca minor</i>
POP	158 MALLOW, POPPY	<i>Callitriche involucrata</i>
SAL	13 SEDUM AUTUMN JOY	<i>Sedum spectabile</i> 'Autumn Joy'
SAL	86 SALVIA, MAY NIGHT	<i>Salvia nemorosa</i> 'May Night'
SDD	132 DAYLILY, STELLA D'ORO	<i>Heimerocallis</i> x 'Stella d'Oro'
SHJ	57 HYSSOP, SUNSET	<i>Agastache rupestris</i>
VAL	33 VALERIAN, RED	<i>Centranthus ruber</i>
WRH	25 HOSTA, WHITE RIMMED	<i>Hosta x undulata</i> 'Albomarginata'

5 LANDSCAPE NOTES

- PRIOR TO INSTALLATION OF SEED, PLANT MATERIAL & SOD, CONTRACTOR TO THOROUGHLY LOOSEN ALL AREAS THAT HAVE BEEN COMPACTED OR DISTURBED BY CONSTRUCTION. CONTRACTOR TO THOROUGHLY INCORPORATE SIX (6) CUBIC YARDS OF COMPOST OR COMPOSTED WEE FREE MANURE PER 1,000 SQUARE FEET TO BED AND SOD AREAS. AREAS TO BE SEED TO RECEIVE FOUR (4) CUBIC YARDS OF AMENDMENT, ORGANIC MATERIAL TO FOLLOW FOLLOWING CHARACTERISTICS:
 ORGANIC MATTER: 25% OR GREATER
 SALT CONTENT: 3.0 mhos/cm MAX.
 pH: 8.5 MAXIMUM
 CARBON TO NITROGEN RATIO: 10:1 TO 25:1
 CONTRACTOR TO PROVIDE A NUTRIENT TEST FROM THE LAST 3 MONTHS PRIOR TO SPREADING. MOUNTAIN PEAT, ASPEN HUMUS, GYPSUM AND SAND WILL NOT BE ACCEPTED.
- TURF GRASS WILL BE SODDED WITH TURF-TYPE TALL FESCUE.
- ALL SEEDED AREAS TO BE SEED WITH SEED MIX SPECIFIED. NO SUPPLEMENTAL IRRIGATION IS SPECIFIED. ANNUAL MOWING TO APPROXIMATELY 6\"/>

6 LANDSCAPE LEGEND

ORNAMENTAL TREES		
SYM	QTY COMMON NAME	BOTANICAL NAME
APP	11 REDBUD, APPALACHIAN RED	<i>Cercis canadensis</i> 'Appalachian Red'
CEO	3 OAK, COLUMBIAN ENGLISH	<i>Quercus robur</i> 'Fastigata'
CHP	13 PEAR, CHARNIER	<i>Pyrus calleryana</i> 'Charnier'
HWIM	12 MAPLE, HARTMAN 'HOT WINGS'	<i>Acer latatum</i> 'Car Ann'
PLP	12 PLUM, PRINCESS KAY	<i>Prunus nigra</i> 'Princess Kay'
STP	7 PEAR, STONEHILL	<i>Pyrus calleryana</i> 'Stonehill'

ORNAMENTAL GRASSES		
SYM	QTY COMMON NAME	BOTANICAL NAME
B48	120 BRASS BLUE GRAMA, BLONDE AMBITION	<i>Bouteloua gracilis</i> 'Blonde Ambition'
BAG	103 BLUE AVENA GRASS	<i>Helictotrichon sempervirens</i>
DFG	362 FOUNTAIN GRASS, DWARF	<i>Pennisetum alopecuroides</i> 'Dwarf'
ING	36 INDIAN GRASS	<i>Sorghastrum nutans</i>
RFC	41 FEATHER REED GRASS, KARL FOERSTER	<i>Calamagrostis x acutiflora</i> 'Karl Foerster'
KOR	87 FEATHER REED GRASS, KOREAN	<i>Calamagrostis brachytricha</i>
KRF	319 FOUNTAIN GRASS, KARLEY ROSE	<i>Pennisetum orientale</i> 'Karley Rose'
LBS	38 BLUESTEM, LITTLE	<i>Schizachyrium scoparium</i> 'The Blues'
PNG	110 MAIDEN GRASS, PURPLE	<i>Miscanthus sinensis</i> 'Purplepines'
PRG	101 RUBY GRASS, PINK CRYSTALS	<i>Melinis nervigata</i>
SKY	73 SKYRACER TALL PURPLE MOOR GRASS	<i>Molinia caerulea</i> 'grandinacea 'Skyracer'
VMG	144 MAIDEN GRASS, VARIEGATED	<i>Miscanthus sinensis</i> 'Variegated'
WWB	91 BLUESTEM, WINDWALKER BIG	<i>Andropogon gerardii</i> 'Windwalker'

5 LANDSCAPE NOTES

6 LANDSCAPE LEGEND

7 SEED MIX

SEED MIX		
SYM	QTY COMMON NAME	BOTANICAL NAME
WHE	WESTERN WHEATGRASS 'ARriba' OR 'NATIVE'	<i>Triticum aestivum</i>
SLN	SLENDER WHEATGRASS 'PRIMA' OR 'NATIVE'	<i>Lolium perenne</i>
THK	THICKSPIKE WHEATGRASS 'CRITANA' OR 'NATIVE'	<i>Lolium perenne</i>
NEE	NEEDLEGRASS 'LORDORIK' OR 'NATIVE'	<i>Lolium perenne</i>
SID	SIDEDOTS GRAMA 'VAUGHN' OR 'NATIVE'	<i>Lolium perenne</i>
LIT	LITTLE BLUESTEM 'PASTURA' OR 'NATIVE'	<i>Lolium perenne</i>
BLU	BLUE GRAMA 'LOVINGTON' OR 'NATIVE'	<i>Lolium perenne</i>
BUF	BUFFALOGRASS 'NATIVE'	<i>Lolium perenne</i>

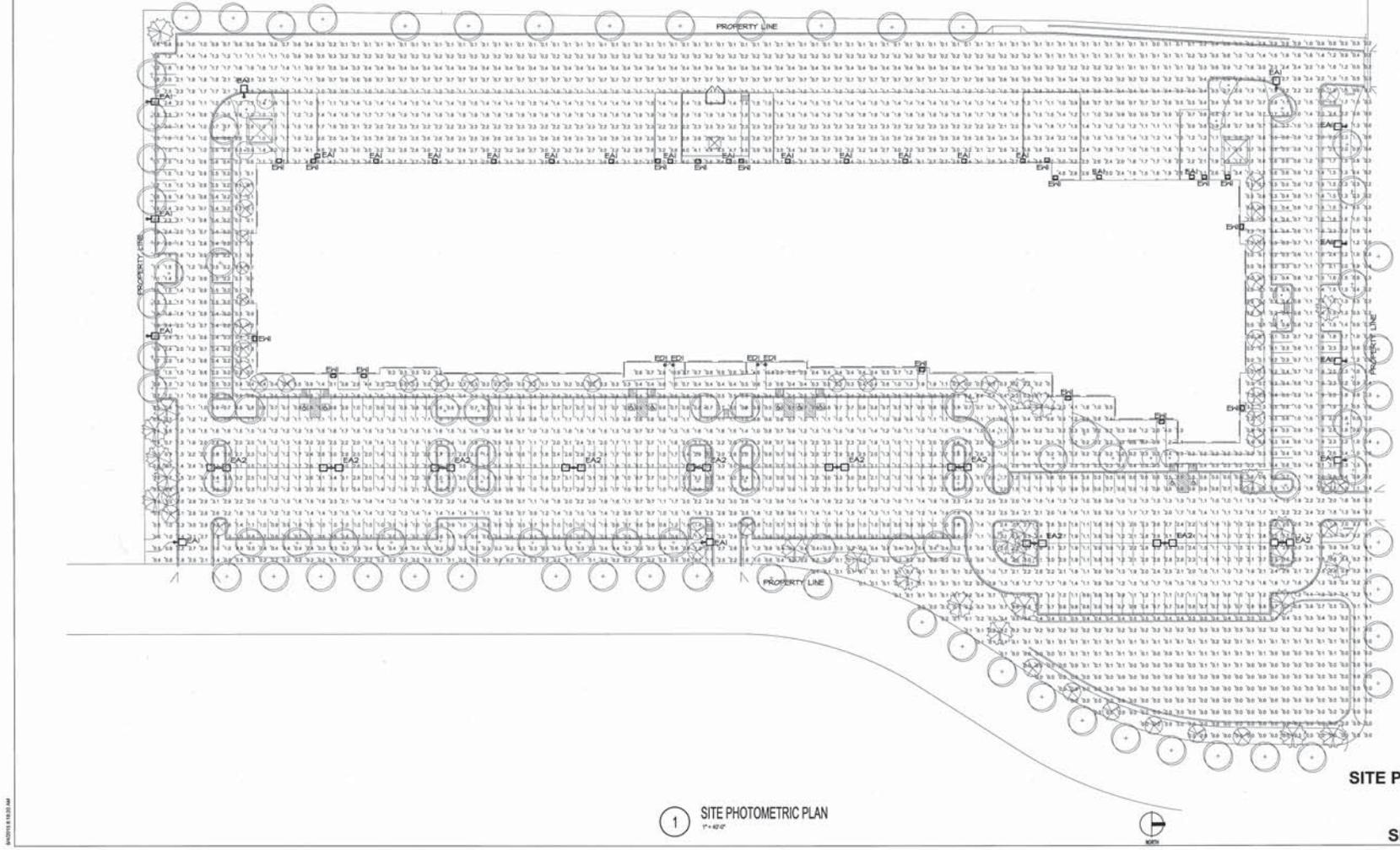
5 LANDSCAPE NOTES

6 LANDSCAPE LEGEND

7 SEED MIX

COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN. CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



1 SITE PHOTOMETRIC PLAN
1"=40'

**SITE PHOTOMETRIC
PLAN
03/04/2016
SHEET 14 OF 15**
633 CTC Blvd. - PUD

CTC FILING NO. 2 FINAL PUD
Lots 3, 4, 5 & 16
633 CTC Blvd 14 of 15

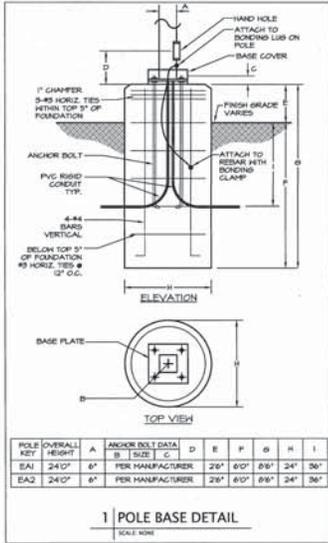
DRAWING NUMBER
PHOTOMETRIC PLAN - SITE PLAN - 14/15

DRAWING NUMBER
PHOTOMETRIC PLAN - SITE PLAN - 14/15

DRAWING NUMBER
PHOTOMETRIC PLAN - SITE PLAN - 14/15

COLORADO TECHNOLOGICAL CENTER FILING NO. 2, LOTS 3,4,5,& 16. 633 CTC BLVD. FINAL PLANNED UNIT DEVELOPMENT

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 16 TOWNSHIP 1 SOUTH,
RANGE 69 WEST, OF THE SIXTH PRINCIPAL MERIDIAN, CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



- GENERAL NOTES**
1. FIXTURE POLE HEIGHTS DO NOT EXCEED 24 FEET ON THE PROPERTY.
 2. ALL LIGHT FIXTURES TO BE CONTROLLED VIA ASTRONOMICAL TIMECLOCK WITH PHOTOCELL TO ALLOW LIGHTS TO BE TURNED "ON" ONLY DURING NON-DAYLIGHT HOURS.
 3. ALL LIGHT FIXTURES THAT ARE VISIBLE FROM ANY STREET RIGHT-OF-WAY OR ADJACENT PROPERTIES ARE FULL-CUTOFF AND SHIELDED TO REDUCE GLARE BEYOND THE PROPERTY LINE. ALL LAMPS DO NOT EXTEND BELOW THE HOUSING OF THE FIXTURE.

LIGHTING FIXTURE SCHEDULE

TYPE	DESCRIPTION	MANUFACTURER	CATALOG NUMBER	VOLTAGE	LAMP	FLUX	HEIGHT	LOCATION	INFORMATION	NOTES
EAI	POLE MOUNTED AREA FIXTURE LED FORWARD MEDIUM HIGH MEDIUM 12'-0" POLE HEIGHT	LITHONIA	D501 LED-400-7000-000-1000 (MULTI-SPARK-0000)	277V	1 30W LED	104	24'	POLE MOUNTED	24' POLE HEIGHT	
EAG	POLE MOUNTED AREA FIXTURE LED FORWARD MEDIUM HIGH MEDIUM 12'-0" POLE HEIGHT	LITHONIA	D501 LED-400-7000-000-1000 (MULTI-SPARK-0000)	277V	1 30W LED	104	24'	POLE MOUNTED	24' POLE HEIGHT	
EAI	POLE MOUNTED AREA FIXTURE LED FORWARD MEDIUM HIGH MEDIUM 12'-0" POLE HEIGHT	LITHONIA	D501 LED-400-7000-000-1000 (MULTI-SPARK-0000)	277V	1 30W LED	104	24'	POLE MOUNTED	24' POLE HEIGHT	
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EAI	POLE MOUNTED AREA FIXTURE LED FORWARD MEDIUM HIGH MEDIUM 12'-0" POLE HEIGHT	LITHONIA	D501 LED-400-7000-000-1000 (MULTI-SPARK-0000)	277V	1 30W LED	104	24'	POLE MOUNTED	24' POLE HEIGHT	
EAI	POLE MOUNTED AREA FIXTURE LED FORWARD MEDIUM HIGH MEDIUM 12'-0" POLE HEIGHT	LITHONIA	D501 LED-400-7000-000-1000 (MULTI-SPARK-0000)	277V	1 30W LED	104	24'	POLE MOUNTED	24' POLE HEIGHT	
EAI	POLE MOUNTED AREA FIXTURE LED FORWARD MEDIUM HIGH MEDIUM 12'-0" POLE HEIGHT	LITHONIA	D501 LED-400-7000-000-1000 (MULTI-SPARK-0000)	277V	1 30W LED	104	24'	POLE MOUNTED	24' POLE HEIGHT	

STATISTICS

DESCRIPTION	SYMBOL	AVG	MAX	MIN	AVG-MIN
PROPERTY LINE	+	0.0 FC	0.0 FC	0.0 FC	N/A
WHOLE SITE	+	11 FC	10.2 FC	0.0 FC	N/A

TYPE EDI

LITHONIA LIGHTING

FEATURES & SPECIFICATIONS

WST LED Architectural Wall Sconce

Specifications

Height	Width	Depth	Weight
1.5"	3.5"	1.5"	0.15 lbs
2.0"	4.0"	2.0"	0.20 lbs
2.5"	4.5"	2.5"	0.25 lbs
3.0"	5.0"	3.0"	0.30 lbs
3.5"	5.5"	3.5"	0.35 lbs
4.0"	6.0"	4.0"	0.40 lbs
4.5"	6.5"	4.5"	0.45 lbs
5.0"	7.0"	5.0"	0.50 lbs
5.5"	7.5"	5.5"	0.55 lbs
6.0"	8.0"	6.0"	0.60 lbs
6.5"	8.5"	6.5"	0.65 lbs
7.0"	9.0"	7.0"	0.70 lbs
7.5"	9.5"	7.5"	0.75 lbs
8.0"	10.0"	8.0"	0.80 lbs
8.5"	10.5"	8.5"	0.85 lbs
9.0"	11.0"	9.0"	0.90 lbs
9.5"	11.5"	9.5"	0.95 lbs
10.0"	12.0"	10.0"	1.00 lbs

TYPE EWI

WST LED Architectural Wall Sconce

Specifications

Height	Width	Depth	Weight
1.5"	3.5"	1.5"	0.15 lbs
2.0"	4.0"	2.0"	0.20 lbs
2.5"	4.5"	2.5"	0.25 lbs
3.0"	5.0"	3.0"	0.30 lbs
3.5"	5.5"	3.5"	0.35 lbs
4.0"	6.0"	4.0"	0.40 lbs
4.5"	6.5"	4.5"	0.45 lbs
5.0"	7.0"	5.0"	0.50 lbs
5.5"	7.5"	5.5"	0.55 lbs
6.0"	8.0"	6.0"	0.60 lbs
6.5"	8.5"	6.5"	0.65 lbs
7.0"	9.0"	7.0"	0.70 lbs
7.5"	9.5"	7.5"	0.75 lbs
8.0"	10.0"	8.0"	0.80 lbs
8.5"	10.5"	8.5"	0.85 lbs
9.0"	11.0"	9.0"	0.90 lbs
9.5"	11.5"	9.5"	0.95 lbs
10.0"	12.0"	10.0"	1.00 lbs

TYPE EAI/EAG

D-Series Size 1 LED Area Luminaire

Specifications

Height	Width	Depth	Weight
1.5"	3.5"	1.5"	0.15 lbs
2.0"	4.0"	2.0"	0.20 lbs
2.5"	4.5"	2.5"	0.25 lbs
3.0"	5.0"	3.0"	0.30 lbs
3.5"	5.5"	3.5"	0.35 lbs
4.0"	6.0"	4.0"	0.40 lbs
4.5"	6.5"	4.5"	0.45 lbs
5.0"	7.0"	5.0"	0.50 lbs
5.5"	7.5"	5.5"	0.55 lbs
6.0"	8.0"	6.0"	0.60 lbs
6.5"	8.5"	6.5"	0.65 lbs
7.0"	9.0"	7.0"	0.70 lbs
7.5"	9.5"	7.5"	0.75 lbs
8.0"	10.0"	8.0"	0.80 lbs
8.5"	10.5"	8.5"	0.85 lbs
9.0"	11.0"	9.0"	0.90 lbs
9.5"	11.5"	9.5"	0.95 lbs
10.0"	12.0"	10.0"	1.00 lbs

TYPE EA3

D-Series Size 2 LED Wall Luminaire

Specifications

Height	Width	Depth	Weight
1.5"	3.5"	1.5"	0.15 lbs
2.0"	4.0"	2.0"	0.20 lbs
2.5"	4.5"	2.5"	0.25 lbs
3.0"	5.0"	3.0"	0.30 lbs
3.5"	5.5"	3.5"	0.35 lbs
4.0"	6.0"	4.0"	0.40 lbs
4.5"	6.5"	4.5"	0.45 lbs
5.0"	7.0"	5.0"	0.50 lbs
5.5"	7.5"	5.5"	0.55 lbs
6.0"	8.0"	6.0"	0.60 lbs
6.5"	8.5"	6.5"	0.65 lbs
7.0"	9.0"	7.0"	0.70 lbs
7.5"	9.5"	7.5"	0.75 lbs
8.0"	10.0"	8.0"	0.80 lbs
8.5"	10.5"	8.5"	0.85 lbs
9.0"	11.0"	9.0"	0.90 lbs
9.5"	11.5"	9.5"	0.95 lbs
10.0"	12.0"	10.0"	1.00 lbs

CTC FILING NO. 2 FINAL PUD
Lots 3, 4, 5, & 16
633 CTC Blvd. 15 of 15

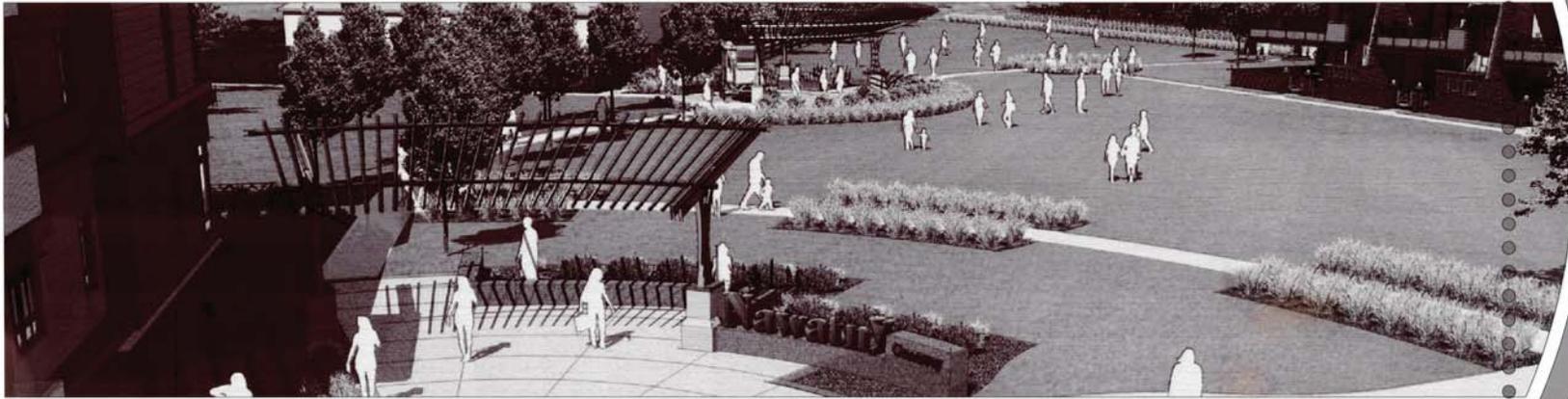
DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER

Final Planned Unit Development Phase 2

A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado



SHEET INDEX

- 1.....Cover
- 2.....Project Introduction
- 3.....Masterplan & General Notes
- 4-6.....MUD Plan, Notes & Standards
- 7.....Horizontal Public Improvements Plan

- 8.....General Landscape Plan
- 9.....Detailed Landscape Plan
- 10.....Emergency Vehicle Access Plan
- 11.....Turning Movement Details
- 12-13..Horizontal Control Plan
- 14-15..Overall Utility Plan

- 16-17....Overall Grading Plan
- 18-28.... Architectural Elevations
- 29.....Street Sections
- 30-31....Site Details
- 32-33....Site Photometric Plan
- 34.....Character Sketches



<p>owner's rep</p> <p>RMCS 21 South Summit Street Longmont, CO 80503 Phone: 720.524.3620</p>	<p>planning & ia</p> <p>people creating spaces</p> <p>pcs group, inc. 1001 16th street, 3b-180 Denver, Co 80205 tel: (303) 531-4905 www.pcsgrouppco.com</p>	<p>engineering</p> <p>ENGINEERING CONSULTANTS Contact: Jason D. Margraf, PE 11101 W. 120th Avenue, Ste 240 Broomfield CO 80021 phone: (720) 975-0177 fax: (720) 460-4906</p>	<p>architecture</p> <p>ORIGURE architecture 3003 Larimer Street Denver, CO 80203 Phone: (303) 861-0704 Fax: (303) 861-9230 www.orichure.com</p> <p>kga studio 950 Spruce Street, Ste 30 Louisville, CO 80027 ph: (303) 442-5882 c: (303) 442-5888 www.kgaarch.com</p>	<p>electrical</p> <p>GIVEN 735 S. Xenon Court, Ste. 201 Lakewood, CO 80228 Phone: (303) 716-1270 Fax: (303) 716-1272</p>
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CITY COUNCIL SIGNATURE BLOCK

APPROVED THIS 17 DAY OF MARCH, 2019, BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO.
MAYOR [Signature] CITY CLERK [Signature] Pgs. 14, SERIAL# 2019

PLANNING COMMISSION CERTIFICATION

RECOMMENDED APPROVAL THIS 15 DAY OF February, 2019, BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO. RESOLUTION NO. 2, SERIES 2119
N/A
CHAIRMAN

CLERK & RECORDER CERTIFICATE - COUNTY OF BOULDER, STATE OF COLORADO

I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT _____ O'CLOCK, _____ M., THIS 20 DAY OF September, OF 2019, AND IS RECORDED IN PLAN FILE _____, FILE _____, RECEPTION _____, PAID _____, FILM NO. 2019-12-22-7.

RECORD _____ DEPUTY _____

OWNERSHIP SIGNATURE BLOCK

BY SIGNING THIS PDP/PUD, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS PDP/PUD. WITNESS OUR HANDS AND SEALS THIS 15 DAY OF September, 2019.

OWNER - TACODA PROPERTIES, INC. A COLORADO CORPORATION,
JUSTIN MCCLURE, VICE PRESIDENT

NOTARY [Signature]

ALLIANCE 1 COLLEGE
SECRETARY, STATE OF COLORADO
NOTARY PUBLIC, COUNTY OF BOULDER
(NOTARY SEAL)

Final Planned Unit Development

louisville, colorado

de lo
DowntownEastLouisville

DELO 2 PUD
1 OF 34

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER

de lo Final Planned Unit Development Phase 2

Downtown East Louisville

A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado

LEGAL DESCRIPTION DELO SUBDIVISION

DOWNTOWN EAST LOUISVILLE (DELO)
A TRACT OF LAND LOCATED IN THE NORTHEAST ¼ OF SECTION 8, TOWNSHIP 1 SOUTH, RANGE 69 WEST OF THE 6TH P.M., DESCRIBED AS FOLLOWS:
BEGINNING AT THE EAST ¼ CORNER OF SAID SECTION 8;
THENCE NORTH 89°14'04" WEST A DISTANCE OF 517.74 FEET ALONG THE SOUTH LINE OF SAID NORTHEAST ¼ OF SAID SECTION 8;
THENCE NORTH 00°18'26" EAST, 30.11 FEET TO THE TRUE POINT OF BEGINNING;
THENCE NORTH 00°12'32" EAST A DISTANCE OF 342.06 FEET;
THENCE ALONG A CURVE TO THE RIGHT WITH A RADIUS OF 330.00 FEET A LENGTH OF 50.81 FEET AND THE CHORD BEARS NORTH 04°27'27" EAST A DISTANCE OF 50.34 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY LINE OF SHORT STREET;
THENCE SOUTH 89°07'00" EAST ALONG THE NORTH RIGHT-OF-WAY LINE OF SHORT STREET A DISTANCE OF 384.17 FEET TO A POINT ON THE EAST RIGHT-OF-WAY LINE OF EAST LAFAYETTE STREET;
THENCE NORTH 39°59'07" WEST ALONG SAID EAST RIGHT-OF-WAY LINE OF EAST LAFAYETTE STREET A DISTANCE OF 422.70 FEET;
THENCE NORTH 34°19'27" WEST ALONG SAID EAST RIGHT-OF-WAY LINE OF EAST LAFAYETTE STREET A DISTANCE OF 88.70 FEET TO THE SOUTHEAST CORNER OF LOT A, BLOCK B OF THE INDUSTRIAL AREA SUBDIVISION;
THENCE NORTH 90°07'00" WEST A DISTANCE OF 5.00 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF CANNON STREET;
THENCE NORTH 00°07'00" EAST ALONG THE WEST RIGHT-OF-WAY LINE OF CANNON STREET A DISTANCE OF 288.13 FEET;
THENCE ALONG A CURVE TO THE LEFT WITH A RADIUS OF 15.00 FEET A LENGTH OF 23.57 FEET AND THE CHORD BEARS NORTH 43°07'41" WEST A DISTANCE OF 21.22 FEET TO A POINT ON THE SOUTH RIGHT-OF-WAY LINE OF GRIFFITH STREET;
THENCE SOUTH 89°08'30" WEST ALONG THE SOUTH RIGHT-OF-WAY LINE OF GRIFFITH STREET A DISTANCE OF 423.30 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF NORTH FRONT STREET;
THENCE SOUTH 87°10'07" EAST ALONG THE WEST RIGHT-OF-WAY LINE OF NORTH FRONT STREET A DISTANCE OF 452.92 FEET;
THENCE NORTH 87°02'17" EAST ALONG THE SOUTH RIGHT-OF-WAY LINE OF NORTH FRONT STREET A DISTANCE OF 40.00 FEET TO A POINT ON THE SOUTH RIGHT-OF-WAY LINE OF EAST LAFAYETTE STREET;
THENCE SOUTH 02°00'00" EAST ALONG THE SOUTH RIGHT-OF-WAY LINE OF EAST LAFAYETTE STREET A DISTANCE OF 353.30 FEET;
THENCE SOUTH 00°07'00" WEST, A DISTANCE OF 29.68 FEET TO THE SOUTHEAST CORNER OF LOT 1, BLOCK C OF THE INDUSTRIAL AREA SUBDIVISION;
THENCE SOUTH 89°07'00" WEST ALONG THE SOUTH LINE OF SAID LOT 1, BLOCK C OF THE INDUSTRIAL AREA SUBDIVISION A DISTANCE OF 283.22 FEET TO A POINT ON THE EASTERLY RIGHT-OF-WAY LINE OF THE COLORADO AND SOUTHERN RAILROADS;
THENCE SOUTH 87°10'07" EAST ALONG SAID EASTERLY RIGHT-OF-WAY LINE OF THE COLORADO AND SOUTHERN RAILROADS A DISTANCE OF 723.88 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY LINE OF SOUTH STREET;
THENCE SOUTH 89°14'04" EAST ALONG SAID NORTH RIGHT-OF-WAY LINE OF SOUTH STREET A DISTANCE OF 163.60 FEET TO THE TRUE POINT OF BEGINNING.
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO.
AREA = 10.69 ACRES

NOTES

1. SURVEY BY ROCK CREEK SURVEYING, DATED NOVEMBER 27, 2012.
2. PROPOSED VEHICULAR CONNECTIONS MAY BE REFINED WITH FUTURE CONSTRUCTION PLAN SUBMITTALS.
3. TIMING AND PHASING OF DEVELOPMENT SHALL BE DEPENDENT UPON MARKET CONDITIONS AND LOCATION OF UTILITIES.

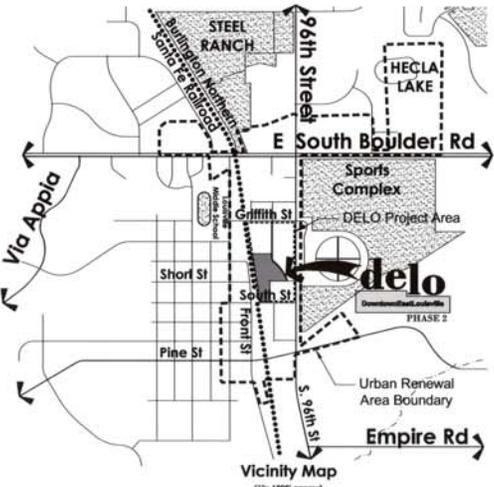
PROJECT DESCRIPTION

PLATED AREA:	110.79 AC.
TOTAL GROSS PROJECT AREA:	114.18 AC.
EXISTING PHASE 1:	3.79 AC.
PHASE 2:	7.38 AC.
MGR:	3.30 AC.
CANNON ST. ROW:	0.14 AC.
E. LAFAYETTE LANE (TRACT E):	0.45 AC.
SOUTH ST. ROW:	0.13 AC.
SHORT ST. ROW:	0.36 AC.
CONCRETE PARCEL:	0.29 AC.
EXCESS DEDICATED PUBLIC LAND PROVIDED:	0.97 AC. (8.5% OF GROSS PROJECT AREA)
OUTLOT A:	0.22 AC.
OUTLOT B:	0.56 AC.
OUTLOT C:	0.30 AC.
OUTLOT F:	0.49 AC.
(FROM INDUSTRIAL AREA SUBDIVISION REPLAT NO. 1)	
CURRENT ZONING:	MIXED USE RESIDENTIAL (MU R)
PROPOSED ZONING:	MIXED USE RESIDENTIAL (MU R)
MAX. NUMBER OF APPROVED RESIDENTIAL UNITS:	190 DU (PHASES 1, 1A & 2)
PHASE 2 ACCESS:	CANNON STREET (VIA GRIFFITH STREET & SOUTH STREET) NORTH FRONT STREET (VIA GRIFFITH STREET) SHORT STREET (VIA STATE HIGHWAY 42)

LAND USE SUMMARY - INDUSTRIAL AREA SUBDIVISION REPLAT NO. 1

TRACT	TOTAL AREA (ACRES)	OWNERSHIP	MAINTENANCE ¹	USE
Tract A	0.22	Takoda Properties Inc.	Takoda Properties Inc.	Private Roadway, Public Access Easement, Exclusive City Utility Easement, Private Utility Easements and Dry Utility Easement
Tract B	0.01	Takoda Properties Inc.	Takoda Properties Inc.	Leased Lane (Private), Emergency and Public Access and Exclusive City Utility Easements
Lot 1	0.29	Concast Cabelado of Colorado II, LLC	Concast Cabelado of Colorado II, LLC	Private Lot for Existing Concast Building and Private Parking Lot
Outlot F	0.49	City of Louisville	City of Louisville / DELO ¹ Commercial H.O.A.	Necessary Greenway, Pedestrian Access, Exclusive City Utility Easement, Concast Private Water and Sanitary Sewer Service Line Easements and Private Storm Sewer Easement

¹ Refer to Subdivision Agreement for delineation of maintenance responsibilities.



LAND USE SUMMARY

TRACT	TOTAL AREA (ACRES)	OWNERSHIP	MAINTENANCE ¹	PRIMARY USES
TRACT A	2.38	DELO Commercial H.O.A.	DELO Commercial H.O.A.	Private Roadway, Pedestrian Access, Emergency and Public Access, Parking, Drainage, Exclusive City Utility Easements and Dry Utility Easements
B	0.45	DELO Commercial H.O.A.	DELO Commercial H.O.A.	Private Roadway, Pedestrian Access, Emergency and Public Access, Lot 7 Industrial Area Subdivision Access, Private Parking, Drainage, Exclusive City Utility Easements and Dry Utility Easements
C	0.09	DELO Commercial H.O.A.	DELO Commercial H.O.A.	Private Roadway, Pedestrian Access, Parking, Drainage, Wet and Dry Utility Easements, Miscellaneous
D	0.03	DELO Commercial H.O.A.	DELO Commercial H.O.A.	Pedestrian Access, Drainage, and Private Utility Easement
E	0.04	DELO Commercial H.O.A.	DELO Commercial H.O.A.	Pedestrian Access, Drainage, Private Utility, and Emergency and Public Access
F	0.15	DELO Townhome H.O.A.	DELO Townhome H.O.A.	Leased Lane (Private), Emergency and Public Access and Exclusive City Utility Easements
G	0.45	DELO Townhome H.O.A.	DELO Townhome H.O.A.	Private Roadway, Public Pedestrian Access, Exclusive City Utility Easements and Dry Utility Easements
OUTLOT				
A	0.22	City of Louisville	City of Louisville / DELO ¹ Commercial H.O.A.	Concast Private, Pedestrian Access, Public Events and Private Utility Easements
B	0.08	City of Louisville	City of Louisville / DELO ¹ Commercial H.O.A.	Necessary Greenway, Pedestrian Access, Exclusive City Utility Easement and Dry Utility Easements
C	0.20	City of Louisville	City of Louisville / DELO ¹ Commercial H.O.A.	Necessary Greenway, Pedestrian Access, Exclusive City Utility Easement and Private Utility Easements

¹ Refer to Subdivision Agreement for delineation of maintenance responsibilities.

PREVIOUSLY PLATTED EAST LAFAYETTE STREET CITY OF LOUISVILLE RIGHT-OF-WAY

TRACT	TOTAL AREA (ACRES)	OWNERSHIP	MAINTENANCE	USE
Necessary Greenway (ROW)	0.42	City of Louisville	City of Louisville	Public Park, Pedestrian Access, Drainage and Wet and Dry Utilities
Tract B	0.45	DELO Commercial H.O.A.	DELO Commercial H.O.A.	Private Roadway, Pedestrian Access, Emergency and Public Access, Lot 7 Industrial Area Subdivision Access, Private Parking, Drainage, Exclusive City Utility Easements and Dry Utility Easements
Portion of Tract F	0.04	DELO Townhome H.O.A.	DELO Townhome H.O.A.	Leased Lane (Private), Emergency and Public Access and Exclusive City Utility Easements
Portion of Block 9	0.08	Takoda Properties Inc.	Takoda Properties Inc.	Block 9

PROPOSED CITY OF LOUISVILLE RIGHT-OF-WAY

ROW	TOTAL AREA (ACRES)	OWNERSHIP	MAINTENANCE	USE
Cannon Street	0.51	City of Louisville	City of Louisville / DELO ¹ Commercial H.O.A.	Public Roadway, Pedestrian Access, Drainage and Wet and Dry Utilities

¹ Refer to Subdivision Agreement for delineation of maintenance responsibilities.



SOUTH STREET PEDESTRIAN GATEWAY PERSPECTIVE



sheet title
Project Introduction

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

planning & architecture
people creating spaces
PCJ GROUP, INC.
1001 16th Street, 38-180
Denver, CO 80265
tel (303) 531-4905
www.pcjgroupco.com

owner's rep
RMCS
21 South Summit Street
Longmont, CO 80503
Phone 720.524.3620

designed by: SOS drawn by: SCD
checked by: JDM project #: 030017

engineering CONSULTANTS
Contact: Jason D. Margraf, PE
11101 W. 120th Ave, Ste 240
Broomfield, CO 80021
ph:(303) 368-5601
fx: (303) 368-5603

architecture
3003 Larimer Street
Denver, CO 80205
ph:(303) 861-5704
WWW.OZarch.com

de lo Final Planned Unit Development Phase 2

Downtown East Louisville

A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado

PRODUCT TYPE KEYMAP (1" = 200')



- PHASE 1 - RELOCATED HISTORIC OFFICE / COMMERCIAL
- PHASE 1 - MULTI-UNIT DWELLING (TOWNHOMES AND MEWS)
- PHASE 2 - MULTI-UNIT DWELLING (TOWNHOMES)
- PHASE 2 - CIVIC (NAWATNY GREENWAY)
- PHASE 2 - MULTI-UNIT DWELLING (APARTMENTS / CONDOMINIUMS)
- PHASE 2 - CIVIC (CALEDONIA PLAZA)
- PHASE 2 - CIVIC (CANNON ST. WOONER)
- PHASE 2 - OFFICE, COMMERCIAL AND EATING & DRINKING CIVIC (SOUTH STREET PEDESTRIAN PLAZA)

GENERAL NOTES AND STANDARDS

1. DELO IS A MASTER PLANNED DEVELOPMENT AND CONSISTS OF MULTIPLE PHASES. THE SECOND PHASE (PHASE 2) IS COMPOSED OF THREE SEPARATE AND DISTINCT PRODUCT TYPES: MULTI-UNIT DWELLING TOWNHOMES, MULTI-UNIT DWELLING APARTMENTS, CONDOMINIUMS, COMMERCIAL OFFICE SPACE, AND EATING & DRINKING ESTABLISHMENTS.
2. ANY AND ALL TRACTS, AS DEPICTED ON SHEETS 4, 5 & 6 AND GENERATED VIA THE DELO SUBDIVISION - RE-PLAT NO. 1, SHALL BE MAINTAINED BY THE HOA. THE DEVELOPER MAY CONDUCT ACTIVITIES INCLUDING BUT NOT LIMITED TO GRADING ON ALL DEDICATED LANDS FOR THE PURPOSES OF CONSTRUCTING PUBLIC AND PRIVATE IMPROVEMENTS.
3. ANY AMENITIES, PROJECT IDENTITY AND WAY-FINDING SIGNAGE ARE CONCEPTUAL IN NATURE AT THE LOCATIONS SHOWN WITHIN THE DEVELOPMENT PLAN. FINAL LOCATIONS SHALL BE DETERMINED DURING THE CONSTRUCTION DOCUMENTATION PROCESS, BUT SHALL CONFORM TO THE STANDARDS WITHIN THIS DEVELOPMENT PLAN.
4. THE HISTORIC HOME, PREVIOUSLY LOCATED AT 1004 GRIFFITH STREET, WAS RELOCATED TO LOT 1, BLOCK 12 AND RECONFIGURED FOR OFFICE OR COMMERCIAL USE AS A PORTION OF DELO PHASE 1. (I.E. THERE ARE NO HISTORIC STRUCTURES WITHIN DELO PHASE 2).
5. ACCESSIBLE SIDEWALKS AND PEDESTRIAN WALKS SHALL BE PROVIDED THAT MEET ADA STANDARDS FOR RUNNING SLOPE AND CROSS SLOPE.
6. IN ADDITION TO ALL USES ALLOWED BY THE MUEP DESIGNATION, THE FOLLOWING USES SHALL BE EXPRESSLY ALLOWED AND CONSIDERED ACCESSORY STRUCTURES: DEVELOPMENT AMENITY ITEMS SUCH AS PLAZAS, PATIOS, AND GARDENS; AND FREE STANDING GARAGES, CARPOIS, PARKING STRUCTURES AND TRASH ENCLOSURES.
7. AMENITY/RECREATION STRUCTURES, IF ANY, ARE NOT INCLUDED IN DENSITY CALCULATIONS, HOWEVER MAY BE INCLUDED IN LOT COVERAGES CALCULATIONS. SUCH STRUCTURES ARE SUBJECT TO BULK AND DIMENSION STANDARDS SPECIFIED FOR ACCESSORY STRUCTURES AS DESCRIBED IN THIS DEVELOPMENT PLAN.
8. PARKING STRUCTURES, CARPOIS, AND PARKING GARAGES, WHETHER ATTACHED TO DWELLING UNITS OR DETACHED, SHALL BE APPROVED AS ACCESSORY STRUCTURES AND USES NECESSARY AND CUSTOMARILY INCIDENTAL TO THE RESIDENTIAL USE. SUBJECT TO BULK AND DIMENSION STANDARDS AS DESCRIBED IN THIS DEVELOPMENT PLAN. PARKING STRUCTURES, GARAGES AND SHARABLE STRUCTURES ARE NOT INCLUDED IN DENSITY CALCULATIONS, HOWEVER ARE INCLUDED IN LOT COVERAGES CALCULATIONS. FURTHERMORE, IT IS UNDERSTOOD THAT UNING SPACES ARE NOT PERMITTED OR ABOVE DETACHED GARAGES, OR AS AN ACCESSORY USE.
9. NO RESTRICTIONS ARE IMPOSED WITH REGARD TO PROJECT PHASING OTHER THAN AS EXPRESSLY SET FORTH IN THE DEVELOPMENT PLAN OR IN ANY DEVELOPMENT AGREEMENT BETWEEN THE OWNER AND THE CITY.
10. THE GROUND FLOOR PROGRAM MAY BE RESIDENTIAL, ANKOR COMMERCIAL, AND SHALL BE INTERCHANGEABLE, PROVIDED THAT THE TOTAL NUMBER OF DWELLING UNITS DOES NOT EXCEED THE MAXIMUM NUMBER OF DWELLING UNITS DESCRIBED HEREIN.
11. MULTI-UNIT DWELLING APARTMENTS SHALL BE ALLOWED TO TRANSFER TO CONDOMINIUMS, AT THE SOLE DISCRETION OF THE DEVELOPER, WITHOUT ADDITIONAL REQUIREMENTS.
12. THE CANNON STREET "WOONER" SECTION MAY BE ADJUSTED TO AN ALTERNATIVE SHARED SPACES/WALKABLE SECTION OR RECONFIGURED BASED ON MARKET CONSTRAINTS, FUNDING, RIGHT-OF-WAY OR SERVICEABILITY LIMITATIONS. ADDITIONAL MAINTENANCE SHALL BE PERFORMED VIA THE DELO COMMERCIAL HOA, HOWEVER, ALL MAINTENANCE SHALL BE IN CONFORMANCE WITH THE APPROVED SUBDIVISION AGREEMENT.
13. ALL IMAGERY IS CONCEPTUAL IN NATURE.
14. THE LIGHTING CONFIGURATION, DESIGN, FUTURE TYPES, ETC. AS DEPICTED HEREIN IS SUBJECT TO FURTHER ANALYSIS, DESIGN AND AVAILABILITY, AND AS SUCH MAY VARY FROM THE FINAL PUD TO FINAL CONSTRUCTION DOCUMENTS. PROPOSED LIGHTING WILL INCLUDE DIRECTIONAL COWYS AND SHALL BE DIRECTED AWAY FROM THE RESIDENCES.
15. ALL DELO PROJECT CALCULATIONS, INCLUDING DENSITY, LANDSCAPING, PARKING, ETC. SHALL BE CALCULATED ON THE AGGREGATE DELO PROJECT AREAS.
16. THIS PUD PROVIDES AND ACCOMMODATES PUBLIC ACCESS AND SIGHT LINES TO THE FUTURE 870 NORTHWEST PLAZA, TO BE RELOCATED WITHIN THE BAY RIGHT-OF-WAY.

BULK & DIMENSION STANDARDS (PHASE 2)

PERMITTED DENSITY	MULTI-UNIT DWELLING TOWNHOMES	MULTI-UNIT DWELLING APARTMENTS, CONDOMINIUMS	OFFICE/COMMERCIAL
	12 DU/AC MIN 30 DU/AC MAX		
MIN. LOT WIDTH	40'	40'	40'
MIN. LOT COVERAGE	40%	40%	40%
MIN. LANDSCAPE COVERAGE	10%	10%	10%
MAX. FLOORFRT	10,000 SF	15,000 SF	15,000 SF
MAX. LENGTH ALONG STREET	300'	300'	300'
MIN. % STREET FRONTAGE	70%	70%	70%
STREET SECTIONS	SHARED SPACES/WALKABLE *		
RELOCATED STRUCTURES			
MIN. & MAX. PUBLIC STREET / ** OUTLOT/TRACT SEIBACK (PRINCIPAL USES)	MAXIMUM: 10' MINIMUM: 0'	MAXIMUM: 10' MINIMUM: 0'	MAXIMUM: 10' MINIMUM: 0'
MIN. SIDE YARD SEIBACK / ** (PRINCIPAL & ACCESSORY USES)	0'	0'	0'
MIN. REAR YARD SEIBACK / ** (PRINCIPAL USES)	5'	20'	20'
MIN. REAR YARD SEIBACK / ** (ACCESSORY USES)	5' (BAR)	0'	0'
BUILDING HEIGHT			
PRINCIPAL USES	MIN: 2 STORIES/33' MAX: 3 STORIES/43'	MIN: 2 STORIES/33' MAX: 3 STORIES/43'	MIN: 1 STORY/16' MAX: 3 STORIES/43'
ACCESSORY USES	30' MAX	30' MAX	30' MAX

* SEE SIMPLE LOTS CREATED WITHIN BUILDINGS SHALL HAVE NO SEIBACK REQUIREMENT BETWEEN INTERNAL UNITS.
 ** ACCESSORY STRUCTURES AS ALLOWED PER APPLICABLE CITY CODE AND PER MASTER PLAN AND GENERAL NOTES SHEET 3.
 ** SHALL BE SUBJECT TO MUDOSG, SECTION 8, RESIDENTIAL PROTECTION AND TRANSITIONAL STANDARDS.
 ** ROOFLINES, OVERHANDINGS, PATIOS, DECKS, PORCHES AND BALCONIES MAY EXTEND INTO THE ADJACENT TRACTS A, C, D & E. SEIBACKS SHALL BE MEASURED FROM THE BUILDING FACEL AND NOT THE ABOVE MENTIONED EXTENSIONS.
 ** THE PLATTED BLOCK LENGTH SHALL NOT BE LESS THAN 40'. TOWNHOMES SHALL BE ALLOWED TO PLAT FIVE SIMPLE LOTS FOR NONMUDAL UNITS, WITH A 24' MINIMUM WIDTH.
 ** TO BE ADHERED TO SOLELY AS A FRONT SEIBACK.
 ** REAR SEIBACK SHALL BE MEASURED FROM PROPERTY LINES, NOT TRACT OR OUTLOT BOUNDARIES.
 ** ALLOWED PER VARIANCE REQUEST.

USE CHART

PHASE	PRINCIPAL USE GROUP	USES	LOCATION
PHASE 2	RESIDENTIAL	MULTI-UNIT DWELLING TOWNHOMES	BLOCK 9
	RESIDENTIAL	MULTI-UNIT DWELLING APARTMENTS, CONDOMINIUMS	BLOCK 13, LOTS 1, 2 & 3 AND BLOCK 14, LOT 1
	OFFICE	PROFESSIONAL AND BUSINESS OFFICES	
COMMERCIAL	PERSONAL SERVICES	RETAIL CONVENIENCE GOODS	BLOCK 13, LOTS 4 & 5
	INDOOR EATING AND DRINKING ESTABLISHMENTS	TEMPORARY SPECIAL EVENTS	
	CIVIC	PUBLIC SQUARES, PLAZAS, GREENWAYS AND COMMUNITY AMENITIES	

* THE ANTICIPATED PRINCIPAL USES EXCEEDS THE MAXIMUM REQUIREMENTS OF TWO USES AND PROVIDES A PUBLIC BENEFIT.
 ** ALL USES PER TABLE 1 OF SECTION 17.14.00.6 ARE ALLOWED, WHICH MAY FURTHER ENVELOPE THE RANGE OF USES WITHIN THE PROJECT.

DENSITY SUMMARY

TOTAL GROSS PROJECT AREA:	114.18 AC. *		
GROSS LAND AREA:	112.26 AC. *		
TOTAL:	112.26 AC.		
	GROSS LAND AREA (ACRES)	UNITS	ANTICIPATED DENSITY
	244 DU (PRELIMINARY PUD, AS APPROVED BY COUNCIL)	13,30 DU/AC	
	35 DU (PHASE 1 AND PHASE 1A)		
	135 DU (PHASE 2)		
	34 DU (NOT USED)		

* THE DENSITY CALCULATION IS CONSISTENT WITH CITY OF LOUISVILLE MUNICIPAL CODE SECTION 17.14.00.6 C.1.A.
 ** TOTAL GROSS PROJECT AREA INCLUDES ALL PLATTED AREA PLUS REBEL RIGHT OF WAY AREA FOR THE BURELBY ADJACENT STREET. GROSS LAND AREA IS SYNCHRONOUS WITH THE AREA CONSISTENT WITH LOUISVILLE MUNICIPAL CODE SECTION 17.14.00.6 C.1.A.
 ** FOR UNUSED DENSITY, PLEASE REFER TO PUBLIC BENEFIT NOTE TO ABOVE.

REQUESTED VARIANCE

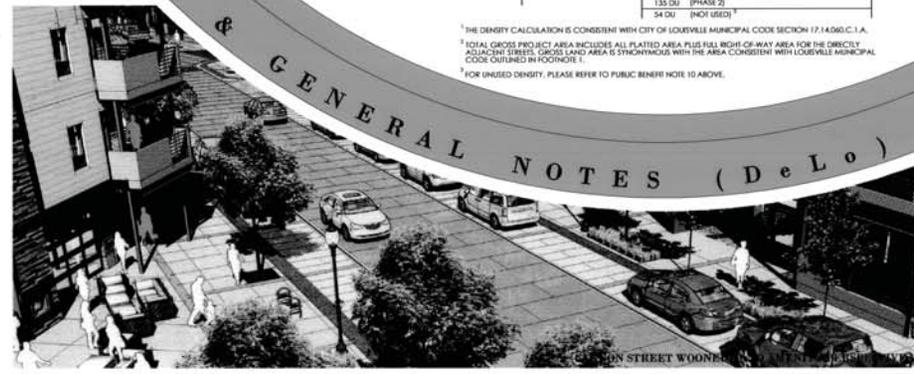
VARIANCE	EXISTING CODE (CITY OF LOUISVILLE)	REQUESTS	LOCATION
STREET SECTIONS	SUBURBAN	SHARED SPACES / WALKABLE	PHASE 2
OFFICE/RETAIL HEIGHT	MIN: 2 STORIES/30' MAX: 3 STORIES/43'	MIN: 1 STORY/16' MAX: 3 STORIES/43'	LOT 1, BLOCK 13
ACCESSORY STRUCTURE(S)	REAR SEIBACK: 20'	REAR SEIBACK: 0'	PHASE 2
INDOOR EATING & DRINKING ESTABLISHMENTS	GROSS FLOOR AREA: 5,000 SF	GROSS FLOOR AREA: 5,000 SF	PHASE 2
SIGNS	MUDDOSG / MUEP	ROOF MOUNTED SIGN	LOT 4, BLOCK 13
SIGNS	MUDDOSG / MUEP	MONUMENT (60 SQ FT)	PHASE 2

PUBLIC BENEFIT

THE DEVELOPMENT OF THE DELO PROJECT YIELDS PUBLIC BENEFITS GENERATED BY THE PLANNING, LAYOUT AND INNOVATION WITHIN THE DESIGN. PUBLIC BENEFITS INCLUDE, BUT ARE NOT LIMITED, TO THE FOLLOWING:

1. THE PROJECT IS THE FIRST IMPLEMENTATION OF THE HIGHWAY 42 REDEVELOPMENT AREA COMPREHENSIVE PLAN AND WILL LIKELY PROVIDE THE IMPULS FOR THE CONTINUATION OF THAT PLAN. THE PUBLIC AND DEVELOPERS Alike, WILL VIEW THE AREA AS AN EXTENSION OF THE DOWNTOWN FABRIC PROVIDING AN ENCOURAGEMENT FOR CONTINUED DEVELOPMENT.
2. THROUGH THE IMPLEMENTATION OF THE DIVERSE USE MIXTURE, THE PROJECT WILL STRENGTHEN AND ENHANCE ADJACENT RESIDENTIAL NEIGHBORHOODS BY PROVIDING HIGH QUALITY MULTI-UNIT DWELLINGS AUGMENTED BY A LOCAL COMMERCIAL, RETAIL AND OFFICE PRESENCE WITH AN EMPHASIS ON THE PEDESTRIAN EXPERIENCE. FURTHERMORE, THE PROJECT WILL PROTECT THE EXISTING RESIDENTIAL NEIGHBORHOODS THROUGH THE MAINTENANCE OF OR THOUGHOUT, SEIBACKS/BACK YARDS, PROVIDING CONSTRUCTION IMPLEMENTING HIGH QUALITY MATERIALS, SHELTERING PARKING LOTS THROUGH THE STRATEGIC SITE LAYOUT AND GENERATING GREENWAYS AND PUBLIC USE AREAS TO INTERJECT CIVIC SPACES.
3. THE DEVELOPMENT OF THE SOUTH STREET UNDERPASS ALONG WITH THE WOONER STREET CONCOURSE WILL COMPLEMENT AND INTEGRATE THE DEVELOPMENT WITH THE HISTORIC DOWNTOWN. THE SITE LAYOUT AND STREET SECTIONS WERE DEVELOPED TO ACCOMMODATE STRONG PEDESTRIAN AND MULTIMODAL CONNECTIONS THROUGH THE USE OF SHARED SPACES AND WILL ALLOW FOR WALKABILITY AND WILL DEVELOPE A SENSE OF NEIGHBORHOOD.
4. THE PROJECT IS ANTICIPATED TO BRING HIGH QUALITY RESIDENTIAL UNITS THAT WILL PROVIDE ADDITIONAL RESIDENCES TO SUPPORT HISTORIC DOWNTOWN BUSINESS AND WILL ENCOURAGE COMMERCIAL OPPORTUNITIES BETWEEN CANNON STREET AND HIGHWAY 42. ADDITIONALLY, 35,000 SQUARE FEET OF COMMERCIAL RETAIL OFFICES USE WILL SERVICE THE AREA AND WILL BOOST THE CITY TAX BASE.
5. THE DEVELOPMENT WILL REMOVE THE INDUSTRIAL ZONING AND REPLACE IT WITH AER ZONING, WHICH PROHIBITS THE INCOMPATIBLE INDUSTRIAL USES (DEVELOPED BY THE REDEVELOPMENT PLAN, THIS PROJECT IS THE CATALYST FOR THE CORE PROJECT AREA AND WILL CREATE THE RIGHT SIZED EXPANSION THAT PLAYS OFF THE HISTORIC DESTINATION OF LOUISVILLE.
6. THE MIX OF USES PROPOSED WITH THE DELO REDEVELOPMENT EXCEEDS THE MINIMUM REQUIREMENTS OF SECTION 17.14.00.6.2, WHICH PROVIDES A PUBLIC BENEFIT AND MEETS THE PURPOSE AND INTENT OF THE MUEP ZONE DESIGN AND THE MUDOSG.
7. THE DEVELOPMENT INTEGRATES PRINCIPLES OF SUSTAINABLE ARCHITECTURE AND ENERGY CONSERVATION THROUGH THE ANTICIPATED IMPLEMENTATION OF ENVIRONMENTALLY RESPONSIBLE AND RESOURCE EFFICIENT TECHNOLOGIES WITH THE OBJECTIVE TO EXPAND AND COMPLEMENT THE CLASSICAL BUILDING DESIGN CONCERNING ECONOMY, URBANITY AND COMFORT. THE MAY BE ACHIEVED THROUGH PHOTOVOLTAIC SOLUTIONS, ENERGY EFFICIENT HEATING AND COOLING, GREEN BUILDING PRODUCTS AND CONSTRUCTION WASTE REDUCTION.
8. THE CANNON STREET EXTENSION WILL PROVIDE ADDITIONAL PEDESTRIAN ACCESS, VIA THE SOUTH STREET GATEWAY TO THE DOWNTOWN EXPERIENCE. FURTHERMORE, PEDESTRIAN PLAZAS, WALKS, GREENWAY AND REFUGE AREAS WILL LINE THE PEDESTRIAN CORRIDORS AND CIRCULATION THROUGHOUT THE PROJECT AND PROVIDE A REGENERATED PUBLIC BENEFIT.
9. THE DELO REDEVELOPMENT WILL PROVIDE AMENITIES THAT WILL BE ENJOYED BY THE PUBLIC SUCH AS PLAZAS, CIVIC SPACES, GREENWAYS FOR COMMUNITY EVENTS AND GATHERING AREAS AS WELL AS THE WOONER CURBLESS ENVIRONMENT. PUBLIC ART LOCATIONS HAVE BEEN PROVIDED TO THE CITY OF LOUISVILLE FOR FUTURE USE AT THIS SCALE. GENERALLY, THE ABOVE MENTIONED PROPOSED AMENITIES SHALL PROVIDE CITY AND PUBLIC BENEFIT.
10. THE TOTAL PROPOSED DENSITY WITHIN PHASE 1, 1A AND 2 IS 225 OF 34 UNITS LESS THAN THE APPROVED DENSITY AND SHALL BE DEEMED A PUBLIC BENEFIT.
11. ALL PUBLIC LAND DEDICATION REQUIREMENTS HAVE BEEN PREVIOUSLY MET FOR THE ENTIRETY OF THE PROJECT. PUBLIC LAND DEDICATION INCLUDED AS A PORTION OF THIS APPLICATION IS IN EXCESS OF THAT TYPICALLY REQUIRED AND PROVIDES PUBLIC BENEFIT. OUTLOTS A, B AND C AS WELL AS OUTLOT 1 FROM THE INDUSTRIAL AREA SUBDIVISION RE-PLAT NO. 1 ARE BEING GENERATED FOR THIS PUBLIC USE. THE TOTAL AREA FOR THESE OUTLOTS IS 0.97 ACRES, WITH THE INCLUSION OF OUTLOT 1 FROM THE INDUSTRIAL AREA SUBDIVISION RE-PLAT NO. 1, THE NAWATNY GREENWAY IS 1.12 ACRES.
12. THE DELO DEVELOPMENT HAS CONTRIBUTED FUNDING FOR THE OFF-SITE DETENTION AND WATER QUALITY FOND THAT WILL SERVICE DELO AND THE CORE PROJECT AREA AND FACILITATING THE FUTURE DEVELOPMENT OF PARCELS WITHIN THE CORE PROJECT AREA THIS ALLOWING FOR THE CREATION OF A PUBLIC REGIONAL GREENWAY AND IS A PUBLIC BENEFIT.

MASTER PLAN & GENERAL NOTES (DeLo)



sheet title

Master Plan & General Notes

submittal

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

designed by: SOS drawn by: SGD
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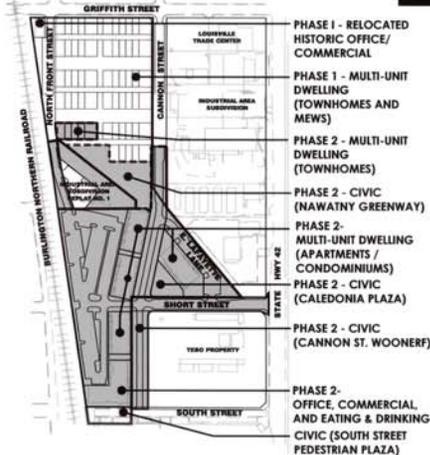
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DowntownEastLouisville

A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado

PRODUCT TYPE KEY MAP (1" = 200')



MULTI-UNIT DWELLING (TOWNHOMES) SUMMARY

LAND USE	MU R
DWELLING UNITS (ALLOWED)	72 DU
DWELLING UNITS PHASE 1(T/A)	33 DU
DWELLING UNITS PHASE 2	3 DU
DWELLING UNITS (NOT USED)	12*

* FOR UNUSED DENSITY, PLEASE REFER TO PUBLIC BENEFIT NOTE 10 ON SHEET 3

PARKING SUMMARY

RESIDENTIAL CRITERIA	DWELLING UNITS (DU)	REQUIRED	PROVIDED ¹	PARKING RATIO
2 BEDROOM & LARGER X 2 SF/DU	3	10	PRIVATE GARAGE = 10; HEAD-IN SURFACE = 38*	
GUEST X 1 SF/DU		2	ON STREET PARKING = 15 BI-CYCLE PARKING = 16 (NOT A PORTION OF THE PROVIDED "STALL" COUNT)	
TOTAL	3 (DU)	12	139¹	2.65¹

¹ TWO CAR GARAGES REQUIRED FOR EACH DWELLING UNIT AS PART OF THE TOWNHOME PRODUCT.
² SHARED PARKING SHALL BE GRANTED BETWEEN ALL TOWNHOME AND OFFICE/COMMERCIAL PRODUCT TYPES.
³ TOTAL PARKING PROVIDED AND PARKING RATIO IS SHOWN IN AGGREGATE BETWEEN PHASES 1, 1A AND 2.

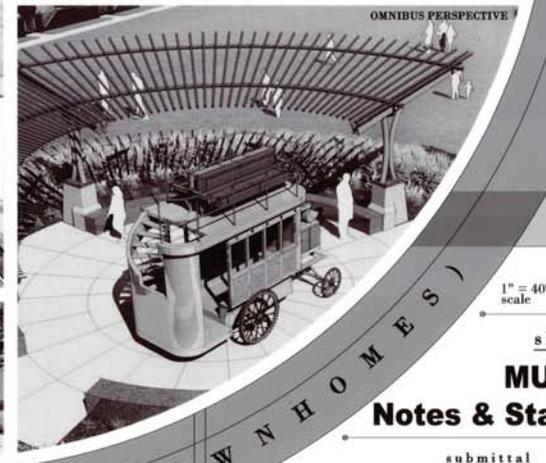
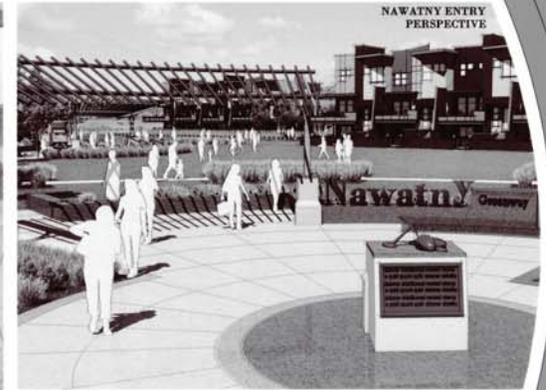
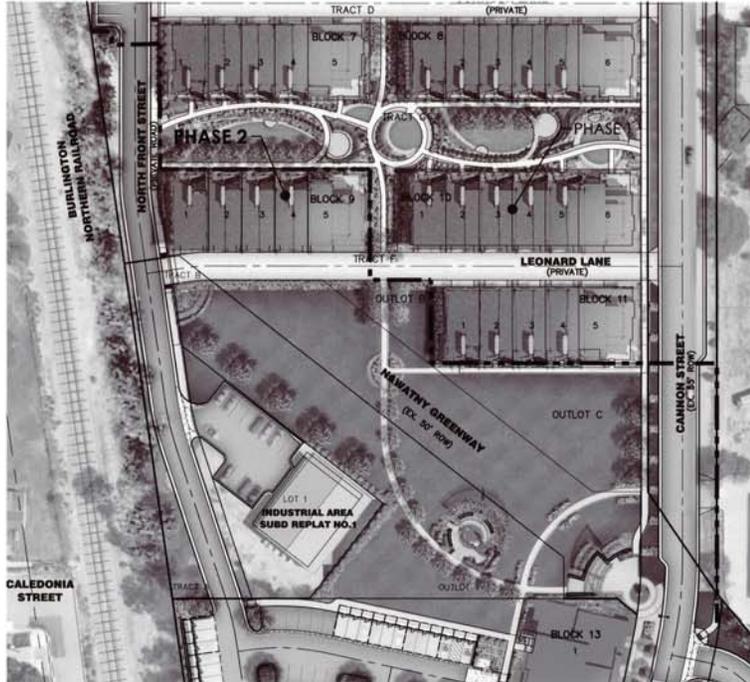
BULK & DIMENSION STANDARDS

MIN. LOT WIDTH	42' F
MIN. LOT COVERAGE	40%
MIN. LANDSCAPE COVERAGE	12%
MAX. FOOTPRINT	10,000 SF
MAX. LENGTH ALONG STREET	200'
MIN. % STREET FRONTAGE	70%
BUILDING SETBACKS	
MIN. & MAX. PUBLIC STREET/OUTLOT/TRACT SETBACK (PRINCIPAL USES)	MAXIMUM: 10' MINIMUM: 0'
MIN. SIDE YARD SETBACK (1 ST PRINCIPAL & ACCESSORY USES)	0'
MIN. REAR YARD SETBACK (PRINCIPAL USES)	0'
MIN. REAR YARD SETBACK (ACCESSORY USES)	5' (LANE)
MAX. BUILDING HEIGHT	
PRINCIPAL USES	MIN. 2 STORIES/33'
ACCESSORY USES ¹	MAX. 3 STORIES/45'

¹ FREE SIMPLE LOTS CREATED WITHIN BUILDINGS SHALL HAVE NO SETBACK REQUIREMENT BETWEEN INTERNAL LINES.
² ACCESSORY STRUCTURES AS ALLOWED PER APPLICABLE CITY CODE AND PER MASTER PLAN AND GENERAL NOTES SHEET 3, REFER TO THE BULK & DIMENSION CHART ON SHEET 3.

TOWNHOMES - NOTES AND STANDARDS

- THE AREA WILL BE ORGANIZED AROUND TWO PRIMARY POINTS OF ACCESS KNOWN AS NORTH FRONT STREET AND CANNON STREET. THIS SITE LAYOUT ENHANCES ACCESS TO PUBLIC PLAZAS, GREEN SPACES, GREENWAYS, AND MULTI-MODAL TRANSPORTATION OPPORTUNITIES.
- PER SECTION 1.4.D, IN THE MUDSIC, MD BLOCK ALLEYS ARE ENCOURAGED BY THE CITY OF LOUISVILLE IN PLACE OF SUCH ALLEYS. GREENWAYS ARE BEING USED IN THE TOWNHOMES ROWHOMES AREA AS AN EFFORT TO BREAK UP THE BLOCK AND SHALL BE AN ACCEPTABLE SUBSTITUTE.
- REFER TO THE LAND USE SUMMARY ON SHEET 2 AND/OR THE SUBDIVISION AGREEMENT FOR TRACT AND OUTLOT OWNERSHIP AND GENERAL MAINTENANCE INFORMATION INCLUDING DELINEATION OF MAINTENANCE RESPONSIBILITIES.
- USES ALLOWED BY RIGHT: SINGLE FAMILY ATTACHED UNITS AND ALL USES AS PERMITTED IN THE LOUISVILLE MUNICIPAL CODE SECTION 17.14.03A, TABLE 1 IN ADDITION TO THOSE NOTED ON THE MASTER PLAN AND GENERAL NOTES, SHEET 3.
- BUILDINGS MAY BE BUILT AT TWO OR THREE STORY HEIGHTS, OR COMBINATIONS THEREOF AND MAY INCLUDE OUTDOOR USING SPACES.
- THE TOWNHOMES SHALL HAVE ADDITIONAL CONDITIONS, COVENANTS AND RESTRICTIONS (CCRs) TO BE DEVELOPED BY THE SELO TOWNHOME H.O.A.
- IT IS ENCOURAGED TO PLANT DROUGHT TOLERANT, LOW GROWING PLANT MATERIAL IN THE LANES BETWEEN THE PLANTING STRIPS CREATED BY APRONS AND EDGE OF CONCRETE LANE, AND GARAGE. EVERGREENS AND DECIDUOUS SHRUB PLANTINGS WILL Aid IN CREATING A MORE INVITING SPACE BY SOFTENING MANY OF THE LANES' HARDSCAPE ELEMENTS, AS LONG AS THEY COMPLY WITH ESTABLISHED SETBACKS.
- THE PROPOSED TOWNHOMES SHALL BE PROTECTED WITH A FIRE SPRINKLER SYSTEM THAT SHALL BE OFF A LOOPED MAIN STREET THAT NO MORE THAN ONE SERVICE OFF A DEAD-END LINE.
- MULTI-FAMILY DWELLING UNITS WITHIN 100 FEET OF BALFOUR ROW WILL INCORPORATE NOISE MITIGATION VIA ENHANCED WALL CONSTRUCTION AND WINDOW SPECIFICATION, INCLUDING A MINIMUM EXTERIOR WALL SOUND TRANSMISSION COEFFICIENT OF 35 FACING THE BALFOUR ROW, WITH TIGHT FITTING TRIPLE PANE WINDOWS, AND SOLID CORE DOORS.
- ALL LANDSCAPING DEPICTED IS CONCEPTUAL AND IS SUBJECT TO CHANGE WITH SUBSEQUENT CONSTRUCTION PLAN SUBMITTALS.
- NORTH FRONT STREET, LEONARD LANE AND THE ASSOCIATED UTILITIES IN THE VICINITY OF THE FUTURE E. LAFAYETTE RIGHT-OF-WAY VACATION AND THAT CROSS THE COMCAST PARCELS SHALL BE CONSTRUCTED PENDING AN AGREEMENT WITH COMCAST. ANY REQUIRED PHASING OF THE INFRASTRUCTURE AFFECTED BY THE AGREEMENT SHALL BE DETERMINED WITH THE SUBSEQUENT CONSTRUCTION DOCUMENTS.



1" = 40'
scale north

sheet title
**MUD Plan,
Notes & Standards**

submittal	no	date	description
	1	08-15-2014	initial submittal
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designed by: SOS drawn by: SCD
checked by: JDM project #: 030017

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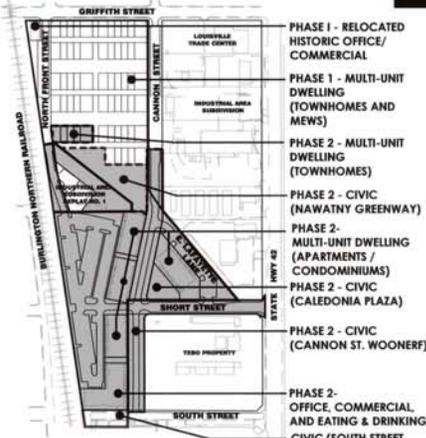
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4 of 34

delo Final Planned Unit Development Phase 2

Downtown East Louisville

A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado

PRODUCT TYPE KEY MAP (1" = 200')



MULTI-UNIT DWELLING (APARTMENTS/ CONDOMINIUMS) SUMMARY

LAND USE	MU-4
DWELLING UNITS (ALLOWED)	172 DU
DWELLING UNITS (PROPOSED)	130 DU
DWELLING UNITS (NOT USED)	42 ²

¹ FOR UNUSED DENSITY, PLEASE REFER TO PUBLIC BENEFIT NOTE 10 ON SHEET 3

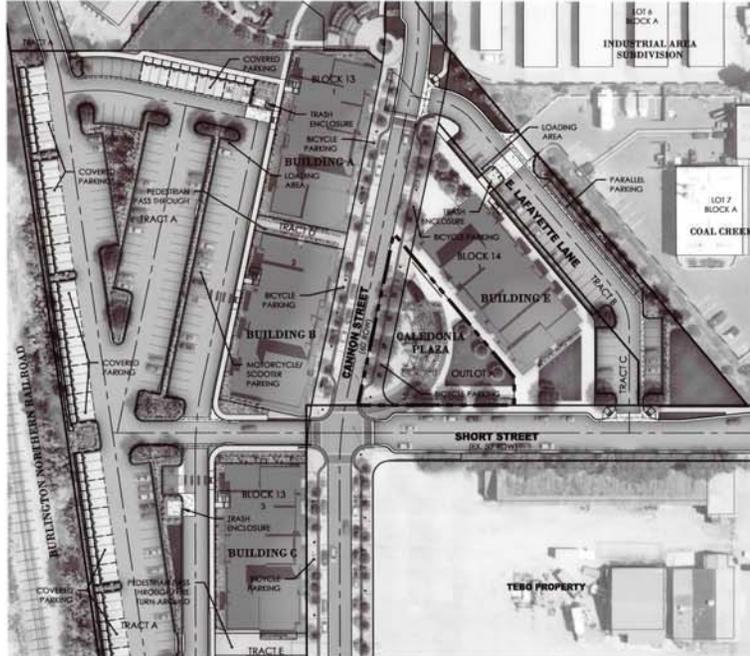
PARKING SUMMARY

RESIDENTIAL CATEGORIES	DWELLING UNITS (DU)	REQUIRED	PROVIDED	PARKING RATIO
STUDIOS X 1 SP/OU	36	36	HEAD IN AND PARALLEL SURFACE +120 HEAD IN COVERED = 77 ON STREET PARALLEL = 49 MOTORCYCLE/SOOTER = 2	
1 BEDROOM X 1 SP/OU	72	72	LOADING = 2 BICYCLE PARKING = 20 (NOT A PORTION OF THE PROVIDED "STALL" COUNT)	
2 BEDROOM X 2 SP/OU	11	22		
3 BEDROOM X 2 SP/OU	11	22		
GUEST X 1 SP/OU		17		
LOADING		2		
TOTAL	130	171	230	1.92

BULK & DIMENSION STANDARDS

	MULTI-UNIT DWELLING APARTMENTS, CONDOMINIUMS
MIN. LOT WIDTH	40'
MIN. LOT COVERAGE	40%
MIN. LANDSCAPE COVERAGE	10%
MAX. FOOTPRINT	15,000 SF
MAX. LENGTH ALONG STREET	200'
MIN. 8' STREET FRONTAGE	70%
BUILDING SETBACKS	
MIN. & MAX. PUBLIC STREET/OUTLET ^{1,2} TRACT SETBACK (PRINCIPAL USES)	MAXIMUM: 10'
MIN. SIDE YARD SETBACK ³ (PRINCIPAL & ACCESSORY USES)	0'
MIN. REAR YARD SETBACK ⁴ (PRINCIPAL USES)	20' ⁵
MIN. REAR YARD SETBACK ⁴ (ACCESSORY USES)	0'
BUILDING HEIGHT	
MAX. BUILDING HEIGHT	MAX. 3 STORIES/45'
PRINCIPAL USES	MAX. 3 STORIES/45'
ACCESSORY USES ⁷	20'

¹ THE SIMPLE LOTS CREATED WITHIN BUILDINGS SHALL HAVE NO SETBACK REQUIREMENT BETWEEN INTERNAL UNITS.
² ACCESSORY STRUCTURES AS ALLOWED PER APPLICABLE CITY CODE AND PER MASTER PLAN AND GENERAL NOTES, SHEET 3.
³ FOOTING, OVERHANGING PORCHES, DECKS, PORCHES AND BALCONIES MAY EXTEND INTO THE ADJACENT TRACTS A, C, D & E. SETBACKS SHALL BE MEASURED FROM THE BUILDING FACE AND NOT THE JOINS MENTIONED EXTENSIVE.
⁴ TO BE ADMINISTERED SOLELY AS A FRONT SETBACK.
⁵ REAR SETBACK SHALL BE MEASURED FROM PROPERTY LINES, NOT TRACT OR OUTLOT BOUNDARIES.



1" = 50' scale



sheet title

MUD Plan, Notes & Standards

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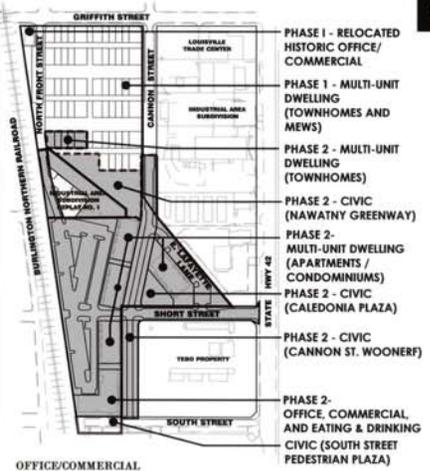
MUD APARTMENTS/CONDOMINIUMS - NOTES AND STANDARDS

- THE APARTMENTS/CONDOMINIUMS WILL BE ORGANIZED AROUND FOUR PRIMARY POINTS OF ACCESS KNOWN AS SHORT STREET, CANNON STREET, NORTH FRONT STREET AND SOUTH STREET. THIS SITE LAYOUT ENHANCES ACCESS TO PUBLIC PLAZAS, GREEN SPACES, GREENWAYS, AND MULTI-MODAL TRANSPORTATION OPPORTUNITIES.
- REFER TO THE LAND USE SUMMARY ON SHEET 2 AND/OR SUBDIVISION AGREEMENT FOR TRACT AND OUTLOT OWNERSHIP AND GENERAL MAINTENANCE INFORMATION INCLUDING DISTRIBUTION OF MAINTENANCE RESPONSIBILITIES.
- USES ALLOWED BY RIGHT: SINGLE FAMILY ATTACHED UNITS AND ALL USES AS PERMITTED IN THE LOUISVILLE MUNICIPAL CODE SECTION 17.14.05A, TABLE 1. IN ADDITION TO THOSE NOTED ON THE MASTER PLAN AND GENERAL NOTES, SHEET 3.
- BUILDINGS MAY BE BUILT AS TWO OR THREE STORY BLDGS, OR COMBINATIONS THEREOF AND MAY INCLUDE OUTDOOR LIVING SPACES.
- THE PROPOSED APARTMENTS/CONDOMINIUMS SHALL BE PROTECTED WITH A FIRE SPRINKLER SYSTEM THAT SHALL BE OFF A LOOPED MAIN SUCH THAT NO MORE THAN ONE SERVICE IS OFF A DEAD-END LINE.
- ALL LANDSCAPING DEPICTED IS CONCEPTUAL AND IS SUBJECT TO CHANGE WITH SUBSEQUENT CONSTRUCTION PLAN SUBMITTALS.
- ALL COVERED PARKING IS CONCEPTUAL AND CAN BE REMOVED, RELOCATED OR MOORED WITH SUBSEQUENT CONSTRUCTION PLAN SUBMITTALS.
- THE ARCHITECT WILL VARY THE COLORS ON THE EXTERIOR BALCONIES OF THE RESIDENTIAL BUILDING TO CREATE VISUAL INTEREST. WE PROPOSE TO USE COMPLEMENTARY COLORS SELECTED OUT OF THE MATERIAL PALETTE FOR EACH BUILDING. THE ACTUAL COLOR ASSIGNMENT WILL BE FINISHED IN THE SUBSEQUENT BUILDING CONSTRUCTION DOCUMENTS.
- ALL BUILDING HEIGHTS RELATIVE TO ASSOCIATED GRADE SHALL BE MEASURED PER THE CITY OF LOUISVILLE MUNICIPAL CODE.

de lo Final Planned Unit Development Phase 2

Downtown East Louisville
A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado

PRODUCT TYPE KEY MAP (1" = 200')



OFFICE/COMMERCIAL SUMMARY

LAND USE	MU-6
MU-6 COMMERCIAL	31,066 SQ-FT

PARKING SUMMARY

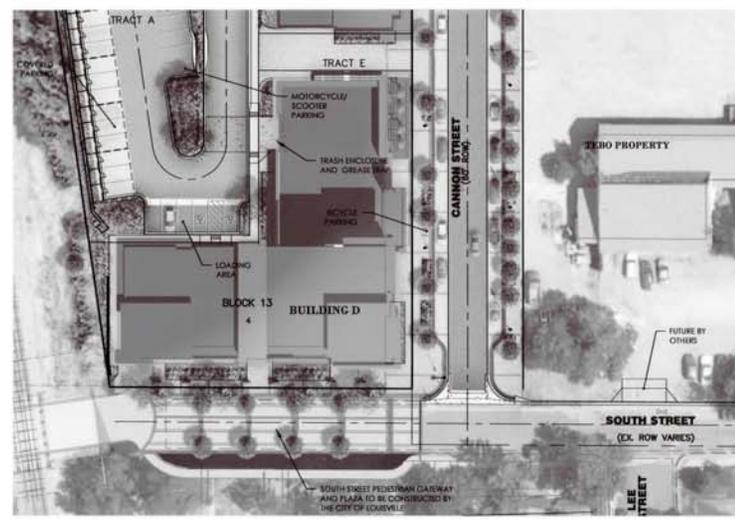
COMMERCIAL CRITERIA	GROSS LEASABLE AREA ¹ (GLA)	REQUIRED	PROVIDED	PARKING RATIO
(OFFICE) 1 SF/200 SF	19,143 SF	39	HEAD IN SURFACE = 40 ON STREET PARALLEL = 14 ² MOTORCYCLE/SCOOTER = 2 LOADING = 1 BICYCLE PARKING = 4 (NOTE A PORTION OF THE PROVIDED "STALL" COUNTS)	
(RESTAURANT) 1 SF/200 SF	5,081 SF	17		
LOADING		1		
TOTAL	24,224 (SF)	57³	57	2.33/ 1,000 SF

¹ GLA IS ASSUMED TO BE 88% OF TOTAL AREA (31,066 SF) AND HAS BEEN SPLIT INTO FLOORS 2 & 3 AS PROFESSIONAL AND BUSINESS OFFICE USE AND FLOOR 1 AS RETAIL / EATING AND DRINKING ESTABLISHMENTS (ONLY ADJACENT TO CANNON STREET WOONERF)
² SHARED PARKING SHALL BE GRANTED BETWEEN ALL APARTMENT/CONDOMINIUM AND OFFICE/COMMERCIAL/RETAIL PRODUCT TYPES.
³ ON-STREET PARKING SHALL ALLOW FOR COMMERCIAL AND RESIDENTIAL PARKING.

BULK & DIMENSION STANDARDS

	OFFICE/COMMERCIAL
MIN. LOT WIDTH	40'
MIN. LOT COVERAGE	40%
MIN. LANDSCAPE COVERAGE	10%
MAX. FOOTPRINT	13,000 SF
MAX. LENGTH ALONG STREET	200'
MIN. S. STREET FRONTAGE	70%
BUILDING SETBACKS	
MIN. & MAX. PUBLIC STREET/OUTLET TRACI SETBACK (PRINCIPAL USES) ¹	MAXIMUM: 10' ²
MIN. SIDE YARD SETBACK ^{1,3,4} (PRINCIPAL & ACCESSORY USES)	0'
MIN. REAR YARD SETBACK ^{1,3} (PRINCIPAL USES)	20' ⁵
MIN. REAR YARD SETBACK ¹ (ACCESSORY USES)	0'
MAX. BUILDING HEIGHT	
PRINCIPAL USES	MIN: 1 STORY/5 FT MAX: 3 STORY/30 FT
ACCESSORY USES ²	20' MAX

¹ SEE SIMPLE LOTS CREATED WITHIN BUILDINGS SHALL HAVE NO SETBACK REQUIREMENT BETWEEN INTERNAL SPACES.
² ACCESSORY STRUCTURES AS ALLOWED PER APPLICABLE CITY CODE AND PER MASTER PLAN AND GENERAL NOTES, SHEET 3.
³ SHALL BE SUBJECT TO MID-2008 SECTION 8.8 RESIDENTIAL PROTECTION AND TRANSITIONAL STANDARDS.
⁴ ROOFING, OVERHANGING, PATIOS, DECKS, PORCHES AND BALCONIES MAY EXTEND INTO THE ADJACENT TRACTS A, C, D & E. SETBACKS SHALL BE MEASURED FROM THE BUILDING FACE AND NOT THE ABOVE MENTIONED EXTENSIONS.
⁵ TO BE ADMINISTERED SOLELY AS A FRONT SETBACK.
⁶ REAR SETBACK SHALL BE MEASURED FROM PROPERTY LINES, NOT TRACT OR OUTLOT BOUNDARIES.



MUD OFFICE, COMMERCIAL, EATING AND DRINKING / RETAIL - NOTES AND STANDARDS

- REFER TO THE LAND USE SUMMARY ON SHEET 2 AND/OR SUBDIVISION AGREEMENT FOR TRACT AND OUTLOT OWNERSHIP AND GENERAL MAINTENANCE INFORMATION. REFER TO SUBDIVISION AGREEMENT FOR DELINEATION OF MAINTENANCE RESPONSIBILITIES.
- USES ALLOWED BY RIGHT: ALL USES AS PERMITTED IN THE LOUISVILLE MUNICIPAL CODE SECTION 17.4.0504, TABLE 1 IN ADDITION TO THOSE NOTED ON THE MASTER PLAN AND GENERAL NOTES, SHEET 3.
- BUILDINGS MAY BE BUILT AT ONE, TWO OR THREE STORY HEIGHTS, OR COMBINATIONS THEREOF.
- THE PROPOSED OFFICE, COMMERCIAL AND RESTAURANT SHALL BE PROTECTED WITH A FES SPRINKLER SYSTEM THAT SHALL BE ON A LOOPED MAIN SUCH THAT NO MORE THAN ONE SERVICE IS OFF A DEAD END LINE.
- ALL LANDSCAPING DEPICTED IS CONCEPTUAL AND IS SUBJECT TO CHANGE WITH SUBSEQUENT CONSTRUCTION PLAN SUBMITTALS.
- OPERABLE GARAGE DOORS ARE INTERCHANGEABLE WITH ALUMINUM CLAD GLASS DOORS AND/OR OTHER ARCHITECTURAL ELEMENTS ON THE GROUND FLOOR.
- ALL BUILDING HEIGHTS RELATIVE TO ASSOCIATED GRADE SHALL BE MEASURED PER THE CITY OF LOUISVILLE MUNICIPAL CODE.

OFFICE, COMMERCIAL AND EATING & DRINKING

1" = 30'
scale



sheet title

MUD Plan,
Notes & Standards

submittal

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

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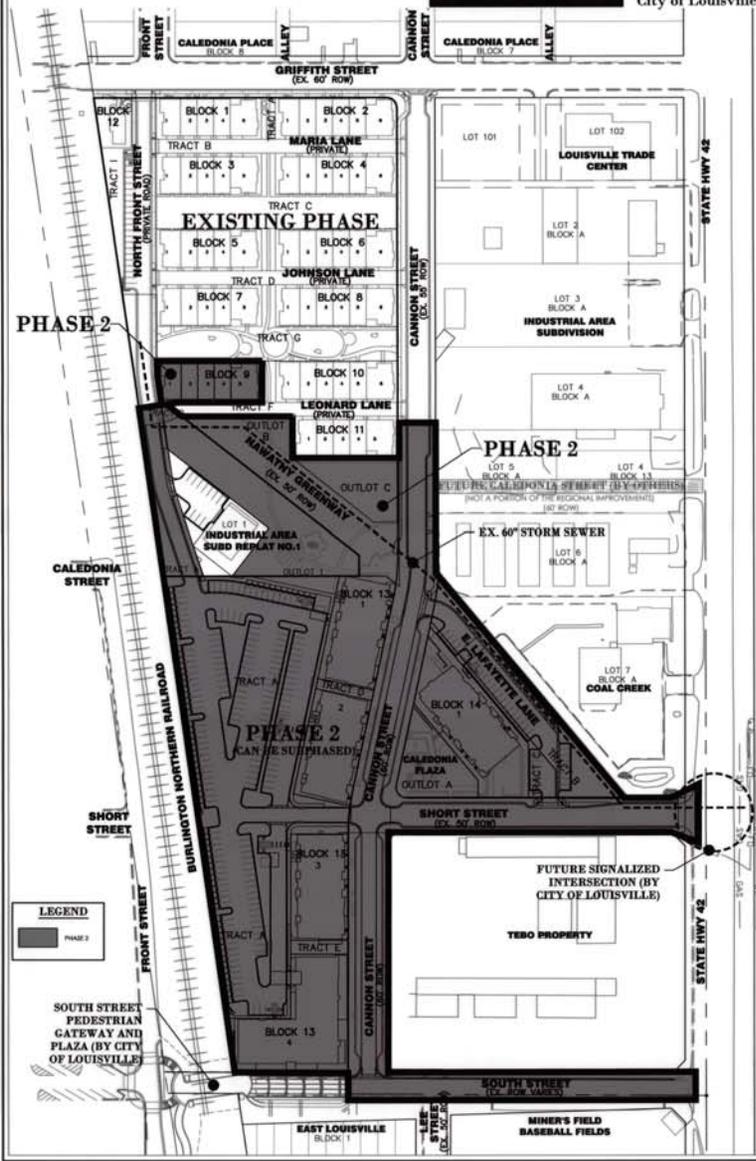
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6 of 34

de lo Final Planned Unit Development Phase 2

Downtown East Louisville

A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado



HORIZONTAL PUBLIC IMPROVEMENT NOTES

HORIZONTAL PUBLIC IMPROVEMENTS FOR THE PROPERTY WILL BE CONSTRUCTED TO SERVICE THE PROJECT AND MAY BE PHASED. THIS PLAN IS NOT MEANT TO DICTATE THE PHASING ASSOCIATED WITH THE PROJECT BEYOND WHAT IS STATED IN THESE NOTES. THE HORIZONTAL PUBLIC IMPROVEMENTS ARE DESCRIBED IN THE SUBSEQUENT TEXT AND SHALL ALLOW FOR THE GENERAL PROJECT NOTES AS FOLLOWS:

- RETAINING WALLS MAY BE REQUIRED FOR THE DEVELOPMENT OF THE PARCELS TO THE STANDARDS AND REQUIREMENTS OF THE CITY. DEVELOPER AND MARKET NEEDS WALLS OVER 3' WILL BE ABANDONED TO THE EXTENT PRACTICAL AND IF FEASIBLE, WALLS OVER 3' SHALL BE TIERED. WALL COLOURS AND MATERIALS SHALL MATCH OR BE COMPLEMENTARY THROUGHOUT THE DELO DEVELOPMENT.
- NO OFF-SITE IMPROVEMENTS ARE REQUIRED OTHER THAN WHAT IS DEPICTED WITH THIS PLAN.
- AN OFF-SITE DRAINAGE FACILITY TO SERVICE THE CORE PROJECT AREA IS REQUIRED FOR DETENTION AND WATER QUALITY. THE REQUIREMENTS OF THAT FACILITY AND THE ASSOCIATED IMPACTS TO THE PROJECT SHALL BE CONSISTENT WITH THE FINALIZED SUBDIVISION IMPROVEMENT AGREEMENT. THE FUND MUST BE OPERATIONAL PRIOR TO CONSTRUCTION ACCEPTANCE.
- PHASING (AND SUB PHASING) OF THE DEVELOPMENT AREA MAY OCCUR IN ANY ORDER AND IS NOT LIMITED EXCEPT AS REQUIRED TO PROVIDE LOOPEE WATER SERVICE, SANITARY SERVICE, TWO POINTS OF EMERGENCY ACCESS AND THAT NO "DEAD END" LONGER THAN 300' WITHOUT A SECONDARY EMERGENCY ACCESS SHALL OCCUR.
- REGIONAL HORIZONTAL PUBLIC IMPROVEMENTS ARE INFRASTRUCTURE THAT PROVIDES REGIONAL AND REDEVELOPMENT BENEFIT AND SHALL BE FUNDED BY THE CITY OF LOUISVILLE. THE URBAN REVENUE AGREEMENT FOR THE APPROVED AGREEMENT BY THE CITY OF LOUISVILLE OR BOND FINANCING, AS NEGOTIATED WITH ALL PARTIES. THE IDENTIFIED INFRASTRUCTURE IS ANTICIPATED TO BE CONSTRUCTED PRIOR TO OR CONCURRENT WITH THE SECOND PHASE OF THE PROJECT. HOWEVER, SUB PHASING MAY OCCUR SO LONG AS EMERGENCY AND UTILITY ACCESS IS MAINTAINED THROUGH EACH PHASE. STREET CROSS SECTIONS AND LANDSCAPE IMPROVEMENTS ARE AGREED TO AND AVAILABLE FOR REVIEW WITHIN THE DOCUMENT.
- THE REGIONAL DRAINAGE CONTRIBUTIONS, AS DEPICTED BY A NEW 8" RCP AND THE ASSOCIATED APPURTENANCES, SHALL BE FUNDED BY CITY OF LOUISVILLE AND CONSTRUCTED BY DELO.

HORIZONTAL PUBLIC IMPROVEMENTS BY STREET

- CANNON STREET**
- 8" WATER LINE BETWEEN EAST LAFAYETTE STREET AND GERRITT STREET
 - DRY UTILITY
 - 3" REGIONAL DRAINAGE FACILITY
 - PHASE 1 AND 1A FLATWORK, PAVING, WAUZE TREE LAMING AND PUBLIC GREENWAYS, WATER, LANDSCAPE AND STORM SEWER IMPROVEMENTS
- REGIONAL IMPROVEMENTS**
- PAVING AND PEDESTRIAN WALKWAYS FROM THE PHASE 1/1A TERMINATION TO SOUTH STREET
 - PUBLIC DRAINAGE FACILITY
 - CALEDONIA PUBLIC PLAZA (OUTLOT A)
 - 8" SANITARY SEWER
 - RELOCATION OF DRY UTILITY
 - PROPOSED 48" RCP STORM SEWER PARALLEL TO EXISTING (SUBJECT TO NOTE 8)
 - IF WATER LINE FROM LAFAYETTE LANE TO SOUTH STREET
 - PUBLIC STORM SEWER AND INLETS FOR LOCAL DRAINAGE WITHIN RIGHT OF WAY
- NANANTY GREENWAY**
- EXISTING IMPROVEMENTS**
- 8" RCP REGIONAL STORM SEWER
 - DRY UTILITY
- REGIONAL IMPROVEMENTS**
- PROPOSED 48" RCP STORM SEWER PARALLEL TO EXISTING (SUBJECT TO NOTE 8)
 - RECONSTRUCT 15" PVC SANITARY SEWER TO REPLACE EXISTING 15" VCP PIPE (DRY UTILITY)
- SHORT STREET**
- EXISTING IMPROVEMENTS**
- 8" RCP REGIONAL STORM SEWER INLETS AT HIGHWAY 42
 - 8" WATER LINE
 - DRY UTILITY
 - STREET PAVING

- REGIONAL IMPROVEMENTS**
- RECONSTRUCT STREET PAVING AND PEDESTRIAN WALKWAYS TO CREATE CANNON STREET WOODCHIEF
 - STORM SEWER IMPROVEMENTS
 - SIGNALIZED INTERSECTION AT HIGHWAY 42 (SEPARATE CITY PROJECT)
 - RELOCATION OF DRY UTILITY
 - REPLACEMENT OF 8" WATERLINE FROM BNP EAST ROW TO HIGHWAY 42
 - ABANDON IN PLACE EXISTING 24" RCP ACROSS HIGHWAY 42 AND RECONSTRUCT EXISTING BOX BASE MANHOLE
 - PROPOSED 48" RCP STORM SEWER PARALLEL TO EXISTING (SUBJECT TO VICINITY OF WEST ROW LINE OF HIGHWAY 42) SUBJECT TO NOTE 8)
 - PROPOSED 8" RCP STORM SEWER PARALLEL TO EXISTING FROM VICINITY OF WEST ROW LINE OF HIGHWAY 42 TO DAYLIGHT, ON THE EAST SIDE OF HIGHWAY 42. TO BE FUNDED BY THE CITY OF LOUISVILLE AND CONSTRUCTED BY DELO
 - REPLACEMENT OF EXISTING 8" WATER LINE FROM VICINITY OF BNP EAST ROW TO THE CONNECTION AT NORTH FRONT STREET (TO BE FUNDED BY THE CITY OF LOUISVILLE AND CONSTRUCTED BY DELO)
- TRACTS A AND C**
- EXISTING IMPROVEMENTS**
- 8" RCP REGIONAL STORM SEWER
 - DRY UTILITY
 - 12" VCP SANITARY TRUNK MAIN
- REGIONAL IMPROVEMENTS**
- PROPOSED 48" RCP STORM SEWER PARALLEL TO EXISTING (SUBJECT TO NOTE 8)
 - RECONSTRUCT 15" PVC SANITARY SEWER TO REPLACE EXISTING 15" VCP PIPE (DRY UTILITY)
- SOUTH STREET**
- EXISTING IMPROVEMENTS**
- FINAL STREET CROSS SECTION WITH WET AND DRY UTILITY
- REGIONAL IMPROVEMENTS**
- REPLACEMENT OF WALKWAY FROM CANNON STREET TO HIGHWAY 42
 - REPLACEMENT OF SANITARY SEWER FROM CANNON STREET TO WEST ROW AT HIGHWAY 42
 - MAIL AND OVERLAY FROM CANNON STREET TO THE WEST POB AT HIGHWAY 42
 - REPLACEMENT OF CURB AND GUTTER FOR THE NORTH 1/2 SECTION. IN USE OF A MILL AND OVERLAY AND CONCRETE SPUR CITY FURNISH. REMOVAL OF THE EXISTING PAVEMENT AND REPLACEMENT WITH FULL DEPTH ASPHALT SHALL OCCUR.
- CALEDONIA PLAZA**
- EXISTING IMPROVEMENTS**
- NOISE
- REGIONAL IMPROVEMENTS**
- LANDSCAPE, HARDSCAPE, ENTRY MONUMENTATION AND SIGNAGE

HORIZONTAL PUBLIC IMPROVEMENTS PLAN

GENERAL PHASING NOTES:

- RETAINING WALLS MAY BE REQUIRED FOR THE DEVELOPMENT OF THE PROJECT TO THE STANDARDS AND REQUIREMENTS OF THE CITY. DEVELOPER AND MARKET NEEDS, WALLS WITHIN PRIVATE TRACTS, ETC. SHALL BE ABANDONED TO 2' TO THE EXTENT PRACTICAL. IF FEASIBLE, PRIVATE WALLS SHALL BE TIERED. WALL COLOURS AND MATERIALS SHALL MATCH OR BE COMPLEMENTARY THROUGHOUT THE DELO DEVELOPMENT.
- CONSTRUCTION OF THE IMPROVEMENTS AND ALL UTILS MAY OCCUR IN ANY ORDER AND IS NOT LIMITED EXCEPT AS REQUIRED TO PROVIDE A LOOPEE WATER SERVICE, SANITARY SERVICE, TWO POINTS OF EMERGENCY ACCESS AND THAT NO "DEAD END" LONGER THAN 300' WITHOUT A SECONDARY EMERGENCY ACCESS SHALL OCCUR.
- IMPROVEMENTS ALONG AND WITHIN THE BNP RIGHT-OF-WAY FROM GERRITT STREET TO SOUTH STREET SHALL BE COORDINATED WITH AND SUPPORTED BY THE CITY FOR INCLUSION WITHIN THE CITY'S AGREEMENT WITH THE CITY TO ALLOW FOR CONSTRUCTION OF THE DEVELOPMENT PROJECTS. THESE ITEMS MAY INCLUDE BUT ARE NOT LIMITED TO: PAVING, CURBS, GUTTERS, AND TRAILER CONNECTIONS. THESE IMPROVEMENTS AND TRAILER CONNECTIONS MAY BE CONSTRUCTED WITHIN THE BNP RIGHT-OF-WAY OF THE DELO DEVELOPMENT. THE DEVELOPER WILL REIMBURSE THE CITY OF ANY DELO SPECIFIC FEES ASSOCIATED WITH THE BNP RIGHT-OF-WAY. ALL IMPROVEMENTS WILL BE BASED OFF THE FINAL PUD AND DETAILED WITHIN THE APPROVED CONSTRUCTION DOCUMENTS.
- PRIVATE IMPROVEMENTS SHALL NOT BE INCLUDED WITHIN THE DELO SUBDIVISION AGREEMENT.
- THE DEVELOPMENT IS NOT RESPONSIBLE FOR ANY MARKET'S FIELD IMPROVEMENTS.
- ALL IMPROVEMENTS DEPICTED HEREIN ARE SUBJECT TO FINAL AGREEMENT WITHIN PROJECTS SUBDIVISION AGREEMENT AND APPROVED CONSTRUCTION DOCUMENTS.
- THE CITY FUNDED IMPROVEMENTS (SUBJECT TO FUNDING BY THE CITY) LOCATED WITHIN THE BNP RIGHT OF WAY OR HIGHWAY 42 RIGHT OF WAY AND THE ASSOCIATED REQUIRED PERMITTING SHALL NOT DELAY THE DELO PHASE PROJECT.

DETENTION AND WATER QUALITY PHASING NOTES:

- A TEMPORARY DETENTION AND WATER QUALITY FACILITY SHALL BE CONSTRUCTED ON-SITE TO SERVICE DELO IN THE EVENT THAT AN OFF-SITE FACILITY TO SERVICE THIS PHASE OF THE PROJECT IS NOT AVAILABLE. THE DESIGN AND SPECIFICS SHALL BE PRESENTED WITHIN THE SUBSEQUENT CONSTRUCTION DOCUMENTS.
- IN THE EVENT THAT AN OFF-SITE FACILITY TO SERVICE THIS PROJECT IS NOT AVAILABLE AT THE TIME OF THE DEVELOPMENT A TEMPORARY OR PERMANENT DETENTION AND WATER QUALITY FACILITY SHALL BE CONSTRUCTED ON-SITE TO SERVICE DELO. THE POND MAY BE CONSTRUCTED IN A PERMANENT CONFIGURATION IN THE EVENT THAT THE TIERING OF THE OFF-SITE FACILITY IS NOT ADVANTAGEOUS TO THE DELO DEVELOPMENT. THE DESIGN AND SPECIFICS SHALL BE PRESENTED WITHIN THE SUBSEQUENT PHASE 2 CONSTRUCTION DOCUMENTS.
- THE FINAL PLANNED UNIT DEVELOPMENT SHALL PLAN FOR AND THE SUBSEQUENT CONSTRUCTION DOCUMENTS SHALL EVALUATE AND DESIGN THE REQUIRED DETENTION AND WATER QUALITY VOLUME FOR THAT PHASE. AT THAT TIME, SHOULD THE OFF-SITE REGIONAL DRAINAGE FACILITY NOT BE COMPLETED, A DETERMINATION TO DESIGN AND CONSTRUCT AN ON-SITE PERMANENT ON-SITE FACILITY SHALL OCCUR.
- IN THE DEVELOPMENT OF THE OFF-SITE DRAINAGE FACILITY TO SERVICE THE REDEVELOPMENT AREA AND THE DELO PROJECT, THE TEMPORARY DETENTION AND WATER QUALITY FACILITY SHALL BE REMOVED AND THE DEVELOPMENT OF A PUBLIC GREENWAY SHALL OCCUR.



sheet title

Horizontal Public Improvements Plan

submittal

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

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7 of 34

DELO 2 PUD
8 OF 34

DRAWING NUMBER

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de lo Final Planned Unit Development Phase 2

Downtown East Louisville

A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado

SUGGESTED PLANT PALETTE

COMMON NAME	COMMON NAME	COMMON NAME	COMMON NAME
DECEDEOUS TREES ACER FREEMAN 'AUTUMN BLAZE' CELEBRE OCCIDENTALIS GLEDETTIA TRICANONIS 'NIPERIS' PLATANUS ACERIFOLIA 'BLOODGOOD' GUEBOS MACROCARPA SPYRACA JAPONICA 'HALA' TILIA AMERICANA 'REDMOND' TILIA CORDATA 'GREENPINE' ULMUS X 'TRICHOPY' * Not to be used as a street tree.	EVERGREEN TREES JUNIPERUS CHINENSIS 'SPARTAN' JUNIPERUS SCOPULORUM 'GRAY CLEAM' JUNIPERUS SCOPULORUM 'MIDORA' PICEA PUNGENS 'GLAUCA' PICEA PUNGENS 'COLORADO GREEN' PRINUS FELIX 'ANDERWOLF'S PYRAMID' PRINUS MUGO	ORNAMENTAL TREES ACER GRIBALTA 'FLAME' ACER LASARICUM AMELANCHIER CANADENSIS 'AURUM BRILLIANCE' CORNAEGUS CRIG-GALLI 'BENNE' CORYLUS X 'MONSIEUR TONIA' MALUS X 'BRANDYWINE' MALUS X 'SPRING SNOW' PRUNUS CEAESAREA 'NEWPORT' PRUNUS VIRGINIANA 'CANADA RED' PRUNUS CALLENTANA 'AUTUMN BLAZE' PRUNUS CALLENTANA 'CHARNIEREE'	ANNUALS/PERENNIALS ACHILLEA LAUREA ACHILLEA X 'MOONBINE' AGASTACHE X 'COBANADO RED' ANTENNARIA LYCOPHYLLA 'VALERIE FINNS' CONYSEUS RIBES 'AUBREY CRICK' COROPHOS VERICOLLATA 'MOONBEAM' COROPHOS X 'SERENA SUNSET' DALEA PURPUREA DELICOSPITHA COOPERI ECHINACIA PURPUREA 'MARGES' ERIGERON SPECIOSUS GALLARDA ABIGATA HEMEROCALLIS X 'STELLA DE ORO' HEUCHERA X 'CHOCOLATE RUFFET' HOSIA X 'LEADING LIGHT' HYPERICUM PERFORATUM RIG. SERICIA LAVANDULA ANGLUSTROIA 'MUNDEAD' LUCIDANTHEMUM X 'SUPERBUM' LUCIDANTHEMUM X 'SUPERBUM' PENTSTEMON PINIFOLIUS RUDEBECKIA FULGIDA 'GOLDSTURM' SALVIA X 'STAYGREEN' MAY NIGHT' SEDUM X 'AURUM JOY'

ITEMIZATION OF LANDSCAPE

PANEL	LANDSCAPE TOTAL	HARDSCAPE	%	SOFTSCAPE	%	NON IRRIGATED	%	HOA	CITY
								WATER USAGE GAL/YR	WATER USAGE GAL/YR
CANNON ST. ROW	20,295.39 SF	20,295.39 SF	87%	2,638.28 SF	13%	0 SF	0%	35,544 Gal./Year	0 Gal./Year
SHORT ST. ROW	6,646.45 SF	1,765.11 SF	27%	861.34 SF	13%	0 SF	0%	13,220 Gal./Year	0 Gal./Year
TRACT B (LAURETTI LANE ROW)	6,823.03 SF	33.33 SF	0%	6,502.70 SF	96%	0 SF	0%	97,526 Gal./Year	0 Gal./Year
BLOCK S (TOWNHOMES)	2,117.77 SF	62.47 SF	3%	1,122.86 SF	48%	1,113.28 SF	48%	34,828 Gal./Year	0 Gal./Year
MAHONY GREENWAY	1,222.76 SF	6,624.93 SF	100%	0 SF	0%	0 SF	0%	47,253 Gal./Year	47,253 Gal./Year
BLOCK 1	41,088.97 SF	14,862.00 SF	36%	23,700.30 SF	58%	2,726.67 SF	7%	35,505 Gal./Year	0 Gal./Year
BLOCK 4	10,348.76 SF	7,917.42 SF	77%	2,291.46 SF	22%	95.88 SF	1%	34,402 Gal./Year	0 Gal./Year
OUTPUT A (CALEDONIA PLAZA)	1,962.07 SF	6,130.73 SF	46%	1,315.34 SF	34%	0 SF	0%	48,675 Gal./Year	48,675 Gal./Year
Total	144,754.19 SF	55,654.89 SF	38%	85,116.50 SF	59%	3,982.80 SF	3%	201,519.60 Gal./Year	719,723.40 Gal./Year

1 MASTER LANDSCAPE PLAN
SCALE: 1" = 100'



LANDSCAPE PLAN

- ### LANDSCAPE KEY
- CANOPY TREES
 - ORNAMENTAL TREES
 - EVERGREEN TREES
 - SOD
 - ROCK MULCH
 - WOOD MULCH
 - EDGER - PERFORATED

GENERAL NOTES

- FINAL CONSTRUCTION DOCUMENTS SHALL CONFORM TO THE REQUIREMENTS OUTLINED WITHIN THE PUD SUBMITTAL.
- LANDSCAPE PLANS ARE SCHEMATIC IN NATURE AND ARE SUBJECT TO MODIFICATIONS TO MEET THE CITY'S REQUIREMENTS, THE DEVELOPER'S PROGRAM, THE BUILDINGS ARCHITECTURE AND TARGET DEMOGRAPHIC, OR OTHER NECESSARY REQUIREMENTS.
- THE LOCATION OF LANDSCAPE PLANTINGS MAY BE ALTERED TO PROVIDE ADEQUATE CLEARANCE FROM THE FINAL LOCATION OF UNDERGROUND UTILITIES. THE BASE OF DECEDEOUS TREES SHALL BE PLANTED NO CLOSER THAN 7' FROM ALL WET UTILITIES UNLESS OTHERWISE DIRECTED BY THE CITY.
- GRASS AREAS DESIGNATED AS IRRIGATED BURN SHALL BE SEEDED OR SOODED WITH A DROUGHT TOLERANT GRASS MIXTURE.
- THE SIZE OF DECEDEOUS TREES SHALL BE A MINIMUM OF 2 1/2" CALIPER AND THE SIZE OF EVERGREEN TREES SHALL BE A MINIMUM OF 6" IN HEIGHT.
- THE SIZE OF DECEDEOUS AND EVERGREEN SHRUBS SHALL BE A MINIMUM OF 3 GALLONS.
- THE QUANTITY AND LOCATION OF LANDSCAPE AND HARDSCAPE ELEMENTS DEPICTED MAY BE ALTERED WITHIN THE CONSTRUCTION DOCUMENTS.
- WHENEVER POSSIBLE, MECHANICAL DEVICES SHALL BE SCREENED WITH LANDSCAPE MATERIAL.
- SITE MONITORING AND AMENITIES, EXCLUDING PUBLIC ART, THROUGHOUT THE DEVELOPMENT WILL BE CONSTRUCTED IN CONJUNCTION WITH ASSOCIATED ADJACENT IMPROVEMENTS.
- LANDSCAPE IMPROVEMENTS SHALL BE DESCRIBED AND INSTALLED WITH THE INTENT TO PRESERVE THE PEDESTRIAN EXPERIENCE WHILE ENSURING PUBLIC SAFETY AND MAINTAINING A HIGH AESTHETIC QUALITY WITHIN THE SITE WHILE COMPLYING WITH THE INTENT SET FORTH WITHIN THE MDCD.
- PLANT DIVERSITY SHOULD BE CONSIDERED WHEN SELECTING STREET TREES. THE MAXIMUM PERCENTAGE OF ANY ONE TREE SPECIES ON ONE SIDE OF THE STREET EXCEEDS 30%.
- EACH STREET TREE SHALL BE IRRIGATED VIA DRIP IRRIGATION OR A DRIP IRING EMITTER.
- COMMON AREAS - ONE TREE AND THREE SHRUBS PER 2,000 SQ FT OF SOFTSCAPE OR A LOW WATER PLANT PALETTE IS PROPOSED. ADDITIONAL PLANTS MAY BE PROPOSED IN COMMON AREAS.
- POSTS AND PLANTERS ARE ENCOURAGED AND SHALL COUNT TOWARDS THE MINIMUM LANDSCAPE COVERAGE CALCULATIONS.
- ANNUAL AND PERENNIAL PLANTINGS SHALL NOT BE USED ON CITY OWNED PARCELS WITHOUT PRIOR APPROVAL FROM THE PARKS DEPARTMENT.
- THE HOA OR ASSOCIATED MANAGEMENT COMPANY IS RESPONSIBLE FOR RESTORATION TO DAMAGED PRIVATE IMPROVEMENTS LOCATED WITHIN THE CITY OF LOUISVILLE EXCLUSIVE OFYR TAXPAYERS DUE TO OPERATOR AND MAINTENANCE, REPAIR AND REPLACEMENT BY THE CITY.
- CITY FORFEITS TO SELECT/QUALIFY TYPE OF DECEDEOUS TREES TO BE PLANTED NEAR (2.5+ FT) CURB AND WALK.
- THE DEVELOPER SHALL BE RESPONSIBLE FOR THE PHYSICAL COST OF THE BRICKGAP PLAN, WATER METER FEE, YIELD AND WATER USAGE FEE SHALL BE WAIVED AND THE CITY'S RESPONSIBILITY.
- PER THE SUBMITTAL AGREEMENT ALL LANDSCAPE SHOWN ON THESE PLANS SHALL BE MAINTAINED AT A WEAT AND ADEQUATE MAINTENANCE. DUE COMMERCIAL HOA OR APARTMENT WILL BE RESPONSIBLE FOR LANDSCAPE MAINTENANCE, REPAIR, REMOVAL AND SNOW REMOVAL OF CURBS AND E.O.W'S. FURTHERMORE, SPECIALTY ITEMS WITHIN CURBS OR E.O.W'S THAT REQUIRE CUSTOM FABRICATION MAY BE SUBSTITUTED OR REPLACED AT THE CITY'S DISCRETION.

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checked by: PMS project #: 030009

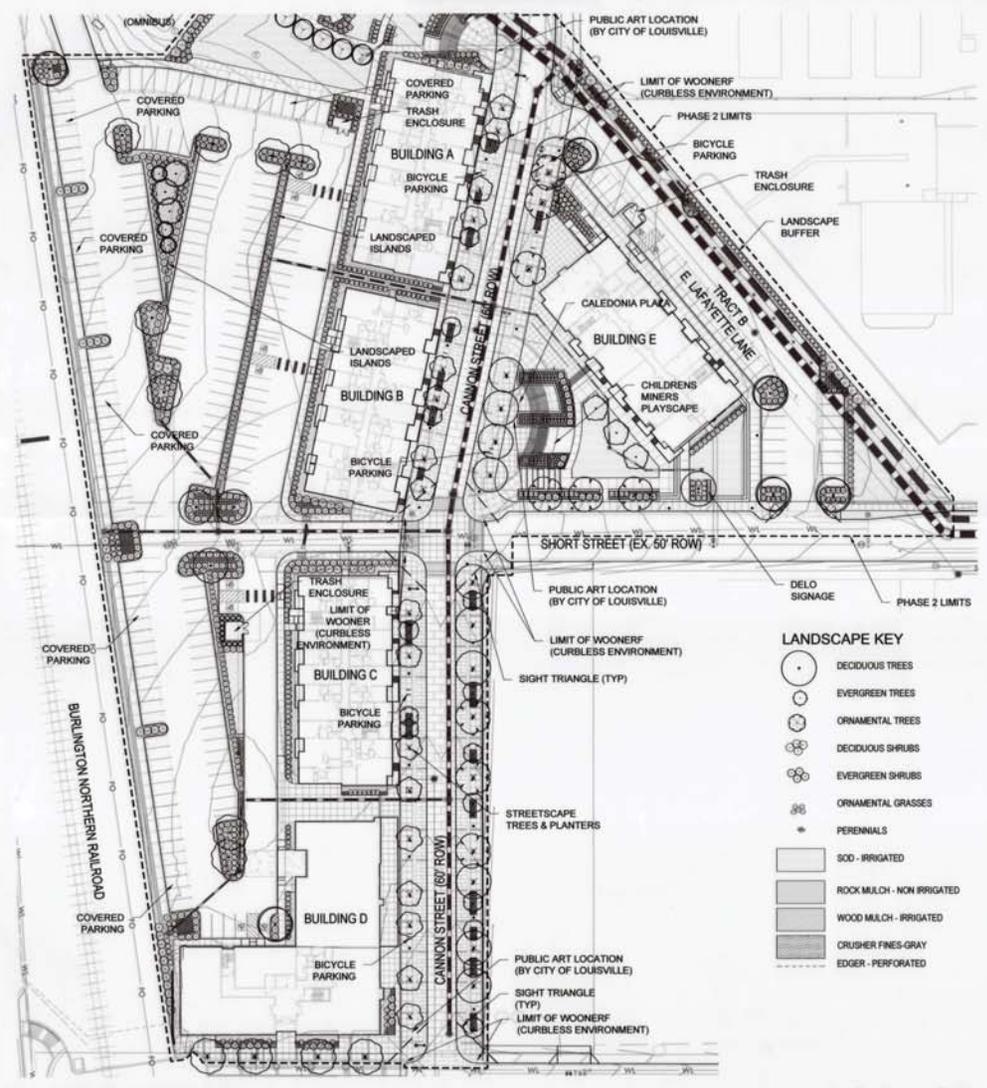
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8 of 34

delo Final Planned Unit Development Phase 2

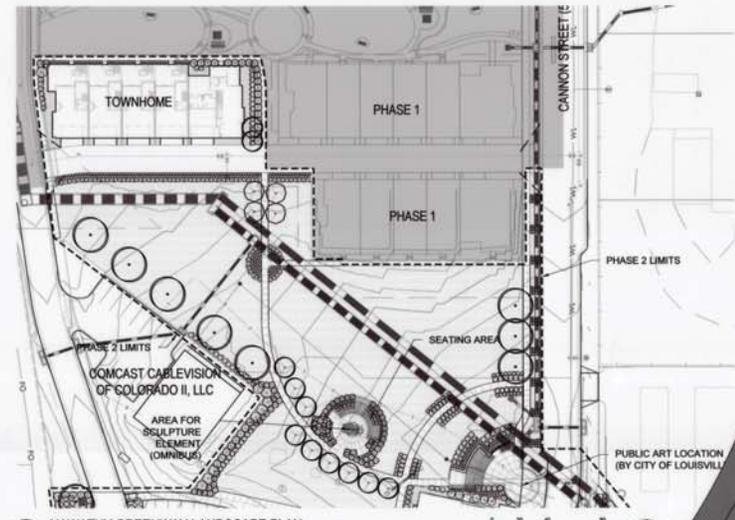
DowntownEastLouisville
A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado



1 SOUTH END LANDSCAPE PLAN
SCALE: 1" = 40'

- LANDSCAPE KEY**
- DECIDUOUS TREES
 - EVERGREEN TREES
 - ORNAMENTAL TREES
 - DECIDUOUS SHRUBS
 - EVERGREEN SHRUBS
 - ORNAMENTAL GRASSES
 - PERENNIALS
 - SOIL - IRRIGATED
 - ROCK MULCH - NON IRRIGATED
 - WOOD MULCH - IRRIGATED
 - CRUSHER FINES-GRAY
 - EDGER - PERFORATED

SCALE: 1" = 40'



2 NAWATNY GREENWAY LANDSCAPE PLAN
SCALE: 1" = 40'

SCALE: 1" = 40'



sheet title
Detailed Landscape Plan

submittal		
no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

designed by: SA drawn by: KLM
checked by: PMS project #: 030009

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tel (303) 531-4905
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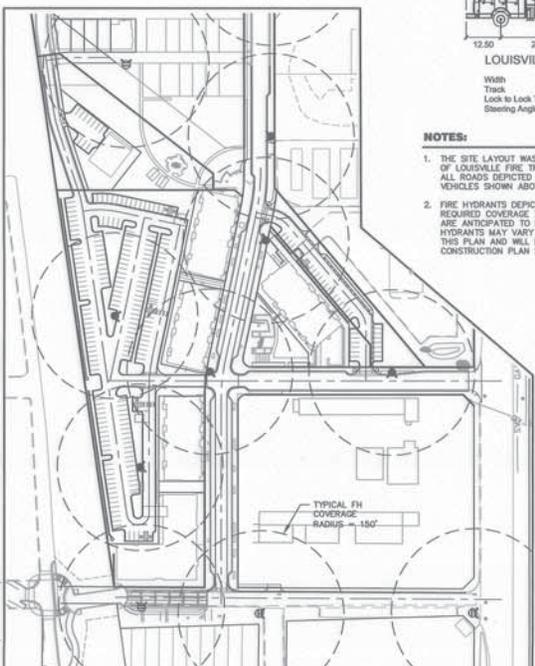
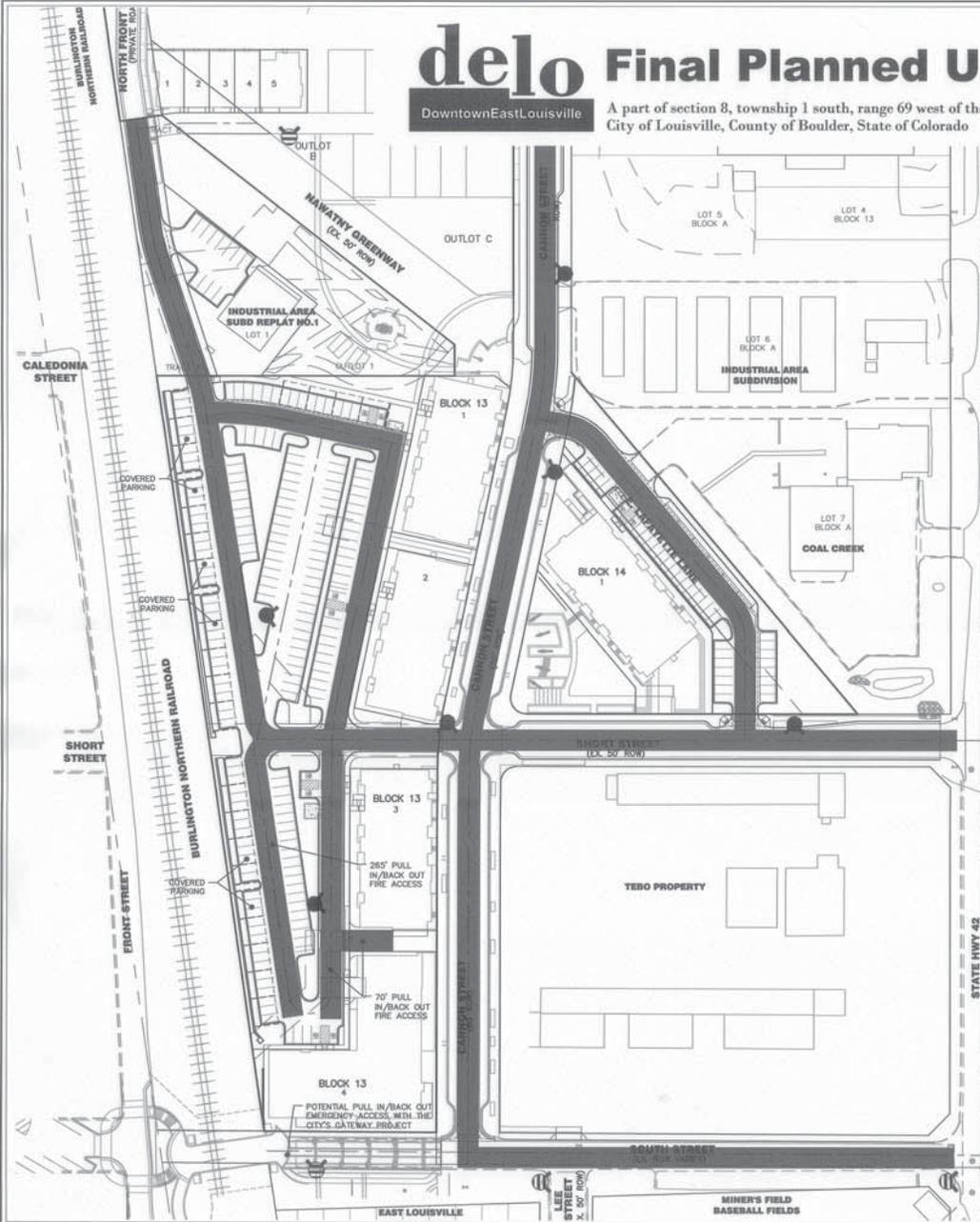
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Downtown East Louisville
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City of Louisville, County of Boulder, State of Colorado



PHASE 2 FIRE HYDRANT COVERAGE
(Scale: 1" = 100')



LOUISVILLE TRUCK 17
feet
Width : 8.00
Track : 8.00
Lock to Lock Time : 5.00
Steering Angle : 45.00

- NOTES:**
1. THE SITE LAYOUT WAS EVALUATED FOR ACCESSIBILITY FOR THE CITY OF LOUISVILLE FIRE TRUCK UTILIZING THE PROGRAM AUTOTURN FOR ALL ROADS DEPICTED IN GRAY. THOSE AREAS ACCOMMODATE THE VEHICLES SHOWN ABOVE.
 2. FIRE HYDRANTS DEPICTED ARE TO PROVIDE THE ANTICIPATED REQUIRED COVERAGE TO SERVICE THE SITE. ALL PRODUCT TYPES ARE ANTICIPATED TO BE SPRINKLED. FINAL LOCATION OF THE FIRE HYDRANTS MAY VARY SLIGHTLY FROM INFORMATION DEPICTED ON THIS PLAN AND WILL BE DETERMINED WITH SUBSEQUENT CONSTRUCTION PLAN SUBMITTALS.

1" = 50'
scale north

sheet title
Emergency Vehicle Access Plan

submittal		
no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-03-2015	third submittal
4	09-01-2015	final submittal

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- LEGEND**
- Emergency Vehicle Access Route (Louisville Truck & Truck 17)
 - Emergency Vehicle Access Route (Louisville Truck)
 - Proposed Fire Hydrant Location
 - Existing Fire Hydrant Location

delo Final Planned Unit Development Phase 2

Downtown East Louisville

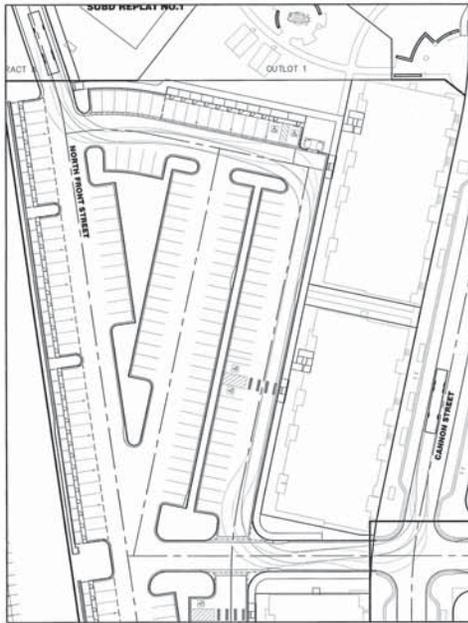
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City of Louisville, County of Boulder, State of Colorado

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11 OF 34

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER



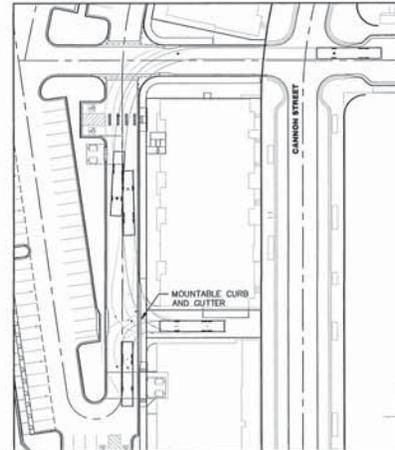
① - FIRE TRUCK 17



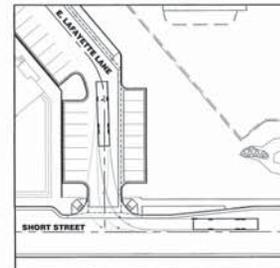
② - FIRE TRUCK 17



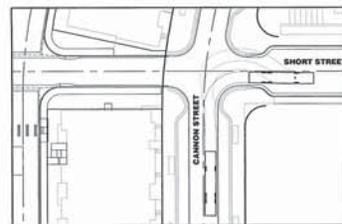
③ - FIRE TRUCK 17



④ - FIRE TRUCK 17



⑤ - FIRE TRUCK 17



⑥ - FIRE TRUCK 17



⑦ - FIRE TRUCK 17



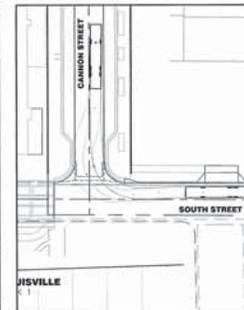
⑧ - FIRE TRUCK 17



⑨ - FIRE TRUCK 17

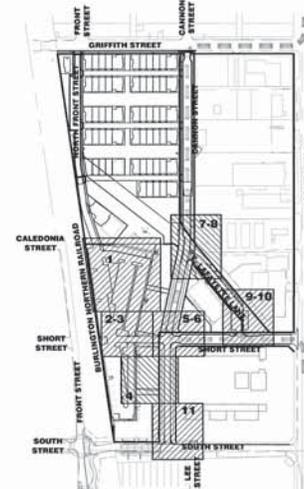


⑩ - FIRE TRUCK 17



⑪ - FIRE TRUCK 17

KEYMAP (1" = 200')



LEGEND

- VEHICLE LADDER/BUMPER OVERHANG
- VEHICLE FRONT TIRES
- VEHICLE REAR TIRES

NOTE:
INCREASED CONCRETE THICKNESS OF 8" IS REQUIRED FOR THE CROSSSPAN/GUTTER PAN AT CANNON STREET AND THE DEPRESSION WALK AND GUTTER PAN AT NORTH FRONT STREET. THE ADDITIONAL CONCRETE THICKNESS SHALL BE SPECIFIED WITHIN THE SUBSEQUENT CONSTRUCTION DOCUMENTS.



sheet title
**Turning
Movement Details**

submittal

no	date	description
1	06-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

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11
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34

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Downtown East Louisville

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City of Louisville, County of Boulder, State of Colorado

LEGEND

CENTER LINE OF STREET
PROPERTY BOUNDARY LINE
RIGHT OF WAY LINE
LOT LINE
EASEMENT LINE

EX. CURB, GUTTER, CROSSSPAN
SIDEWALK & RAMP

CURB, GUTTER, CROSSSPAN
SIDEWALK & RAMP

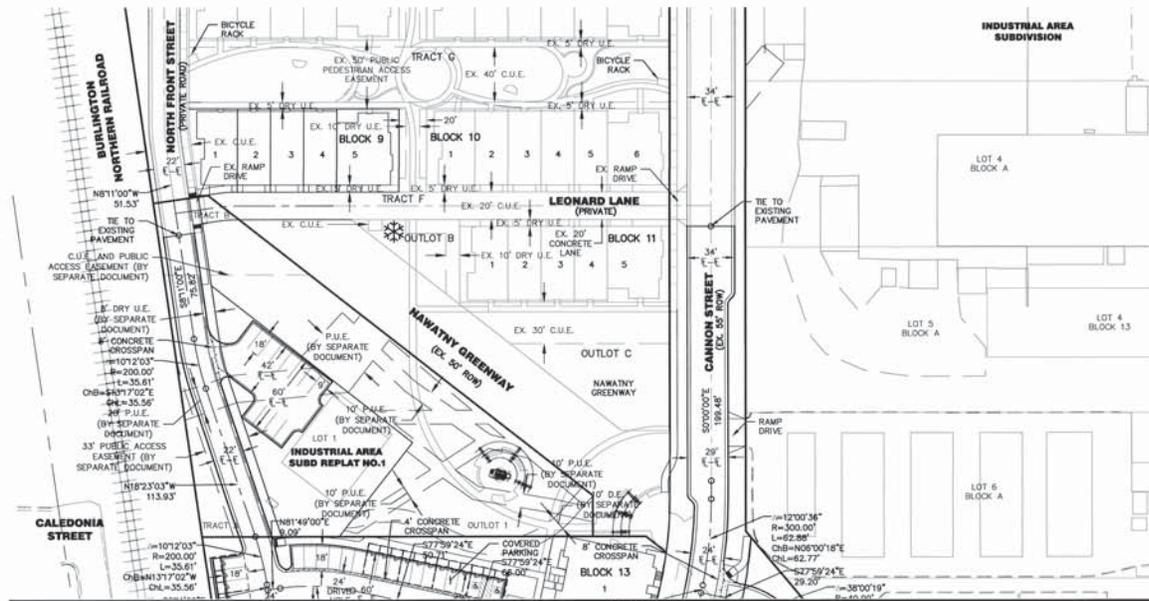
POTENTIAL SNOW STORAGE LOCATION

NOTES

- DESIGNS FOR GREENSPACE AND GREENWAYS ARE CONCEPTUAL AND ARE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. THE FINAL DESIGN FOR THESE AREAS WILL BE PRESENTED IN SUBSEQUENT SUBMITTALS AND CONSTRUCTION DOCUMENTS.
- GREENSPACE IMPROVEMENTS WILL BE DESIGNED TO ACCOMMODATE ACCESS FOR THE CONTINUED MAINTENANCE OF EXISTING UTILITIES WITHIN THE VACATED EAST LAFAYETTE STREET RIGHT-OF-WAY.
- THE HORIZONTAL LAYOUT PRESENTED IN THIS PLAN IS A REPRESENTATION OF THE DESIGN INTENT FOR THE DELO DEVELOPMENT. SPECIFIC ELEMENTS ARE SUBJECT TO REVISION IN ORDER TO ACCOMMODATE UNFORESEEN CIRCUMSTANCES AND CHANGING PROJECT CONDITIONS.
- THE WOONERF DESIGN PROPOSED FOR CANNON STREET IS A REPRESENTATION OF THE DESIGN INTENT. SPECIFIC ELEMENTS ARE SUBJECT TO MODIFICATION WITH THE SUBSEQUENT CONSTRUCTION PLAN DOCUMENTS.
- CANNON STREET RIGHT-OF-WAY SHALL BE DEDICATED VIA THE FINAL DELO SUBDIVISION RELAT NO. 1 OR SHALL BE OBTAINED VIA SEPARATE INSTRUMENT TO ENSURE THE REQUIRED IMPROVEMENT TIMING TO SUPPORT THE DELO PHASE 2 DEVELOPMENT.
- THE REQUIRED ACCESS AND CONSTRUCTION EASEMENTS THROUGH THE COMCAST PARCEL SHALL BE OBTAINED PRIOR TO THE EXTENSION OF NORTH FRONT STREET. HOWEVER, THE EXTENSION SHALL NOT LIMIT THE TIMING OF THE PHASE 2 CONSTRUCTION SO LONG AS TWO POINTS OF EMERGENCY ACCESS ARE PROVIDED.

ABBREVIATIONS

- C.U.E. = CITY EXCLUSIVE UTILITY EASEMENT
P.U.E. = PRIVATE DRAINAGE EASEMENT
ROW = RIGHT-OF-WAY
E = FLOWLINE



SEE SHEET 13



sheet title

Horizontal Control Plan

submittal

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

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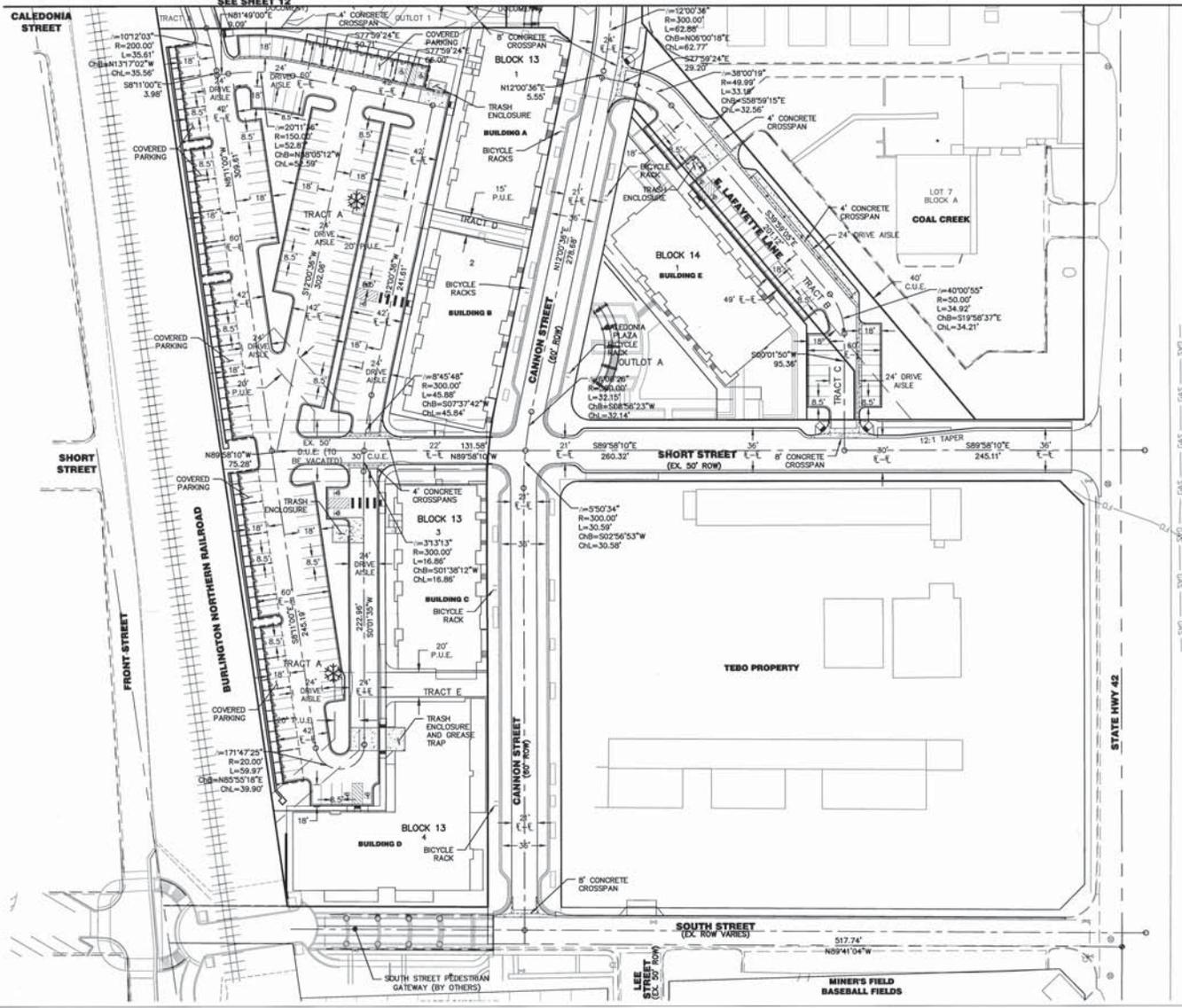
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delo Final Planned Unit Development Phase 2

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City of Louisville, County of Boulder, State of Colorado



LEGEND

- CENTER LINE OF STREET
- PROPERTY BOUNDARY LINE
- RIGHT OF WAY LINE
- LOT LINE
- EASEMENT LINE

- ### NOTES
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ABBREVIATIONS

C.U.E. = CITY EXCLUSIVE UTILITY EASEMENT
P.U.E. = PRIVATE DRAINAGE EASEMENT
ROW = RIGHT-OF-WAY
E = FLOWLINE



sheet title

Horizontal Control Plan

submittal

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

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sheet
13
of
34

DELO 2 PUD
13 OF 34

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER

DELO 2 PUD
14 OF 34

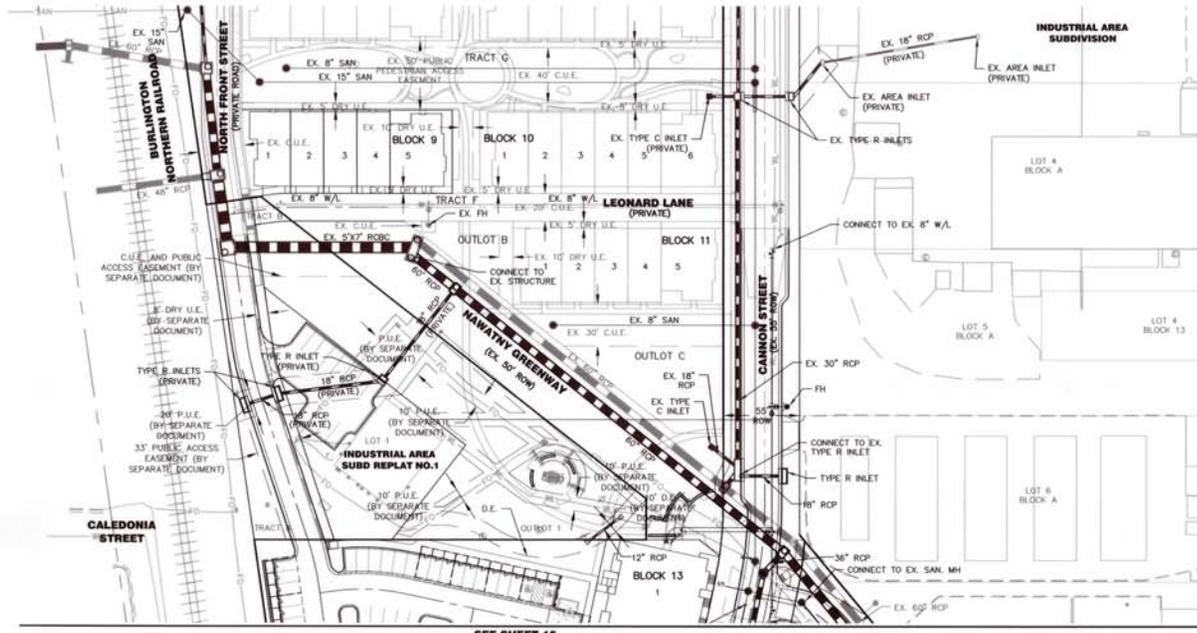
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DRAWING NUMBER

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Downtown East Louisville A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado



SEE SHEET 15

LEGEND

- CENTER LINE OF STREET
- PROPERTY BOUNDARY LINE
- RIGHT OF WAY LINE
- LOT LINE
- EASEMENT LINE
- WATERLINE W/ GATE VALVE & TEE
- SANITARY SEWER W/ MANHOLE
- STORM SEWER W/ INLET & MANHOLE
- EX. STORM SEWER W/ INLET & MANHOLE
- EX. WATERLINE W/ VALVE & TEE
- EX. SANITARY SEWER W/ MANHOLE
- EX. OVERHEAD ELECTRICITY LINE
- EX. FIBER OPTIC LINE
- EX. UNDERGROUND ELECTRICITY LINE
- EX. GAS LINE
- EX. TELEPHONE LINE
- EX. FENCE LINE
- EX. CURB, GUTTER, CROSSSPAN SIDEWALK & RAMP
- CURB, GUTTER, CROSSSPAN SIDEWALK & RAMP

NOTES

- THE UTILITY LAYOUT PRESENTED IN THIS PLAN IS A REPRESENTATION OF THE DESIGN INTENT FOR THE DEVELOPMENT. SPECIFIC ELEMENTS ARE SUBJECT TO REVISION IN ORDER TO ACCOMMODATE UNFORSEEN CIRCUMSTANCES AND CHANGING PROJECT CONDITIONS.
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ABBREVIATIONS

- RCP = REINFORCED CONCRETE PIPE
- W/L = WATER LINE
- SAN = SANITARY SEWER
- C.U.E. = CITY EXCLUSIVE UTILITY EASEMENT
- P.U.E. = PRIVATE DRAINAGE EASEMENT



sheet title
Overall Utility Plan

submittal		
no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

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checked by: JDM project #: 030017

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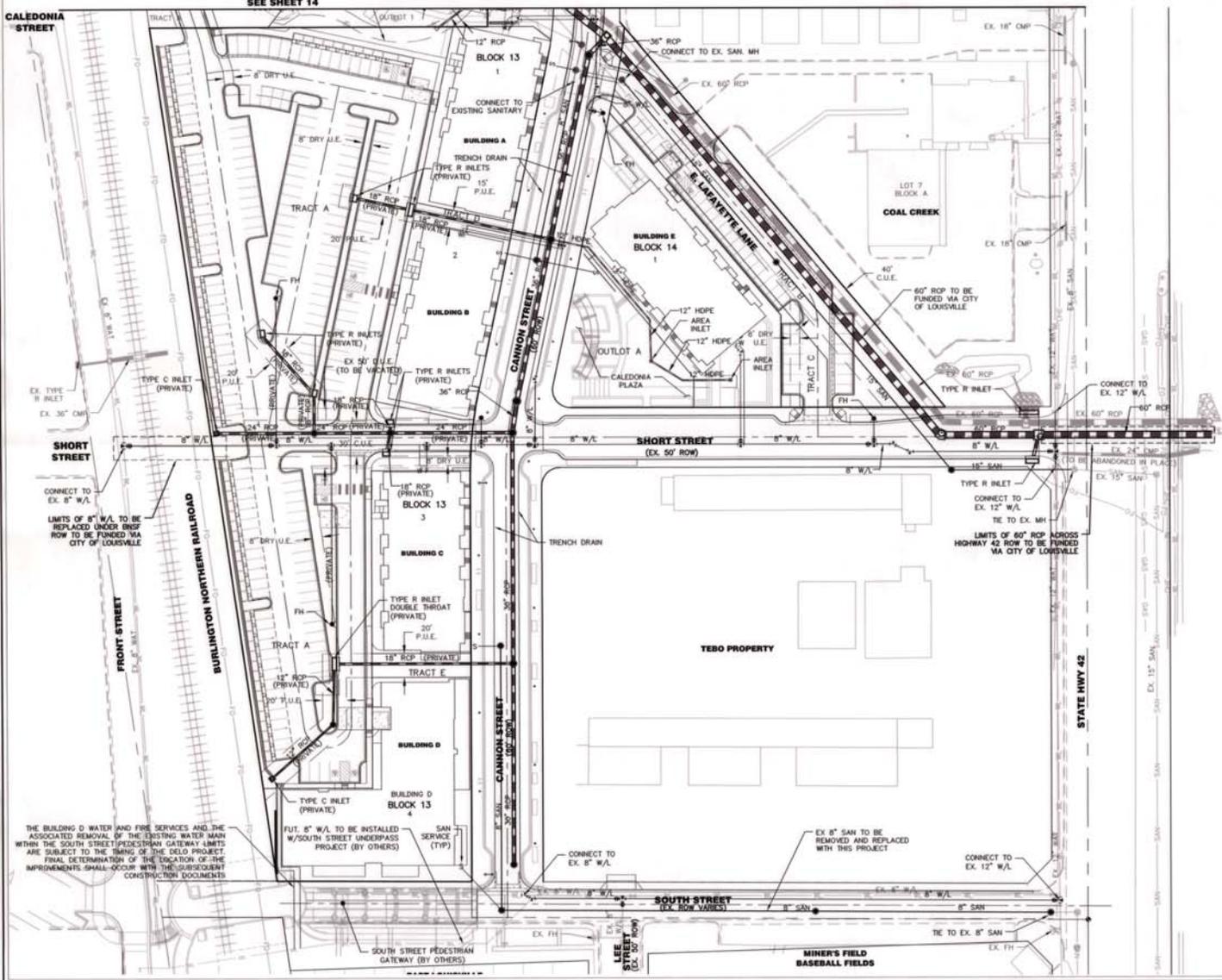
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Downtown East Louisville

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City of Louisville, County of Boulder, State of Colorado



LEGEND

- CENTER LINE OF STREET
- PROPERTY BOUNDARY LINE
- RIGHT OF WAY LINE
- LOT LINE
- EASEMENT LINE
- WATERLINE W/ GATE VALVE & TEE
- SANITARY SEWER W/ MANHOLE
- STORM SEWER W/ INLET & MANHOLE
- EX. STORM SEWER W/ INLET & MANHOLE
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- EX. UNDERGROUND ELECTRICITY LINE
- EX. GAS LINE
- EX. TELEPHONE LINE
- EX. FENCE LINE

EX. CURB, GUTTER, CROSSSPAN
SIDEWALK & RAMP

- ### NOTES
1. THE UTILITY LAYOUT PRESENTED IN THIS PLAN IS A REPRESENTATION OF THE DESIGN INTENT FOR THE DEVELOPMENT. SPECIFIC ELEMENTS ARE SUBJECT TO REVISION IN ORDER TO ACCOMMODATE UNFORSEEN CIRCUMSTANCES AND CHANGING PROJECT CONDITIONS.
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- ### ABBREVIATIONS
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 - W/L = WATER LINE
 - SAN = SANITARY SEWER
 - C.U.E. = CITY EXCLUSIVE UTILITY EASEMENT
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sheet title
Overall Utility Plan

submittal

no	date	description
1	05-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

designed by: SOS drawn by: SCD
checked by: JDM project #: 030017

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15
of
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16 OF 34

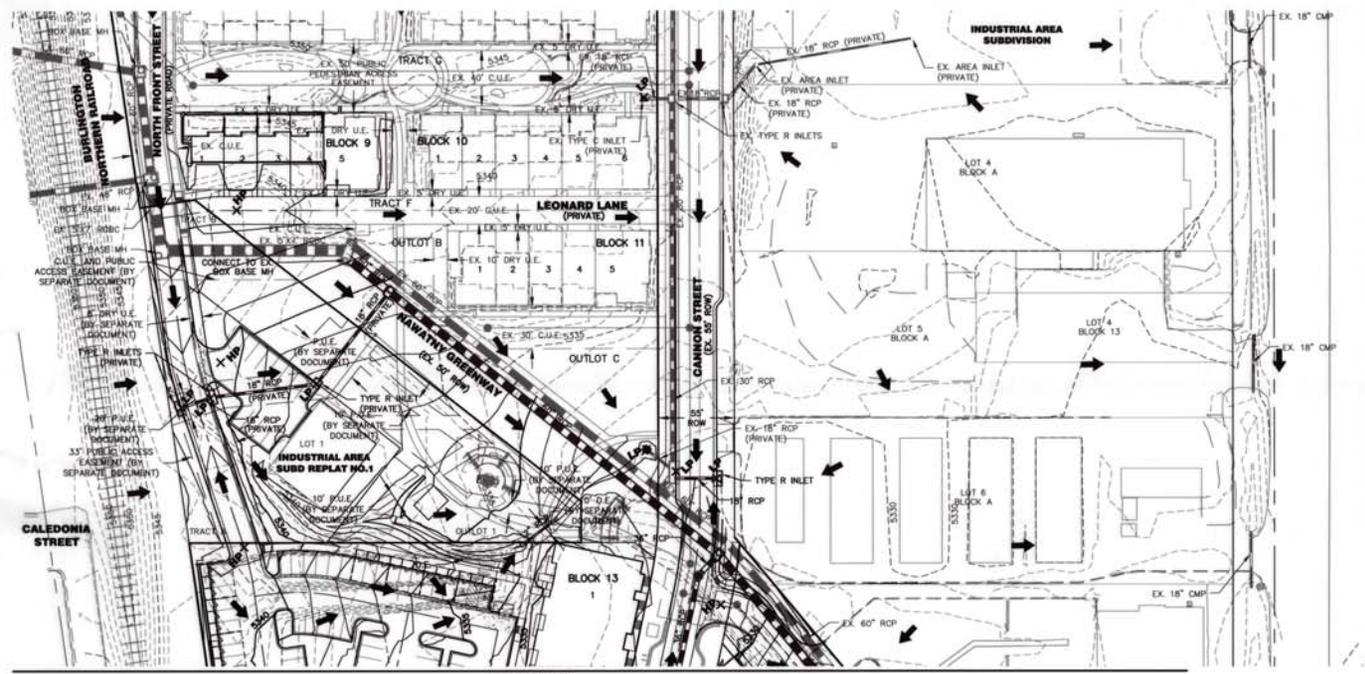
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DRAWING NUMBER

DRAWING NUMBER

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Downtown East Louisville A part of section 8, township 1 south, range 69 west of the 6th p.m. City of Louisville, County of Boulder, State of Colorado



SEE SHEET 17

LEGEND

- CENTER LINE OF STREET
- PROPERTY BOUNDARY LINE
- RIGHT OF WAY LINE
- LOT LINE
- EASEMENT LINE
- - - PROPOSED CONTOURS 5000
- - - EXISTING CONTOURS 5200
- xHP / xLP HIGH POINT / LOW POINT
- FLOW DIRECTIONAL ARROW
- STORM SEWER W/ INLET & MANHOLE
- EX. STORM SEWER W/ INLET & MANHOLE
- EX. FENCE LINE
- EX. CURB, GUTTER, CROSSSPAN, SIDEWALK & RAMP
- CURB, GUTTER, CROSSSPAN, SIDEWALK & RAMP

ABBREVIATIONS

- RCP = REINFORCED CONCRETE PIPE
- LP = LOW POINT
- HP = HIGH POINT
- U.E. = UTILITY EASEMENT
- C.U.E. = CITY EXCLUSIVE UTILITY EASEMENT
- P.U.E. = PRIVATE UTILITY EASEMENT

1" = 40' scale
north

sheet title
Overall Grading Plan

submittal		
no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

designed by: SOS drawn by: SCD
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16 of **34**

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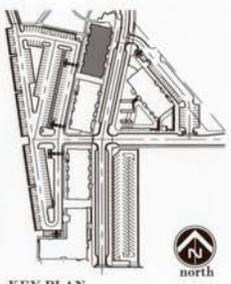
DELO BUILDING A - NORTH ELEVATION
1/8" = 1'-0"



PERSPECTIVE



DELO BUILDING A - EAST ELEVATION
1/8" = 1'-0"



KEY PLAN

DELO PHASE II BUILDING MATERIAL PALETTE

- L51 CEMENT LAP SIDING OR STUCCO
- L52 CEMENT LAP SIDING OR STUCCO
- M51 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- M52 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- M53 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- M54 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- W51 HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN
- BR1 BRICK, RUNNING BOND
- BR2 BRICK, RUNNING BOND
- SN1 STONE VENEER
- SS1 STANDING SEAM METAL SIDING / ROOF
- SS2 STANDING SEAM METAL SIDING / ROOF
- W01 ALUMINUM CLAD WINDOW FRAME
- W02 VINYL GLAZED WINDOW FRAME
- SP1 SPANDREL PANEL
- ST1 ALUMINUM STOREFRONT SYSTEM
- TP1 PAINTED ACCENT TRIM
- MB1 PAINTED FAUX METAL EXPOSED BEAM
- GR1 POWDERCOATED METAL GUARDRAIL

NOTE: ALL MATERIALS & COLORS FROM THE DELO PHASE II BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION PROCESS.

THE ARCHITECT WILL VARY THE COLORS ON THE EXTERIOR BALCONIES OF THE RESIDENTIAL BUILDINGS TO CREATE VISUAL INTEREST BY PROPOSING TO USE COMPLEMENTARY COLORS SELECTED OUT OF THE MATERIAL PALETTE FOR EACH BUILDING. THE ACTUAL COLOR ASSIGNMENT WILL BE FINALIZED IN SUBSEQUENT BUILDING CONSTRUCTION DOCUMENTS.



scale north

sheet title

Architectural Elevations

submittal

no	date	description
1	05-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal

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www.pcsgroupco.com

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18 of 34

de lo Final Planned Unit Development Phase 2

Downtown East Louisville
A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado



DELO PHASE II BUILDING MATERIAL PALETTE

LS1 CEMENT LAP SIDING OR STUCCO	SS1 STANDING SEAM METAL SIDING / ROOF
LS2 CEMENT LAP SIDING OR STUCCO	SS2 STANDING SEAM METAL SIDING / ROOF
MS1 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL	W01 ALUMINUM CLAD WINDOW FRAME
MS2 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL	W02 VINYL CLAD WINDOW FRAME
MS3 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL	SP1 SPANDREL PANEL
MS4 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL	ST1 ALUMINUM STOREFRONT SYSTEM
WS1 HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN	TP1 PAINTED ACCENT TRIM
BR1 BRICK, RUNNING BOND	MB1 PAINTED FAUX METAL EXPOSED BEAM
BR2 BRICK, RUNNING BOND	GR1 POWDERCOATED METAL GUARDRAIL
SN1 STONE VENEER	

NOTE:
ALL MATERIALS & COLORS FROM THE DELO PHASE II BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION PROCESS.



sheet title
Architectural Elevations

submittal

no	date	description
1	08-15-2014	initial submittal
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19
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34

de lo Final Planned Unit Development Phase 2

Downtown East Louisville
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City of Louisville, County of Boulder, State of Colorado



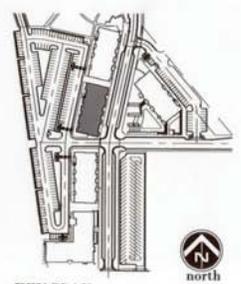
DELO BUILDING B - NORTH ELEVATION
1/8" = 1'-0"



PERSPECTIVE



DELO BUILDING B - EAST ELEVATION
1/8" = 1'-0"



KEY PLAN

DELO PHASE II BUILDING MATERIAL PALETTE

- | | |
|---|---------------------------------------|
| LS1 CEMENT LAP SIDING OR STUCCO | SS1 STANDING SEAM METAL SIDING / ROOF |
| LS2 CEMENT LAP SIDING OR STUCCO | SS2 STANDING SEAM METAL SIDING / ROOF |
| MS1 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | W01 ALUMINUM CLAD WINDOW FRAME |
| MS2 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | W02 VINYL CLAD WINDOW FRAME |
| MS3 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | SP1 SPANDREL PANEL |
| MS4 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | ST1 ALUMINUM STOREFRONT SYSTEM |
| W51 HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN | TP1 PAINTED ACCENT TRIM |
| BR1 BRICK, RUNNING BOND | MB1 PAINTED FAUX METAL EXPOSED BEAM |
| BR2 BRICK, RUNNING BOND | GR1 POWDERCOATED METAL GUARDRAIL |
| SN1 STONE VENEER | |

NOTE: ALL MATERIALS & COLORS FROM THE DELO PHASE II BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION PROCESS.



scale north

sheet title Architectural Elevations

submittal		
no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal

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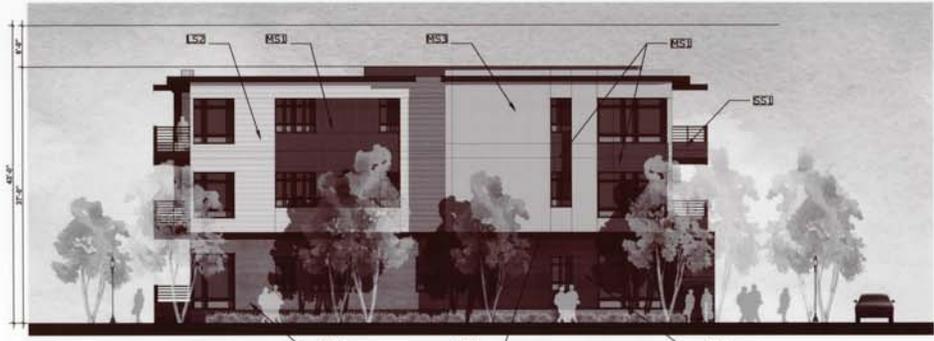
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20 of 34

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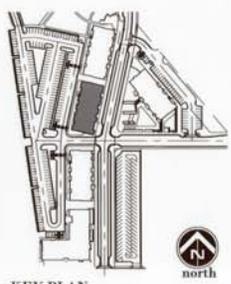
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DELO BUILDING B - SOUTH ELEVATION
1/8" = 1'-0"



DELO BUILDING B - WEST ELEVATION
1/8" = 1'-0"



DELO PHASE II BUILDING MATERIAL PALETTE

- LS1 CEMENT LAP SIDING OR STUCCO
- LS2 CEMENT LAP SIDING OR STUCCO
- MS1 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS2 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS3 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS4 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- WS1 HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN
- BR1 BRICK, RUNNING BOND
- BR2 BRICK, RUNNING BOND
- SN1 STONE VENEER
- SS1 STANDING SEAM METAL SIDING / ROOF
- SS2 STANDING SEAM METAL SIDING / ROOF
- W01 ALUMINUM CLAD WINDOW FRAME
- W02 VINYL CLAD WINDOW FRAME
- SP1 SPANDREL PANEL
- ST1 ALUMINUM STOREFRONT SYSTEM
- TP1 PAINTED ACCENT TRIM
- MB1 PAINTED FAUX METAL EXPOSED BEAM
- GR1 POWDERCOATED METAL GUARDRAIL

NOTE:
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BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE
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scale north
sheet title
Architectural Elevations

submittal

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal

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21 of 34

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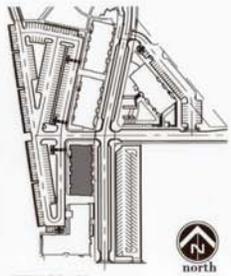
DELO BUILDING C - NORTH ELEVATION
1/8" = 1'-0"



PERSPECTIVE



DELO BUILDING C - EAST ELEVATION
1/8" = 1'-0"



KEY PLAN

DELO PHASE II BUILDING MATERIAL PALETTE

- LS1 CEMENT LAP SIDING OR STUCCO
- LS2 CEMENT LAP SIDING OR STUCCO
- MS1 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS2 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS3 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS4 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- WS1 HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN
- BR1 BRICK, RUNNING BOND
- BR2 BRICK, RUNNING BOND
- SN1 STONE VENEER
- SS1 STANDING SEAM METAL SIDING / ROOF
- SS2 STANDING SEAM METAL SIDING / ROOF
- W01 ALUMINUM CLAD WINDOW FRAME
- W02 VINYL CLAD WINDOW FRAME
- SP1 SPANDREL PANEL
- ST1 ALUMINUM STOREFRONT SYSTEM
- TP1 PAINTED ACCENT TRIM
- MB1 PAINTED FAUX METAL EXPOSED BEAM
- GR1 POWDERCOATED METAL GUARDRAIL



scale

sheet title

Architectural Elevations

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1	08-15-2014	initial submittal
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22 of **34**

NOTE: ALL MATERIALS & COLORS FROM THE DELO PHASE II BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION PROCESS.

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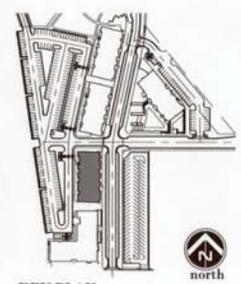
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DELO BUILDING C - SOUTH ELEVATION
1/8" = 1'-0"



DELO BUILDING C - WEST ELEVATION
1/8" = 1'-0"



KEY PLAN

DELO PHASE II BUILDING MATERIAL PALETTE

- | | |
|---|---------------------------------------|
| LS1 CEMENT LAP SIDING OR STUCCO | SS1 STANDING SEAM METAL SIDING / ROOF |
| LS2 CEMENT LAP SIDING OR STUCCO | SS2 STANDING SEAM METAL SIDING / ROOF |
| M51 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | W01 ALUMINUM CLAD WINDOW FRAME |
| M52 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | W02 VINYL CLAD WINDOW FRAME |
| M53 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | SP1 SPANDREL PANEL |
| M54 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL | ST1 ALUMINUM STOREFRONT SYSTEM |
| W51 HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN | TP1 PAINTED ACCENT TRIM |
| BR1 BRICK, RUNNING BOND | MB1 PAINTED FAUX METAL EXPOSED BEAM |
| BR2 BRICK, RUNNING BOND | GR1 POWDERCOATED METAL GUARDRAIL |
| SN1 STONE VENEER | |

NOTE:
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sheet title Architectural Elevations

submittal		
no	date	description
1	06-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal

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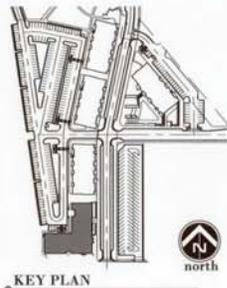
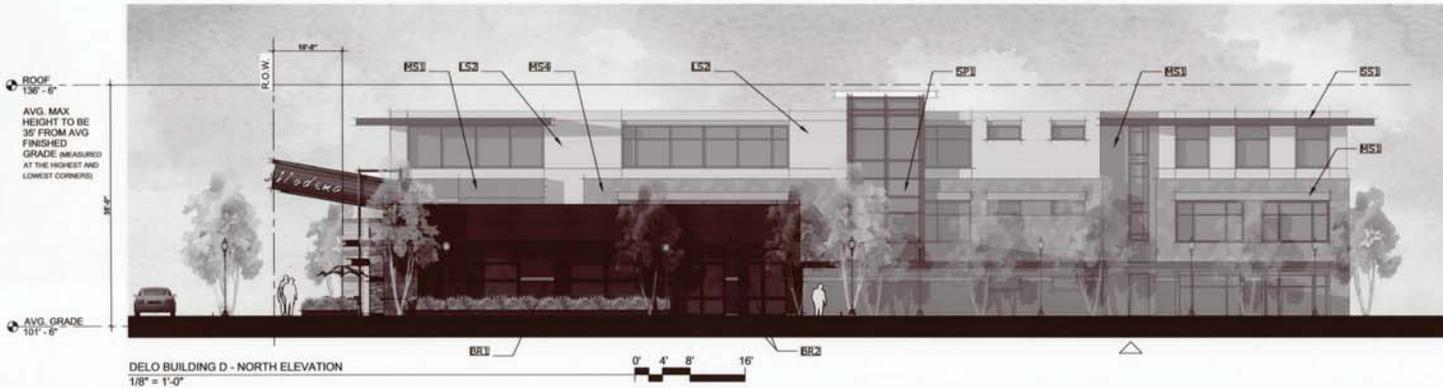
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DELO PHASE II BUILDING MATERIAL PALETTE

- LS1. CEMENT LAP SIDING OR STUCCO
- LS2. CEMENT LAP SIDING OR STUCCO
- MS1. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS2. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS3. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS4. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- WS1. HORIZONTAL TONGUE AND GROOVE WOOD RAINGREEN
- BR1. BRICK, RUNNING BOND
- BR2. BRICK, RUNNING BOND
- SN1. STONE VENEER
- SS1. STANDING SEAM METAL SIDING / ROOF
- SS2. STANDING SEAM METAL SIDING / ROOF
- W1. ALUMINUM CLAD WINDOW FRAME
- W2. VINYL CLAD WINDOW FRAME
- SP1. SPANDREL PANEL
- ST1. ALUMINUM STOREFRONT SYSTEM
- TP1. PAINTED ACCENT TRIM
- MB1. PAINTED FALX METAL EXPOSED BEAM
- GR1. POWDERCOATED METAL GUARDRAIL



NOTE: ALL MATERIALS & COLORS FROM THE DELO PHASE II BUILDING MATERIAL PALETTE ARE ELIGIBLE FOR USE IN THE CONSTRUCTION DOCUMENTATION PROCESS.



sheet title

Architectural Elevations

submittal

no	date	description
1	08-15-2014	initial submittal
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3	02-02-2015	third submittal

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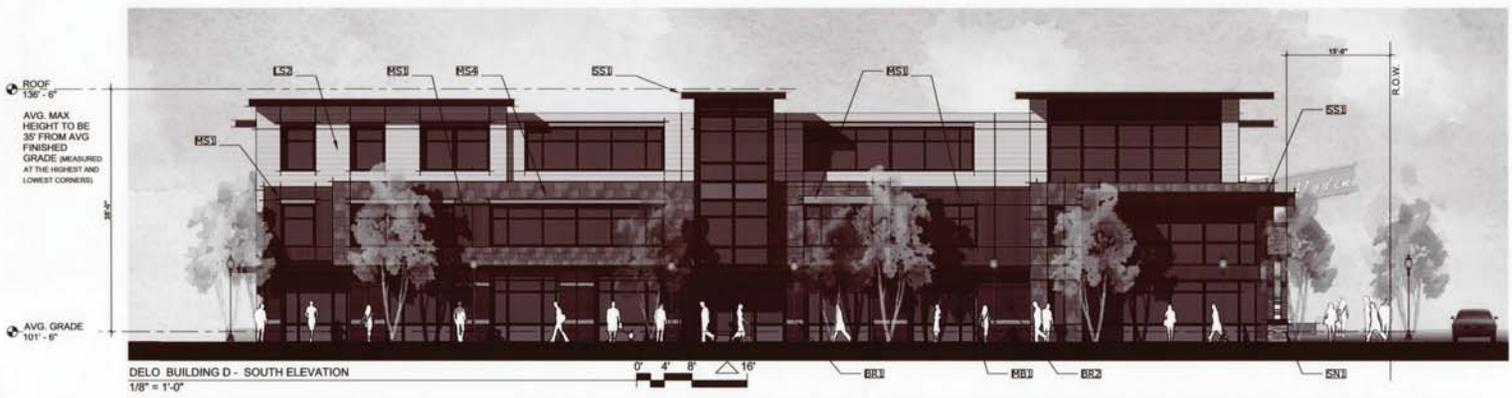
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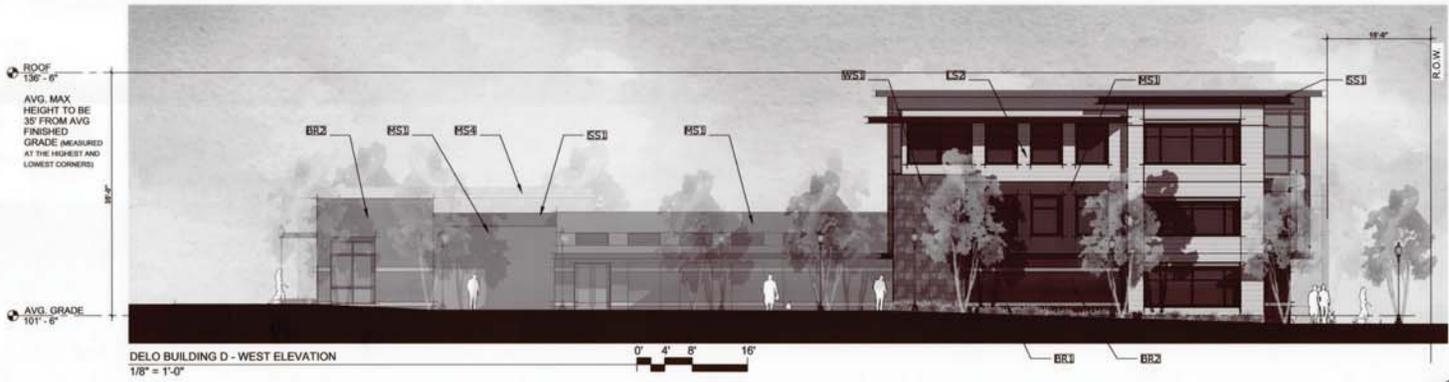
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24 of 34

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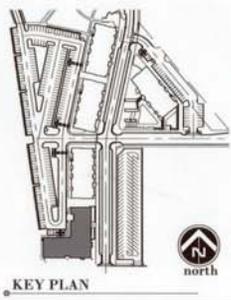
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DELO BUILDING D - SOUTH ELEVATION
1/8" = 1'-0"



DELO BUILDING D - WEST ELEVATION
1/8" = 1'-0"



DELO PHASE II BUILDING MATERIAL PALETTE

- LS1. CEMENT LAP SIDING OR STUCCO
- LS2. CEMENT LAP SIDING OR STUCCO
- MS1. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL.
- MS2. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL.
- MS3. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL.
- MS4. VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL.
- WS1. HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN
- BR1. BRICK, RUNNING BOND
- BR2. BRICK, RUNNING BOND
- SN1. STONE VENEER
- SS1. STANDING SEAM METAL SIDING / ROOF
- SS2. STANDING SEAM METAL SIDING / ROOF
- W01. ALUMINUM CLAD WINDOW FRAME
- W02. VINYL CLAD WINDOW FRAME
- SP1. SPANDREL PANEL
- ST1. ALUMINUM STOREFRONT SYSTEM
- TP1. PAINTED ACCENT TRIM
- MB1. PAINTED FAUX METAL EXPOSED BEAM
- GR1. POWDERCOATED METAL GUARDRAIL

NOTE:
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scale
sheet title
Architectural Elevations

submittal		
no	date	description
1	08-15-2014	initial submittal
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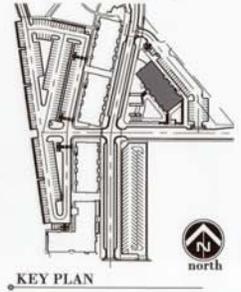
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DELO BUILDING E - SOUTH ELEVATION
1/8" = 1'-0"



DELO BUILDING E - WEST ELEVATION
1/8" = 1'-0"



KEY PLAN

DELO PHASE II BUILDING MATERIAL PALETTE

- LS1 CEMENT LAP SIDING OR STUCCO
- LS2 CEMENT LAP SIDING OR STUCCO
- MS1 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS2 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS3 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- MS4 VERTICAL OR HORIZONTAL CORRUGATED METAL SIDING OR METAL PANEL
- WS1 HORIZONTAL TONGUE AND GROOVE WOOD RAINSCREEN
- BR1 BRICK RUNNING BOND
- BR2 BRICK RUNNING BOND
- SN1 STONE VENEER
- SS1 STANDING SEAM METAL SIDING / ROOF
- SS2 STANDING SEAM METAL SIDING / ROOF
- W61 ALUMINUM CLAD WINDOW FRAME
- W62 VINYL CLAD WINDOW FRAME
- SP1 SPANDREL PANEL
- ST1 ALUMINUM STOREFRONT SYSTEM
- TP1 PAINTED ACCENT TRIM
- MB1 PAINTED FAUX METAL EXPOSED BEAM
- GR1 POWDERCOATED METAL GUARDRAIL

NOTE:
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scale north

sheet title Architectural Elevations

submittal		
no	date	description
1	08-15-2014	initial submittal
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3	02-02-2015	third submittal

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FRONT ELEVATION



SIDE STREET / MEWS ELEVATION I



ALLEY ELEVATION



SIDE STREET / MEWS ELEVATION II



PERSPECTIVE

NOTE:
ARCHITECTURAL ILLUSTRATIONS FOR THE TOWNHOME
PRODUCT ARE PER PREVIOUS APPROVED SUBMITTALS
ASSOCIATED WITH THIS PRODUCT TYPE.



north

sheet title
**Architectural
Elevations**

submittal

no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

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designed by: SA drawn by: KLM
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34

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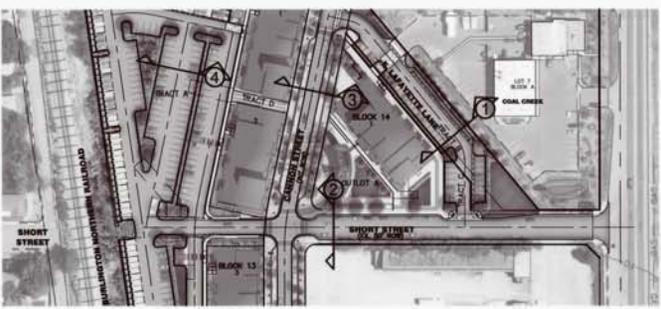
Downtown East Louisville
A part of section 8, township 1 south, range 69 west of the 6th p.m.
City of Louisville, County of Boulder, State of Colorado



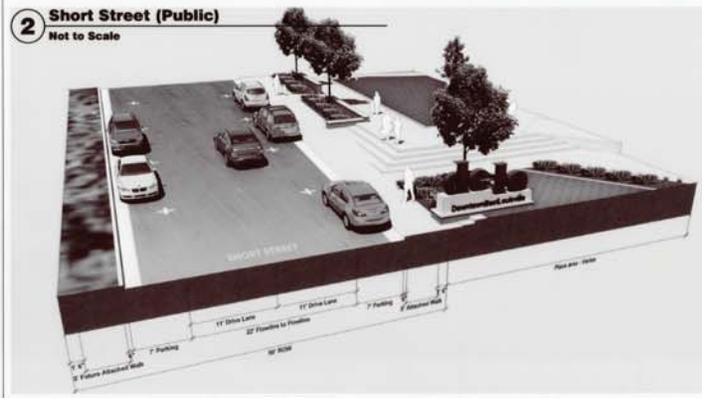
1 East Lafayette Lane (Private)
Not to Scale



3 Cannon Street (Apartments/Condominiums)
Not to Scale



NOTES:
1. ALL STREET TREES DEPICTED ON THE TYPICAL SECTIONS ARE ILLUSTRATIVE ONLY. PLEASE REFERENCE LANDSCAPING SHEETS FOR ALL LANDSCAPE LOCATIONS.
2. ACCESS IS GRANTED OVER AND ACROSS ALL PAVED AREAS FOR EMERGENCY, PUBLIC AND PRIVATE VEHICULAR ACCESS, VIA THE PLAT.



2 Short Street (Public)
Not to Scale



4 Private Parking (Apartments/Condominiums)
Not to Scale



CANNON STREET PERSPECTIVE

1" = 60' scale
north

sheet title
Street Sections

submittal		
no	date	description
1	08-15-2014	initial submittal
2	11-21-2014	second submittal
3	02-02-2015	third submittal
4	09-01-2015	final submittal

designed by: SOS drawn by: SCD
checked by: JDM project #: 030017

planning & ia
people creating spaces
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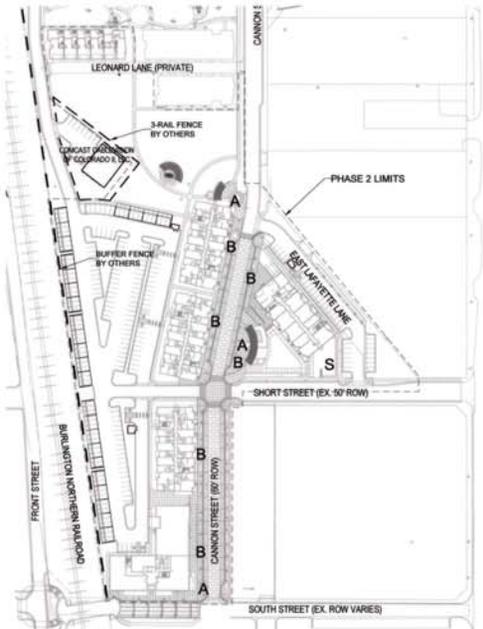
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29
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DowntownEastLouisville
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- KEY**
- S MONUMENT SIGN
 - A PUBLIC ART LOCATION (BY CITY OF LOUISVILLE)
 - B BICYCLE RACK
 - - - BUFFER FENCE BY OTHERS
 - - - 3 RAIL FENCE BY OTHERS
 - CARPORT
 - TRASH ENCLOSURE

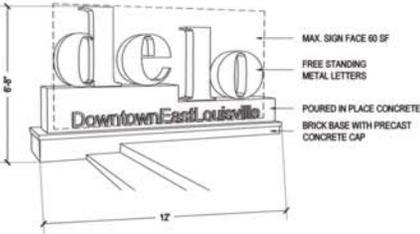


1 SITE ELEMENTS DIAGRAM
SCALE: 1"=100'

2 ENTRY MONUMENTATION

SCALE: NTS
PERSPECTIVE SKETCH
SIGNAGE DESCRIPTION
TO PROVIDE A CLEAR IDENTIFICATION OF PROPOSED BUSINESSES, BUILDINGS, AND THE COMMUNITY BY ADDING VISUAL INTEREST, WAYFINDING AND ENHANCING THE CHARACTER OF THE SITE. DELO PROPOSES USING HIGH QUALITY AND DURABLE MATERIALS THAT ARE APPROPRIATE FOR AN URBAN SETTING.

- DESIGN NOTES & STANDARDS**
- BUILDINGS SHALL HAVE LOCATIONS ON THE COMMERCIAL AREAS OF THE FACADE THAT ARE SPECIFICALLY DESIGNED TO ACCOMMODATE CHANGEABLE TENANT SIGNAGE INCLUDING WALL SIGNS, PROJECTING SIGNS AND WINDOW SIGNS.
 - STRUCTURE, MATERIALS, DETAILING AND POWER SOURCES SHALL BE PROPOSED WITH CONSIDERATION OF SIGNAGE INSTALLATION REQUIREMENTS AND SHALL BE READILY ADAPTABLE AND REPAIRABLE AS TENANT SIGNAGE NEEDS CHANGING.
 - LOCATIONS FOR COMMUNITY MONUMENTATION AND ILLUMINATED SIGNAGE SHALL BE ORIENTED TO THE PUBLIC RIGHT-OF-WAY AND SHALL AVOID FACING RESIDENTIAL USES WHEREVER POSSIBLE.
 - ORIENTATION OF ANY ILLUMINATED SIGN OR LIGHT SOURCE SHALL BE DIRECTED OR SHIELDED TO REDUCE LIGHT TRESPASS AND GLARE.
 - SMALL SCALE SIGNS PROJECTING FROM THE BUILDING FACE, PERPENDICULAR TO THE PUBLIC RIGHT-OF-WAY, SHALL BE CONSIDERED APPROPRIATE IN PEDESTRIAN AREAS.
 - STREET LIGHTING SHOULD BE DESIGNED TO INCORPORATE BANNERS, BANNERS CAN BE USED TO PROMOTE SPECIAL EVENTS THROUGHOUT DELO.
 - SIGNS SHOULD CREATIVELY USE TWO AND THREE-DIMENSIONAL FORM, PROFILE, AND ICONOGRAPHIC REPRESENTATION (E.G. LIGHTING, TYPOGRAPHY, COLOR, AND MATERIALS) IN EXPRESSING THE CHARACTER OF THE USE, THE IDENTITY OF THE DEVELOPMENT, THE CHARACTER OF THE NEIGHBORHOOD, AND THE ARCHITECTURE OF THE BUILDING.
 - SIGNS SHOULD FIT WITHIN THE ARCHITECTURAL FEATURES OF THE FACADE AND COMPLEMENT THE BUILDING'S ARCHITECTURE.
 - SIGNS SHOULD NOT OVERLAP OR CONCEAL ARCHITECTURAL ELEMENTS.
 - INDIRECT AND EXTERNAL LIGHT SOURCES SHALL BE THE PREFERRED OPTION WHERE LIGHTING IS REQUIRED.
 - SIGNS SHOULD BE ORGANIZED ON BUILDINGS SO AS TO NOT VISUALLY CLUTTER THE STREETSCAPE.
 - THE SELECTIVE USE OF NEON SIGNAGE IS ALLOWED AND SHOULD BE USED IN LIMITED VOLUME TO ENSURE THAT THEY DO NOT BECOME VISUALLY OBTRUSIVE AND DOMINATE THE STREET FRONTAGE. THIS COULD COME IN THE FORM OF MULTI-COLORED TUBE-FORM LIGHT WITH EXPOSED TUBES, WHERE TUBING IS VISIBLE ATOP A SIGN, OR FORMS LETTERS IN THE SIGN, OR INDIRECT WHERE TUBES ILLUMINATE PAINTED OR OTHER DECORATIVE SURFACES.



PERSPECTIVE SKETCH

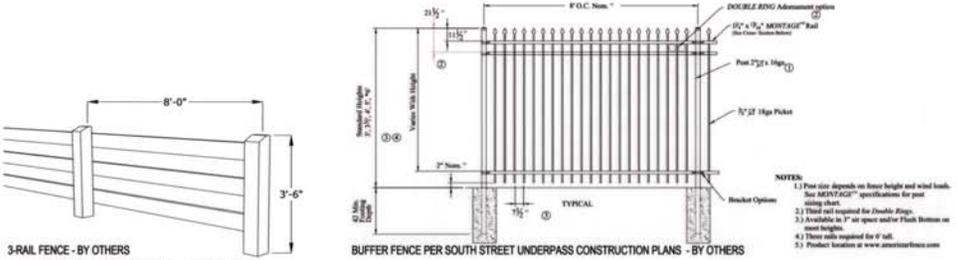
3 PUBLIC SPACE

SCALE: NTS
PERSPECTIVE SKETCH
PEDESTRIAN SPACES DESCRIPTION
THE PROJECT ANTICIPATES A PLAZA AREA AT THE INTERSECTION OF CANNON STREET AND SHORT STREET AT THE PRIMARY ENTRY INTO THE PROJECT OFF OF HWY 42. THE PLANTING IS ANTICIPATED TO BE OF AN URBAN NATURE, AND THE OVERALL DESIGN IS LIKELY TO INCLUDE INTERPRETIVE DISPLAYS, BENCHES, SITE FURNITURE, AND IRRIGATED PLANTINGS. THE SECOND PRIMARY PUBLIC PEDESTRIAN EXPERIENCE IS THE PROPOSED UNDERPASS AT SOUTH STREET. THE PROMENADE WILL PROVIDE A CLEAR AND ENJOYABLE PEDESTRIAN LINK TO THE SOUTHERN PORTION OF THE PROJECT. ITS ANTICIPATED TO HAVE MORE RETAIL USES IN THIS AREA WHICH PROPOSED RESTAURANTS AND BOUTIQUE SHOPS CAN TAKE ADVANTAGE OF.

- NOTES & STANDARDS**
- TO PROVIDE OPEN SPACES SUCH AS PLAZAS AND PUBLICLY ACCESSIBLE COURTYARDS, OR PROMENADES THAT SERVE AS AREAS FOR RELAXATION, COMMUNITY INTERACTION AND CREATE VARIETY AND INTEREST IN THE PEDESTRIAN REALM.
 - TO INTRODUCE ELEMENTS OF NATURE INTO THE URBAN ENVIRONMENT.
 - TO ALLOW FOR ADDITIONAL SPACE ADJACENT TO BUILDINGS TO ACCOMMODATE SPECIAL AMENITIES SUCH AS CAFE SEATING, SCULPTURE AND PLANTERS.
 - TO PROVIDE ORGANIZING FEATURES FOR GROUPS OF BUILDINGS.
 - TO ESTABLISH LINKS IN A SYSTEM OF COMMON AREAS WITHIN AND OUTSIDE OF THE SITE DESIGN.
 - DESIGN OF PLAZAS, PUBLICLY-ACCESSIBLE COURTYARDS AND EXPANDED SIDEWALKS SHALL TAKE INTO CONSIDERATION EASE OF MAINTENANCE AND SNOW REMOVAL.
 - SPECIALTY ITEMS WITHIN CITY OWNED OR MAINTAINED PROPERTY THAT REQUIRE CUSTOM FABRICATION MAY BE SUBSTITUTED OR REPLACED AT THE CITY'S DISCRETION.

SCREENING & FENCING
SCREENING AND BUFFERING AREAS BETWEEN SERVICE AREAS AND PLAZAS, STREET SCAPES, PEDESTRIAN CORRIDORS AND PARKS, WILL AID TO THE AESTHETICS OF THE COMMUNITY.

- DESIGN NOTES & STANDARDS**
- PROPOSED FENCING SHALL NOT ADVERSELY AFFECT THE MAINTENANCE OF CITY FACILITIES.
 - FENCING ALONG THE PROPERTY BOUNDARY SHOULD BE ALLOWED IF NEEDED TO AID IN THE SEPARATION AND PROTECTION OF ADJACENT USES.
 - DETAILED LOCATIONS FOR FENCES WILL BE FINALIZED WITHIN THE CONSTRUCTION DOCUMENTS
 - THE FENCE DESIGN ALONG THE BURLINGTON NORTHERN ROW SHALL BE CONSISTENT AND IN THE SAME VERNACULAR WITH THAT RAIL FENCE SELECTED IN THE FINAL DESIGN OF THE HIGHWAY 42 GATEWAY STUDY AND WILL NOT BE AN EXPENSE OF THE DEVELOPER.



4 FENCING CONCEPTS - BY OTHERS
SCALE: NTS

NOTE:
ENTRY MONUMENTS, PROJECT IDENTITY AND SITE DETAILS INCLUDING FENCING, CARPORT AND ARBORS ARE CONCEPTUAL IN NATURE. FINAL DESIGN DETAILS TO BE PROVIDED DURING CONSTRUCTION DOCUMENTS.



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Site Details

submittal

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designed by: SA
checked by: PMS
drawn by: KLM
project #: 030009

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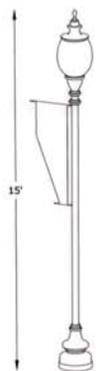
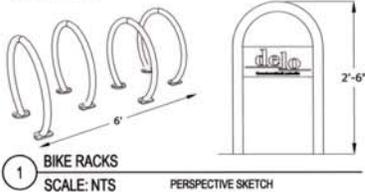
Downtown East Louisville

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SITE DETAILS AND FURNISHINGS - KIT OF PARTS
THE SITE FURNITURE IN DELO SHALL BE CONSISTENT WITH THAT OF THE SITE FURNITURE IN DOWNTOWN LOUISVILLE.

THE DEVELOPMENT OF THIS PROMINENT SITE WILL SERVE TO STRENGTHEN AND UNIFY THE SURROUNDING AREA. LIGHTING, STREET FURNISHINGS, PLANTERS, TREE GRATES, ETC. WILL ENHANCE AND UNIFY THE OVERALL PROJECT, CONTRIBUTING TO THE SENSE OF PLACE AND OVERALL CHARACTER. THE INTENT IS TO BENEFIT ALL USERS WITH A THEMATIC, WELL LIT, SAFE AND PLEASANT ENVIRONMENT. A KIT-OF-PARTS HAS BEEN DEVELOPED AS A STARTING POINT FOR THE DESIGN OF THE STREET FURNISHINGS WITHIN THE THE STREETScape AND PUBLIC AREAS AND WILL HELP TO GUIDE THE SITE PLANNING PROCESS WHICH FOLLOWS THIS DOCUMENT. THE FURNISHINGS BEING SHOWN ARE INTENDED TO PORTRAY THE CHARACTER AND QUALITY FOR DELO'S STREETScape.

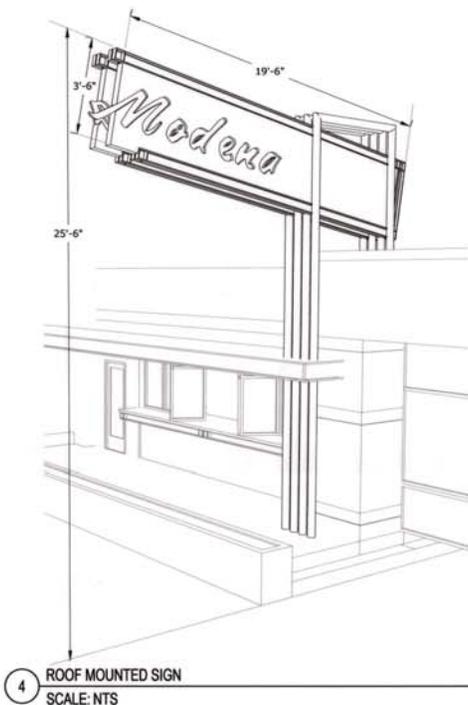
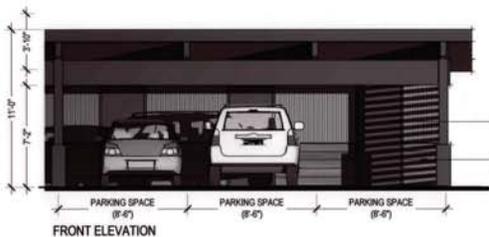
SITE DETAILS AND FURNISHINGS ARE ENCOURAGED TO BE CATALOG ITEMS TO FACILITATE EFFICIENT AND COST EFFECTIVE REPLACEMENT BY THE CITY WHEN NECESSARY.



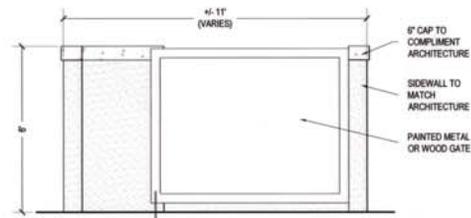
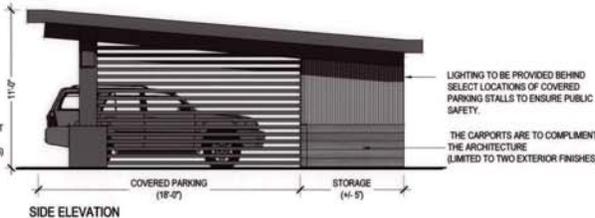
LIGHTING DESCRIPTION
LIGHTING IS COMPRISED OF THE FOLLOWING CATEGORIES:
PARKING LOT LIGHTING, STREET LIGHTING, BUILDING LIGHTING, PLAZA/PEDESTRIAN LIGHTING AND LANDSCAPE LIGHTING.
LIGHTING SHALL CONSIST OF QUALITY FIXTURES THAT ARE BOTH APPEALING AND PROVIDE SAFETY FOR BOTH PEDESTRIANS AND AUTOMOBILES. LIGHTING SHALL COMPLEMENT THE BUILDING ARCHITECTURE, SIGNAGE, PARKING, LANDSCAPING AND PLAZA DESIGNS. FIXTURES ARE TO BE USED THAT REDUCE GLARE AND MINIMIZE IMPACT TO SURROUNDING PROPERTIES.

DESIGN NOTES & STANDARDS

- PARKING LOT LIGHTING IS TO BE OF A ZERO CUTOFF TYPE AND BE NO TALLER THAN 25 FEET IN HEIGHT. THE FIXTURES ARE TO BE IN THE STYLE, TYPE AND COLOR THAT COMPLEMENT THE ARCHITECTURE AND CHARACTER OF THE PROJECT.
- STREET LIGHTING IS TO BE ZERO OR PARTIAL CUTOFF TYPE AND BE NO TALLER THAN 15 FEET IN HEIGHT AND MATCH THE CHARACTER OF OLD TOWNS LIGHT FIXTURES.
- BUILDING MOUNTED LIGHTING IS ENCOURAGED TO ENHANCE THE ADJACENT SIDEWALKS AS WELL AS THE ARCHITECTURE ITSELF. LIGHTING MAY BE USED TO ENHANCE IMPORTANT ARCHITECTURAL FEATURES OF THE BUILDING IT SERVES.
- LANDSCAPE LIGHTING IS TO ENHANCE THE LANDSCAPE FEATURE IT SERVES, AS WELL AS TO FURTHER ENHANCE THE SAFETY OF THE PEDESTRIAN AREA.
- SIGNAGE LIGHTING: SEE SIGNAGE NOTES UNDER THE ENTRY MONUMENT DESIGN NOTES & STANDARDS.



NOTE:
ENTRY MONUMENTS, PROJECT IDENTITY AND SITE DETAILS INCLUDING FENCING, CARPORT AND ARBORS ARE CONCEPTUAL IN NATURE. FINAL DESIGN DETAILS TO BE PROVIDED DURING CONSTRUCTION DOCUMENTS.



THE ROOF MOUNTED SIGN WITHIN DELO IS PROPOSED TO BE AN ICONIC SIGN AND SHALL EVOKE A SENSE OF QUALITY AND UNIQUE VISUAL APPEARANCES. IT MAY, OR MAY NOT, HAVE HISTORIC SIGNIFICANCE AS LONG AS:

- THE SIGN, BY ITS DESIGN, CONSTRUCTION AND LOCATION, WILL NOT HAVE A SUBSTANTIAL ADVERSE EFFECT ON ABUTTING PROPERTY OR THE PERMITTED USE THEREOF, AND WILL CONTRIBUTE TO THE CITY'S UNIQUE CHARACTER AND QUALITY OF LIFE.
- THE SIGN EXHIBITS UNIQUE OR RARE CHARACTERISTIC THAT ENHANCE THE STREETScape OR IDENTIFY DOWNTOWN LOUISVILLE AND IT CLEARLY PROVIDES A UNIQUE ARCHITECTURAL STYLE AND APPEARANCE.
- THE SIGN CONTRIBUTES TO THE CULTURAL CHARACTER OF THE STREETScape OR THE COMMUNITY AT LARGE.
- THE SIGN AND ALL PARTS, PORTIONS, AND MATERIALS SHALL BE MAINTAINED AND KEPT IN GOOD REPAIR. THE DISPLAY SURFACE OF ALL SIGNS SHALL BE KEPT CLEAN, NEATLY PAINTED, AND FREE FROM RUST AND CORROSION. THE MAXIMUM COPY AREA FOR THE COMBINATION OF ANY WALL, MARQUEE, AWNING/CANOPY, ROOF AND FREE STANDING SIGNS IS TWO (2) SQUARE FEET (SF) OF SIGN AREA FOR EVERY ONE (1) LINEAR FOOT OF PRIMARY FRONTAGE PLUS ONE (1) SF OF SIGN AREA FOR EVERY ONE (1) LINEAR FOOT OF SECONDARY FRONTAGE FOR CORNER LOTS, OR PARCELS ADJOINING A PARK SPACE, CIVIC PLAZA, OR DESIGNATED WALKWAY. ILLUMINATION SHALL BE ALLOWED PER THE DOWNTOWN LOUISVILLE SIGN MANUAL AND SHALL INCLUDE:
 - NEON LIGHTING
 - REVERSE CHANNEL / HALO LIGHTING
 - INDIRECT LIGHTING



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Site Details

submittal

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checked by: PMS project #: 030009

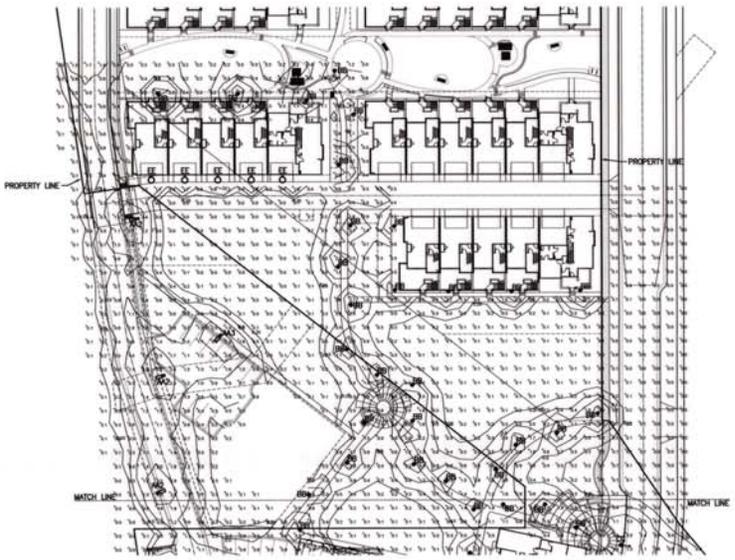
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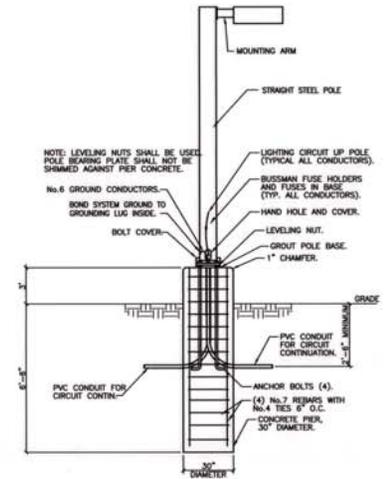
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31
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34

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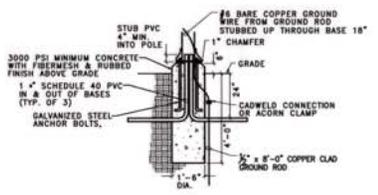
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1 Site Photometric Plan
32 NORTH
1"=40'



2 TALL POLE DETAIL
32 N.T.S.



3 SHORT POLE DETAIL
32 N.T.S.



sheet title
Site Photometric Plan

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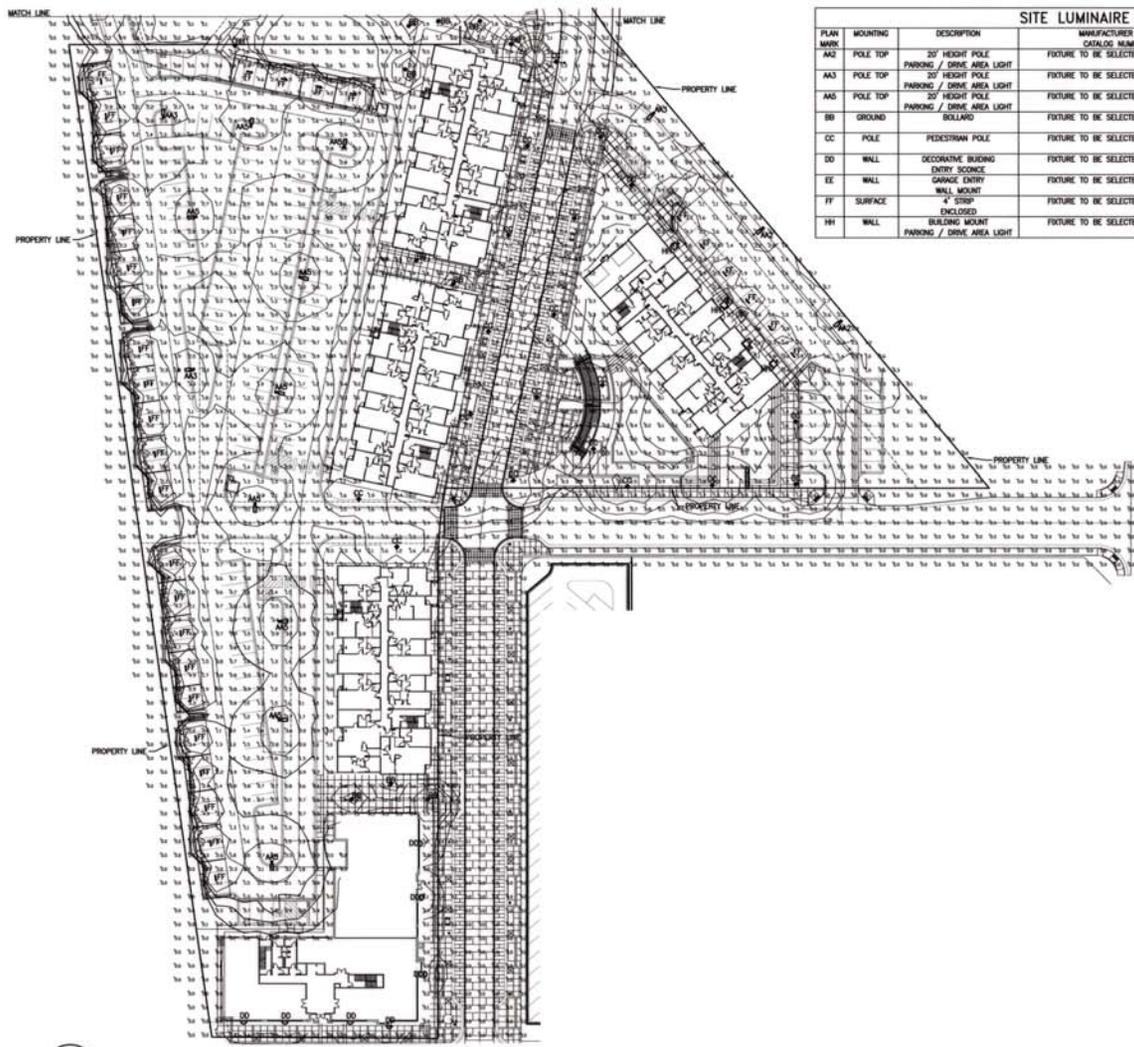
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33 OF 34

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MATCH LINE, SEE SHEET 22 FOR CONTINUATION



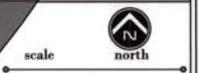
SITE LUMINAIRE SCHEDULE											
PLAN MARK	MOUNTING	DESCRIPTION	MANUFACTURER AND CATALOG NUMBER	VOLTAGE	NO. OF LAMPS	LAMP TYPE	INITIAL LUMENS	CONTROLS	MOUNTING / POLE HEIGHT	FULL CUT OFF	REMARKS
AAZ	POLE TOP	20' HEIGHT POLE PARKING / DRIVE AREA LIGHT	FIXTURE TO BE SELECTED BY OWNER	208	1	150W MH	14000	PHOTOCELL	20'	YES	HOUSESIDE SHIELD STRUT STEEL ROUND
AAS	POLE TOP	20' HEIGHT POLE PARKING / DRIVE AREA LIGHT	FIXTURE TO BE SELECTED BY OWNER	208	1	150W MH	14000	PHOTOCELL	20'	YES	HOUSESIDE SHIELD STRUT STEEL ROUND
AAS	POLE TOP	20' HEIGHT POLE PARKING / DRIVE AREA LIGHT	FIXTURE TO BE SELECTED BY OWNER	208	1	150W MH	14000	PHOTOCELL	20'	YES	STRUT STEEL ROUND
BB	GROUND	PEDESTRIAN POLE	FIXTURE TO BE SELECTED BY OWNER	208	1	70W MH	5300	PHOTOCELL	4'	YES	STRUT STEEL ROUND
CC	POLE	PEDESTRIAN POLE	FIXTURE TO BE SELECTED BY OWNER	208	1	70W MH	5300	PHOTOCELL	12'	YES	STRUT STEEL ROUND
DD	WALL	DECORATIVE BUILDING ENTRY SCIENCE GARAGE ENTRY	FIXTURE TO BE SELECTED BY OWNER	120	2	28W CFL	3600	PHOTOCELL	6'	YES	ABOVE GARAGE
EE	WALL	DECORATIVE BUILDING ENTRY SCIENCE GARAGE ENTRY	FIXTURE TO BE SELECTED BY OWNER	120	1	28W CFL	1800	PHOTOCELL	6'	YES	ABOVE GARAGE
FF	SURFACE	4" STRIP ENCLOSED BUILDING MOUNT PARKING / DRIVE AREA LIGHT	FIXTURE TO BE SELECTED BY OWNER	208	1	32W TB	3050	PHOTOCELL	6'	NO	UNDER COVERED PARKING
HH	WALL	ENCLOSED BUILDING MOUNT PARKING / DRIVE AREA LIGHT	FIXTURE TO BE SELECTED BY OWNER	120	1	70W MH	5200	PHOTOCELL	10'	YES	

DRAWING NUMBER

DRAWING NUMBER

DRAWING NUMBER

1 Site Photometric Plan



scale

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Site Photometric Plan

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Character Sketches

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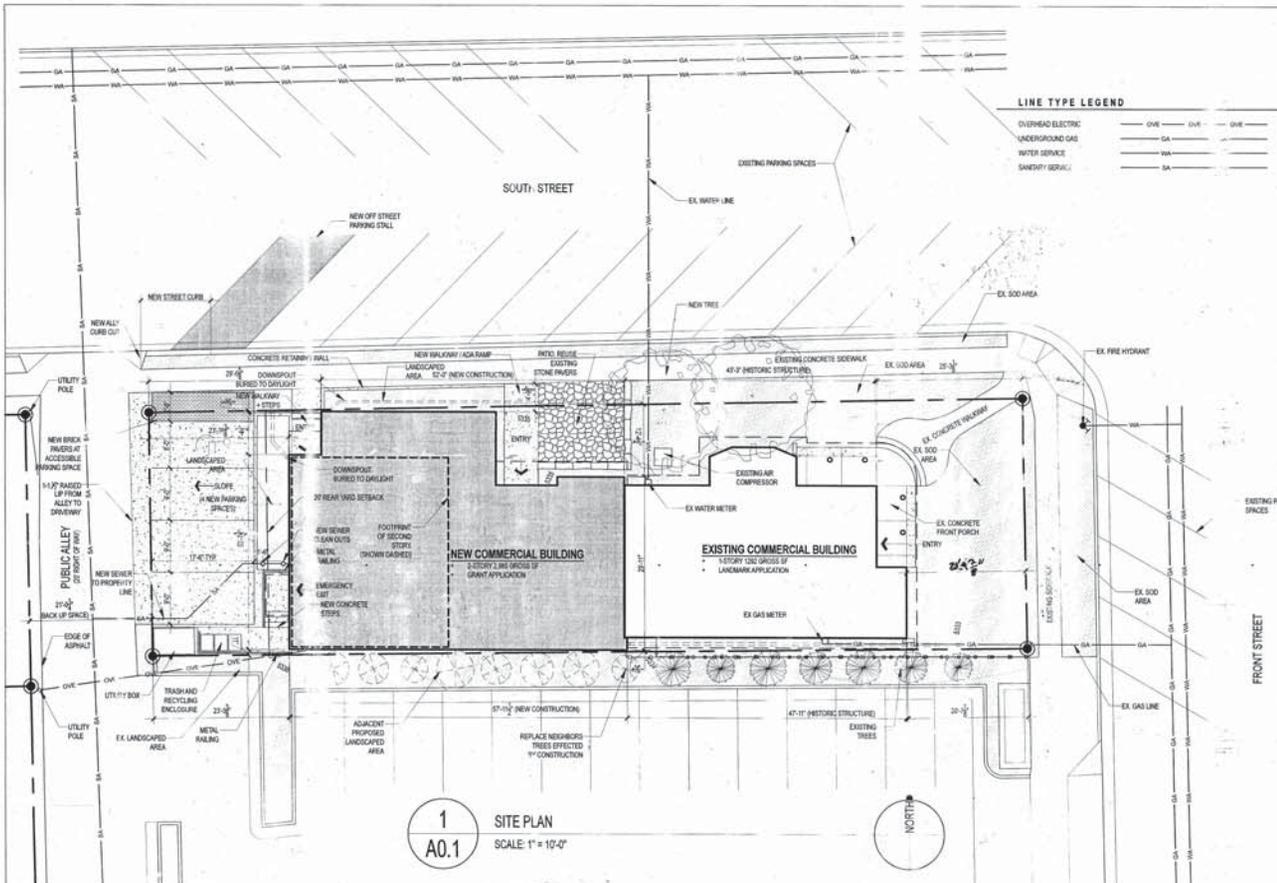
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PROJECT ANALYSIS

PROJECT DESCRIPTION:
THE ADDITION OF A 2-STORY COMMERCIAL BUILDING TO AN EXISTING 1-STORY HISTORIC COMMERCIAL BUILDING

LEGAL DESCRIPTION:
LOT 1, BLOCK 1, TOWN OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

LOT SIZE:
6,373 SF

ZONING DISTRICT:
CC (COMMUNITY COMMERCIAL CENTRAL BUSINESS DISTRICT)

DESIGN STANDARDS AND GUIDELINES:
2012 INTERNATIONAL BUILDING CODE
DESIGN HANDBOOK FOR DOWNTOWN LOUISVILLE
DOWNTOWN LOUISVILLE FRAMEWORK PLAN (TRANSITION AREA)

TYPE OF CONSTRUCTION:
EXISTING CONSTRUCTION TYPE V-B
NEW CONSTRUCTION TYPE V-A

OCCUPANCY TYPE:
MIXED OCCUPANCY BM

OCCUPANCY LOAD:

EXISTING COMMERCIAL SPH	EV ACCESS STAIR (BUSINESS+100SF)	1,376 SF	14 PEOPLE
NEW MAIN LEVEL COMMERC	CE (MERCANTILE+100SF)	1,883 SF	32 PEOPLE
NEW UPPER LEVEL COMMERC	NICE (BUSINESS+100SF)	1,615 SF	11 PEOPLE

TOTAL OCCUPANCY LOAD: 67 PEC

PROJECT CALCULATIONS	CITY REQUIREMENTS / STANDARDS	PROPO.
FLOOR AREA (GROSS)		
EXISTING COMMERCIAL BLDG	N/A	1,202 SF
NEW COMMERCIAL ADDITION (1 ST LEVEL)	N/A	2,019 SF
NEW COMMERCIAL ADDITION (2 ND LEVEL)	N/A	128 SF
UPPER LEVEL DECK	N/A	200 SF
TOTAL GROSS FLOOR AREA	N/A	4,529 SF
FLOOR AREA RATIO	1.30	0.71
BUILDING LOT COVERAGE SF	N/A	3,011 SF
BUILDING LOT COVERAGE %	N/A	53%

PARKING REQUIREMENTS (NEW COMMERCIAL ADDITION IS LESS THAN 999 SF):
NEW AND EXISTING COMMERCIAL BUILDING: 0 SPACES
NEW MULTI-FAMILY BUILDING: 4 SPACES

BUILDING HEIGHT: 35' / 24'-6"

SETBACKS:
FRONT YARD: 0' / 2'-0"
SIDE YARD - NORTH: 0' / 0'-0"
SIDE YARD - SOUTH: 0' / 0'-0"
REAR YARD: 20' / 15'-0"

IMPERVIOUS / PERVIOUS AREA CALCULATIONS:

EXISTING IMPERVIOUS AREA	2,754.6 SF
EXISTING PERVIOUS AREA	3,618.5 SF
PROPOSED IMPERVIOUS AREA	4,833 SF
PROPOSED PERVIOUS AREA	1,535.5 SF

VICINITY



DAJDESIGN

922A MAIN STREET
LOUISVILLE, CO 80007
P. 303.527.1100
WWW.DAJDESIGN.COM

DESIGN DEVELOPMENT
945 FRONT STREET, LOUISVILLE, COLORADO
LOT 1, BLOCK 1, TOWN OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

SITE PLAN,
PROJECT
ANALYSIS

07/09/2015
08/25/2015
10/09/2015

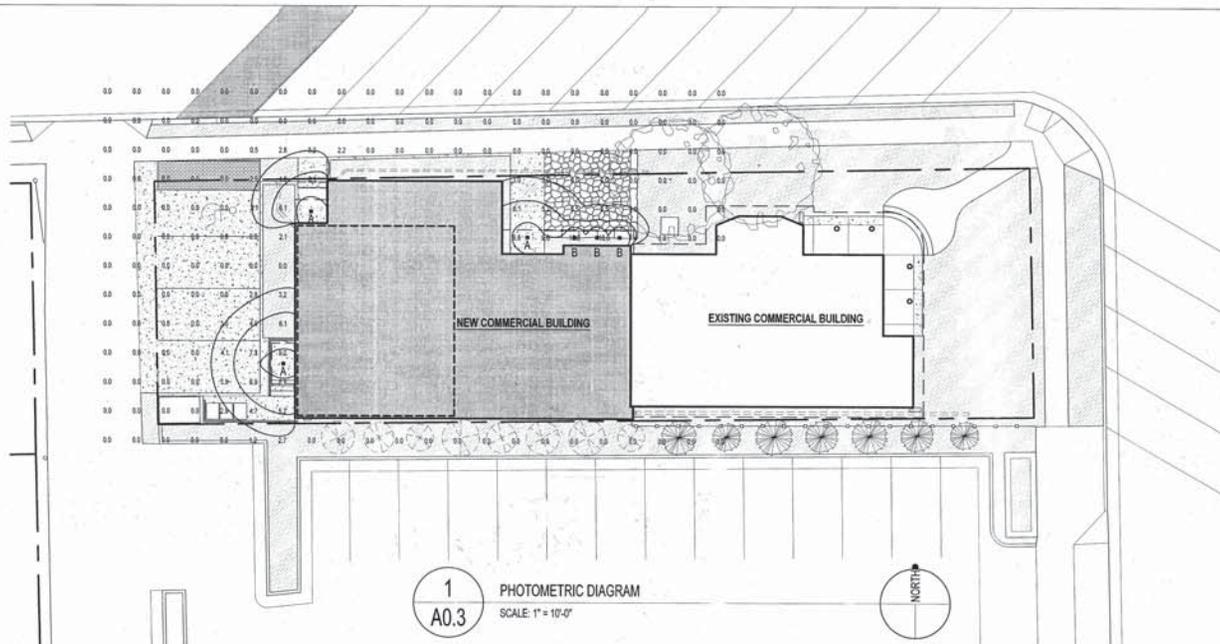
DESIGN DEVELOPMENT
10/09/2015

40.1

OWNER'S SIGNATURE BLOCK:
BY: I, THE OWNER, KNOWLEDGANTLY ACCEPT ALL THE REQUIREMENTS AND INTENT SET FORTH IN THIS INSTRUMENT.
DATE: 8th DAY OF March 2016
SHERPI MURGAJIS
KERRY HOLLE (NOTARY SEAL)
9/21/2018

CLERK AND RECORDER CERTIFICATE (COUNTY OF BOULDER, STATE OF COLORADO)
I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT _____ O'CLOCK _____ M. THIS 15th DAY OF MARCH 2016 AND IS RECORDED IN BOOK _____ PAGE _____ FILE NO. 3505804 RECEIVED

CITY COUNCIL CERTIFICATE
APPROVED THIS 20th DAY OF October 15 BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO
REGULATION NO. 74 SERIES 2015
Mayor Signature: Carol Hanson
CITY CLERK SIGNATURE



LIGHTING KEY

- A. 4" SQUARE LED RECESSED CAN, WIDE ANGLE OPTICS
- B. 4" SQUARE LED RECESSED CAN, NARROW ANGLE OPTICS

ESSENTIA
LED LIGHTING

ESA-ADS-414-C

DESCRIPTION
Downlight luminaire with 4" square aperture, designed for 14 high output LED's maximum. Two plane optical assembly provides a broad, even light distribution, combining low brightness, with maximum visual comfort and efficiency. Three light distributions available - narrow, medium, and wide.

FEATURES

- Customized color 14 high output LED's, intended to be within a 2-step Macadam ellipse. See table for specific color tolerance (ΔE).
- Aseal and Tiled Aseal (TA) used apply on each individual LED to maximize light efficiency through aperture.
- Light distribution available in narrow, medium, or wide.
- Lower glare in the fixture - low glare, 60°C beam, 60°C beam, and 60°C beam.
- Self-cleaning Clear Frosted standard.
- Precision machined plastic cover ensures precise that the lower cover is held in position.
- Recessed cover features wide to minimize glare light.
- 2" aperture depth to accommodate all standard and select trim settings and provide flexibility in mounting options.
- Custom hangers for custom ceiling of LED's.
- Provided with dark mounting brackets for optional mounting channels.
- High Efficiency, constant current driver (120-277VAC input, 2700K drive current).
- 90-100 dimming, dimming 100% - 10% full-range continuous dimming.
- 60-100, tested for dimming for 10,000 hours and many locations.
- Tamperly protection.

Notes:

Color Tolerances

2700K	±10
3000K	±10
3500K	±10
4000K	±10

LED Architectural Downlight - Square 4" Aperture

Quantity	Product	Aperture	LED Type	Beam	Beam Spread	Mounting	Color	Notes
1	ESA ADS	4"	14	C	WD	SSC	2700K	

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 Made in the U.S.A. of U.S. and imported parts.
 NOTE: All data subject to change without notice. Rev. Date: 01-07-12/11

ITEM: 2019 Planning Commission Work Plan

PLANNER: Rob Zuccaro, AICP, Director of Planning & Building Safety

SUMMARY:

The Planning Commission has requested a review and discussion of the 2019 Commission work plan. The following pages include a summary table of 2019 work priorities adopted by City Council that will involve the Planning Commission. Attached for reference is the complete 2019 City Council Work Plan. In addition, staff has provided the following guiding documents to inform the work plan discussion:

- *Strategic Planning Framework:* Each City Council work plan project is categorized by “Critical Success Factor,” which aligns with a recently adopted Strategic Planning Framework, developed by City Staff and endorsed by City Council. The Strategic Planning Framework is attached for reference and staff will provide more background on the plan development and its anticipated role in City operations.
- *City Program Goals and Objectives:* Also included for each City Council project is the designated “Program.” These designations reflect budgetary categories and are broken out into Program Goals, Subprograms, and Subprogram Objectives. The Community Design, Transportation and Economic Prosperity Program Goals and Objectives are attached for reference.
- *City of Louisville Comprehensive Plan:* The Comprehensive Plan is reviewed and updated on a 10-year cycle and is the City’s main policy document on land use, infrastructure and programs. Often, there are more specific policy documents that should align with the Comprehensive Plan, including small area plans, the transportation master plan, and parks and trails plans. The Comprehensive Plan may be updated prior to the 10-year cycle if necessary. Municipal Code Sec. 17.64.020 specifically allows the Planning Commission to initiate a Comprehensive Plan amendment.

Staff is seeking direction on any additional projects or initiatives the Commission may wish to explore. These may include ideas for study sessions on topics of interest, specific zoning or subdivision code amendments or comprehensive plan amendments that could be explored or initiated.

Critical Success Factor/ Priority Initiative	Program	Issue	Priority	Planning Commission (PC) Involvement
 Reliable Core Services **Priority Initiative	Transportation	Transportation Master Plan – implement recommendations from TMP and discuss future funding considerations.	High	PC will review draft document at one or more public meetings and provide feedback and recommendations prior to presentation to City Council
 Reliable Core Services	Community Design	Design Guidelines and Sign Code Update – improve ability of existing businesses and property owners to be successful by implementing changes to City's commercial and industrial design guidelines and sign code.	High	PC will review the draft documents at one or more public meetings and provide feedback and recommendations prior to presentation to City Council
 Vibrant Economic Climate **Priority Initiative	Economic Prosperity	Implement Recommendations from McCaslin Redevelopment Study to support redevelopment within area.	High	Staff anticipates that PC will review a General Development Plan amendment at a future Public Hearing that will reflect implementation of the Redevelopment Study.
 Reliable Core Services	Community Design	Marijuana Regulations Update – discuss potential changes to current regulations.	Medium	COMPLETE – PC reviewed the revised marijuana regulations in early 2019. City Council adopted revised regulations for sales, testing and manufacturing and pulled out regulations for cultivation. Council intends to place the cultivation regulations on the ballot in November along with an excise tax requirement.
 Collaborative Regional Partner	Community Design	Affordable Housing funding in collaboration with Boulder County and participation in Countywide affordable housing strategies.	Medium	If City Council wishes to pursue zoning related incentives or requirements, PC will review and make recommendations and the codes.
 Reliable Core Services	Community Design	Height Calculations – amend height calculation requirements. Current regulations are difficult to interpret and enforce.	Medium	PC will review possible changes to the height calculations and make recommendations to City Council. Staff anticipates a work session with PC prior to drafting the regulations.
 Vibrant Economic Climate	Economic Prosperity	Redevelopment of Phillips 66 Property - Staff will develop options/tools to understand the market, the development potential and benefits to the community.	Medium	PC will review possible zoning or comprehensive plan amendments related this property.
 Reliable Core Services	Community Design	PUD Review and Waiver Criteria – consolidate and update criteria.	Lower	PC will review possible changes to the criteria and make recommendations to City Council. Staff anticipates a work session with PC prior to drafting the regulations.

Critical Success Factor/ Priority Initiative	Program	Issue	Priority	Planning Commission (PC) Involvement
 Reliable Core Services	Community Design	Dark Sky Lighting – consider dark sky lighting code changes for residential properties, and further education.	Lower	PC will review possible changes to the city's codes and make recommendations to City Council. Staff anticipates a work session with PC prior to drafting the regulations
 Reliable Core Services	Other	PUDs/Developments Projects	N/A	PC will hold public hearings and make recommendations to City Council on any non-administrative development review applications.

ATTACHMENTS:

1. 2019 City Council Work Plan
2. Strategic Planning Framework
3. Transportation, Community Design and Economic Prosperity Program Goals and Subprograms
4. Comprehensive Plan

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
1.	 Reliable Core Services **Priority Initiative	Transportation	Transportation Master Plan – implement recommendations from TMP and discuss future funding considerations.	3 meetings	High	1 st , 2 nd , 3 rd
2.	 Reliable Core Services	Utilities	Trash Hauler RFP - select contractor for collection of single-family residential trash, recyclables and compostables. Policy discussion about waste diversion and composting and approval of the contract.	2 meetings	High	1 st
3.	 Financial Stewardship and Asset Mgmt **Priority Initiative	Recreation	Recreation/Senior Center Assessment/Fees - review finances, fees and budgets to ensure sound financial structure/fiscal sustainability of Recreation Fund.	2 – 3 meetings	High	2 nd , 3 rd
4.	 Financial Stewardship and Asset Mgmt **Priority Initiative	Recreation	Golf Course Assessment/Fees – review finances, fees, budgets and water policies to ensure sound financial structure/fiscal sustainability of Golf Fund.	2 – 3 meetings	High	2 nd , 3 rd

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
5.	 Reliable Core Services	Community Design	Design Guidelines and Sign Code Update – improve ability of existing businesses and property owners to be successful by implementing changes to City’s commercial and industrial design guidelines and sign code.	2 meetings	High	2 nd , 3 rd
6.	 Vibrant Economic Climate **Priority Initiative	Economic Prosperity	Implement Recommendations from McCaslin Redevelopment Study to support redevelopment within area.	3 – 4 meetings	High	1 st , 2 nd , 3 rd , 4 th
7.	 Financial Stewardship and Asset Mgmt **Priority Initiative	Administration and Support Services	Fiscal and Revenue Policies – review and update fiscal policies, including Rec and Senior Center, Golf Course and Open Space acquisition. Review reserve policy for acquisitions.	2 meetings	High	2 nd , 3 rd
8.	 Financial Stewardship and Asset Mgmt	Administration and Support Services	2020 Budget – finalize and adopt 2020 operating and capital budget.	3 meetings	High	2 nd , 3 rd

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
9.	 Financial Stewardship and Asset Mgmt	Administration and Support Services	TABOR Revenue Options – explore options for excess sales/use tax collected for operations and maintenance for recreation facilities expansion.	1 meeting	Medium	2 nd
10.	 Reliable Core Services	Transportation	South Boulder Road Connectivity – update on alternatives and approval of design.	1 meeting	Medium	2 nd
11.	 Reliable Core Services	Transportation	Paving Update – review results of updated Pavement Condition Index (PCI) inventory and scores and incorporate specific measurable goals and long-term funding strategies.	1 Study Session, 1 meeting	Medium	1 st , 3 rd
12.	 Reliable Core Services	Utilities	Water, Sewer and Storm Rates – update utility rate model/rate classes.	1 meeting	Medium	2 nd
13.	 Quality Programs and Amenities **Priority Initiative	Parks	Improve Medians/Landscaping – increase efforts to improve the City’s medians and landscaping infrastructure, including forestry.	1 memo, 1 meeting	Medium	2 nd , 3 rd
14.		Open Space and Trails	Open Space/Parks Enforcement – Revisions to Municipal Code for enforcement on open space and parks.	2 meetings	Medium	2 nd , 3 rd

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
	Quality Programs and Amenities					
15.	 Quality Programs and Amenities	Open Space and Trails	Coyote Run – Update and implementation of landslide mitigation.	2 meetings	Medium	1 st , 3 rd
16.	 Quality Programs and Amenities	Open Space and Trails	Open Space Management Plan/Vision – Conduct baseline assessment and determine desired level of service to maintain and improve open space now and into the future.	1 Study Session, 1 meeting	Medium	3 rd , 4 th
17.	 Quality Programs and Amenities	Open Space and Trails	Open Space zoning - Rezoning of existing parks and open space lands	2 - 3 meetings	Medium	2 nd , 3 rd
18.	 Reliable Core Services	Community Design	Marijuana Regulations Update – discuss potential changes to current regulations.	1 – 2 meetings	Medium	1 st
19.	 Quality Programs and Amenities	Community Design	Miners Cabins – complete the relocation/restoration of miners’ cabins.	1 meeting	Medium	2 nd

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
20.	 Collaborative Regional Partner	Community Design	Affordable Housing funding in collaboration with Boulder County and participation in Countywide affordable housing strategies.	2 meetings/ memo updates	Medium	2 nd , 3 rd (timing based on county conversations)
21.	 Reliable Core Services	Community Design	Height Calculations – amend height calculation requirements. Current regulations are difficult to interpret and enforce.	2 meetings	Medium	3 rd
22.	 Quality Programs and Amenities	Community Design	Historic Preservation Funding – review of Historic Preservation Funding Grant Program.	1 meeting	Medium	2 nd
23.	 Vibrant Economic Climate	Economic Prosperity	Redevelopment of Phillips 66 Property - Staff will develop options/tools to understand the market, the development potential and benefits to the community.	2 – 3 meetings	Medium	3 rd , 4 th
24.	 Vibrant Economic Climate	Economic Prosperity	Review BAP Policies – establish administrative policies for Business Assistance Program and review focus of program.	1 meeting	Medium	3 rd
25.	 Vibrant Economic Climate	Economic Prosperity	LRC Update – update and further collaboration with Louisville Revitalization Commission (i.e. capital projects, opportunities, redevelopment efforts).	1 meeting	Medium	1 st , 2 nd

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
26.	 Engaged Community **Priority Initiative	Administration and Support Services	New Technology/Engagement Tools – seek input from City Council on new communication tools (i.e. website redesign, mobile application).	1 Study Session	Medium	2 nd
27.	 Supportive Technology **Priority Initiative	Administration and Support Services	Middle Mile Network – develop plan for completion of City’s middle mile fiber network.	Study Session or Regular Meeting Discussion/ Direction	Medium	3 rd
28.	 Financial Stewardship and Asset Mgmt	Administration and Support Services	2021-2022 Budget Process – investigate and proposed changes to City’s budget process for implementation for 2021-2022 biennial fiscal year budget cycles.	1 Study Session, 1 – 2 meetings	Medium	2 nd , 3 rd
29.	 Quality Programs and Amenities	Administration and Support Services	Energy Future Collaboration Update –update on Energy Future Collaboration between City and Xcel and implementation of goals/strategies.	1 Regular Meeting	Medium	4 th
30.	 Quality Programs and Amenities	Administration and Support Services	Evaluation of all City Council appointees: City Manager, City Attorney, Judge and Prosecuting Attorney prior to 2020 appointments by Council.	2 Regular Meetings	Medium	3 rd , 4 th (CM)

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
31.	 Quality Programs and Amenities	Administration and Support Services	Council Work Plan preparation	1 Regular Meeting	Medium	4 th
32.	 Quality Programs and Amenities	Recreation	Senior Services Update – update on County-wide aging plan and senior services programming.	Memo and 1 Study Session	Lower	1 st , 4 th
33.	 Quality Programs and Amenities	Cultural Services	Funding for Public Art – LCC recommendations for creating revenue stream for public art and other options for expanding public art program.	1 Study Session	Lower	3 rd
34.	 Reliable Core Services	Community Design	PUD Review and Waiver Criteria – consolidate and update criteria.	1 Regular Meeting	Lower	2 nd
35.	 Reliable Core Services	Community Design	Dark Sky Lighting – consider dark sky lighting code changes for residential properties, and further education.	2 Regular Meetings	Lower	3 rd , 4 th
36.	 Quality Programs and Amenities	Open Space and Trails	Open Space zoning: Consideration of annexation of open space and enclaves	1 meeting	Lower	3 rd

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
37.	 Healthy Workforce	Administration and Support Services	Strategic Plan Implementation – update on implementation of Strategic Plan	1 Study Session / memos	Lower	2 nd
38.	 Engaged Community	Administration and Support Services	Board & Commission Interviews/Appointments: - Review process for Board & Commission Appointments - Conduct interviews for Boards & Commissions and determine appointments.	2 meetings	Lower	2 nd , 4 th
39.	 Reliable Core Services	Administration and Support Services	Council Salary Survey – review results of biannual City Council salary survey and potentially create a policy structure around the issue.	1 meeting	Lower	3 rd
	 Reliable Core Services	Other	PUDs/Developments Projects to be Submitted – once applicant has satisfied all submittal requirements and proposal has been reviewed by the Planning Commission, staff will present for consideration.	N/A	N/A	N/A
	 Collaborative Regional Partner **Priority Initiative	Other	Consider Regional Partnerships – continue to consider shared service opportunities with neighboring municipalities (i.e. multi-purpose fields, northwest rail).	N/A	N/A	N/A
	 Reliable Core Services	Other	Consent Items – staff processes small/non-controversial issues by adding to consent agenda for consideration. Council sometimes	N/A	N/A	N/A

2019 Louisville City Council Work Plan

Number ⁱ	Critical Success Factor/ Priority Initiative	Program	Issue	Time Allotment (# Regular Meeting, Study Session and/or Memo)	Priority (High, Medium or Lower)	1 st /2 nd /3 rd /4 th Quarter
			removes these items from consent agenda and discusses during regular meeting.			
	 Reliable Core Services	Other	Municipal Code Updates – staff drafts and presents updates to Municipal Code as part of ongoing efficiency efforts.	N/A	N/A	N/A
	 Reliable Core Services	Other	Unanticipated Issues - each year numerous issues arise that cannot be reasonably foreseen that require Council consideration.	N/A	N/A	N/A

ⁱ Number for reference only. Does not represent priority of item on work plan or within high/medium/lower category.



City of Louisville

Strategic Planning Framework



Introduction

The purpose of the Strategic Plan is to outline how the City can best serve our residents now and into the future. The Strategic Plan will serve as a road map for our organization, to strengthen our organizational culture, and to serve as a communication tool for the community to understand the strategic vision and operating guidelines of the organization.

As an internal, guiding document, the Strategic Plan outlines our operating guidelines for the organization as a whole—our Vision, Mission and Values, as well as our Critical Success Factors—and will help align our organizational culture with the work that we do. In addition, the Strategic Plan includes Priority Initiatives that capture the City’s key priorities for the next one to two years (aligned with the biennial budget process) in each of the Critical Success Factor areas. The City has many initiatives ongoing throughout the year, in addition to the daily operations required to run the City. The Priority Initiatives represent those projects or initiatives occurring in the next one to two years that are above and beyond our daily operations, which represent an increased level of service, have new or additional dedicated resources and funding, and help advance the City’s vision. Together, these elements demonstrate to our residents what we plan to accomplish, and the manner in which we commit to doing our work.

The development of a Strategic Plan has been a priority for City Council and the City Manager, to serve as a singular, guiding document that aligns with the City’s Comprehensive Plan, program-based budget, Home Rule Charter and other planning documents to reflect one unified vision for the organization. Existing plans are still relevant, and will continue to provide direction in key areas of our work.

In addition, the City of Louisville continues to move forward with its program-based budget structure, which includes program areas with specific goals, and sub-programs with detailed objectives. Our progress in meeting these goals and objectives are measured on an annual basis through our Key Performance Indicators (KPIs), and the Strategic Plan reflects how our Priority Initiatives are aligned with these program areas. In essence, the program/sub-program areas reflect all the work of the City that’s performed on a day to day basis, the Priority Initiatives reflect those high-priority efforts that represent an increased financial and resource investment over a period of time, and the Strategic Plan reflects how we do our work.

Thank you for reading this document. We hope it will quickly become a useful tool that becomes an integral part of our organizational operations, and which also will serve to inform our residents about the work we do.

Vision

The City of Louisville – dedicated to providing a vibrant, healthy community with the best small town atmosphere.

Values

Innovation

Leading and embracing change and transformation through creative thinking, learning, and continuous improvement.

Collaboration

Proactively engaging colleagues and other stakeholders in developing solutions through open communication.

Accountability

Fulfilling our responsibilities, owning our actions, and learning from our mistakes.

Respect

Treating people, processes, roles, and property with care and concern.

Excellence

Doing our best work and exceeding expectations with responsive, efficient, and effective customer service.

Mission

Our commitment is to protect, preserve, and enhance the quality of life in our community.

Critical Success Factors



Financial Stewardship and Asset Management



Reliable Core Services



Vibrant Economic Climate



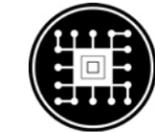
Quality Programs and Amenities



Engaged Community



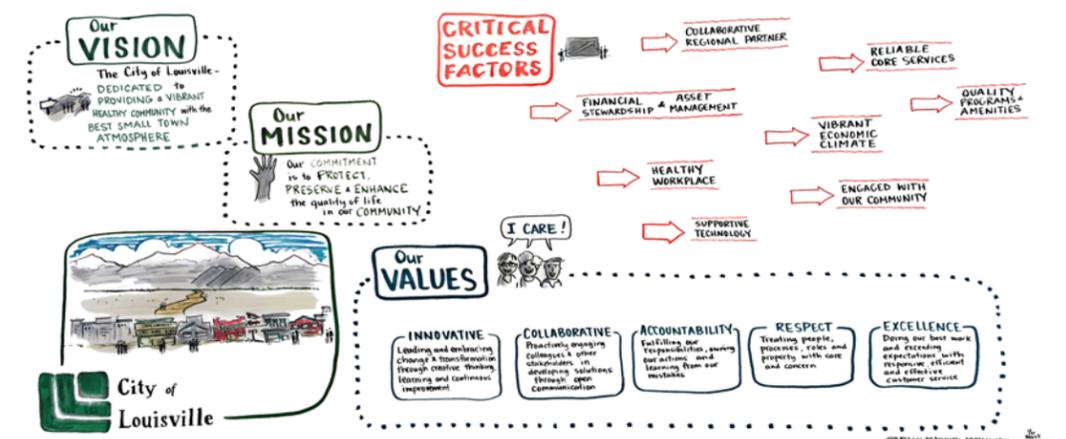
Healthy Workforce



Supportive Technology



Collaborative Regional Partner



Critical Success Factors and 2019 – 2020 Priority Initiatives



Financial Stewardship and Asset Management

The City of Louisville has established financial policies and internal controls to ensure financial sustainability and financial resiliency, and to safeguard the City's assets. The City's recurring revenues are sufficient to support desired service levels and proactively maintain critical infrastructure and facilities. The City practices long-term financial planning through a comprehensive budget process to proactively adjust for changes in financial forecasts. City employees are trusted stewards of the public's money and assets.

2019 – 2020 Priority Initiatives:

- Review and update fiscal policies. (Administration & Support Services)*
- Review finances, fees, and budgets to ensure sound financial structure and fiscal sustainability for the new Recreation Center Fund and Golf Fund. (Administration & Support Services, Recreation)
- Continue implementation of the City's enterprise resource planning (ERP) system, including the implementation of utility billing and electronic time sheets. (Administration & Support Services)



Reliable Core Services

Louisville is a safe community that takes comfort in knowing core services, such as police, roads, water and basic maintenance, are fair, effective, consistent, and reliable. Excellent customer service is provided in the delivery of all City services. The City is prepared for emergencies and offers residents peace of mind knowing basic municipal services are planned for and carried out.

2019 – 2020 Priority Initiatives:

- Complete the City's Transportation Master Plan and identify and implement key investments that will improve the City's transportation infrastructure. (Transportation, Community Design)
- Complete infrastructure improvements outlined in the Capital Improvement Plan, including Citywide paving management upgrades, new water treatment pump station replacing Sid Copeland, and water and sewer line replacement. (Transportation, Utilities)
- Increase efforts to improve the City's medians and landscaping infrastructure, including forestry resources. (Parks, Transportation)
- Complete renovations at the Police Department facility to expand the City's Emergency Operations Center. (Public Safety & Justice)



Vibrant Economic Climate

Louisville promotes a thriving business climate that provides job opportunities, facilitates investment, and produces reliable revenue to support City services. Our unique assets enhance the City's competitive advantage to attract new enterprises, and Louisville is a place people and businesses want to call home.

2019 – 2020 Priority Initiatives:

- Implement recommendations from the McCaslin Area Market Study to support redevelopment within the area. (Economic Prosperity, Community Design)
- Develop a plan to increase proactive retail recruitment for the City of Louisville. (Economic Prosperity)



Quality Programs and Amenities

Excellent programs and amenities sustain the unique experience of living in Louisville. The community enjoys quality facilities and public spaces as well as cultural and educational services that reflect our heritage and are accessible for all. Program performance is evaluated on a regular basis. Opportunities exist to support a healthy mind, healthy body, and healthy community.

2019 – 2020 Priority Initiatives:

- Transition Recreation and Senior Center programming and services to reflect the increased demand associated with the newly expanded facility. (Recreation)
- Complete upgrades to two City playgrounds, and infield improvements at the Louisville Sports Complex. (Parks, Recreation)
- Increase natural resource management activities on City Open Space with the addition of new natural resources staff, including improving native vegetation, increasing weed control, and evaluating the effectiveness of management efforts. (Open Space and Trails)
- Increase programming and hours at the Louisville Historical Museum, and increase program marketing and outreach to grow attendance and participation in all City cultural events. (Cultural Services)

*The City of Louisville has a program based budget and Key Performance Indicators that reflect progress on all program goals. This denotes the program area with which these priority initiatives are associated.



Engaged Community

Louisville residents are informed, involved, engaged, and inspired to be active in community life. The City provides formal and informal opportunities to participate in civic life and transparently shares information using a variety of efficient and accessible approaches.

2019 – 2020 Priority Initiatives:

- Further develop the City's public information and involvement program through additional staffing and resources. (Administration & Support Services)
- Increase transparency around the City's budget, Strategic Plan, and budget program goals through dashboards and other reporting tools. (Administration & Support Services)
- Explore new technology and engagement tools (i.e. mobile application, engagement platform, etc.) to ensure accessible participation for all members of the community. (Administration & Support Services)

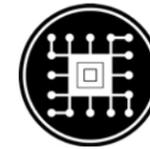


Healthy Workforce

Louisville employees are high-performing public servants characterized as dedicated, engaged self-starters who embody established organizational values and excel in their roles and responsibilities. The City is a healthy workplace that provides competitive compensation and benefits and offers professional development and lifelong learning opportunities for its employees. City employees know they are valued, and they are recognized and rewarded for excellence. Louisville is a place where employees can have a voice in decisions, so collective success is ensured.

2019 – 2020 Priority Initiatives:

- Leverage additional staffing and resources to develop an organizational development and training program that will support our culture of continuous learning, succession planning, and leadership development. (Administration & Support Services)
- Develop a workplace culture initiative that promotes the organizational culture of I CARE and reflects the strategic plan. (Administration & Support Services)



Supportive Technology

Louisville utilizes stable, proven, and relevant technology to enhance and automate City services and to improve the overall customer experience when possible. The use of technology allows the City to make decisions based on accurate and supportable datasets. Supportive technology fosters a culture of learning and innovation.

2019 – 2020 Priority Initiatives:

- Develop a plan for completion of the City's middle-mile fiber network. (Administration & Support Services)
- Utilize additional staffing resources to support data-driven decision-making by training staff to fully leverage technology systems by accessing available data. (Administration & Support Services)
- Implement and build upon existing technology applications and systems that will enhance City services, including Police Department Records Management, Laserfiche records retention, Planning Department Energov, Recreation Center RecTrak, GIS, and other system upgrades. (Administration & Support Services, Public Safety & Justice, Community Design, Recreation)



Collaborative Regional Partner

Louisville is recognized as a regional leader on collaborative issues that cross jurisdictional lines. The City partners with neighboring communities to solve regional problems and to further leverage resources. Louisville cultivates and maintains strong relationships with regional entities and organizations, leads and participates in collective efforts to address issues of mutual interest, and shares ideas and best practices to improve services.

2019 – 2020 Priority Initiatives:

- Work with regional partners to develop approaches to address transportation funding needs. (Administration & Support Services, Transportation)
- Strengthen relationships with local schools and school district. (Administration & Support Services)
- Consider shared service opportunities with neighboring municipalities. (Administration & Support Services)



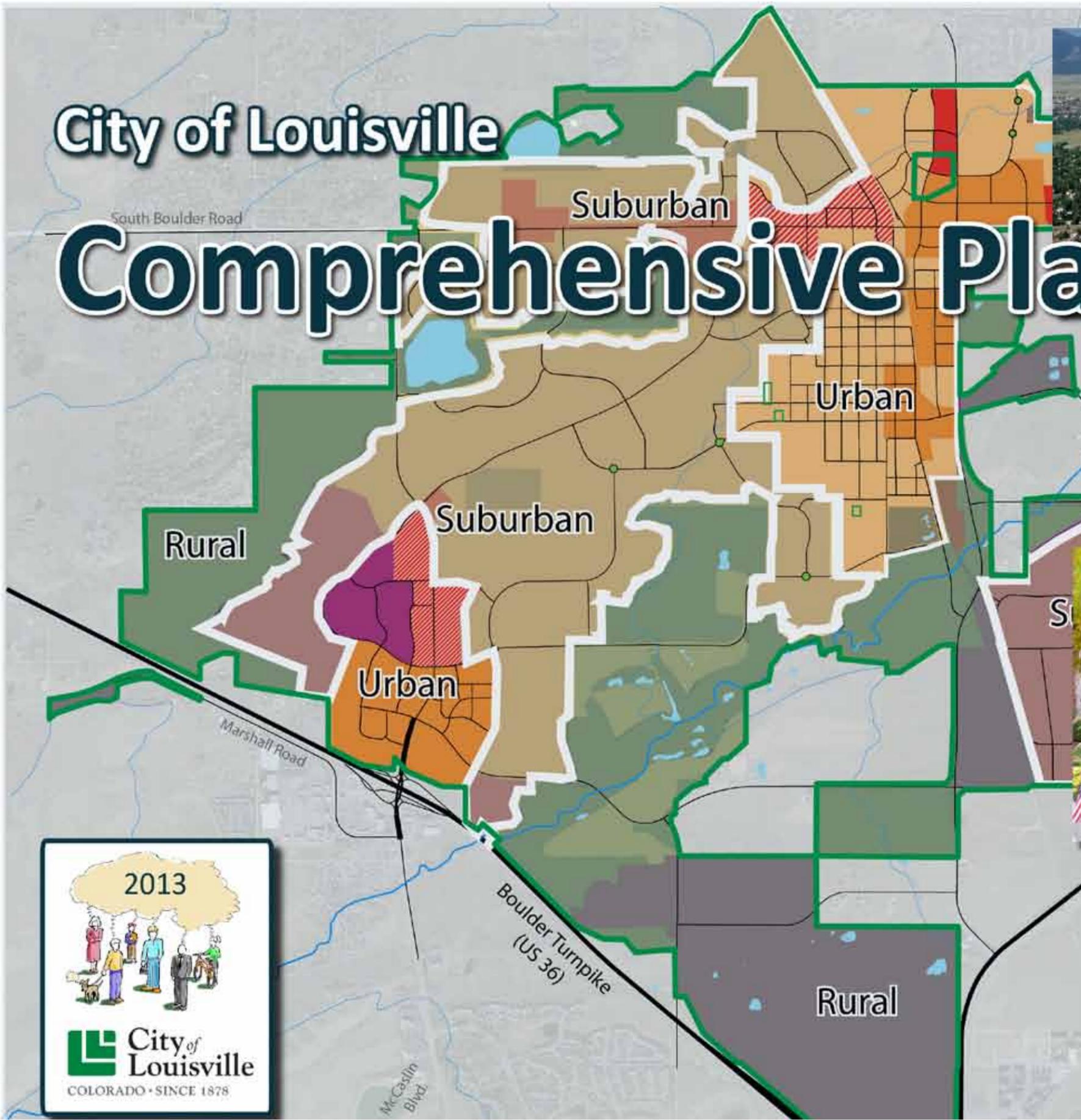
City of Louisville Programs, Goals and Sub-Programs

Programs	Goals	Sub-Programs	Sub-Program Objectives
Transportation	A safe, well-maintained, effective and efficient multi-modal transportation system at a reasonable cost.	Planning and Engineering	Design infrastructure to adopted standards that meets the transportation needs of the City. Collaborate with partner agencies (RTD, CDOT) to ensure residents have adequate multimodal transportation options. Proactively redesign the street network as regulations and technology change our transportation needs over time.
		Transportation Infrastructure Maintenance	Conserve natural resources by maintaining streets cost-effectively before they reach a point of rapid failure. To ensure a high quality of life and to provide services equitably, no street will be in poor condition. Streets and intersections are monitored, maintained, and adequately lit to move people, bikes and cars safely and efficiently. All arterial and collector streets have marked bicycle lanes. All streets have well maintained sidewalks.
		Streetscapes	Safe, visually appealing, appropriately lit and inviting streets, sidewalks and publicly-owned areas adjacent to streets and sidewalks.
		Snow & Ice Removal	Safe traveling conditions for pedestrians and motorists; cost effective snow and ice control services; assist Police, Fire and Emergency Medical Services in fulfilling their duties; safe, passable streets, school bus routes and hard surface trails; safe access to City facilities; and snow cleared within 24 hours from sidewalks that are the City's responsibility.
		Public Works Administration	
Community Design	Sustain an inclusive, family-friendly community with a small-town atmosphere; effective and efficient building services; and effective preservation of the City's historic structures through a voluntary system.	Community Design	A well-connected and safe community that is easy for all people to walk, bike, or drive in. Neighborhoods that are rated highly by residents and thriving commercial areas. An open and inclusive long-range planning process with significant public participation.
		Development Review	Review development applications and enforce the building, zoning and subdivision laws of the city to promote public health, safety, comfort, convenience, prosperity, general welfare and consumer protection.
		Historic Preservation	Provide incentives to preserve the historic character of old town to encourage the promotion and preservation of Louisville's history and cultural heritage. Provide incentives and processes to preserve historic buildings.
Economic Prosperity	Promote a thriving business climate that provides job opportunities, facilitates investment and produces reliable revenue to support City services.	Business Retention and Development	Maintain positive business relationships throughout the community and assist property owners, brokers, and companies in finding locations and/ or constructing new buildings in the City. Attract and retain a diverse mix of businesses that provide good employment opportunities for Louisville residents.



City of Louisville

Comprehensive Plan



2013

City of Louisville
COLORADO • SINCE 1878



Adopted
May 7, 2013
Resolution 18, Series 2013



“Whatever you can do or dream, you can begin it.
Boldness has genius, power, and magic. Begin it now.”

- Johann Wolfgang Von Goethe

CITY COUNCIL

Bob Muckle - Mayor
Hank Dalton - Mayor- Pro Tem (Ward 3)
Emily Jasiak - (Ward 1)
Jay Keany - (Ward 1)
Susan Loo - (Ward 2)
Frost Yarnell - (Ward 2)
Ron Sackett - (Ward 3)

PLANNING COMMISSION

Jeffrey Lipton - Chairman
Chris Pritchard - Vice Chairman
Ann O'Connell - Secretary
Cary Tengler
Jeff Moline
Scott Russell
Steven Brauneis

CITY BOARDS AND COMMISSIONS

Board of Adjustment
Building Code Board of Appeals
Business Retention & Development Committee
Cultural Council
Finance Committee
Golf Course Advisory Board
Historic Preservation Commission
Historical Commission
Horticultural & Forestry Advisory Board
Housing Authority
Library Board of Trustees
Local Licensing Authority
Open Space Advisory Board
Revitalization Commission
Sustainability Advisory Board
Youth Advisory Board

INTEREST GROUPS

Louisville Chamber of Commerce
Downtown Business Association
Centennial Valley Business Association
Colorado Technology Center Metropolitan District
Citizens Action Committee
Centennial Heights West HOA

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ACKNOWLEDGEMENTS *i*

INTRODUCTION **1**

 Purpose 1

 Background 1

 How to use this Plan 2

THE PROCESS **3**

 Outreach 3

THE PLANNING CONTEXT **5**

 A Quick History 5

 The Context 6

 Demographics

 Existing Land Uses

 Natural Environment

 Built Environment

 Block Structure

 Municipal Utilities and Infrastructure

 Building Inventory

 Circulation

 Market Opportunities

 Fiscal Analysis

 Stability and Change

THE VISION STATEMENT AND CORE COMMUNITY VALUES **17**

 Vision Statement 17

 Core Community Values 18

 Character Zones 19

 Development Patterns

 Urban Pattern

 Suburban Pattern

 Rural Pattern

 Development Types

 Centers

 Corridors

 Neighborhoods

 Special Districts

 Parks and Open Space

THE FRAMEWORK **24**

 Street Types and Land Use

 Downtown and the Highway 42 Revitalization District - Center 25

 McCaslin Boulevard (South of Cherry) - Center 27

 Highway 42 and South Boulder Road - Center 29

 South Boulder Road and Highway 42 - Corridor 31

 McCaslin Boulevard (North of Cherry) - Corridor 33

 Centennial Valley and Coal Creek Business Park - Special District 35

 Colorado Technology Center (CTC) - Special District 35

 96th and Dillon - Special District 35

 Phillips 66 - Special District 35

 Empire Road - Special District 35

 Neighborhoods 37

 Transportation, Mobility, and Accessibility 39

 Background and History

 Analysis and Recommendations

 Community Heritage 43

 Parks, Recreation, Trails and Open Space 45

 Municipal Infrastructure 47

 Energy 47

 Community Services 48

 Schools

 Library Services

 Police and Fire Services

 Health Services

 Solid Waste Services

 Community Events

 Arts and Culture

 The Economic Development and Fiscal Health 49

 Economic Development

 Fiscal Health

POLICY ALIGNMENT & IMPLEMENTATION **51**

 Small Area Plans and Neighborhood Plans

 Louisville Municipal Code Amendments

 The City's Operating and Capital Improvement Budget

 The Zoning Map

 Existing Zoning Agreements

 Compliance with Intergovernmental Agreements

 Policy Alignment 52

 Implementation Action Items 52



Introduction

Louisville, Colorado from its beginnings as a mining town in 1878 to today has become one of the most livable small towns in the United States. Louisville's evolution will continue to be influenced by changes in environmental factors; economic conditions; social and demographic profiles; and physical influences (i.e. US 36 changes) occurring in Louisville, neighboring jurisdictions and the greater Denver metropolitan region.

Clearly, the City's leaders, residents, property owners, and businesses have done an exceptional job. The positive results of the City's Citizen Survey place Louisville in the highest echelon of municipalities in the United States for citizen satisfaction. However, cities and their environments do not remain static and Louisville's opportunities and challenges in maintaining a high quality of life are continually evolving and transforming.

Purpose

The Comprehensive Plan is the City's tool intended to guide, integrate and align governing regulations, infra-

structure investments, and City services with community values, needs and civic priorities. Louisville's Comprehensive Plan provides the citizens a voice in envisioning and guiding the City's continual evolution.

The Comprehensive Plan is the official statement of the City's Vision and corresponding Core Community Values. The policies contained within the Plan cover a broad range of subject matter related to the long-range (20 year) physical growth of the City. Nine elements function to complement each other in directing future policy decisions towards implementing the Community's Vision and preserving vital community attributes and service levels. These include:

1. Community Form, Character, and Urban Design
2. Neighborhoods and Housing
3. Transportation, Mobility, and Accessibility
4. Community Heritage
5. Parks, Recreation, Trails and Open Space (reference Parks Recreation Open Space and Trails

6. Master Plan (PROST -2011))
7. Municipal Infrastructure
8. Energy
9. Community Services
10. The Economy and Fiscal Health

Background

Louisville's first Comprehensive Plan was adopted in 1973 when the City had only 2,600 residents, and was then updated in 1975. New Comprehensive Plans were adopted in 1983 (updated in 1989) and 2005 (updated in 2009). The 2012 Comprehensive Plan update will further strengthen the Comprehensive Plan in two key ways:

1) Better meet today's unique challenges that were not factors in 2005 and 2009.

Several conditions that influence the City's ability to implement the Community's Vision have changed, or emerged. These conditions include:

a. Redevelopment vs. new development – The General Development Plan (GDP) approval for Phillips 66 and the Planned Unit Development (PUD) approval of North End and Steel Ranch entitle the City's last large vacant parcels for development. Future change in Louisville will come almost exclusively in the form of redevelopment. Previous Comprehensive Plans noted the shift in growth patterns; but, they did not provide the necessary tools for the community to adequately review, discuss, and respond to inevitable future infill development requests.

Development issues and concerns of an expanding greenfield community are quite different than those of a redeveloping infill community. Louisville's previous policies generally align with those of an expanding greenfield community. Previous policies focused on measuring, accommodating and mitigating the impact of new development on the capacity of the City's infrastructure, services and quality of life.

In a redeveloping infill community, the capacity of community infrastructure and services is still a concern. However, efficiency—the ability to achieve economies of scale by using existing infrastructure to serve existing

customers at a lower unit cost to each customer—also becomes a consideration. Because infill development can positively or negatively affect existing land uses, understanding how the design, physical character and other aspects of an infill project affect the adjacent neighbors and the City as a whole is critical to determining how the project will impact the existing quality of life.

This Comprehensive Plan provides not only the flexibility and guidance to address redevelopment in the HWY 42 Revitalization District and Downtown, but throughout the City as well. The Plan provides clear policies to guide redevelopment as the McCaslin Boulevard and South Boulder Road corridors age and as infill residential rehabilitation pressures continue to increase in all established residential neighborhoods.

b. Regional traffic and City transportation policy – As new development continues in surrounding jurisdictions, Louisville will experience a decreasing share of local traffic on its street network. Future transportation investments in the City will be challenged to accommodate demands for regional traffic mobility and at the same time address livability and economic viability concerns within Louisville.

Louisville's transportation policies and regulations were designed for an expanding community, and do not adequately address the realities of a landlocked and redeveloping City. The City's transportation regulations have begun to shift away from a focus on regional mobility concerns designed to accommodate vehicular traffic, roadway capacity, and safety features for higher speed environments. Louisville's new transportation priorities will be aligned with multimodal transportation, roadway efficiency, property access, and safety features for slower speed environments.

This Comprehensive Plan recognizes the inherent conflicts between regional mobility needs, local property access and quality of life requirements, and aims to provide a balance between community and transportation policies which effectively guide future investments within Louisville.

Please circle the number that comes closest to your opinion about the quality of life in Louisville:						Total	National comparison	Front Range comparison
	Excellent	Good	Fair	Poor				
How do you rate Louisville as a place to live?	78%	20%	2%	0%	100%		Much above	Much above
How do you rate Louisville as a place to raise children?	77%	20%	2%	0%	100%		Much above	Much above
How do you rate the overall quality of life in Louisville?	67%	30%	2%	0%	100%		Much above	Much above
How do you rate your neighborhood as a place to live?	62%	33%	5%	0%	100%		Much above	Much above
How do you rate Louisville as a place to retire?	51%	35%	11%	3%	100%		Much above	Much above
How do you rate Louisville as a place to work?	37%	37%	19%	7%	100%		Much above	Much above

* Source – City of Louisville Citizen Survey – May 2012



c. **The economy and realities of retail growth** – The downturn in the economy since 2008 and the new realities of regional retail competition, access/visibility of retail sites and new retailing practices require more community based approach to economic development and future sales tax revenues.

Revenue generating regional retail development has moved into adjacent communities of Broomfield, Superior, and Lafayette. Future retail growth trends suggest a continued consolidation and shift in retail away from Louisville, particularly toward communities along the US 36 and the I-25 North corridor. The McCaslin Boulevard Corridor south of Cherry Street remains attractive to regional retail opportunities. However, the form of regional retail has changed significantly since the early 1990s and the original Centennial Valley development approval.

This Comprehensive Plan addresses the evolving pattern of regional retail opportunities near US 36 and the general shifting of regional retail opportunities to formulate guiding policies which ensure the City's future fiscal and economic health.

d. **Neighborhood issues and concerns** – Previous Comprehensive Plans have been silent on neighborhood issues and concerns. The City's residential housing stock is aging and rehabilitation issues within residential areas challenge City resources on a daily basis.

Outside of the Old Town Overlay District, the City's residential areas are governed by independent planned unit developments (PUDs). While these PUDs are comprehensive, they are not equipped to assist the City in providing coherent neighborhood plans and strategies for issues such as: housing rehabilitation, cut-through traffic, safe routes to school, aging infrastructure, and monitoring and maintenance of community services.

This Comprehensive Plan outlines a new city-wide neighborhood planning policy with specific planning areas to ensure proper attention is given to the City's unique and diverse neighborhoods.

2) *Better clarify the Community's Vision in terms of community character and physical design to provide the public and staff with a common language and tools to review and discuss redevelopment requests*

The City of Louisville is a diverse community with a number of unique character areas. Other than Downtown and Old Town, the previous Comprehensive Plans did not identify, differentiate, or celebrate, these unique character areas as they relate to the Community Vision.

Clearly, South Boulder Road and its proximity to adjacent land uses are very different than Centennial Valley and its adjacent land uses. The neighborhoods near Davidson Mesa are different from those near Fireside Elementary. The Comprehensive Plan now clarifies and celebrates the differences and outlines policies which guide recommended changes in the Louisville Municipal Code (LMC) that will regulate the form of buildings and community character in each of Louisville's neighborhoods and different commercial districts.

How to Use this Plan

The Comprehensive Plan is a conceptual guide to review and take action on land use initiatives in the City of Louisville. The document is divided into five sections.

- The first section, the Process, describes the public involvement and community outreach efforts used to generate the Comprehensive Plan.
- The second section, the Planning Context, describes the current conditions of the City along with the key trends and challenges facing the City.
- Sections 3 and 4, the Vision Statement and Core Community Values and the Framework, identify the Community Vision, a Conceptual Land Use Framework and specific policies for the structural elements of the Comprehensive Plan.
- The final section of the document, **Policy Alignment and Implementation**, outlines the City's administration and implementation of the Comprehensive Plan.

It is important to note that the Comprehensive Plan is not regulatory. It is an advisory document. Since the Comprehensive Plan does not have the force of law, the City must rely on other regulatory measures to implement the Comprehensive Plan. The Louisville Municipal Code (LMC) is the primary regulatory tool available to the City. Specifically, Buildings and Construction (Chapter 15), the Louisville Subdivision (Chapter 16) and Zoning Ordinances as adopted (Chapter 17) and the zoning map of the City. Additional documents include Small Area Plans, Neighborhood Plans, the Annual Operating and Capital Budget and the Capital Improvement Program.

The LMC chapters on Buildings and Construction, Subdivision, Zoning ordinances, along with the official zoning map control the allowed uses of land as well as preservation and construction requirements and design and bulk standards. The official zoning map reflects a number of zone districts which govern where uses by right and uses by special review may be located. The Subdivision and Zoning ordinances should correspond to the goals and policies of the Comprehensive Plan to ensure that incremental development decisions reflect the Community Vision. All land use applications are reviewed for conformance with the Louisville Municipal Code. All annexations and rezonings are reviewed for conformance with the Louisville Municipal Code and conceptual consistency with the Comprehensive Plan.

The Framework Plan is a map which reflects preferred land use patterns and community character zones for specific geographical areas. The designations are illustrative and are not intended to depict specific uses, densities, or yard and bulk standards for parcel specific locations.

Uses, densities, and yard and bulk standards for individual parcels are conceptual and will be refined in small area and neighborhood plans and implemented through changes to the Louisville Municipal Code.

Louisville Municipal Code Section 17.62.050 (Time for review) states "A review and updating of the comprehensive plan shall occur at least every four years. Ad-

ditional reviews of the comprehensive plan may occur more often as necessary". A Plan review provides the City an opportunity to update the Community Vision and Core Community Values Principles and Policies. Based on this principle, the next review of the Plan shall occur in 2017.



The Process

The process of drafting this Comprehensive Plan represents the results of the collaborative efforts of community stakeholders: residents, business owners and operators, public and private organizations in the City, as well as the City Council, Planning Commission, and all of the City's Citizen boards and commissions. This Comprehensive Plan Update was developed by City staff following a five-phase process of Desire, Discovery, Design, Discussion, and Documentation.

The first phase of work, **Desire**, focused on updating the City's Vision Statement and corresponding Core Community Values to guide the entire process. The second phase, **Discovery**, allowed City staff and its consultants to discover the functioning of the community, its economic variables, physical characteristics, and regulatory framework. The third phase, **Design**, brought the Planning Team and the community together to draft specific alternative physical framework options for consideration. The fourth phase of work, **Discussion**, allowed City staff to test and refine each alternative and facilitate a community dialog to identify a preferred framework plan which best represents the City's Vision

Statement and Core Community Values. The last phase, **Documentation**, allowed City staff to finalize the document and outline specific implementation strategies.

Outreach

The City utilized an extensive community outreach process for the Comprehensive Plan. Staff participated in and facilitated over 60 public meetings along with a continuous on-line discussion through the www.EnvisionLouisvilleCO.com web-site with over 160 participants. The complete outreach effort involved over 500 participants and specifically included:

Envision Louisville CO – Interactive Website - The City engaged MindMixer, an Omaha, NE firm, to develop, support and maintain a website capable of hosting web-based town hall meetings promoting an exchange of information and ideas related to the 2012 Comprehensive Plan Update. Over one hundred sixty (160) participated in the on-line discussions.

The first 90 days of the on-line discussions focused exclusively on the Louisville Vision Statement and the

Community Core Values. The second 90 days focused on the Framework Plan and concerns related to specific areas within the City. The final 90 days of conversations related to the drafting of specific elements within the Comprehensive Plan. This simple platform generated a broad audience, a more inclusive dialog and effective community participation.

Community Design Charrette & Public Meetings - A series of public meetings and workshops were held to engage the community on key decision points. The public meeting process included:

Public Kick-off - Vision Statement and Core Community Values Meeting – March, 2012 (DESIRE) - A public kick-off meeting was held as an introduction of the planning process and included a "post-it" note exercise to gather public ideas and input related to the City's Vision Statement and Core Community Values. During the exercise attendees were asked to write down what they value the most in the City.

Community Design Charrette and Open House – August 27-30, 2012 (DESIGN) - A four-day design workshop was organized as a series of meetings and presentations open to the public to develop and refine alternative Framework Plans which would guide the City's growth for the next 20-years. The charrette started with a public presentation and round table discussions. The discussions were designed to facilitate the public in generating alternative Framework Plans. The second day of the charrette was open to the public and concluded with an evening public meeting which allowed the public to refine specific Framework Plan alternatives generated the first night. Day three was open to the public as alternative Framework Plan options were presented to and refined by the City's senior management team. The charrette concluded on the fourth day with a public presentation, where the results of the four-day effort were presented and a community dialog was initiated to identify a preferred 20-year framework Plan for the City's Comprehensive Plan.

Public Meeting - October, 2012 (DESIRE & DISCOVERY) - A final public meeting presented the four refined



Framework Plan options generated during the design charrette. Specific impacts associated with each alternative were presented and discussed. A community dot exercise was conducted to facilitate community feedback on a preferred alternative.

City Board and Commission Meetings (DESIRE & DISCOVERY) – The Comprehensive Planning effort included two rounds of public meetings with each of the City’s sixteen Citizen boards and commissions. The meetings were organized with the Desire and Discovery Phases of work. The first round of meeting focused on the modification and creation of the City’s Vision Statement and Core Community Values. The second round of meetings focused on the alternative Framework Plan options generated during the Community Design Charrette.

Special Meetings (DESIRE & DISCOVERY) – Concurrent with the meetings conducted with the City’s boards and commission, Planning Staff facilitated two rounds of meetings with specific stakeholder and interest groups. The meetings were organized with the Desire and Discovery phases of work. The first round of meeting focused on the modification and creation of the City’s Vision Statement and Core Community Values. The second round of meeting focused on the physical Framework Plan options generated during the Community Design Charrette. These meetings included presentations and discussions with the Louisville Chamber of Commerce, the Downtown Business Association (DBA), the McCaslin Business Association, The Colorado Technology Center Business Association, Koelbel Properties, and Citizen Action Committee.

City Council and Planning Commission Study Sessions and Meetings (DOCUMENTATION) – Fourteen Study Sessions or Public Hearings were conducted with the Louisville Planning Commission and City Council. Five items were forwarded to the Planning Commission and City Council. Each item represented key decisions in the generation of the 2012 Comprehensive Plan. After the project scoping, the first item brought to the Planning Commission and City Council was the City’s updated Vision Statement and corresponding Core Community

Values for endorsement. Following the Community Design Charrette staff forwarded a recommendation of the Community Framework Plan for endorsement.

The Draft Plan was reviewed by the Planning Commission in two study sessions and the Final document was forwarded to City Council and approved by Resolution 18, Series 2013



The Planning Context

A QUICK HISTORY

Louisville was founded on October 24, 1878, when Louis Nawatny, a manager for the Welch mining operations, laid out a town site near the newly opened coal field and named it after himself. The new settlement was stimulated by the railroad and depended upon it to transport coal. Mining for coal was the genesis for many of the towns in eastern Boulder County.

Louisville grew vigorously with the rapid industrialization of the area's mines. In the wake of a post-Civil War migration, the town's first settlers came from such places as the United States, the United Kingdom, Austria, and Germany, among others. Later, in the 1890s, Italian and Eastern European immigrants, in search of mining work, began populating the area. By 1911, eleven additional residential subdivisions were added to original Louisville. The layout of the town and its population of roughly 2,000 would remain unchanged for several decades. Most houses were small, wood frame structures, with tidy yards, vegetable gardens and space to raise chickens and rabbits in the back.

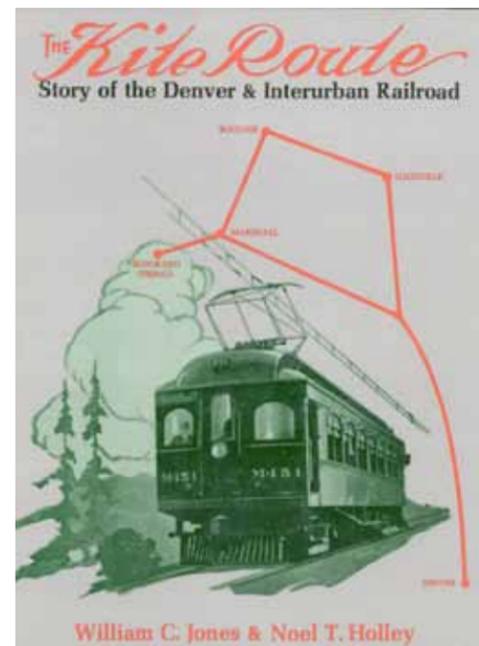
Despite the ethnic differences among groups, most residents lived in harmony. Louisville was homogeneous in that nearly everyone was similarly situated in economic terms. Mining for coal didn't make miners rich, but one could make enough to support a family if one lived modestly. Given the modest incomes, people made do with what they had. Even houses were relocated to where they could be put to better use.

Saloons and billiard halls assumed a very important role in the community. The town boasted an amazing number of drinking establishments, which acted as meeting, eating, sleeping, and relaxing spots. Since Louisville's bars catered to the rough-and-tumble mining crowd, they were restricted by town ordinance to Front Street. By 1908, at least thirteen saloons were in operation along three blocks of Front Street.

The "Denver & Interurban Rail Road." or "The Kite Route" began serving Louisville with electric transportation in 1908. It brought fast, clean, quiet, efficient trans-



City of Louisville - Land Use and Transportation: 1878 to 1909



portation to the town. The Interurban system was established between Boulder and Denver, including a single stop in Louisville. Operations ended in 1926 because of competition from busses and cars.

After World War I, U.S. mines began to close. Simply, the industry found itself with too much supply. Rising competition from other fuels further threatened the coal industry. Coal and railroad revenues further declined with the construction of a natural gas pipeline from Texas to Denver in 1928 and with the gaining popularity of the automobile.

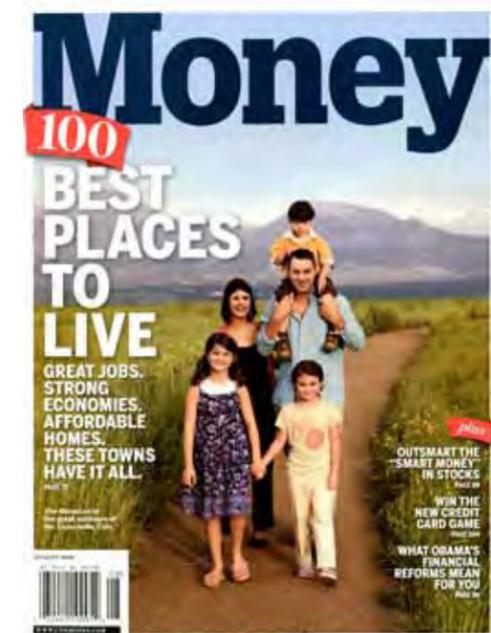
As the last mines were closing in the 1940s and 1950s, Louisville experienced a critical transition. Although the mine closures were a dreaded occurrence, it was only with the end of the coal mining era that Louisville was able to evolve into a modern city. Voters in 1951 approved a bond issue to fund a sewage system, bringing an end to the use of outhouses, and the town paved its streets. The last mine closed in 1955. The Rocky Flats Nuclear Weapons Facility, southwest of Louisville, and other new technology industries, became the area's new primary employers. StorageTek would become a major employer starting in the 1970s.

In 1962, Louisville became a City of Second Class, as defined by the state, having exceeded the state's 2,500 population limit for towns. Modern subdivisions began to be added and the population grew to 19,400. An emphasis on commercial growth along McCaslin Boulevard and South Boulder Road led to many of the historic buildings downtown being left intact.

In 1978, Louisville celebrated the 100th anniversary of its founding with a year of activities, a proclamation from the Governor, a special Labor Day parade, and a commemorative medal. The reflection by many on the community's history led to the establishment of the Louisville Historical Commission in 1979 and the opening of the city-owned Louisville Historical Museum. Twelve Louisville structures were selected to be listed on the National and State Registers of Historic Places. Louisville became a Home Rule City in 2001.



City of Louisville - Land Use and Transportation: 1910 to 2012



The Planning Context

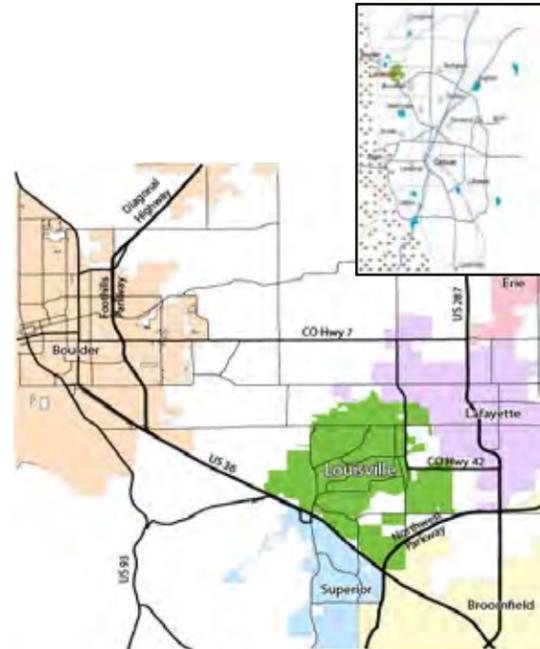
Preserving the past is important to the residents of Louisville. The Louisville Historic Preservation Commission was established in 2002 and a historic preservation ordinance was approved in 2005. Voters in 2008 approved an increase in sales tax for the creation of the Louisville Historic Preservation Fund.

Parks and Open Spaces are also critical components to the desirability of Louisville. The City manages approximately 2,000 acres of open lands. These lands provide visual buffers between local municipalities, support many species of wildlife and diverse plant communities, provide recreational activities through an extensive trail network, and allow agricultural backdrop by maintaining private farming activities in rural areas. The Louisville Open Space Advisory Board was established in 2000. Voters in 2002 and again in 2012 established and continued an increase in the sales tax to fund acquisition, development, and maintenance of parks and open spaces.

Louisville began to achieve national recognition for being among the best places to live in the 2000's. Money Magazine, in its biennial listings of the Best Places to Live in the United States for smaller towns and cities, listed Louisville, Colorado as #5 in 2005; #3 in 2007; and #1 in both 2009 and 2011. Bert Sperling's 2006 book Best Places to Raise Your Family: Experts Choose 100 Top Communities That You Can Afford listed Louisville as the "best of the best" at #1. In 2012, Family Circle magazine placed Louisville among the top ten "Best Towns for Families" based on a survey of 3,335 municipalities with populations ranging from 11,000 to 150,000.

THE CONTEXT

Louisville is now a city of approximately 18,400 people and is roughly 8.0 square miles in size. Louisville is located in southeastern Boulder County, about 6 miles east of the City of Boulder and 19 miles northwest of Denver. US Highway 36 forms the southwest border of Louisville, and the Northwest Parkway runs adjacent to the southeast corner of the City, connecting Louisville to US Interstate 25 (I-25). The Interlocken Business Park and the Rocky Mountain Metropolitan Airport



are located southeast of the City of Louisville along US Highway 36. The City of Louisville lost population since the 2000 census because of an aging population and an overall reduction in average household sizes.

Many physical, social, economic and political elements influence Louisville's continued evolution. This section of the Comprehensive Plan describes the basic elements which influence Louisville's current form and physical character as well as what elements are expected to influence the City's evolution over the next 20 years.

The description of these planning elements will be city-wide and divided into six primary areas: Natural Environment, Demographic Conditions, Built Environment, Circulation System, Land Uses, and Market Opportunities. The Planning Context will conclude with key findings, along with an identification of where Louisville is expected to experience change and extended stability over the next 20 years.

Demographics

Staff and the consultant team performed a baseline demographic and economic profile to identify factors which will influence future market conditions and

economic opportunities for the City of Louisville over the next 20 years. This is a summary of a more comprehensive analysis. A complete demographic analysis is documented under separate title and is included as an appendix to the Comprehensive Plan.

The demographic analysis used a regional approach to include the characteristics of households and employment opportunities within commuting distances of Louisville. For comparison purposes and broader geographic context, Boulder County and the State of Colorado are profiled as primary peer geographies. Where appropriate, the cities of Lafayette, Superior, Broomfield and Denver are profiled as secondary geographies.

Population and Households

The City of Louisville actually saw a decrease in its population from 2000 to 2010. However, Boulder County experienced a 1.1% increase, compared to a 9.7% increase for the nation over the same period. The cities of Superior and Broomfield saw astounding population and household increases from 2000 to 2010. The state experienced relatively robust increases in population of 13.6% and households of 15.6%.

Despite a decline in population, the number of households in Louisville increased 5.1% over the decade. This dichotomy occurred in large measure due to the 8% decrease in average household size throughout the City.

Race and Ethnicity

The majority of the population of Louisville is white (86%), with those of Hispanic origin making up the second largest group (7%). Louisville has a higher percent-

age of white residents than Boulder County as a whole (79%) and much higher than the Denver metro area average (52%).

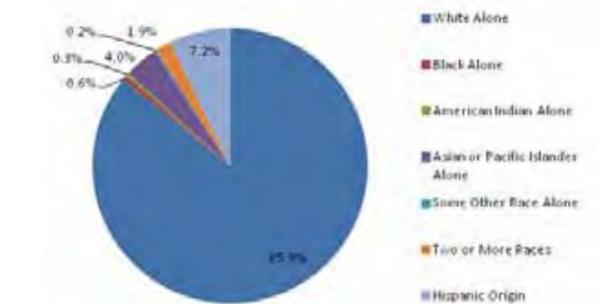
Age Levels

The median age of Louisville's residents is higher than that of the peer geographies. This aging population corresponds to smaller household sizes as children leave the household. Louisville's median age falls within the 25-55 age bracket, which comprises the majority of the employed population. The lowest 2010 median age among peer geographies is 31.7, in the City of Superior.

	2000	2010	Change
City of Louisville	35.8	38.9	8.7%
City of Lafayette	33.8	37.0	9.5%
City of Superior	30.6	31.7	3.6%
City of Broomfield	33.8	36.7	8.6%
Boulder County	33.5	35.3	5.4%
City of Denver	33.1	33.7	1.8%
State of Colorado	34.4	35.8	4.1%

Source: US Census

Median Age



Race and Ethnicity

Population and Households

Jurisdiction	Population			Households			Avg. HH Size		
	2000	2010	Change	2000	2010	Change	2000	2010	Change
City of Louisville	18,868	18,376	-2.6%	7,165	7,529	5.1%	2.62	2.41	-8.0%
City of Lafayette	23,197	24,453	5.4%	8,844	9,632	8.9%	2.54	2.62	3.1%
City of Superior	9,011	12,483	38.5%	3,381	4,496	33.0%	2.67	2.78	4.1%
City of Broomfield	38,272	55,889	46.0%	13,833	21,414	54.8%	2.77	2.60	-6.1%
Boulder County	291,288	294,567	1.1%	114,793	117,629	2.5%	2.45	2.44	-0.4%
City of Denver	554,636	600,158	8.2%	251,435	263,107	4.6%	2.27	2.22	-2.2%
State of Colorado	4,301,261	4,887,061	13.6%	1,659,308	1,918,959	15.6%	2.53	2.49	-1.6%

Source: US Census



The Planning Context

Household Income

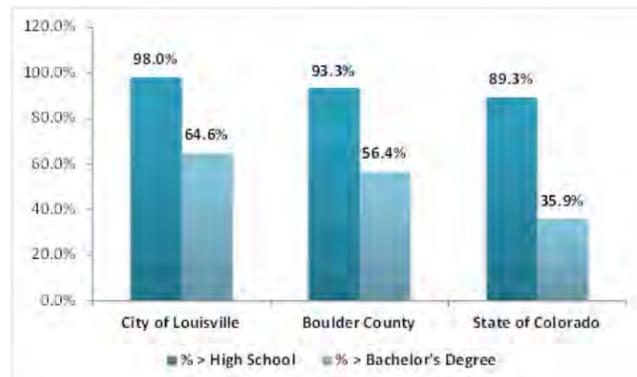
Residents of Louisville enjoy a level of household income nearly 25 percent higher than the median Boulder County income and approximately 44 percent higher than the state's median income, based on 2010 median household income. The highest median household income among peer jurisdictions in 2010 is the City of Superior, at \$96,130.



Median Income

Educational Attainment

Louisville's population is very well-educated relative to nearby populations, with approximately 64 percent of the population achieving bachelor's degrees or higher, compared to 56 percent in the County and 36 percent in the State. The percentage of high school graduates is also higher, at 98 percent in Louisville compared to 93 percent and 89 percent in the County and State, respectively. A highly-educated workforce is a key element to attracting and retaining high technology industries and advanced professional employers, as well as diversifying the economic base of an area.



Educational Attainment

Employed Population

Louisville's generally well educated employed population over 16 years of age is comprised of 81 percent white collar workers, 11 percent service workers, and 7 percent blue collar workers. Over 22 percent of the white collar workers are employed in the management/business/financial sector, while the majority (36 percent) is in the professional sector.

Occupation Category	Share
Total	10,136
Management, business, science and arts occupations	60.1%
Service occupations	11.6%
Sales and office occupations	20.2%
Natural resources, construction and maintenance occupations	4.0%
Production, transportation, and material moving occupations	4.1%

Source: U.S. Census; TischlerBise

Employment Sectors

Inflow/Outflow Characteristics

Although Louisville had a net daily inflow of 1,023 workers in 2010, 92 percent of its 11,159 at-place employees commuted into their jobs from outside of the city. Conversely, 91 percent of Louisville's employed workforce of 10,136 commuted to jobs outside of the city. Only 918, or 9 percent of Louisville's workforce, lived and worked in Louisville.

Labor Market Size	Count	Share
Employed in the City of Louisville	11,159	100.0%
Living in the City of Louisville	10,136	90.8%
Net job inflow (+) or outflow (-)	1,023	

Labor Force Efficiency	Count	Share
Living in the City of Louisville	10,136	100.0%
Living and employed in Louisville	918	9.1%
Living in Louisville but employed outside	9,218	90.9%

Employment Efficiency	Count	Share
Employed in the City of Louisville	11,159	100.0%
Living and employed in Louisville	918	8.2%
Employed in Louisville but living outside	10,241	91.8%

Source: U.S. Census Bureau OnTheMap Application; TischlerBise

Labor Inflow / Outflow

Existing Land Uses

Louisville's geographic expansion is near completion. All first generation development has been planned and entitled for the City. Open space and inter-governmental agreements limit Louisville's future expansion to the approximately 12 acres of the Alkonis Property in the

northeast portion of the City near the Steel Ranch Sub-division.

The principal land use in the community is residential low-density, encompassing approximately 26% of the City's total land area. Open space is also a significant contributor to the City of Louisville's physical form and quality of life. Approximately 26% of the City's land area is dedicated to open space, parks, and public spaces.

Currently, nearly 20% of the City's developable land remains vacant. Low-density residential land uses encompass 53% of the total built environment in the City (9 million square feet). The next largest built land uses are: industrial (13%); office (9%); various retailing land uses (8%).

Future growth in the City will focus on infill development. Louisville will now experience second-and-third generation development. Growth trends for the future have shifted from expansion to reinvestment, refurbishment, and redevelopment. Louisville's building stock will continue to age and will require continued improvement and reinvestment to remain economically viable. In the residential land use categories, Louisville has a higher proportion of single family units to multifamily units than its surrounding geographies, at 78 percent compared to 71 percent in Boulder County and 72 percent in the State.

Land Use	Built SF	Lot SF	Built % of Total	Land % of Total
Residential Low Density	9,504,062	50,560,307	53.9%	26.5%
Industrial	2,380,013	9,915,625	13.5%	5.2%
Office	1,608,285	6,420,221	9.1%	3.4%
Residential High Density	1,208,383	3,229,609	6.9%	1.7%
Residential Medium Density	651,142	2,522,050	3.7%	1.3%
Vacant	638,026	36,560,214	3.6%	19.1%
Multi-Tenant Retail	263,566	1,227,664	1.5%	0.6%
Hotel	256,867	748,987	1.5%	0.4%
Single Tenant Retail	247,273	1,514,086	1.4%	0.8%
Mixed Use Commercial	246,747	1,358,985	1.4%	0.7%
Large Format Retail	232,542	1,021,325	1.3%	0.5%
Public Service/ Institutional	206,691	16,737,125	1.2%	8.8%
Stand Alone Restaurant	100,544	621,915	0.6%	0.3%
Entertainment	53,742	399,183	0.3%	0.2%
Agricultural	18,626	6,768,074	0.1%	3.5%
Mixed Use Residential	8,848	42,469	0.1%	0.0%
Mobile Home	1,782	694,901	0.0%	0.4%
Open Space/Parks	1,780	50,696,337	0.0%	26.5%
Total	17,628,919	191,039,078	100.0%	100.0%

Existing Land Uses

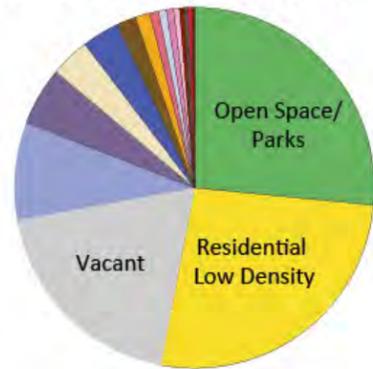
	City of Louisville		Boulder County		State of Colorado	
	Estimate	Percent	Estimate	Percent	Estimate	Percent
Total housing units	7,814		125,768		2,176,600	
Occupied housing units	7,529	96.4%	117,629	93.5%	1,918,959	88.2%
Owner occupied	5,537	73.5%	75,189	63.9%	1,296,670	67.6%
Renter occupied	1,992	26.5%	42,440	36.1%	622,289	32.4%
Avg. HH size of owner occupied unit	2.67		2.51		2.57	
Avg. HH size of renter occupied unit	1.68		2.13		2.31	
Median value of owner occupied units	\$361,200		\$353,300		\$236,600	
Single family units	6,125	78.4%	88,853	70.6%	1,558,501	71.6%
Multifamily units	1,561	20.0%	33,000	26.2%	517,228	23.8%
Mobile homes	128	1.6%	3,915	3.1%	99,621	4.6%

Source: US Census

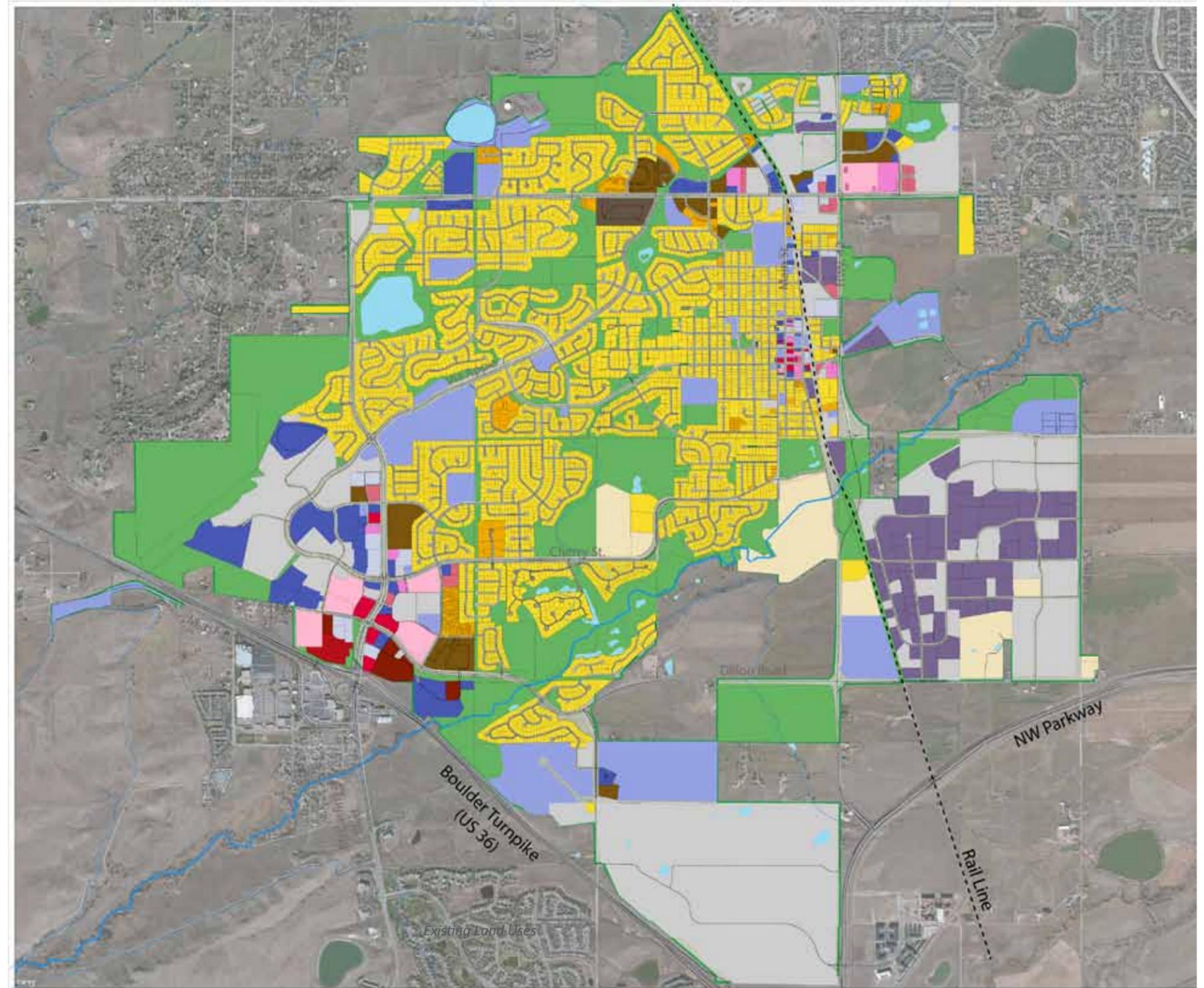
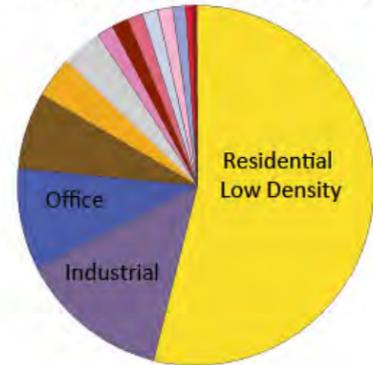
Housing and Household Information



Land Associated with Each Land Use



Built Square Footage of Each Land Use



Existing Land Use

The Planning Context

Natural Environment

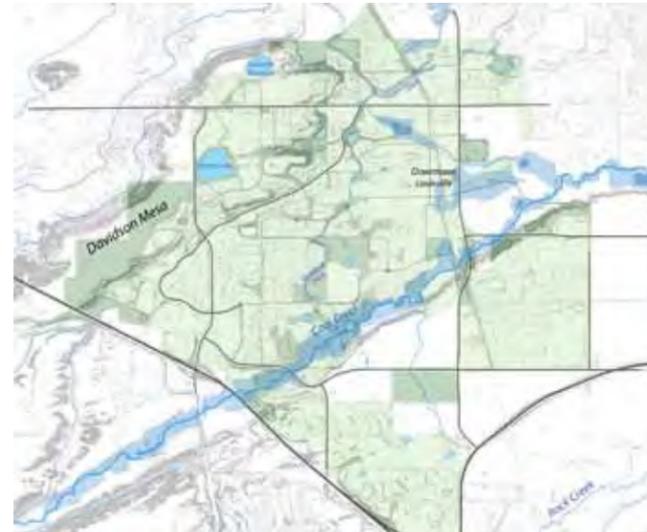
Louisville is located in southeastern Boulder County, generally centered on Coal Creek within the Colorado Piedmont Section of the Great Plains, east of the foothills to the Rocky Mountains. The landform-defining drainage in the Louisville area is the southwest-to-northeast trending Coal Creek. Uplands to the northwest of Coal Creek comprise the drainage divide with the South Boulder Creek drainage basin, and the uplands to the southeast straddle the drainage divide with Rock Creek. Other defining physical features include Davidson Mesa and the slope leading to it in the northwest of the City, as well as the small water bodies throughout the City, most notably Harper Lake.

The area lies eight to ten miles east of the Front Range of the Southern Rocky Mountains. The elevation ranges from about 5,250 feet on the eastern edge of Coal Creek to about 5,530 feet atop Davidson Mesa on the western side of the City.

The City is situated over the Laramie formation at the western end of the Boulder-Weld coalfield, one of the oldest coal mining areas in the Western United States. Coal was mined from the lower part of the Laramie Formation where coal seams were 5-8 feet thick and only 30-40 feet below the ground surface. Many areas of the City of Louisville have been undermined (Maps illustrating the City's undermining are available for review upon request).

With an average elevation of 5,370 feet, the climate of Louisville can be described as a high plains, continental climate, with light rainfall and low humidity. The climate is modified considerably from that expected of a typical high plains environment because of the nearby mountains. Winds are channeled from the Continental Divide down the Front Range and can be severe. Prevailing winds are generally from the west.

The average high temperature in July is 88°F, and the average low temperature in January is 14°F (Weatherbase, 2002). Annual precipitation averages 16 inches. Relative humidity is about 30-35% in summer and about 40-50% in winter. Periods of drought are frequent, usually occur-



Natural Features



ring in the fall and winter. The growing season is approximately 140 days long, with the average date of the first killing frost being September 28th. The last killing frost occurs around May 11 (USDA, 1975).

The grasslands of the Colorado Front Range Piedmont are “shortgrass prairie” and represent a response to predominant dryness as well as historic stress in the form of heavy grazing periods by domestic livestock associated with early settlement.

While grassland habitats around Louisville decreased in both extent and quality, the high quality of life offered by Louisville’s attractive surroundings made the 1980’s and 1990’s a time of rapid suburban expansion. Farms were purchased for development of subdivisions and retail space to support the influx of families moving to Louisville.

Riparian corridors in the area are mostly protected from development through floodplain regulations and open space acquisitions. The loss of adjacent open terrain and the introduction of many invasive plant species have compromised their suitability for many riparian wildlife species.

A few grassland areas on Louisville open space continue to support prairie wildlife, especially areas that are too steep to have been farmed. Some riparian areas on Louisville open space continue to support uses that predated settlement, even though they have been modified by the loss of adjacent habitat, increased human disturbance, and competition with human-tolerant urban wildlife. Other areas of open space have been so highly modified or so impacted by development that they no longer sustain significant use by non-urban species.

Built Environment

The built environment of Louisville, like the natural environment, informs how the physical development of the City will fit with the community’s character and evolve over time. Three elements of the built environment were examined for the Louisville Comprehensive Plan: the *block pattern*; *municipal infrastructure*; and the *building inventory*.



The Planning Context

Block Pattern

The City's street network, or block pattern, is the skeleton of the community. The block pattern dictates the development flexibility and ultimately the physical character of the community. The block pattern establishes the street network and street hierarchy of the community, which in turn dictate the mass, scale, and orientation of buildings. Together, the streets and buildings determine the City's walkability.

As existing streets are improved and new streets are proposed in the Comprehensive Plan, it is important to understand the block pattern that is envisioned will establish the character of development and redevelopment for years to come.

The City's existing block pattern creates three distinctive character zones within Louisville: *urban*, *suburban*, and *rural*. Downtown and Old Town (built before 1960) and the newer subdivisions of North End and Steel Ranch (built since 2008) have established interconnected streets with smaller block patterns and supporting alleys. The block structure in the northeastern portion of the City dictates smaller property parcels, interconnected smaller streets and a more walkable urban character.

Contrasting Downtown and Old Town are the suburban (less walkable) areas of the City along South Boulder Road and McCaslin Boulevard and everything built between 1961 and 2007. The character of these suburban and rural areas of town is influenced by their limited street networks and larger arterials, creating single purpose suburban retailing and employment environments.

A problem with suburban block patterns is that after 10 to 15 years, the retail centers built upon them are outperformed by newer competition. Significant public investment is then needed to reshape the blocks to accommodate a variety of retailing formats and land development patterns, allowing the retail centers to successfully compete again.

Block patterns and infrastructure inform an area's building inventory, development patterns, and land use types. It is important for the Comprehensive Plan to

enable the development of more urban block patterns, building stock and community supported land uses. Urban block patterns, like that in Old Town and Downtown Louisville, have high resiliency and flexibility in accommodating development and redevelopment over time. Typical suburban block patterns have not demonstrated similar resiliency.



Block Pattern

Municipal Utilities and Infrastructure

Municipal utilities and infrastructure (water, sewer, and storm water) are critical in defining the economic vitality and physical character of the City. Their capacity defines the growth potential of the City. Their placement and design contribute to the physical character of the City.

Louisville's water supply originates from two primary sources: South Boulder Creek and the Northern Colorado Water Conservancy District consisting of the Colorado Big Thompson and Windy Gap projects.

The City is treating 4,000 acre-feet (AF) of water a year, with peak demands approaching 9.0 million gallons per day (mgd). Raw water from the City's established sources is treated and distributed to individual businesses and residences from the City's two water treatment facilities: *the Howard Berry Plant and the North Plant*. Currently, both plants operate at or under capacity.



Raw Water Sources

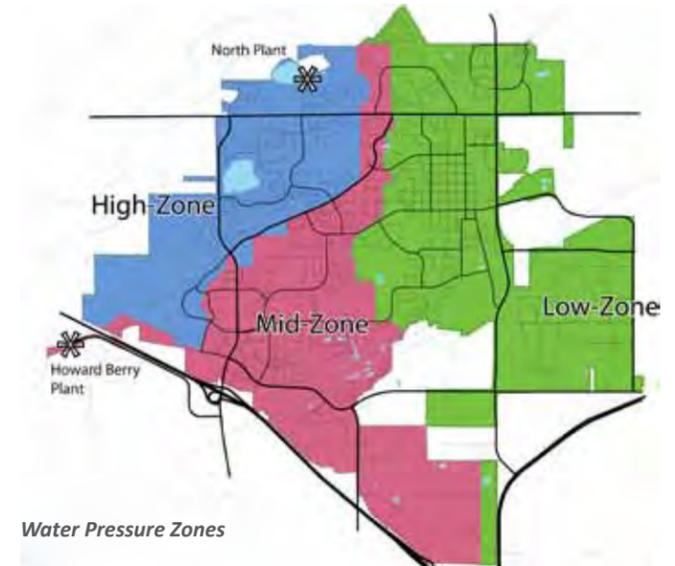
The two water treatment plants have a combined treatment capacity of 13 mgd. Together, the two facilities serve three pressure zones within the City. A water system capacity analysis examined both demand and location of the projected build-out of the City as well as the 20 year market forecast.

The existing water supply and treatment capacity are sufficient to accommodate the expected 20-year development absorption assumptions of the Framework.

However, it is important to note, the Howard Berry Plant may require additional capacity to serve the projected build-out of the mid and lower water pressure zones of the City. The primary driver of future water demand will be the office and industrial uses expected in the Centennial Valley, the Phillips 66 property, and the Colorado Technology Center (CTC).

The Wastewater Treatment Plant provides sanitary sewage treatment for the City of Louisville. There is a surplus of sanitary treatment capacity currently on-line to serve the projected demand of the City as reflected in the Framework.

The Sanitary Treatment Plant is currently operating at a daily average of 2 million gallons per day (mgd) or 59% of its capacity. Historically, the plant has seen flows as high as 2.8 mgd. Additional treatment capacity was added in 1999 giving the plant a maximum permitted capacity of 3.4 mgd.



Water Pressure Zones

The Wastewater Treatment Plant has reached the end of its useful life based upon the age of the facility and upcoming regulatory water quality requirements.

Construction is currently being planned for the Wastewater Treatment Plant to meet regulatory and growth requirements. Improvements to transmission mains and lift stations will be needed with build out of the Colorado Technology Center and the Phillips 66 property.

There are also limitations in the sanitary sewer pipes located in the Downtown and Old Town areas. The pipes in this area are the original vitrified clay pipes, constructed in the mid 1900s. As the pipes have aged, they have begun to break down. The City annually replaces portions of these pipes with PVC pipes to maintain the integrity of the collection system.



Waste Water Treatment Plant Improvement Timelines



The Planning Context

The City's Engineering Department has an ongoing maintenance program for inspecting storm drainage facilities. The department also provides detailed hydraulic modeling to identify any deficiencies and what improvements are necessary.

The City is currently following the Louisville/Boulder County Outfall System Plan, as completed in 1982, for necessary improvements to the stormwater system. Developers are responsible for completing elements of the outfall system to meet the City's land development and engineering codes.

Overall, the City is positioned well to serve the needs of the Framework at build out. However, as the City continues to age, infrastructure that has deteriorated or become obsolete will need to be replaced or rehabilitated.

Building Inventory

The City of Louisville's building inventory reflects the diversity, economic stability and physical character of the City. According to the 2010 U.S. Census, there were 7,529 occupied housing units in Louisville out of a total of 7,814, for a vacancy rate of 3.6%. Approximately 74% of the occupied units were owner occupied, compared to 64% in Boulder County and 68% in the State. Louisville's median home value of \$361,200 for owner occupied units was slightly higher than Boulder County at \$353,300, and significantly higher than the state's median value of \$236,600. The highest median housing value among peer jurisdictions in 2010 is the City of Superior at \$389,300.

The bulk of Louisville's building stock was constructed in the three decades between 1970 and 2000 when 84% of the total inventory was delivered. The County and State saw an upsurge of residential construction starting in the 1960s that remained relatively robust past year 2000.

Louisville's building stock is generally divided into four eras of construction. These periods of construction generated distinctively different patterns of development and architectural styles. No single architectural

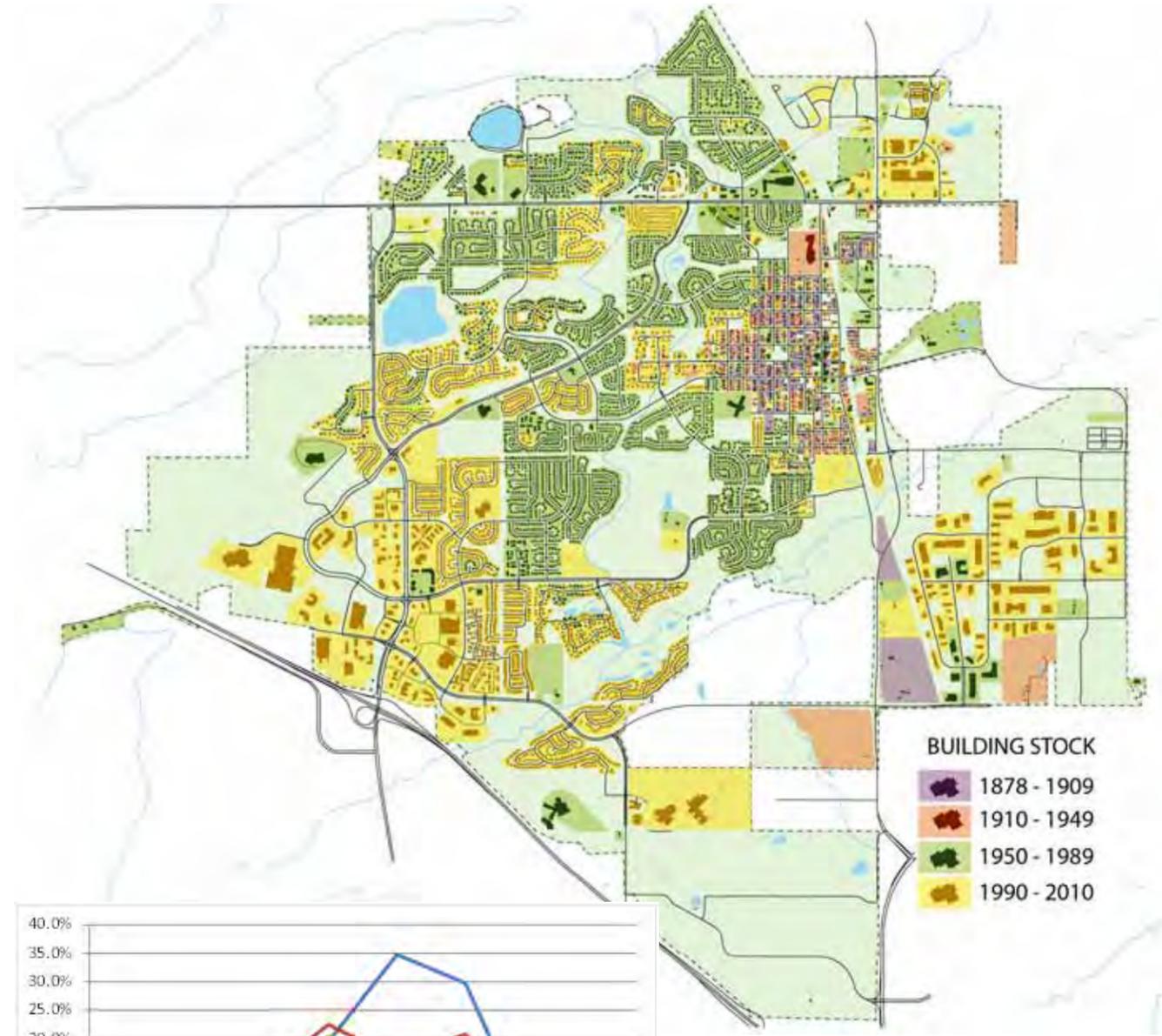


Building Figure Ground

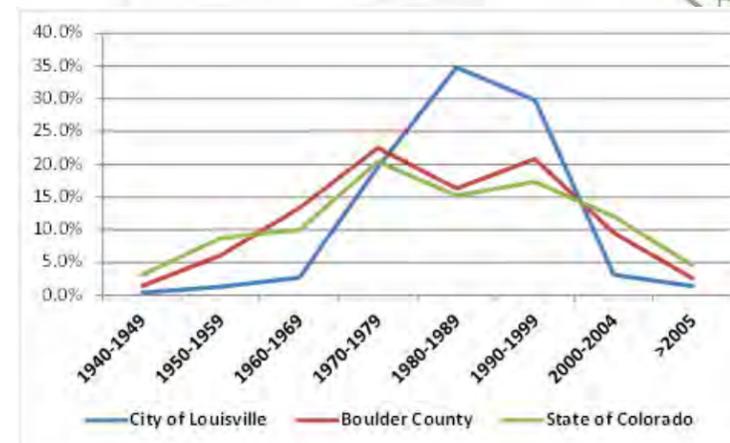
style dominates the Louisville architectural vernacular City-wide, or within any individual era of construction. The development pattern of the City clearly shifted from a pedestrian character and orientation in Old Town and Downtown Louisville (pre-1950) to a vehicle base orientation and character for development after 1950.

Louisville adopted a historic preservation ordinance in 2005 and voters approved an increase in sales tax for the creation of the Louisville Historic Preservation Fund in 2008. The historic preservation ordinance's designation of historic resources is voluntary for buildings over 50 years old. Revenues from the one-eighth percent sales tax are to be retained and spent exclusively within the "Historic Old Town Overlay District" and "Downtown Louisville" to preserve the unique charm and character of historic Old Town Louisville. This revenue source is meant to:

- Provide incentives to preserve historic resources, including funding of programs to identify and attempt to preserve buildings which qualify for listing on the Louisville Register of Historic Places with the consent of the property owner;



BUILDING STOCK
 1878 - 1909
 1910 - 1949
 1950 - 1989
 1990 - 2010



Percentage of Existing Buildings by Construction Date

Age of Building Stock



The Planning Context



Example Buildings Built Between 1878 and 1909



Example Buildings Built Between 1910 and 1949



Example Buildings Built Between 1950 and 1989

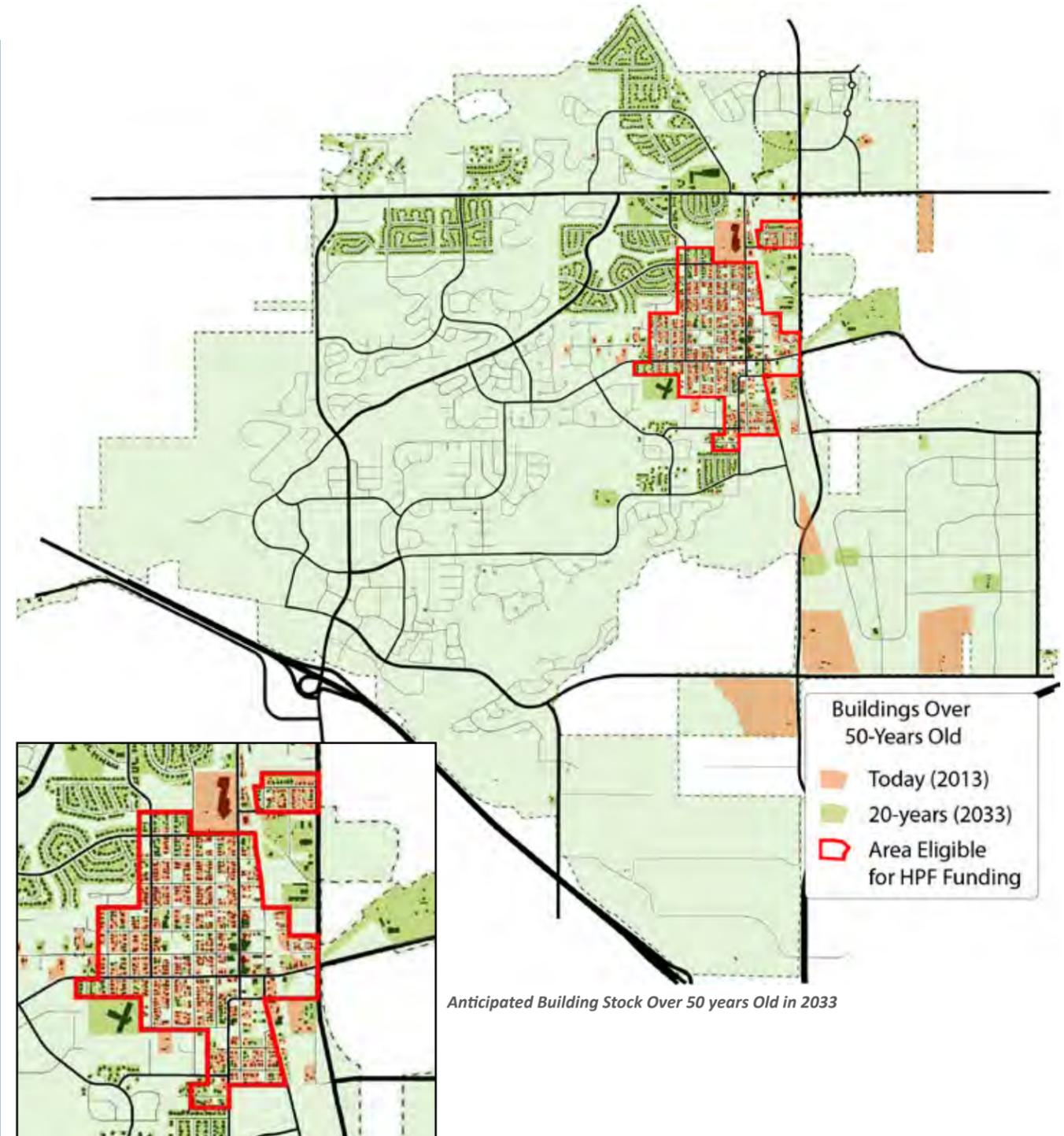


Example Buildings Built Between 1990 and 2012

- above mandatory requirements; and
- For city staff time to administer the programs.

As Louisville’s building stock continues to age, more of the City’s buildings will become eligible as historic resources. Currently, buildings over 50 years of age are generally constrained to the building stock of Downtown Louisville and Old Town Louisville. However, over the 20 year life of this Comprehensive Plan, it is expected the total number of eligible historic resources will nearly double, including many homes in North Louisville and along South Boulder Road. Under the existing preservation ordinance, these resources will not be eligible for money from the Historic Preservation Fund.

- Provide incentives to preserve buildings that contribute to the historic character of historic Old Town Louisville but do not qualify for listing on the Louisville Register of Historic Places, with such buildings to be treated the same as historic buildings but with lower priority;
- Provide incentives for new buildings and developments within historic Old Town Louisville to limit mass, scale, and number of stories; to preserve setbacks; to preserve pedestrian walkways between buildings; and to utilize materials typical of historic buildings,



The Planning Context

Circulation

Louisville is a maturing municipality in which growth trends and traffic patterns are shifting from an expansion focus to an infill orientation. Louisville is situated within rapidly developing east Boulder County, between the residential areas of Lafayette, East Boulder County and Erie, and the employment centers of Boulder, Interlocken, and the US 36 Corridor serving Denver. Louisville’s arterial street network provides the primary access routes between these residential and employment areas.

Staff and the consultant team conducted a complete multi-modal transportation analysis for Louisville. Four significant observations have emerged from the transportation analysis when compared to the City’s Vision Statement and Core Community Values.

Street Vehicle Capacity

Staff plotted the Average Daily Traffic (ADT) volumes for the year 2035 on the Louisville Street Network for the preferred Framework Option. Staff then used the Institute of Transportation Engineers (ITE) generalized level of service (LOS) guidelines to document any vehicle capacity concerns with the projected 20 year build out of the City. Vehicle LOS is most commonly used to analyze a roadway’s performance by categorizing vehicle traffic flow throughout the day, or during the periods of heaviest use, typically the morning and evening commute. Vehicle LOS is measured using letters from A to F.

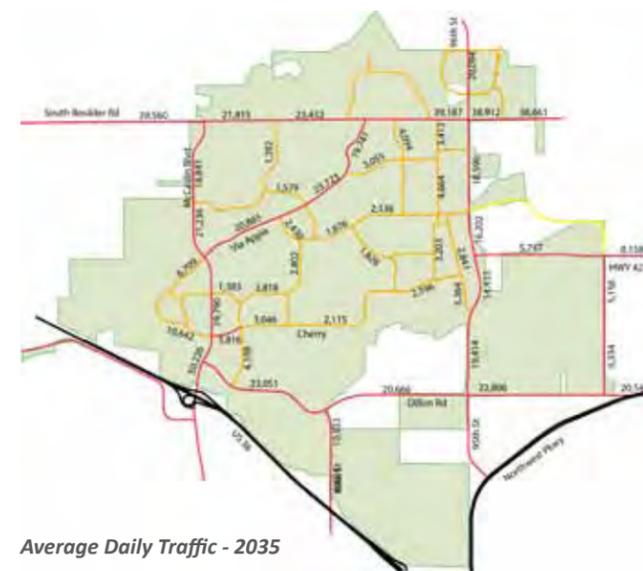
Vehicle based LOS does not measure a pedestrian’s, or bicyclist’s quality of trip. However, the size and speed of roadway affects the quality of a pedestrian’s and bicyclist’s trip experience. Generally, a larger and faster roadway corresponds with a higher vehicle LOS. Conversely, a smaller and slower roadway corresponds generally with a higher pedestrian’s and bicyclist’s quality of experience and a generally lower vehicle LOS. The transportation profession recommends LOS A to LOS C in rural communities, LOS C to D in suburban communities, and LOS C to F in urban communities.

A goal of this Comprehensive Plan is to maintain vehicle LOS C unless to maintain LOS C it would be necessary to

widen the street or make other capacity modifications in a way that would conflict with these desired small town transportation qualities:

- Pedestrians of all ages and abilities should be able to safely and comfortably walk along, or across a street, arterial corridor, or intersection, as well as wait for public transit.
- Bicyclists of all ages and abilities should be able to safely and comfortably ride along, or across a street, arterial corridor, or intersection.
- All streets, arterial corridors and intersections are designed and function to be compatible with the City’s desired character zone identified in the Framework.
- Streets, arterial corridors and intersections do not negatively affect the adjacent neighborhoods, historic assets, or natural resources.

Based on these criteria, the majority of the City’s streets have the capacity to accommodate the 20 year forecasted traffic volumes for the preferred Framework at LOS C. However, several of the City’s arterials will operate at LOS D. It is important to note the anticipated regional cut-through traffic in the year 2035 causes traffic volumes on the arterials to exceed LOS C standards, regardless of any additional development in Louisville. Staff



believes that the required vehicle capacity modifications necessary to maintain LOS C conflict with Louisville’s small town transportation quality expectations.

Regional vs. Local Traffic

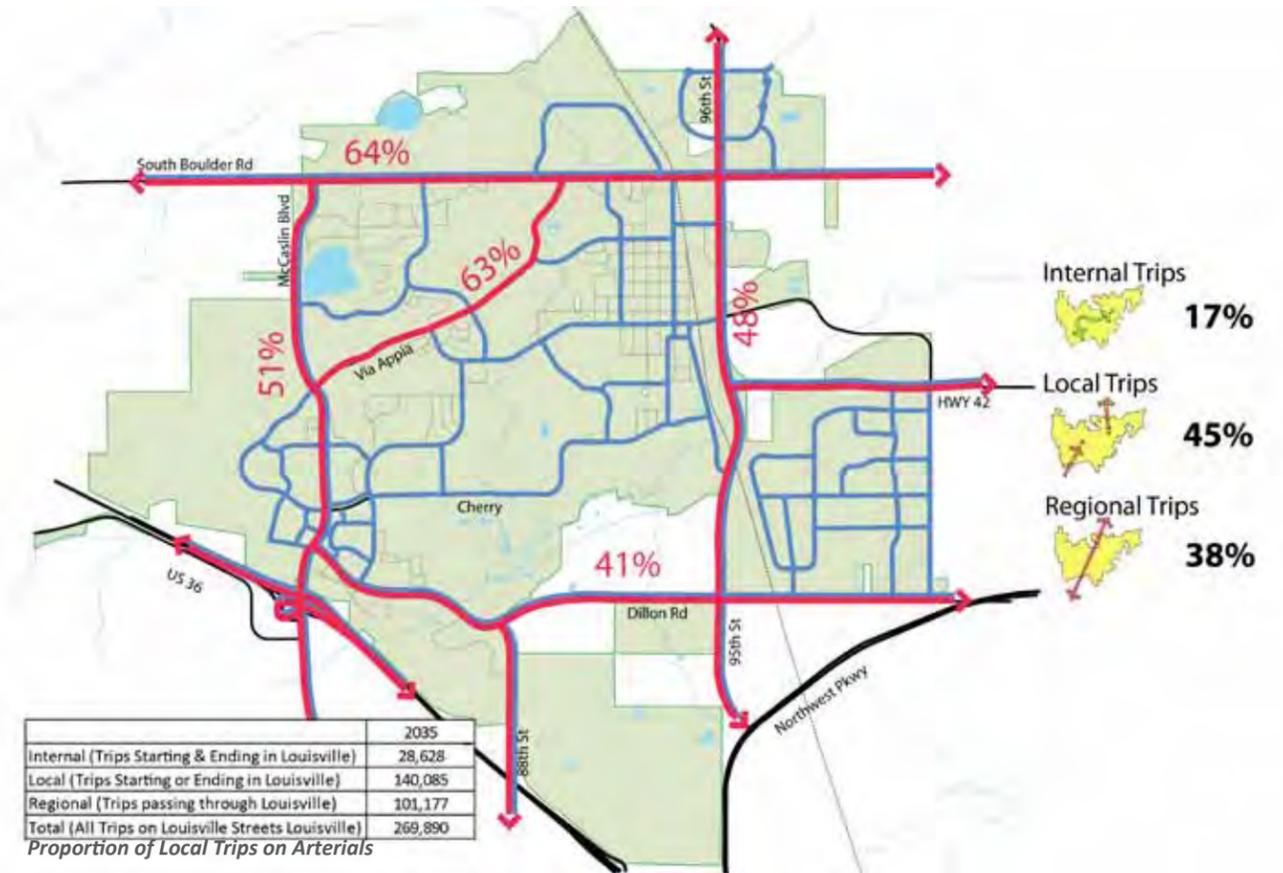
Staff conducted a Select Link Analysis of the 2035 DRCOG Transportation Model. A select link analysis identifies where the origins and destinations of car trips using Louisville streets occur. Louisville’s share of traffic on its own roadways is decreasing. In 2035, 38% of all trips on Louisville streets will have neither an origin nor destination in Louisville. More relevant is that regional traffic on Louisville arterial streets in 2035 will account for 40% to 65% of all traffic. As residential areas in East Boulder County and employment areas in Boulder and the US 36 Corridor continue to increase, Louisville’s share of traffic on its own roadways will continue to decrease. Only 10% of Louisville’s employment base lives

in Louisville. A key transportation strategy for Louisville should be to improve local connectivity and transportation choices internal to the City.

Transportation Nodes and Economic Opportunities

The City of Louisville has three transportation nodes with varying degrees of economic opportunities: McCaslin Boulevard and US 36, South Boulder Road and Highway 42, and Pine Street and Highway 42. These transportation nodes generate intersecting traffic volumes that retailers are attracted to because of visibility and drive-by opportunities. It is important for the City to recognize and capitalize on these opportunities.

Neighborhood Centers: South Boulder Road and Highway 42 along with McCaslin Boulevard (north of Cherry), represent neighborhood retailing centers. Traffic volumes within these centers will range between 30,000



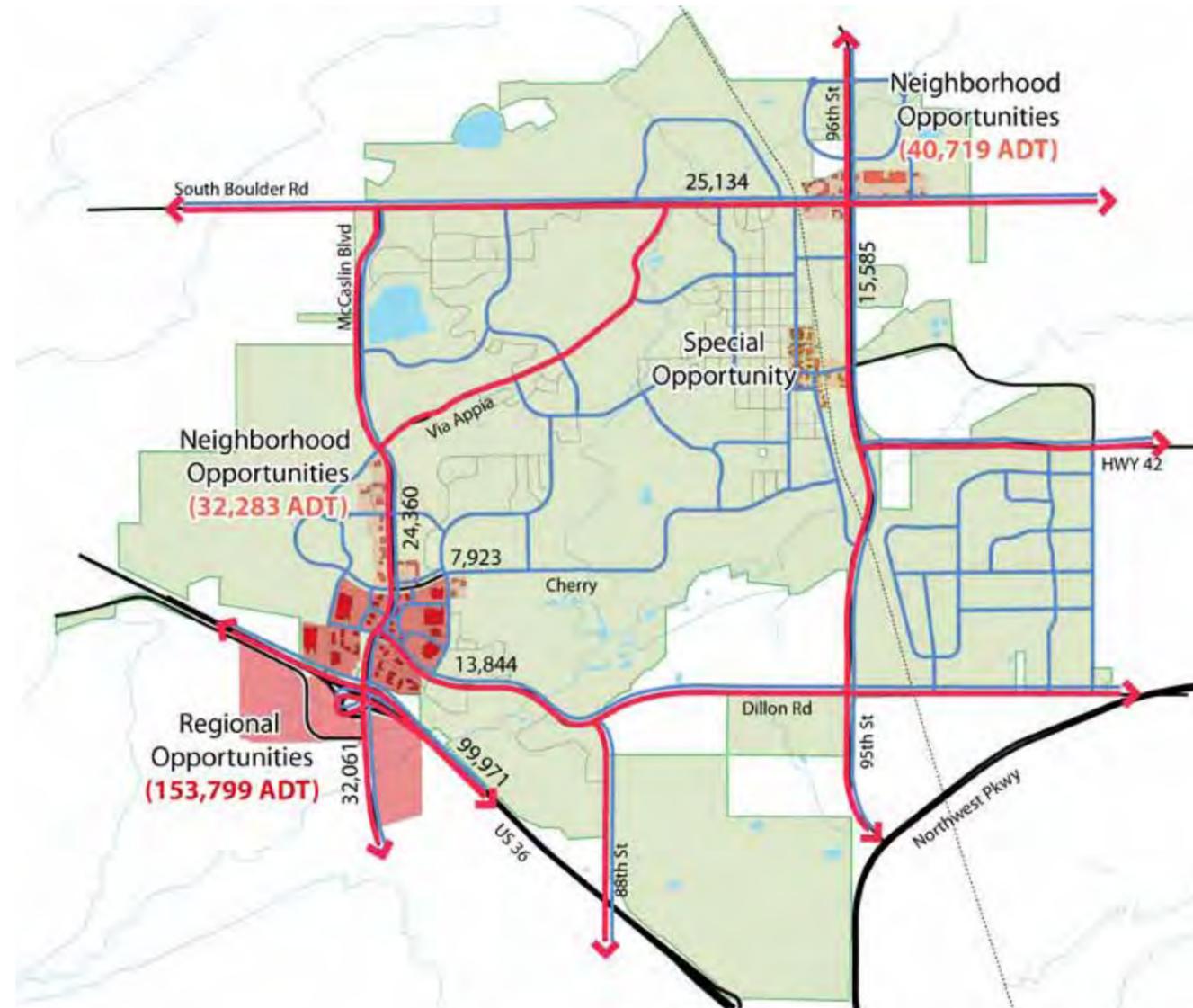
The Planning Context

and 40,000 vehicles daily by the year 2035. Generally, retailing will be limited to neighborhood opportunities.

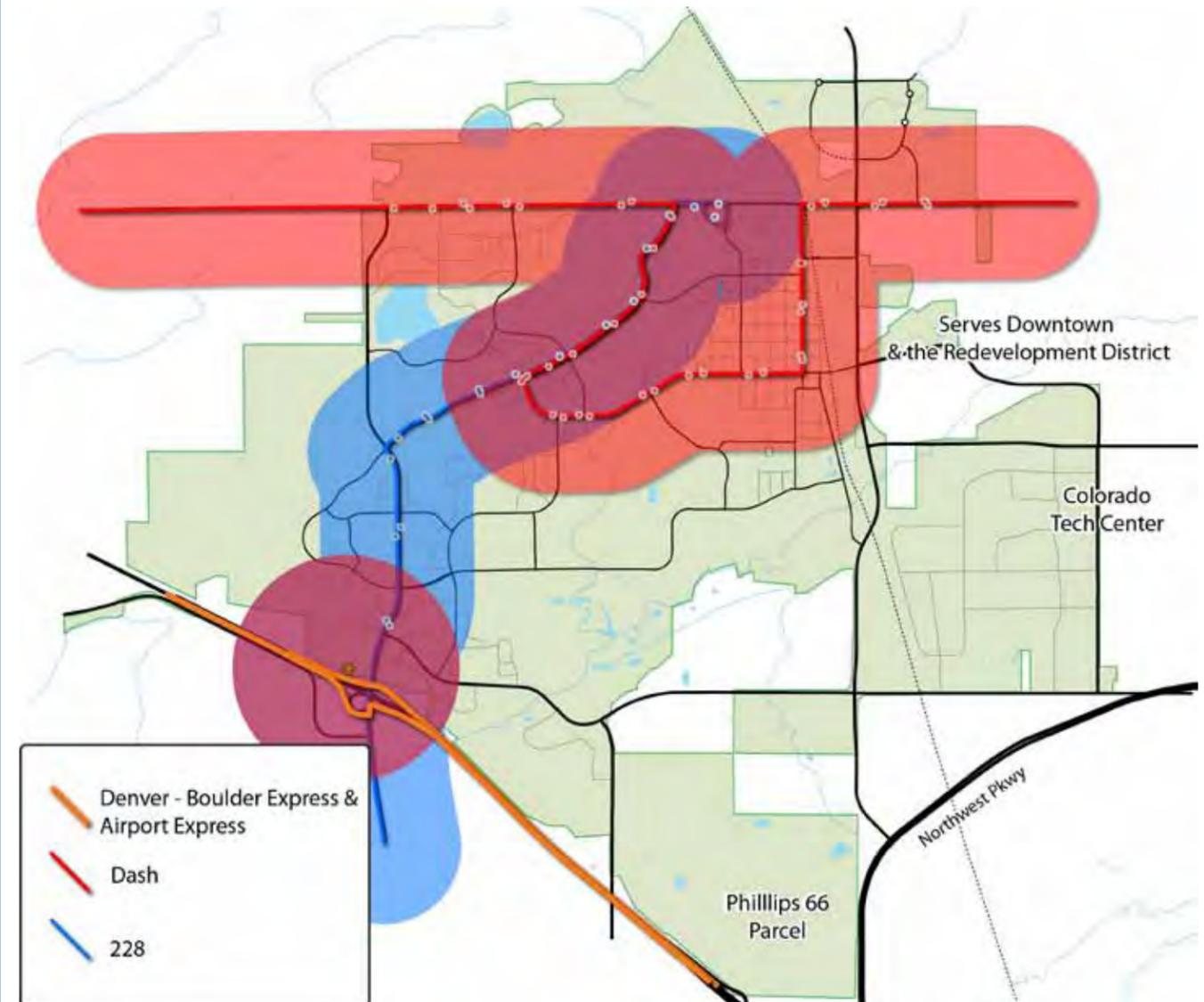
Regional Center: Regional retailing opportunities exist along McCaslin Boulevard south of Cherry Street to the US 36 interchange. In total, 150,000 vehicle trips travel through this transportation node daily.

Transit Service

Currently, the entire southeastern portion of the City has no local transit service, including Avista Hospital, the Colorado Technology Center, and the Phillips 66 and Monarch Campus properties. All are critical employment areas to the City and the entire metro region.



The Strength of Retail Opportunities Influenced by Average Daily Traffic Volumes



Transit Service

The Planning Context

Market Opportunities

The City of Louisville contracted with Tischler Bise to complete a demographic and economic market study for the City which is included as an appendix to the Comprehensive Plan. The following is a brief overview of the market opportunities of the major land uses in the City. The Market Study does not imply the development projections are to be achieved in the Plan.

Retail

The Economic and Market Assessment indicates there is a surplus of approximately 3 million square feet of retail within a 15 minute drive shed of Louisville. The assessment goes on to suggest it will take between nine and ten years of population growth in the trade area to fill this excess retail space. Based on these findings, the study concludes that the demand for new retail development at the community shopping center scale and higher (100,000 SF and higher) will be soft in Louisville for the next nine to ten years.

Although the study concludes that demand for larger scale retail in the trade area will be weak for the next ten years, there are opportunities to capitalize on emerging market trends to regain lost retail base. Areas like Downtown and the Revitalization District are positioned well to capitalize on emerging market trends favoring mixed use walkable environments. The zoning is in place and infrastructure improvements like the South Street Gateway and the HWY 42 Gateway Project will enable these areas to develop in line with emerging market trends. However, the zoning and current development patterns in Centennial Valley and the McCaslin Boulevard corridor provide little flexibility for new development patterns. Residential mixed use is not currently permitted, and the regulations encourage larger lot, automobile-centered development.

Office/R&D/Flex Space

The majority of Louisville's office, research and development, and flex space is located in either the Colorado Technology Center (CTC) or Centennial Valley. There are approximately 2.3 million square feet of occupied space in CTC and a great deal of vacant land zoned for additional industrial development including office, research and development, and flex space. The market study suggests the CTC is positioned well in the region and will continue to experience moderate growth for the foreseeable future. Centennial Valley has approximately 425,000 square feet of vacant office space, and the market study indicates it is not likely that additional

speculative office space will be built in this area until the vacant space is occupied.

Residential

The City of Louisville's residential housing market is constrained by a scarcity of developable land. As currently zoned, the City does not have additional land for greenfield residential development within city limits. The Alkonis parcel in the northeast corner of the City is the last significant parcel of land identified for annexation with the potential for residential development. Opportunities for infill residential development are constrained by a lack of land supply and current zoning regulations which restrict residential development or do not allow it at all.

Despite a scarcity of residential land for development, the Economic and Market Assessment indicates there is significant demand for residential units in Louisville, as evidenced by the rapid and sustainable sales of homes at Steel Ranch and North End. Opening up additional areas for residential development, either through rezoning, or revised development regulations, would likely result in additional residential development as demand is quite strong.

Fiscal Analysis

Staff worked with an economic and fiscal consultant, Tischler Bise, to assess the fiscal impacts of the Comprehensive Plan over the next 20 years. The complete study is included as an appendix to this plan. At build out, the preferred Framework will produce a balanced amount of residential units, and retail, industrial, and office square footage. However, over the next 20 years the market will only construct a portion of each of these build out scenarios. Additionally, some of the newly constructed square footage and residential units will be added in greenfield locations, while other units and square footage will be constructed in infill locations. The following table outlines the additional square footage and residential units that the fiscal study projects could be built in the next twenty years.

Greenfield development and infill development have different fiscal impacts on the city. For example, a new residential subdivision on the outskirts of town will require the construction of new roads that will need to be maintained by the city, and may require additional police resources. An infill site will likely not need additional roads. The City's current fiscal model does not account for the potential savings of infill development. The fiscal study attached to this plan includes cost adjustments to Operating and Capital Costs for infill de-

Use	Net New
Single Family Residential (Units)	224
GreenField	141
Infill	83
Multi-Family Residential (Units)	967
GreenField	273
Infill	694
Retail (Sq. Ft.)	200,000
GreenField	25,000
Infill	175,000
Industrial (Sq. Ft.)	375,000
GreenField	
Infill	375,000
Office (Sq. Ft.)	450,000
GreenField	250,000
Infill	200,000

20 Year Market Forecast

Source: Source: City of Louisville; TischlerBise

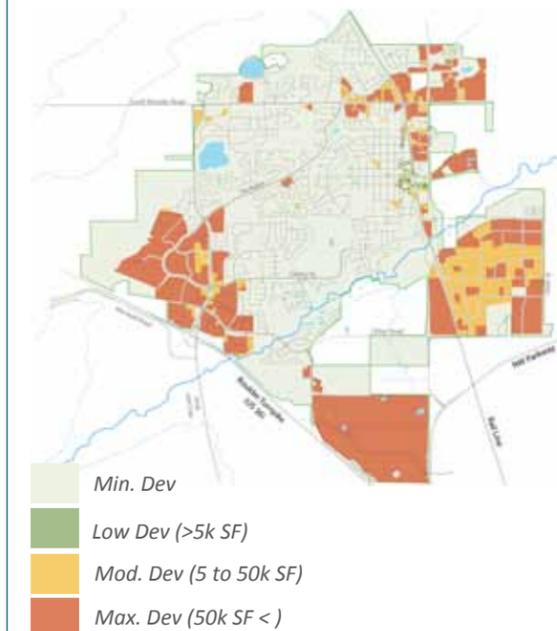
velopment. Based on the discount assumptions in the report, Tischler Bise completed an analysis of operating and capital fiscal impacts for the 20 year build out. The model indicates the proposed land use mixture in this comprehensive Plan is essentially fiscally neutral. Annual operations revenue will be slightly under expenditures by approximately \$93,000 and that annual capital budget will experience a slight surplus of approximately \$115,000 annually. These are rough assumptions based on one out of countless possible build-out scenarios.

City of Louisville, Colorado Fiscal Impact Analysis Summary of Annual Operating and Capital Estimates		
		Total
OPERATING SUMMARY		
Revenue	\$	2,171,664
Expenditures	\$	2,264,780
Total	\$	(93,116)
CAPITAL SUMMARY		
Revenue	\$	875,996
Expenditures	\$	758,349
Total	\$	117,647

Stability and Change

The three largest land uses in the City are: residential, parks and open space, and vacant or undeveloped. Together these uses comprise approximately three-quarters of the land in the City. On the properties that have been developed, residential makes up more than half of the built square footage in the City, followed by industrial and office, together totaling about one-quarter of the City's built square footage.

The Louisville Municipal Code (LMC), Chapter 17 - Zoning, dictates the amount of development allowed within Louisville. Staff analyzed the LMC with respect to each lot to determine how much development is allowed in addition to what currently exists. This analysis shows a large portion of the City is entitled to additional development.



Areas with Substantial Buildout Capacity

Most of the entitled development is within retail corridors along South Boulder Road and McCaslin Boulevard; special office and industrial districts of Centennial Valley, the Colorado Technology Center (CTC), and Phillips 66; and within the Downtown and the HWY 42 Redevelopment district. It should be noted, the analysis simply indicates what additional development is allowed and not what the retail, office, and residential markets can absorb.

Several variables influence the likeliness of property developing or redeveloping. One is the ratio between the building



value and the total property value. If the building value is a relatively small portion of the total value, then the property is probably not being used close to its full potential and redevelopment is likely. However, the improved value to property value ratio is not an indicator of immediate development. Many other factors unique to each property also influence the likelihood of development. For example, if a property is owned free and clear, without any debt, this analysis falls short.



- Improvement values (50%) of total Property Values
- Improvement values (40 to 50%) of total Property Values
- Improvement values (30 to 40%) of total Property Values
- Improvement values (>30%) of total Property Values

Areas with High Development Pressures

Areas with the highest development pressures are typically vacant like some in the CTC and Centennial Valley; however, many older under-developed properties are experiencing significant reinvestment pressure along South Boulder Road and within Old Town.

Staff mapped the allowed additional development in the City with the building to property value ratio for all properties to identify areas experiencing change today and that will likely experience change in the future as the real estate market recovers.

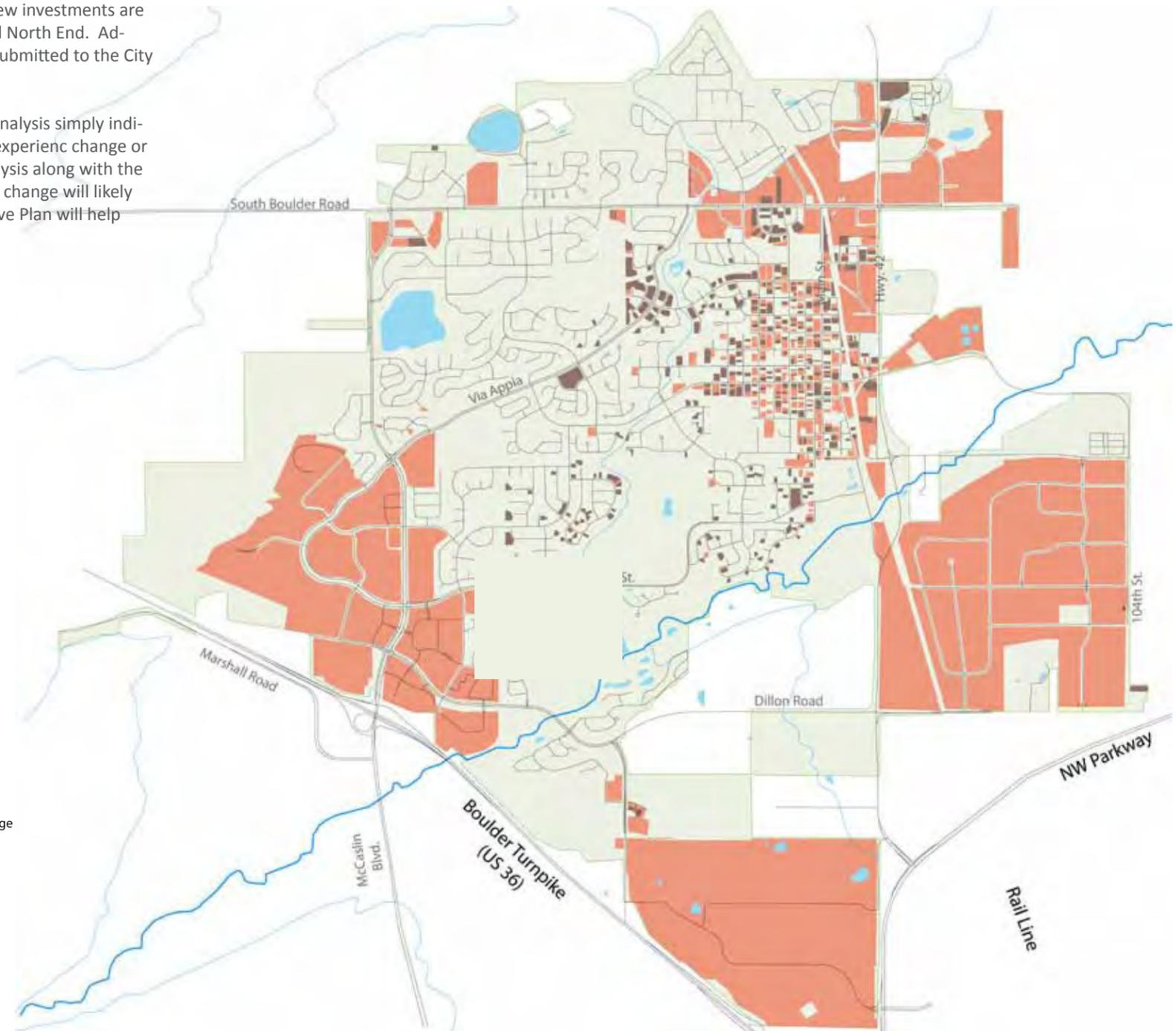
The majority of Louisville is stable; however, some specific areas are experiencing, or will likely experience, change. Downtown, over the last few years, has experienced substantial

reinvestment to its building stock. The Old Town neighborhood is also experiencing significant reinvestment with new houses replacing many of the older homes. This analysis also indicates large residential reinvestments may begin occurring in neighborhoods outside of Old Town. New investments are also occurring in the CTC, Steel Ranch, and North End. Additional development requests are being submitted to the City for property along South Boulder Road.

As a caveat, it is important to realize this analysis simply indicates which areas of the City are likely to experience change or should anticipate future change. This analysis along with the economic market study will indicate when change will likely occur by land use type. The Comprehensive Plan will help guide that change to the City's benefit.

Areas of Stability and Change

- Areas of Stability
- Areas of Incremental Change
- Areas of Change



The Vision Statement and Core Community Values

The 20 Year Plan for the City of Louisville has two primary components which guide the direction and implementation of the 2012 Comprehensive Plan Update.

The first key component is the Vision Statement and Core Community Values. The Vision Statement and Core Community Values are supported by the second key component, the Framework Plan.

Louisville's Vision Statement and Core Community Values define how the City sees itself and identify characteristics that should be carried into the future. The Vision Statement and Core Community Values were developed through extensive public outreach and represent the views of residents, business and property owners, and elected and appointed officials. The Vision Statement and Core Community Values serve as the rubric against which the Framework Plan was developed and how future City policies and decisions should be evaluated. All of the recommendations, principles, and policies in this Comprehensive Plan are designed to further the goals of the Vision Statement and Core Community Values.

The Framework Plan illustrates Louisville's community character and development expectations verbalized in the Vision Statement and Core Community Values. Together, the Vision Statement and Core Community Values visualized by the Framework Plan represent the long-range integrated land use, transportation and natural resource vision for the City.



Vision Statement

Established in 1878, the City of Louisville is an inclusive, family-friendly community that manages its continued growth by blending a forward-thinking outlook with a small-town atmosphere which engages its citizenry and provides a walkable community form that enables social interaction. The City strives to preserve and enhance the high quality of life it offers to those who live, work, and spend time in the community. Louisville retains connections to the City's modest mining and agricultural beginnings while continuing to transform into one of the most livable, innovative, and economically diverse communities in the United States. The structure and operation of the City will ensure an open and responsive government which integrates regional cooperation and citizen volunteerism with a broad range of high-quality and cost-effective services.



The Vision Statement and Core Community Values

Core Community Values

The following Core Community Values are the foundation upon which the City of Louisville will make decisions and achieve the Community's vision.

We Value...



A Sense of Community . . . where residents, property owners, business owners, and visitors feel a connection to Louisville and to each other, and where the City's character, physical form and accessible government contribute to a citizenry that is actively involved in the decision-making process to meet their individual and collective needs.



Our Livable Small Town Feel . . . where the City's size, scale, and land use mixture and government's high-quality customer service encourage personal and commercial interactions.



A Healthy, Vibrant, and Sustainable Economy . . . where the City understands and appreciates the trust our residents, property owners, and business owners place in it when they invest in Louisville, and where the City is committed to a strong and supportive business climate which fosters a healthy and vibrant local and regional economy for today and for the future.



A Connection to the City's Heritage . . . where the City recognizes, values, and encourages the promotion and preservation of our history and cultural heritage, particularly our mining and agricultural past.



Sustainable Practices for the Economy, Community, and the Environment . . . where we challenge our government, residents, property owners, and our business owners to be innovative with sustainable practices so the needs of today are met without compromising the needs of future generations.



Unique Commercial Areas and Distinctive Neighborhoods . . . where the City is committed to recognizing the diversity of Louisville's commercial areas and neighborhoods by establishing customized policies and tools to ensure that each maintains its individual character, economic vitality, and livable structure.



A Balanced Transportation System . . . where the City desires to make motorists, transit customers, bicyclists and pedestrians of all ages and abilities partners in mobility, and where the City intends to create and maintain a multimodal transportation system to ensure that each user can move in ways that contribute to the economic prosperity, public health, and exceptional quality of life in the City.



Families and Individuals . . . where the City accommodates the needs of all individuals in all stages of life through our parks, trails, and roadway design, our City services, and City regulations to ensure they provide an environment which accommodates individual mobility needs, quality of life goals, and housing options.



Integrated Open Space and Trail Networks . . . where the City appreciates, manages and preserves the natural environment for community benefit, including its ecological diversity, its outstanding views, clear-cut boundaries, and the interconnected, integrated trail network which makes all parts of the City accessible.



Safe Neighborhoods . . . where the City ensures our policies and actions maintain safe, thriving and livable neighborhoods so residents of all ages experience a strong sense of community and personal security.



Ecological Diversity . . . where the City, through its management of parks and open space and its development and landscape regulations, promotes biodiversity by ensuring a healthy and resilient natural environment, robust plant life and diverse habitats.



Excellence in Education and Lifelong learning . . . where the City allocates the appropriate resources to our library services and cultural assets and where the City actively participates with our regional partners to foster the region's educational excellence and create a culture of lifelong learning within the City and Boulder County.



Civic Participation and Volunteerism . . . where the City engages, empowers, and encourages its citizens to think creatively, to volunteer and to participate in community discussions and decisions through open dialogue, respectful discussions, and responsive action.



Open, Efficient and Fiscally Responsible Government . . . where the City government is approachable, transparent, and ethical, and our management of fiscal resources is accountable, trustworthy, and prudent.



The Vision Statement and Core Community Values

CHARACTER ZONES

This Comprehensive Plan Update introduces a new language and format to the community's Framework. The intent of the change is to clarify and illustrate the community's expectations related to the City's land use function, form, and character in the Framework, and to ensure the City's Vision Statement and Core Community Values are properly translated and illustrated in the Comprehensive Plan. The new language simplifies the format of the Framework into character zones. The character zones are described by two variables: development patterns and development types.

Development Patterns

Three development patterns are found in Louisville: *urban*, *suburban*, and *rural*. These development patterns reflect the look and feel of the City. Development patterns dictate how streets are laid out; how property parcels are subdivided; how buildings are designed and arranged on a site; and how parks and public spaces are integrated into the community.

Specifically, the development patterns in the Framework establish guidelines for Small Area and Neighborhood Plans to implement specific regulations within the Louisville Municipal Code (LMC). The specific elements the development patterns influence include:

Building Form and Design

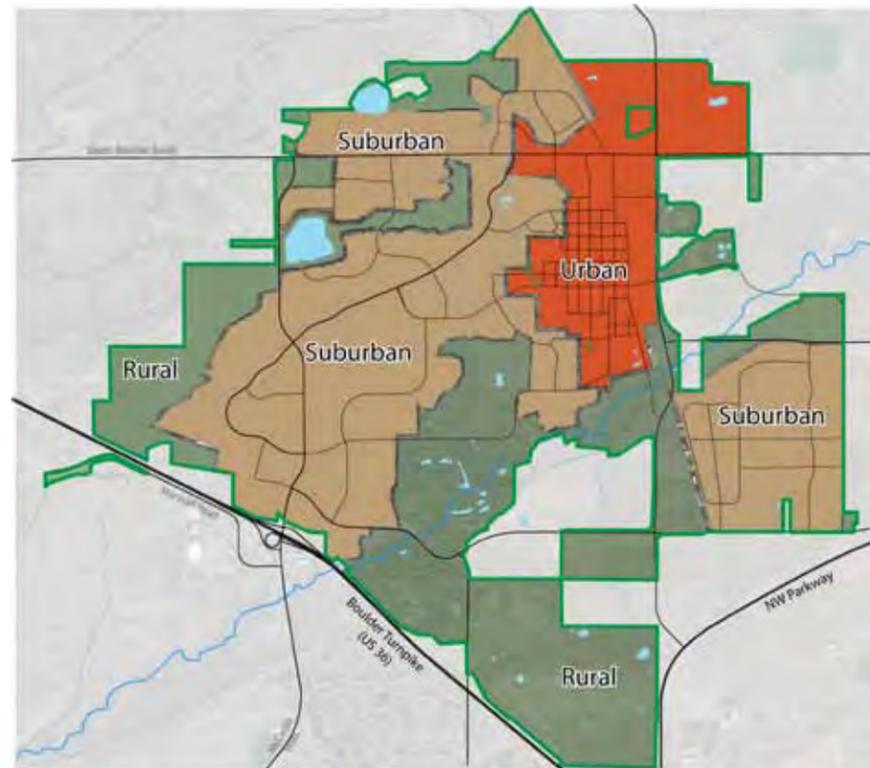
- Building Heights
- Building Mass and Scale
- Building Orientation

Infrastructure

- Streets
- Blocks
- Storm Water Facilities
- Public Spaces and Trails

Design Standards

- Yard & Bulk
- Parking Ratios
- Site Design



Urban Pattern

The urban portions of Louisville are found in the north-east quadrant of the City and are generally more compact and walkable. The majority of the urban development pattern occurred in Louisville prior to 1960. Some urban development patterns have occurred since 2008. The urban areas of the City include: Downtown, Old Town, North End and Steel Ranch. Generally, the urban pattern of development includes the following distinguishing design characteristics.

Streets

- Interconnected street network (smaller blocks)
- Alley / rear loaded properties
- Multimodal (Vehicle, pedestrian, bike, transit)
- Reduced speeds
- Balanced civic and mobility responsibilities

Parcels

- Smaller parcels

Building Design and Orientation

- Street Orientation
- Pedestrian mass, scale, and details

Civic & Public Infrastructure

- Integrated
- Multi-purpose
- Formal landscape



Example Figure Ground - Downtown & Old Town Louisville

The Vision Statement and Core Community Values

Suburban Pattern

The suburban portions of Louisville generally evolved between 1960 and 2008 and are found along: Via Appia; McCaslin Boulevard; South Boulder Road; Centennial Valley; and within the Colorado Technology Center. The suburban patterns of development are typically more spread-out and multimodal when compared to urban patterns of development. Generally, suburban patterns of development include the following distinguishing design characteristics.

Streets

- Disconnected street network (larger blocks)
- Street loaded properties
- Multimodal (Vehicular, Pedestrian, Bike, Transit)
- Higher speeds
- Mobility role larger than civic role

Parcels

- Larger parcels

Building Orientation

- Oriented towards property
- Vehicular mass, scale, and details

Civic & Public Infrastructure

- Separated
- Single-purpose
- Informal landscape



Example Figure Ground - McCaslin Boulevard & Centennial Valley

Rural Pattern

The rural portions of Louisville generally occur along the perimeter of City in the form of open space. However, rural development patterns have also emerged around the Coal Creek Golf Course, 96th Street and south of Dillon Road and include the Phillips 66 property. The rural patterns of development are typically more separated and vehicular based when compared to urban and suburban patterns of development. Generally, rural patterns of development include the following distinguishing design characteristics.

Streets

- No street network (no block pattern)
- Street loaded properties
- Vehicular and bicycle design (pedestrian needs supported by trail network)
- Higher speeds
- Mobility priority

Parcels

- Larger parcels

Building Orientation

- Natural resource orientation
- Vehicular mass, scale, and details

Civic & Public Infrastructure

- Separated
- Single-purpose
- Native landscape



Example Figure Ground - Avista, Monarch Campus, & Phillips 66 Property



The Vision Statement and Core Community Values

DEVELOPMENT TYPES

Five development types occur throughout Louisville: *centers, corridors, neighborhoods, special districts, and parks/open space*. These development types reflect the type of uses and activities; density, or intensity of development; and the amount of public infrastructure desired in different areas of the City.

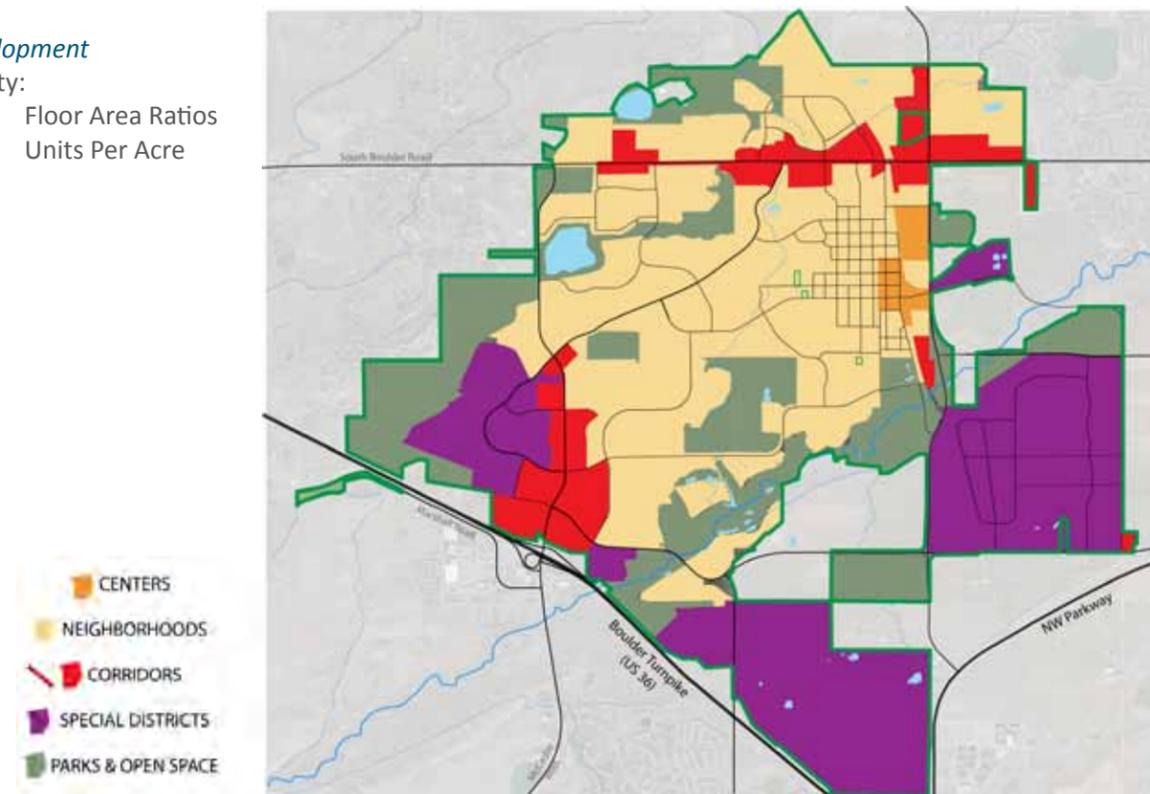
Specifically, the development types in the Framework will establish guidelines for Small Area and Neighborhood Plans to implement specific regulations within the Louisville Municipal Code (LMC). The specific elements the development types influence include:

Land Use Mix

- Retail
- Commercial
- Residential
- Industrial
- Civic/Institutional

Allowed Development

- Density:
 - Floor Area Ratios
 - Units Per Acre



Centers

Downtown Louisville and its relationship with the Old Town neighborhood represent the City's only current center. The City's Framework identifies the emergence of two additional centers: one around South Boulder Road and Highway (HWY) 42, and the other near McCaslin Boulevard and US 36, south of Cherry Street.

Centers are defined by their mixture of uses (retail, commercial, and residential), street interconnectivity, and integrated public spaces. A center's physical design is that of a destination, or gathering point for city-wide activities. Centers are connected to and oriented toward their adjacent land uses. Centers typically have the greatest retailing opportunities. Centers feature integrated public spaces with a recognized public space, or focal point. Centers also have the highest potential for a vertical mix of uses.



The Vision Statement and Core Community Values

Corridors

Corridor development types are similar to center development types in the mixture and intensity of land uses. Corridors differ from centers in their shape, connectedness to adjacent land uses, and public space integration. Generally, corridor development types occur along arterial roadways in a linear form and are disconnected from adjacent land uses. Corridor development types are expected to develop along: McCaslin Boulevard north of Cherry Street and south of Via Appia; along South Boulder Road and along HWY 42, north of Hecla Drive.

Corridors typically have strong retail, commercial and multi-family development opportunities. Corridors lack integrated public spaces and typically do not have a focal point and central gathering area. Corridors typically feature a linear, not horizontal, mixture of uses. Generally, their architectural character is defined by the primary arterial roadway.



Neighborhoods

Neighborhoods are the most abundant development type in the City of Louisville. Neighborhoods are predominantly residential land uses. Neighborhoods range from less dense large lot single family neighborhoods to higher density multi-family communities. Neighborhoods have public spaces either integrated within, or adjacent to them. Neighborhoods are generally sized by a ½ mile diameter (10 minute walk) and have well defined edges and boundaries.

A key component of this Comprehensive Plan update is the introduction of a recommended city-wide neighborhood planning initiative. The neighborhood plans are tailored toward the needs of individual neighborhood. They will ensure the neighborhoods remain livable, stable and successful as the region continues to grow and the City continues to evolve.



The Vision Statement and Core Community Values

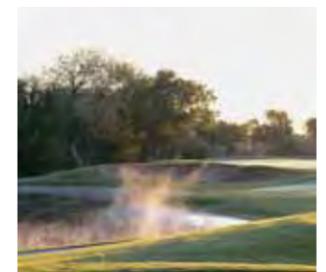
Special Districts

Special Districts are unique development types customized to a particular location and development opportunity. Special Districts are predominantly a single use development, typically involving either industrial or office land uses. Special Districts range in density and intensity. Public spaces are seldom integrated within the development and are more often adjacent, or nearby the special district. Special districts within Louisville include: Centennial Valley, Coal Creek Business Park, Phillips 66 and the Colorado Technology Center.



Parks and Open Space

Parks and Open Spaces are development types to be considered in Louisville. Parks and Open Spaces are predominantly a single institutional or civic use, in which retailing and entertainment opportunities may be temporarily allowed through a license agreement with the City. Parks and Open Spaces range in size and activity levels. The Parks and Open Spaces system is guided by the Parks Recreation Open Space and Trails (PROST) Master Plan, a companion document to the Comprehensive Plan.



THE FRAMEWORK

The Framework uses the new character zone language outlined in the previous section to graphically represent the City of Louisville's adopted Vision Statement and Core Community Values. The Framework also represents a Long-Range Integrated Land Use, Transportation and Natural Resource Plan for the City. These elements provide a specific strategy for enabling the City to review and modify its land development regulations and assist in prioritizing the City's Capital Improvement Program. Together, the Vision Statement, the Core Community Values and the Framework establish community expectations and provide policy guidance for the anticipated areas of change and stability in the City.

The Framework's composition of land uses enables a place for existing and future residents to live, work, shop, and play. The composition of uses ensures a fiscal balance to maintain the City's high quality of services. The Framework also positions the City to capitalize on sound market strategies that will allow the City's revenue generating land uses to stay competitive with neighboring municipalities and the surrounding region.

The core component of the Framework is the identification and development of three mixed use urban centers in the City over the next twenty years.

1. Downtown / the Highway 42 Revitalization District;
2. Highway 42 and South Boulder Road; and,
3. McCaslin Boulevard.

The Framework also designates McCaslin Boulevard (North of Cherry Street and South of Via Appia), South Boulder Road (east of Via Appia), and HWY 42 (north of South Boulder Road) as urban corridors. The special districts of the City are defined to include Centennial Valley, Coal Creek Business Park, the Colorado Technology Center, 96th Street, Dillon Road, and the Phillips 66 property.

The plan identifies various suburban, urban, and rural neighborhoods throughout the City and outlines the parks and open space areas within the City. The follow-

ing section describes what is envisioned through the City's Vision Statement and Core Community Values and graphically represents it within the Framework.

Street Types and Land Use

The land uses envisioned in the Framework's Center and Corridor development types, are determined by the street types in each area. This Comprehensive Plan identifies four types of streets in the Center and Corridor development types: Retail Primary and Secondary Streets and Mixed Use Primary and Secondary Streets.

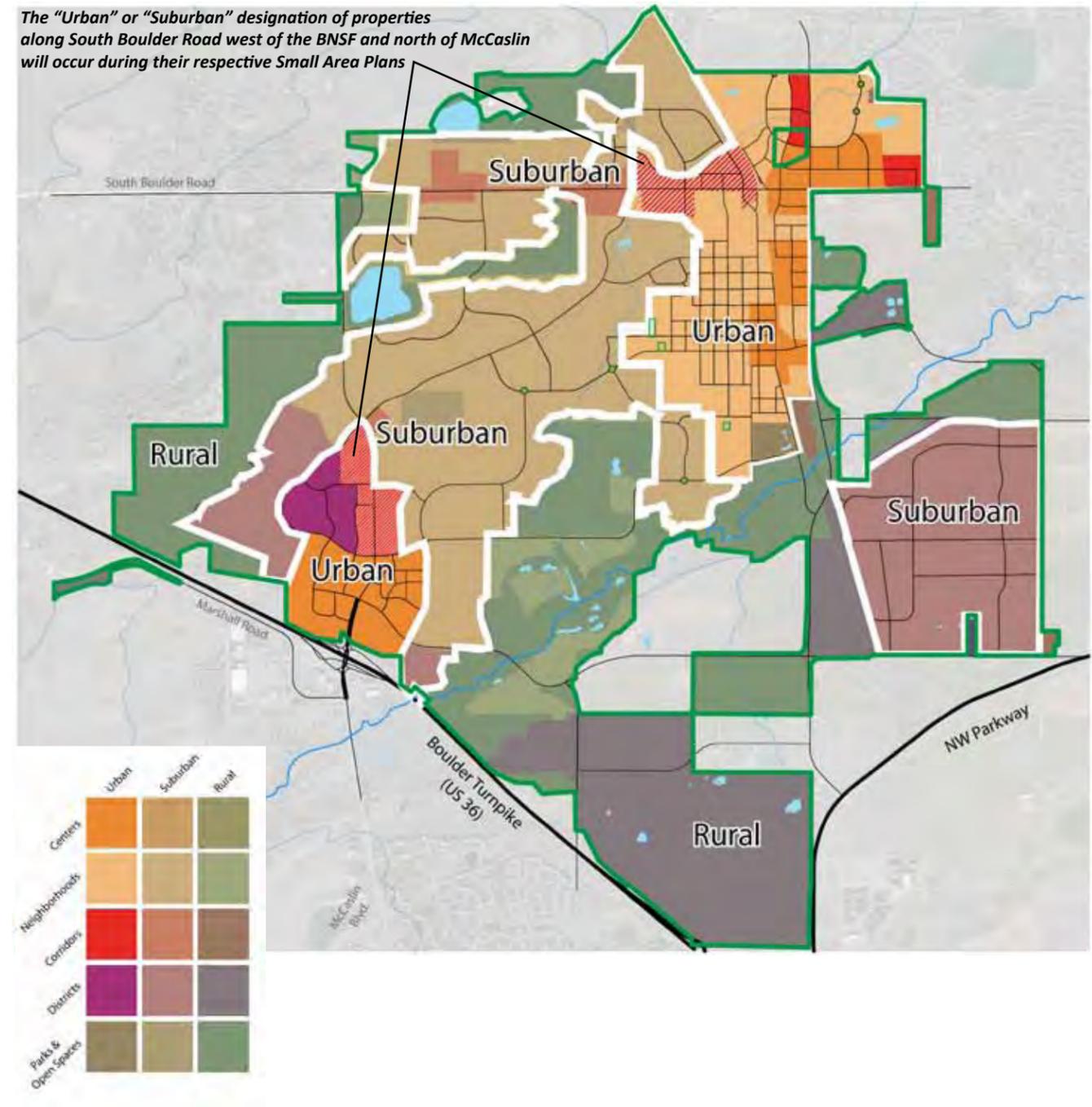
Retail Primary Streets are those streets best positioned for retail success. The traffic volumes and visibility these streets provide requires the provision of retail land uses on the ground floor of the buildings adjacent to them. Other commercial uses may be located on a second story, above the ground floor retail use. Residential land uses are not found on Retail Primary Streets.

Retail Secondary Streets have the potential for retail success, but their location and traffic volumes suggest that other commercial uses, such as office, may present a more economically viable land use option. Retail land uses should be clustered in key locations on secondary streets where visibility and access exist. Residential land uses are not found on Retail Secondary Streets.

Mixed Use Primary Streets are those streets that are located and designed for a mix of complementary uses. These streets may function as the center of a larger mixed use district, and as such are ideally situated for pedestrian activated ground floor commercial uses. Residential uses may occupy the upper floors of a mixed use building on a Mixed Use Primary Street.

Mixed Use Secondary Streets are found in mixed use districts, but they are not located in the heart, or center, of the district. The location of the streets and the corresponding reduced traffic volumes suggest that uses other than retail or office may be more appropriate on the ground floor of buildings fronting the street. Residential uses may be the sole use in a building located on a Mixed Use Secondary Street.

The "Urban" or "Suburban" designation of properties along South Boulder Road west of the BNSF and north of McCaslin will occur during their respective Small Area Plans



The Framework

DOWNTOWN AND THE HIGHWAY 42 REVITALIZATION DISTRICT

The combination of Downtown Louisville and the HWY 42 Revitalization District is the only one of the three urban centers identified in the Framework that currently operates as an urban center. Historic Downtown Louisville presently has a mix of land uses within a walkable and integrated urban pattern. Future efforts in this center will continue to encourage a healthy and vibrant downtown consisting of a mix of supporting businesses and residences. This Framework looks to build on the success of Downtown Louisville in the HWY 42 Revitalization District.

The existing HWY 42 Revitalization Plan calls for a mix of residential housing types, commercial retail and office areas, and parks and public spaces on the east side of the railroad tracks. As the Downtown and HWY 42 Revitalization District Urban Center continues to evolve, focus should be placed on policy and infrastructure improvements which enable these two areas to evolve as one well connected and cohesive urban center.

Land Use Mix

The Downtown and Highway 42 Revitalization District Urban Center is intended to include a mix of uses through the entirety of the center, and within individual buildings. The Center will include a mix of Mixed Use Primary and Secondary Streets, and the land uses envisioned will follow those highlighted in the following table. The assignment of the street types in this sub-

Land Use	Street Type			
	Retail		Mixed Use	
	Primary	Secondary	Primary	Secondary
Retail	G	A	E	A
Office	A*	A	E	A
Residential	N	N	A*	A
Industrial	N	N	N	N
Institutional	A	A	A	A

- A Allowed
- A* Allowed above ground floor
- E Either retail or office required on ground floor
- G Required on ground floor
- N Not allowed

district will be determined during a separate Planning initiative.

Parking: Shared parking environment where visitors park once and visit multiple locations without moving their automobile.

Fiscal Performance: Land use mix demonstrates positive fiscal benefits

Density Range:
Floor Area Ratio: 1.0 – 2.0 with an overall average of 1.5
Unit per Acre: Up to 25 DU/Acre

Building Height: 2-3 Stories

Building Form and Design

- Buildings front the street and the ground floor is activated on primary retail streets.
- Human-scaled buildings.
- Pedestrian design detailing on all building ground floors and around public gathering spaces.
- The growth of the Center will preserve the character and scale of the neighborhoods within the Old Town Overlay District (Little Italy, Miners Field, and Old Town).

Infrastructure

Streets: Reduced speed and multimodal
Block Length: 300-400 Feet
Public Spaces and Trails: Interconnected and integrated into the urban center and nearby open spaces

Design Standards

Downtown - Downtown Framework; Downtown Design Handbook; and, Downtown Parking and Pedestrian Action Plan.
Revitalization District - Mixed Use Development Design Standards and Guideline and Highway 42 Framework Plan.

Policies

- Continue to recognize historic buildings are an integral part of downtown’s character and success, and develop a Preservation Master Plan for residential and commercial structures with historic eligibility.
- Encourage a diversity of housing types and provide a transition in scale from higher density uses in the core of the Urban Center to the adjacent neighborhoods.
- Promote the development of additional public parking and parking management strategies to efficiently use parking resources, ensure a walkable environment, and alleviate potential parking constraints as the Urban Center continues to redevelop.
- Continue to promote the vitality of the downtown through marketing (such as new identification and directional signs) and collaboration with the Chamber of Commerce, Business Retention and Development Committee, and the Downtown Business Association, as well as supporting destination venues such as the Louisville Street Faire, the Steinbaugh Pavilion, Memory Square, the Louisville Arts Center and the Community Park.
- Encourage business diversity through strategic public infrastructure improvements and business assistance which encourages new private investment and business development.
- Complete the necessary street network, pedestrian, and bicycle connections between the Downtown Area and the Highway 42 Revitalization District to provide travel choices, stabilize existing neighborhoods and create one cohesive urban center.
- Promote safe connections for all transportation modes across major transportation corridors and between adjacent commercial areas.

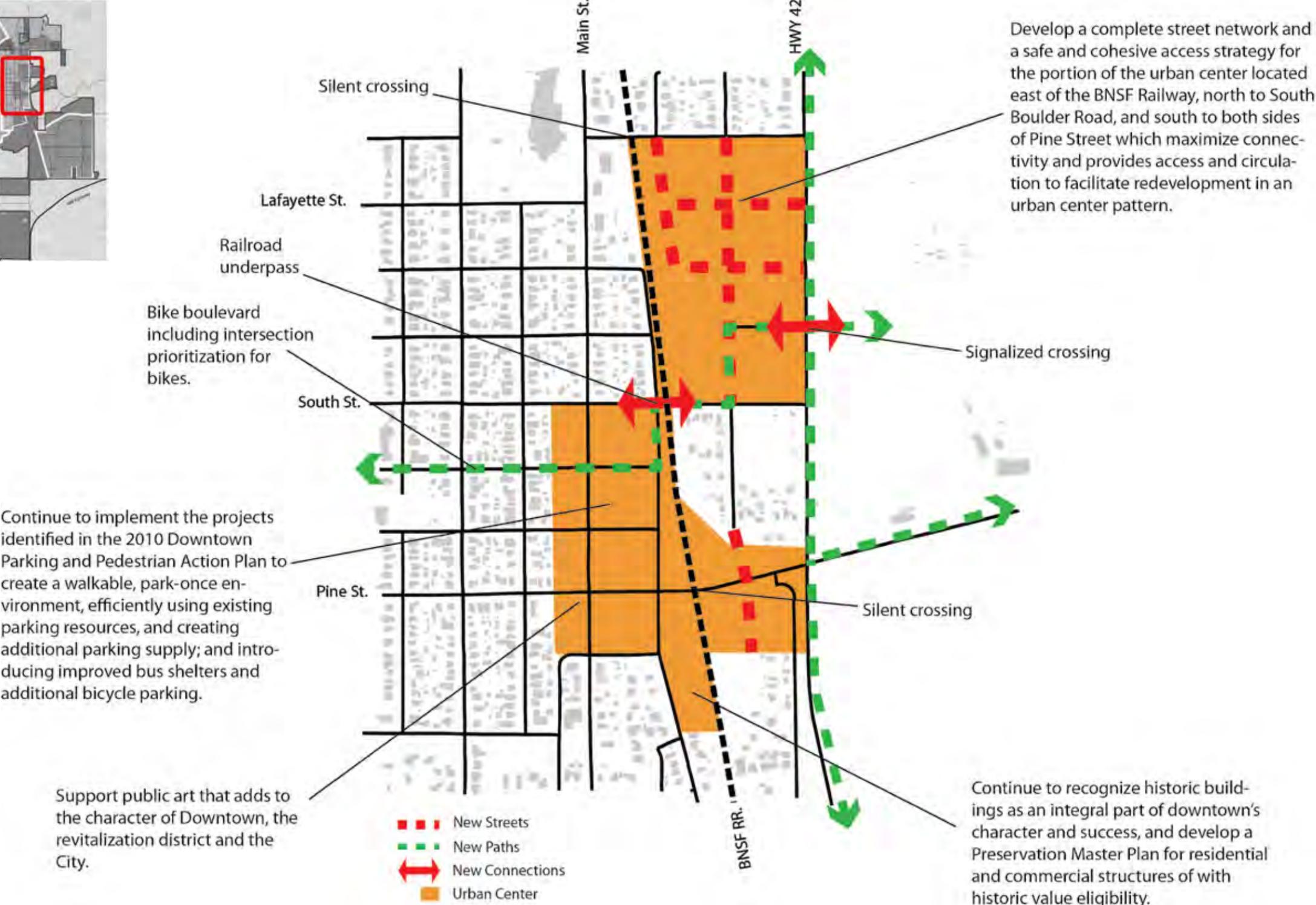
Pedestrian crossings should be completed across HWY 42 and under the existing rail tracks to ensure safe pedestrian passage.

- Develop a complete street network and a safe and cohesive access strategy for the portion of the urban center located east of the BNSF Railway, north to South Boulder Road, and south to both sides of Pine Street which maximizes connectivity and provides access and circulation to facilitate redevelopment in an urban center pattern.
- Promote the health of downtown through a traditional development pattern and pedestrian scaled redevelopment including expansion of business and housing opportunities.
- Continue to implement the projects identified in the 2010 Downtown Parking and Pedestrian Action Plan to create a walkable park once environment, efficiently using existing parking resources, creating additional parking supply; and introducing improved bus shelters and additional bicycle parking.
- Support public art initiatives which add to the character of Downtown, the Revitalization District and the City.
- Street network enhancements should only occur concurrent with the approved development, or redevelopment of a property, or neighborhoods.





Location Map



The Framework

MCCASLIN BOULEVARD (SOUTH OF CHERRY)

The McCaslin Boulevard Urban Center will serve as the focal point for a regionally significant commercial activity center. Future public and private investment is needed to transform this area from an auto oriented suburban retail center, to a walkable mixed-use transit supportive urban center. As properties redevelop over time, attention will be given to enabling a more interconnected block structure that introduces a walkable street network, and the possibility of a mixture of uses, to an area that currently consists of large single purpose properties. The block structure in the McCaslin Boulevard Urban Center will allow for larger blocks than those found in Old Town, but basic connectivity through the Center will be enhanced over time.

The forthcoming Diverging Diamond Interchange and Bus Rapid Transit (BRT) center located at the McCaslin and US Highway 36 interchange will provide increased vehicle capacity and regional transit options that will support higher intensity development infill opportunities. While the entire Urban Center will benefit from the enhanced transit service along US 36, the area surrounding the BRT stop should realize a higher development potential. The McCaslin Boulevard Urban Center shall remain the City of Louisville's primary retailing center and will have the highest intensity of development in the City.

Land Use Mix

The McCaslin Boulevard Urban Center shall remain the City's primary retail center that is supported by a mix of land uses including office and residential. The center will support a vertical mix of land uses with single use residential buildings permitted only in proximity to and a relationship with adjacent to existing residential areas. The Center is intended to include Retail Primary and Secondary Streets and Mixed Use Primary and Secondary Streets. The location and classification of these streets will be determined during the creation of a small area plan for the McCaslin Boulevard Urban Center.

Parking: Majority on-site private parking associated with a particular use. Shared

Land Use	Street Type			
	Retail		Mixed Use	
	Primary	Secondary	Primary	Secondary
Retail	G	A	E	A
Office	A*	A	E	A
Residential	N	N	A*	A
Industrial	N	N	N	N
Institutional	A	A	A	A

A Allowed
 A* Allowed above ground floor
 E Either retail or office required on ground floor
 G Required on ground floor
 N Not allowed

parking facilities encouraged in the vicinity to the BRT Station.

Fiscal Performance: Land use mix demonstrates strong fiscal benefits

Density Range:

Floor Area Ratio: Average of 1.0
Unit per Acre: Up to 30 DU/Acre

Building Height: 2-3 Stories. A 4th story allowed only if view sheds are preserved, shading impacts are mitigated, and the public realm is not adversely impacted.

Building Form and Design

1. Ground floor oriented towards the street
2. Ground floor activated with retail and commercial uses and pedestrian scaled development
3. Provide buildings which transition in scale from adjacent uses

Infrastructure

Streets: Reduced speed and multi-modal
Block Length: 300-600 Feet
Public Spaces and Trails: Public gathering spaces and focal points on both sides of McCaslin Boulevard. Trails integrated into the urban center and transitioning to Davidson Mesa.

Design Standards

Future development will be guided by a Small Area Plan which will allow for flexibility in the urban center to enable emerging market retail, office, residential and mixed use trends to develop as long as the desirable form of the center is maintained.

The Commercial Development Design Standards and Guidelines (CDDSG) currently guide design in the urban center. These guidelines were created for an auto-centric suburban single-use commercial environment, and do not provide flexibility for a changing commercial retail market. The small area plan will address building placement, block structure, landscaping, and signage requirements consistent with the urban center character, and shall replace the CDDSG in governing the design character of the Urban Center.

Policies

1. Build upon the planned Diverging Diamond Interchange and the BRT Station to provide a higher intensity mix of interdependent and compatible land uses with quality access to transit opportunities.
2. Encourage higher intensity transit oriented development within proximity of the BRT station.
3. New residential uses should first be introduced in proximity to and a relationship with existing residential areas.
4. Introduce public gathering spaces on both the east and west side of McCaslin Boulevard which will help to create an identity for the area and allow for public events.
5. Retain commercial retail land supply and promote the retention of existing commercial development as a primarily regional retail center.
6. Enhance the City's regional retail opportunities at the US 36 and McCaslin Boulevard interchange.

7. Emphasize retention of commercial retail uses as a component of any transit oriented development.
8. Increase pedestrian connectivity across McCaslin Boulevard and between employment centers, retail areas, and public land areas within the Urban Center transforming McCaslin Boulevard from a barrier, to the feature that connects both sides of the urban center.
9. Promote safe connections for all transportation modes across major transportation corridors and between adjacent commercial areas.
10. Provide safe pedestrian crossings of McCaslin Boulevard to assist in the integration of both sides of the street. Promote site planning design standards that support and facilitate pedestrian and bicycle access and alternative modes of transportation.
11. New gateway features and wayfinding should reinforce the McCaslin Boulevard interchange area as a primary entryway to the City.
12. Support public art and amenities that add to the character of the McCaslin Boulevard Urban Center and the City.
13. Areas west of McCaslin Boulevard should not include any Mixed Use streets.
14. Residential development may be allowed east of McCaslin if it is incorporated into a development proposal which provides exceptionally strong fiscal and economic benefits to the City.





Location Map

Increase pedestrian connectivity across McCaslin Boulevard and between employment centers, retail areas, and public land areas within the Urban Center transforming McCaslin from a barrier into being a feature that connects both sides of the urban center.



New residential uses should first be introduced in proximity to and a relationship with existing residential areas.

Introduce public gathering spaces on both the east and west side of McCaslin which will help to create an identity for the area and allow for public events.

Build upon the planned Diverging Diamond Interchange and the Bus Rapid Transit Station to provide a higher intensity mix of interdependent and compatible land uses with easy access to transit opportunities.

New gateway features and wayfinding should reinforce the McCaslin interchange area as a primary entryway to the City.

The Framework

HIGHWAY 42 AND SOUTH BOULDER ROAD

The Highway 42 and South Boulder Road Urban Center will bring the separate parcels surrounding the Highway 42 and South Boulder Road intersection into one cohesive center. As properties redevelop in this area, attention will be paid to introducing a more connected street grid creating smaller parcels which relate to one another in an urban and walkable mixed use environment. Commercial land uses and higher density residential uses will concentrate along the South Boulder Road and Highway 42 intersection while lower density residential uses should locate away from the main arterials to provide a transition to the existing neighborhoods.

Land Use Mix

The Highway 42 and South Boulder Road Urban Center is intended to include a mix of uses. This center will include a mix of Retail Primary and Secondary Streets and Mixed Use Primary and Secondary Streets. The location and classification of these streets will be determined during the creation of a small area plan for the Highway 42 and South Boulder Road Urban Center.

Land Use	Street Type			
	Retail		Mixed Use	
	Primary	Secondary	Primary	Secondary
Retail	G	A	E	A
Office	A*	A	E	A
Residential	N	N	A*	A
Industrial	N	N	N	N
Institutional	A	A	A	A

- A Allowed
- A* Allowed above ground floor
- E Either retail or office required on ground floor
- G Required on ground floor
- N Not allowed

Parking: On-site private parking associated with a particular use. Allowance for shared parking agreements

Fiscal Performance: Land use mix demonstrates positive fiscal benefits

Density Range:
Floor Area Ratio: Average of 1.0 FAR
Unit per Acre: Up to 30 DU/Acre

Building Height: 2-3 Stories

Building Form and Design

1. Ground floor oriented towards the street.
2. Ground floor activated with retail and commercial uses and pedestrian scaled development.
3. Provide buildings which transition in scale to adjacent neighborhoods.

Infrastructure

Streets: Slow speed and multimodal with emphasis on creating livable and urban arterial roadways (South Boulder Road and HWY 42).

Block Length: 300-400 Feet

Public Spaces and Trails: Public gathering spaces and focal points on both sides of HWY 42 interconnected and integrated into the urban center and transitioning through the center to the surrounding trail network and open space.

Design Standards

A small area plan should be completed to further define the desired form of development in the Highway 42 and South Boulder Road Urban Center. The majority of the center is currently regulated by the Commercial Development Design Standards and Guidelines (CDDSG). These guidelines were created for an auto-centric suburban commercial environment, and they do not address the type of urban center development envisioned in this Comprehensive Plan. The small area plan will address building placement, block structure, landscaping, and

signage requirements consistent with the urban center character and shall replace the CDDSG in governing the design character of the Urban Center.

New design guidelines should be created which address building placement, block structure, landscaping, and signage requirements City-wide consistent with proposed character zones of the City. The Mixed Use Development Design Standards and Guidelines will continue to provide design guidance for the portion of the center located in the Revitalization District.

Policies

1. Include a mix of low to higher density residential and commercial neighborhood services.
2. Transition from higher intensity uses at the core of the center to lower density uses at the neighborhoods on the periphery of the center
3. To encourage the economic health of existing shopping centers, leverage public investment for infrastructure improvements and business assistance packages to stimulate private redevelopment.
4. Focus on community retail opportunities at the intersection of South Boulder Road and HWY 42 which serve a smaller trade area than those found at a regional retail center.
5. Introduce new roadway network in the center to enable the area to operate as a connected urban center. Medium to high density residential areas should be located with proximity to and pedestrian access to public transportation, neighborhood parks and trail connections and commercial services.
6. As redevelopment occurs, introduce roadway network to enable a variety of redevelopment possibilities. The City should cooperate with the City of Lafayette and Boulder County to secure access between Hecla Lake, Waneka Lake, and Coal Creek.

7. Create a high degree of trail and open space connectivity reinforcing the east/west connectedness of a regional trail system to Hecla Lake and north/south connectedness to Downtown and Coal Creek regional trail.
8. Explore realigning Main Street on the western edge of the urban center to consolidate access near the railroad tracks and introduce a Gateway to the HWY 42 and South Boulder Road urban center and Downtown Louisville.
9. Connect the Highway 42 and South Boulder Road Urban Center to the rest of Louisville by the introduction of new roads, trail connections, and pedestrian crossings of the railroad tracks, South Boulder Road, and HWY 42.
10. Encourage development of new commercial retail services in the Urban Center where the location and scale of such development is consistent with design standards developed for the HWY 42 corridor and the character of the immediate neighborhood.
11. Louisville Plaza shopping center should not include any Mixed Use streets.





Location Map

Introduce new roadway network in the center to enable the area to operate as a connected urban center.

Explore realigning Main Street on the western edge of the urban center to consolidate access near the railroad tracks and introduce a Gateway to the HWY 42 and South Boulder Road urban center and Downtown Louisville.



Create a high degree of trail and open space connectivity reinforcing the east/west connectedness of a regional trail system to Hecla Lake and north/south connectedness to Downtown and Coal Creek regional trail.

Encourage the development of new commercial retail services in the Urban Center where the location and scale of such development is consistent with design standards developed for the Highway 42 corridor and the character of the immediate neighborhood.

Focus on community retail opportunities at the intersection of South Boulder Road and Highway 42 which serve a smaller trade area than those found at a regional retail center.

The Framework

SOUTH BOULDER ROAD AND HIGHWAY 42 CORRIDORS

South Boulder Road Suburban Corridor (West of Via Appia)

South Boulder Road begins as a Suburban Corridor at City limits and remains one as it travels east to Via Appia. As a Suburban Corridor, South Boulder Road's main function is to move all modes of transportation through the corridor and to provide access to the neighborhoods and commercial uses surrounding the corridor. The South Boulder Road Suburban Corridor contains a horizontal mix of uses including residential and commercial. The parcels in the suburban corridor are mainly connected along South Boulder Road and the land uses are setback from the roadway or buffered from it through landscaping. In this fashion, South Boulder road serves as an edge between the uses on either side of it. Safe pedestrian and bicycle crossings at key locations are needed to safely connect both sides of the corridor.

South Boulder Road Urban Corridor (East of Via Appia)

The South Boulder Road Urban Corridor runs adjacent to South Boulder Road beginning at Via Appia and extending east to the railroad tracks where it feeds into the Highway 42 and South Boulder Road Urban Center. After leaving the Urban Center, South Boulder Road transitions back to an urban corridor until it leaves City limits. The urban corridor section of South Boulder Road begins the transition of the road from a suburban edge where the road is a division between land uses on either side of it, to an urban seam where the land uses in the corridor begin to engage with the road instead of turning their back on it. Development in the urban corridor section of South Boulder Road has a high degree of linear (east/west) connectivity between parcels and transitions to adjacent neighborhoods at the back of the corridor through the scaling down of buildings and the introduction of landscape buffers. The South Boulder Road urban corridor provides a transition to the Downtown and the Revitalization District urban center, and the Highway 42 and South Boulder Road urban center.

Highway 42 Urban Corridor

The Highway 42 Urban Corridor begins at the City limits adjacent to Paschal Drive and continues south on the

west side of Highway 42 until transitioning to the urban Center at Hecla Drive. This urban corridor focuses on commercial opportunities including office and neighborhood retail along with higher density housing in close proximity to the roadway. The land uses along the corridor will transition and provide connections to the lower density residential uses found on the outer edge of the corridor. Pedestrian and bicycle safe connections will be constructed across Highway 42 to connect users to the amenities on either side of the corridor, and provide regional trail connectivity.

Land Use Mix

Urban Corridors include a mix of uses including residential, commercial, retail, and park land. The South Boulder Road Corridor and Highway 42 Corridor is a combination of Mixed Use Primary and Secondary Streets. The location and classification of these street segments will be determined during the creation of a small area plan for the Highway 42 and South Boulder Road Corridors. The following table provides an overview of the land uses envisioned in the South Boulder Road and Highway 42 Corridors.

Parking: Majority on-site private parking associated with a particular use. Allowance for shared parking agreements in urban corridors.

Land Use	Street Type			
	Retail		Mixed Use	
	Primary	Secondary	Primary	Secondary
Retail	G	A	E	A
Office	A*	A	E	A
Residential	N	N	A*	A
Industrial	N	N	N	N
Institutional	A	A	A	A

A Allowed
 A* Allowed above ground floor
 E Either retail or office required on ground floor
 G Required on ground floor
 N Not allowed

Fiscal Performance: Land use mix demonstrates positive fiscal benefits in the urban corridor, and may demonstrate neutral fiscal returns in suburban corridors.

Density Range:

Floor Area Ratio - Urban Corridors:

Fronting the Arterial – Up to 1.0 FAR

Not fronting the Arterial - Up to .5 FAR

Floor Area Ratio - Suburban Corridors: Less than .25 FAR

Units per Acre - Urban Corridors: Up to 25 DU/Acre

Units per Acre - Suburban Corridors: Up to 15 DU/Acre

Building Height:

Urban Corridors: 2-3 Stories

Suburban Corridors: 2 Stories

Building Form and Design

Urban Corridors: Ground floor is oriented towards the Arterial Road and/or a secondary street. Provide buildings which transition in scale and mass to adjacent neighborhoods on the back of the property

Infrastructure

Streets - Urban Corridor Arterials: Reduced speed accommodating all modes and including safe pedestrian and bicycle crossings

Street - Suburban Corridor Arterials: Higher speed streets with safe pedestrian and bicycle crossings at key locations

Block Length - Urban Corridor: 300-400 Feet

Block Length - Suburban Corridor: 300-600 Feet

Public Spaces and Trails: Integrated into and transitioning through the corridor

Design Standards

There is currently no cohesive design guidance for the urban and suburban corridors in the City. The Commercial Development Design Standards and Guidelines (CDDSG) regulate commercial development, and various planned unit developments and other residential zoning standards govern residential development. The small area plan for the corridor will address building placement, block structure, landscaping, and signage require-

ments consistent with the urban center character and shall replace the CDDSG in governing the design character of the Urban Corridor.

New design guidelines should be created which address building placement, block structure, landscaping, and signage requirements City-wide consistent with proposed character zones of the City.

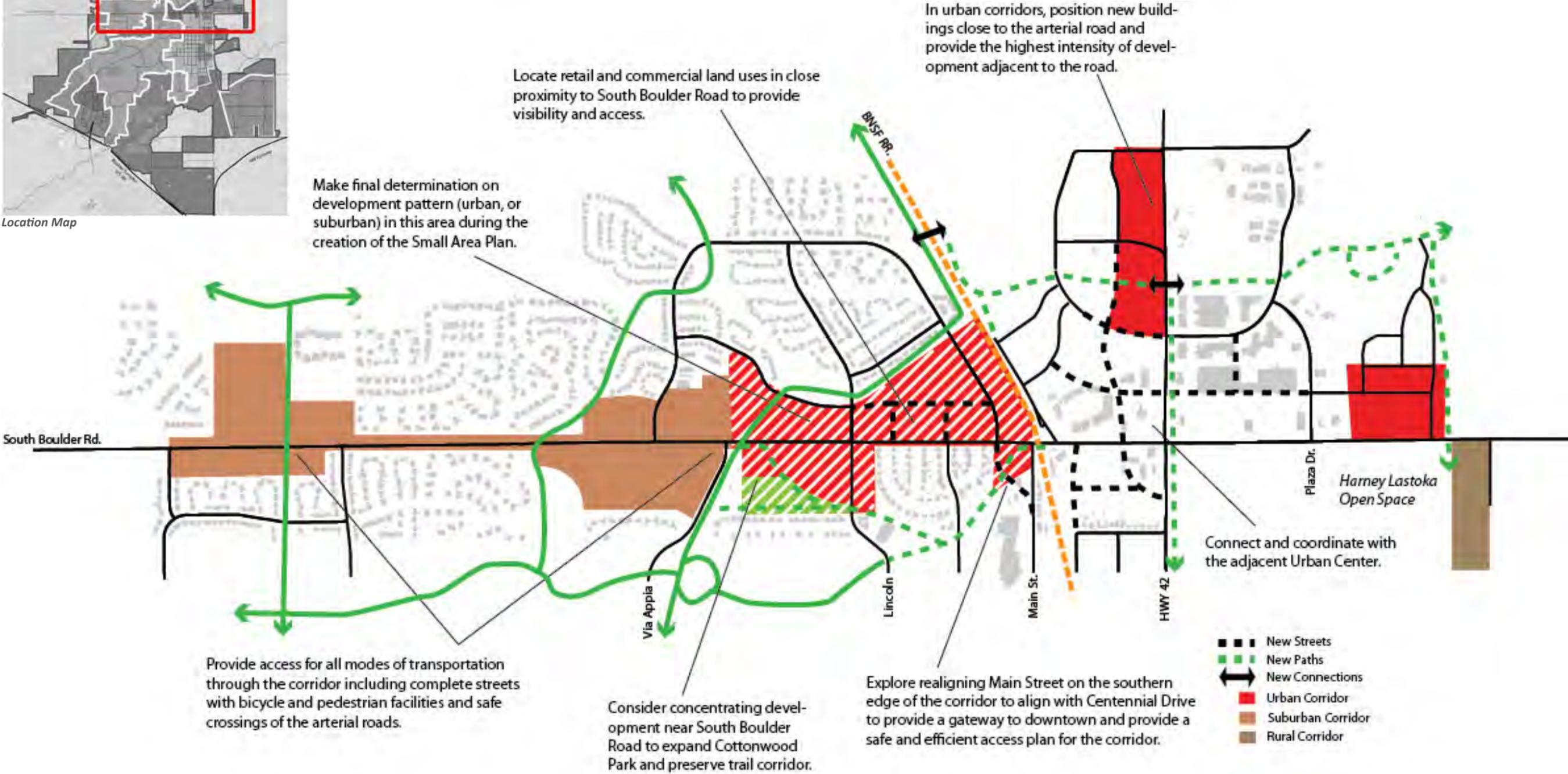
Polices

1. In urban corridors, position new buildings close to the arterial road and provide the highest intensity of development adjacent to the road.
2. Use form-based design regulations to focus on establishing a street presence along the arterial corridors
3. Locate retail and commercial land uses in close proximity to South Boulder Road to provide visibility and access.
4. Explore realigning Main Street on the southern edge of the corridor to align with Centennial Drive to provide a gateway to downtown and provide a safe and efficient access plan for the corridor.
5. Provide access for all modes of transportation through the corridor including complete streets with bicycle and pedestrian facilities and safe crossings of the arterial roads.
6. Develop a comprehensive signage and way finding strategy for the corridor.





Location Map



The Framework

MCCASLIN BOULEVARD CORRIDOR (North of Cherry Street)

McCasin Boulevard transitions from an urban center to an urban corridor from Cherry Street north to Via Appia. The land uses in this corridor will focus on the activity generated by McCasin Boulevard and will include a mix of residential, commercial and neighborhood retail uses. Linear (north/south) connections will be maintained between individual parcels in the corridor. Safe pedestrian and bicycle crossings of McCasin Boulevard will be implemented to enable safe access between the businesses, offices, and residences on either side. The McCasin Boulevard Urban Corridor transitions to a Suburban Corridor at the southeast corner of Via Appia and McCasin.

Land Use Mix

Urban Corridors include a mix of uses including residential, commercial, retail, and park land. The McCasin Boulevard Corridor is a combination of Mixed Use Primary and Secondary Streets. The location and classification of these street segments will be determined during the creation of a small area plan for the McCasin Boulevard Corridor. The following table provides an overview of the land uses envisioned in the McCasin Boulevard Corridor.

Land Use	Street Type			
	Retail		Mixed Use	
	Primary	Secondary	Primary	Secondary
Retail	G	A	E	A
Office	A*	A	E	A
Residential	N	N	A*	A
Industrial	N	N	N	N
Institutional	A	A	A	A

- A Allowed
- A* Allowed above ground floor
- E Either retail or office required on ground floor
- G Required on ground floor
- N Not allowed

Parking: Majority on-site private parking associated with a particular use. Allowance for shared parking agreements.

Fiscal Performance: Land use mix demonstrates positive fiscal benefits.

Density Range:
Floor Area Ratio:
 Fronting McCasin Boulevard – Up to 1.0 FAR
 Not fronting McCasin Boulevard - Up to .5 FAR
Units per Acre: Up to 30 DU/Acre

Building Height: 2-3 Stories

Building Form and Design
 Ground floor is oriented towards McCasin Boulevard and/or a secondary street. Provide buildings which transition in scale to adjacent neighborhoods.

Infrastructure
Streets – McCasin Boulevard: Transitioning to lower speeds which accommodate all modes of travel in an urban environment, and including safe bicycle and pedestrian crossings.
Block Length: 300-600 Feet
Public Spaces and Trails: Integrated into and transitioning through the corridor

Design Standards
 There is not currently cohesive design guidance for the McCasin Boulevard urban corridor. The Commercial Development Design Standards and Guidelines regulate new commercial development, and various planned unit developments and other residential zoning standards govern residential development. Unified standards should be created that help to create a cohesive linear corridor with a mix of uses. Setbacks and landscaping standards should be revised to enable visibility of commercial structures and a unified signage and wayfinding program should be implemented.

The small area plan for the corridor will address building placement, block structure, landscaping, and signage

requirements consistent with the urban center character and shall replace the CDDSG in governing the design character of the Urban Corridor.

Form-based design regulations should be used to focus on establishing a street presence along McCasin Boulevard with both single use commercial buildings and mixed use residential buildings.

New design guidelines should be created which address building placement, block structure, landscaping, and signage requirements City-wide consistent with proposed character zones of the City.

Policies

1. Position new buildings close to the street and provide the highest intensity of development on the Roadway. Interconnect corridor parcels through cross access easements to enable pedestrian and bicycle mobility between uses.
2. Retail and Commercial land uses should be located in close proximity to McCasin Boulevard to provide visibility and access.
3. Use form-based design regulations to focus on establishing a street presence along the arterial corridors.
4. Introduce a unified signage and wayfinding program to provide a gateway to the City of Louisville and establish and identity for the corridor.
5. Provide access for all modes of transportation through the corridor including complete streets with bicycle and pedestrian facilities and safe crossings of McCasin Boulevard.
6. No Mixed Use streets should be designated north of Centennial Pavillion shopping center.





Location Map

Make final determination on development pattern (urban, or suburban) in this area during the creation of the Small Area Plan.

Retail and Commercial land uses should be located in close proximity to McCaslin Boulevard to provide visibility and access.

Davidson Mesa Open Space

Provide access for all modes of transportation through the corridor including complete streets with bicycle and pedestrian facilities and safe crossings of McCaslin Boulevard.

McCaslin Blvd.

Via Appia

Introduce a unified signage and way-finding program to provide a gateway to the City of Louisville and establish and identity for the corridor.

Century Dr.

Interconnect corridor parcels through cross access easements to enable pedestrian and bicycle mobility between uses.

Cherry St.

Connect and coordinate with adjacent Urban Center.

McCaslin Blvd.

- ■ ■ New Streets
- ■ ■ New Paths
- Urban Corridor
- Suburban Corridor
- Connectivity Seam

The Framework

SPECIAL DISTRICTS

Centennial Valley and Coal Creek Business Park

Centennial Valley is an office park special district located between McCaslin Boulevard and the Davidson Mesa Open Space. The portion of the Centennial Valley Business Park located to the west of Centennial Parkway is suburban and consists of single use large office parcels. The portion of the Special District located to the east of Centennial Parkway is urban and consists of smaller office parcels that are interconnected and have direct bicycle and pedestrian access to the McCaslin Boulevard urban center and urban corridor. The Coal Creek Business Park is a suburban office park Special District located adjacent to Dillon Road.

Colorado Technology Center (CTC)

The Colorado Technology Center Suburban Special District is located in the southeastern corner of the City and includes a mix of industrial, office, and research and development facilities. This Special District is a key employment center for the City and will continue to be in the future. Design standards will serve to buffer land uses of differing intensities in the special district, and maintain a high quality employment center that responds to the needs of businesses.

96th and Dillon

The 96th Street and Dillon Road Rural Special District serves as the rural gateway to the City of Louisville. The area will include a mix of commercial, institutional, and industrial uses. The uses in this special district will be separated and buffered from the surrounding roads to maintain the appearance of a rural entryway to the City.

Phillips 66

The Phillips 66 Rural Special District is located in the southern portion of the City and is currently vacant. The land in this location is a unique subarea of the City which contains vital community facilities that provide critical services to the City and also presents a unique regional development opportunity. Due to the isolated nature of this special district, it is somewhat self-contained. However, the district will remain connected to the region through US 36 and to the rest of Louisville

through pedestrian and bicycle trails.

Empire Road

The Empire Road rural special district is situated adjacent to municipal recreational fields (Louisville’s baseball and Lafayette’s future soccer) and the Mayhoffer agricultural lands. The district serves as a rural gateway to downtown Louisville and provides direct access for Old Town residents to Boulder County’s open space and the Coal Creek Trail. The area includes the City’s Wastewater Treatment Plant and the Municipal Services Building. The uses and buildings in this special district should celebrate rural entryway to Downtown Louisville and facilitate recreational connections to the Coal Creek Trail.

Land Use Mix

Each Special District’s land use mix is unique and customized to each individual area. Generally the land use mix within each area is:

Residential: Not Allowed

Retail: Encouraged in locations where the use can capitalize on the activity in the special district, or traffic on surrounding roads.

Office: Allowed as the single use on a parcel, or as part of a mixed commercial/industrial building

Industrial: Allowed as the single use on a parcel, or as part of a mixed commercial/industrial building

Institutional: Allowed

Parking: On-site private parking associated with a particular use.

Fiscal Performance: Land use mix demonstrates neutral fiscal benefits and positive economic benefits

Density Range:

Floor Area Ratio - Urban: Up to .75 FAR

Floor Area Ratio - Suburban: Up to .5 FAR

Floor Area Ratio - Rural: Up to .25 FAR

Building Height:

Urban: 2-3 Stories

Suburban: 2-3 Stories

Rural: 3 stories. Additional stories permitted if structures are clustered and located out of the public view shed and buffered by surrounding topography and Open Space.

Building Form and Design

Buildings are oriented towards the property they sit on and serve the unique use requirements of the property.

Infrastructure

Streets: Varied Speeds

Block Length:

Urban: 300-600 Feet

Suburban: 1,000 – 2,000 Feet

Rural: No defined block structure

Public Spaces and Trails: Serving the periphery of the district.

Policies

1. Articulate and define Special Districts’ specific character expectations in customized general development plans adopted by City Council.
2. Create walkable special districts that are connected to the rest of the City through sidewalks and pedestrian and bicycle paths.
3. Encourage internal services which meet the daily needs of the people working in the district.
4. Establish new design guidelines, replacing the CDDSG and IDDSG, to address building placement, block structure, landscaping, and signage requirements City-wide consistent with proposed character zones of the City.

5. Use form-based design regulations to focus on establishing a street presence along McCaslin Boulevard with both single use commercial buildings and mixed use residential buildings.





The Framework

NEIGHBORHOODS AND HOUSING (NH)

The established residential neighborhoods of Louisville are often overlooked but are of paramount importance to the citizens of Louisville residing in them. The City’s residential housing stock is aging and rehabilitation issues within residential areas will create challenges that the City must be prepared to meet. Outside of Old Town, the City’s residential areas are governed by independent Planned Unit Developments (PUDs). While these PUDs are comprehensive, they are not equipped to assist the City in providing coherent neighborhood plans and strategies for issues such as: housing rehabilitation, cut-through traffic, safe routes to school, aging infrastructure, and monitoring and maintenance of community services.

Changes in adjacent commercial and industrial land uses, particularly infill redevelopment, will also impact neighborhoods, requiring the establishment of compatible design criteria. The neighborhoods must also meet the housing goals of the City, for both current and future residents.

This Comprehensive Plan therefore recommends creating plans for each neighborhood and initiating a housing policy conversation in the City to aid in addressing these and other issues.

The residential areas of Louisville have been characterized into nine neighborhoods. The starting point was circles with half-mile radii, representing a reasonable walking distance. The neighborhoods were then formed around these circles based on geography, connectivity, housing stock, and the input of residents at the charrette and elsewhere. They are as follows:

Davidson Mesa – the homes on top of the mesa in the northwest corner of the City, stretching to both sides of South Boulder Road and bounded on the south and east by Coyote Run open space. The area is mostly larger-lot single-family homes, with a few duplexes and some office uses along South Boulder Road.

North Louisville – the central residential area north



of South Boulder Road, with the north open space to the west and the BNSF railway to the east. The area consists of single-family homes, townhomes, apartment units, and commercial and retail developments along South Boulder Road.

Hecla – the newer homes on either side of HWY 42, north of South Boulder Road and east of the BNSF railway. The area includes apartments, townhomes, single-family homes, senior housing, and significant retail development around South Boulder Road and HWY 42.

Lake Park – the houses around Lake Park on Via Appia, bounded by Coyote Run open space to the west, South Boulder Road to the north, and Old Town to the south and east. The area has apartments, townhomes, mobile homes, and single-family homes.

Hillside – the houses on the slope of Davidson Mesa, with Via Appia to the south and Coyote Run to the north, stretching across McCaslin Boulevard to the homes on the west. The area is all single-family homes, mostly on larger lots.

Old Town – the central area comprised of the Old Town Overlay Zone District, the Central Business District, and

the Mixed Use Overlay District, as well as the newer subdivisions immediately west of Old Town. The area has a diverse mix of single-family houses, both new and old, and multi-family dwellings, as well as commercial areas along Main Street and at South Boulder Road.

Fireside – the homes around Fireside Elementary, extending from Cherry Street to Via Appia and McCaslin Boulevard to Warembourg open space. The area includes mostly single-family homes, but also some apartments and townhomes.

South Louisville – the houses south of Downtown and north of Dutch Creek open space, with Warembourg open space to the west. The area is almost entirely single-family homes, with a few duplexes and townhomes.

Coal Creek – the area along Coal Creek and the golf course, south of Cherry Street and east of Dahlia Street. The area consists of single-family homes, townhomes, and apartments.

PRINCIPLE NH-1. Planning Commission shall develop and City Council shall adopt a process for the creation, adoption, and implementation of Neighborhood Plans to define and preserve the unique special qualities of each neighborhood.

Policy NH-1.1: The preparation of Neighborhood Plans may be initiated by the City at the request of residents with concurrent support from City Council.

Policy NH-1.2: The residents, property owners, and business owners within the neighborhood shall be integrally involved in the creation of the plan, and will work with staff to complete the plans that are presented to City Council for adoption.

Policy NH-1.3: The Neighborhood Planning Areas shall include the residential areas, as identified in the accompanying map, as well as the local shops and businesses that serve the area and the public facilities such as parks and schools.

PRINCIPLE NH-2. The Neighborhood Plans shall include

definitive steps to be taken by the City, including but not limited to changes in zoning or other regulatory codes and improvements in physical and social infrastructure.

Policy NH-2.1: Topics to be addressed in Neighborhood Plans include:

- Addressing issues and concerns identified by residents.
- Transitions between the neighborhood and adjacent neighborhoods and commercial and industrial areas.
- Documenting existing neighborhood character and defining desired future character.
- Compatibility of existing zoning and PUDs with current and future development.
- The adequacy and appropriateness of the street network and street design.
- Facilities for pedestrians and cyclists, including sidewalks and multi-use paths.
- Availability of parking, both on street and off street.
- Other physical infrastructure needs, including water and sewer, power and gas, telephone, cable, and internet, and other civic amenities.
- Neighborhood safety, especially safe routes to school.
- Access to parks, open space, and recreation facilities.
- Provision of and access to social and cultural services.
- Access to public transportation.

PRINCIPLE NH-3. Neighborhood Plans shall be compatible with this Comprehensive Plan and other adopted goals and policies for the City.

Policy NH-3.1: Street designs shall comply with the City’s complete streets policy and allow appropriate amounts of traffic at appropriate speeds.

Policy NH-3.2: Streets shall form an interconnected network.

Policy NH-3.3: Transportation facilities shall provide mul-



timodal accessibility for users of all ages and abilities.

Policy NH-3.4: Diverse housing opportunities shall be available for residents of varying income levels.

Policy NH-3.5: The preservation of significant historic resources shall be encouraged.

Policy NH-3.6: Neighborhood Plans shall be compatible with the City's environmental, economic, and social sustainability.

Policy NH-3.7: Neighborhood Plans shall contribute to the sense of place and community that defines Louisville.

PRINCIPLE NH-4. The character and identity of existing residential neighborhoods should be maintained while allowing for evolution and reinvestment.

Policy NH-4.1: Housing in existing neighborhoods should be compatible with neighborhood plans.

Policy NH-4.2: Zoning designations should allow for reasonable reinvestment in existing houses while maintaining the character of the neighborhood and Louisville.

Policy NH-4.3: The voluntary preservation of historic structures should continue to be encouraged.

Policy NH-4.4: Mixed-income developments should be encouraged.

Policy NH-4.5: New developments should be compatible with existing neighborhoods and the Framework.

Policy NH-4.6: Community organizations and activities that encourage and provide housing rehabilitation and neighborhood improvements should be supported.

Policy NH-4.7: Housing should support vibrant retail and commercial centers that serve local residents.

PRINCIPLE NH-5. There should be a mix of housing types and pricing to meet changing economic, social,

and multi-generational needs of those who reside, and would like to reside, in Louisville.

Policy NH-5.1: Housing should meet the needs of seniors, empty-nesters, disabled, renters, first-time homebuyers and all others by ensuring a variety of housing types, prices, and styles are created and maintained.

Policy NH-5.2: The City should continue to work with Boulder County Housing Authority and others to ensure an adequate supply of affordable housing is available in Louisville.

Policy NH-5.3: Higher density housing should be located primarily in the centers and corridors of the Framework.

Policy NH-5.4: Potential measures to increase housing type and price diversity should be evaluated, including allowing accessory dwelling units in established neighborhoods only if the essential character of the neighborhood is can be preserved.

Policy NH-5.5: Regional changes to job and housing markets should continually be evaluated to address regional opportunities and constraints.

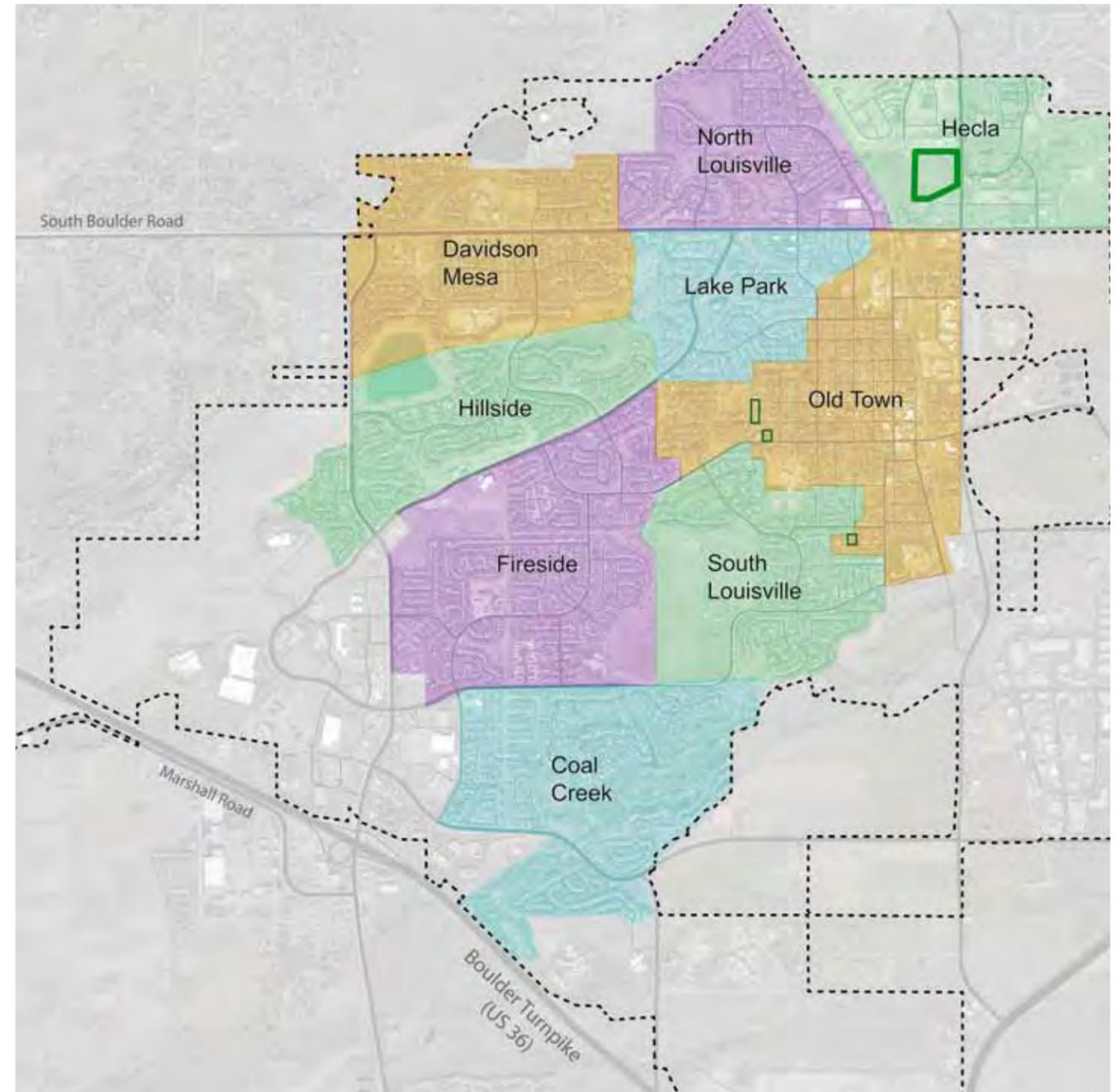
Policy NH-5.6: New housing should address defined gaps in the housing market that exist today and into the future.

Policy NH-5.7: The City should define standards for low income and affordable housing units, and consider reducing or waiving building permit and impact fees for all qualifying projects.

PRINCIPLE NH-6. The City should define City-wide goals for affordable and low-income housing through a public process.

Policy NH-6.1: The City should determine to what extent it would like to allow, encourage, or incentivize affordable and low-income housing.

Policy NH-6.2: The City should develop specific and achievable actions to meet the defined goals.



Neighborhood Planning Areas

The Framework

TRANSPORTATION, MOBILITY, & ACCESSIBILITY (TMA)

Transportation infrastructure is the foundation of city building. The form, function and character of Louisville's transportation infrastructure and adjoining land uses are intrinsically linked – starting with the first Boulder County roads, inter-urban rail between Denver and Boulder, to the Boulder Turnpike and its interchanges. Louisville's urban form and community character are dictated by its transportation systems. Streets provide the means and conveyance of circulation. Streets establish the block structure, organize land uses, and influence the architectural qualities of buildings. Streets are Louisville's most immediate and accessible public space, linking parks and schools to our neighborhoods.

Background / History

Since 1878, the City of Louisville's community form, character, and urban design have been influenced by its transportation investments. There are generally five stages of transportation investments and corresponding land use development, community growth and changes in Louisville's community character.

Stage 1: The Embryonic Phase of Development: The historic core of Louisville grew incrementally between the 1880s and the 1960s. The City's urban form was based on the local mining industry and was guided by the presence of the rail line and the "Kite Route", Denver's inter-urban railroad service to Boulder.

The pattern of Louisville's early development was very walkable and formed what is known today as Downtown and Old Town. Louisville's growth during this time period was primarily residential, organically expanding the original town's street grid. Commercial development stayed within Downtown. Local groceries, goods, and services were provided to the public from various stores in Downtown including Joe's and Ideal Markets. The form of Louisville adhered to an urban pattern of development which better accommodated pedestrians and established Louisville's cherished small town character.

Stage 2: Major Road Infrastructure is developed: Louisville's urban pattern changed dramatically in 1952 with the opening of the Boulder Turnpike and again in the 1960s when the toll for the Turnpike was removed and

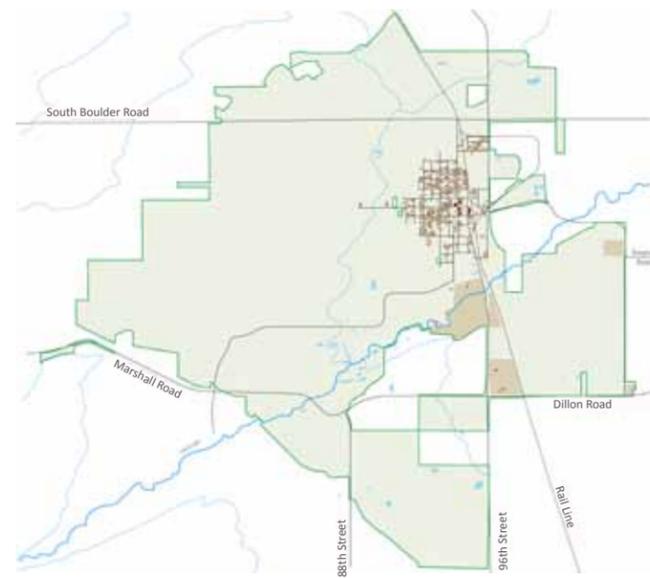
McCaslin Boulevard was first built. Between the 1960s and 1980s, Louisville experienced a significant period of growth and expansion, more than doubling the size of the City. Many new residential subdivisions were developed and the form of the City changed from urban, pedestrian-based design, to suburban, reflecting the mobility of the automobile.

The Boulder Turnpike (US 36) and South Boulder Road improvements increased the accessibility of Louisville to the Denver-Boulder region. In 1978, The Village Square Shopping Center was the first commercial development outside of Downtown and took advantage of the situation by providing a state-of-the-art grocery store capable of serving the Louisville households along with the regional customers commuting along South Boulder Road. As a result, retail services in Downtown were cannibalized by a better located regional competitor. Downtown retail eventually lost economic viability.

Stage 3: Retailing of the suburbs: Mass suburbanization of the Front Range, Boulder County, and Louisville followed the major transportation improvements between 1980 and 1995. HWY 42 was realigned; better connect-

ing Louisville to Broomfield and HWY 287. McCaslin Boulevard was widened with a reconfigured interchange at US 36. Additional retail uses were approved and constructed along McCaslin Boulevard (Sam's Club) and South Boulder Road. Louisville Plaza (King Soopers and K-Mart) was located strategically at the intersection of HWY 42 and South Boulder Road, where it was capable of serving both Louisville and Lafayette residents along with the regional customers traveling on the two arterials. Louisville became the regional retail center of east Boulder County.

Stage 4: Employment Growth: Regional Employment growth, between 1995 and 2005, followed the newly constructed households. Growth in the Centennial Valley, Colorado Technology Center, and Interlocken (Broomfield) altered traffic patterns. Boulder was no longer the primary employment center. New transportation investments, namely the 96th Street / HWY 42 connector (over the BNSF rail line) and the Northwest Parkway significantly altered north-south travel in Louisville and East Boulder County. The new connection acknowledged the emerging commuting traffic to and from Interlocken, and the US 36 Corridor.



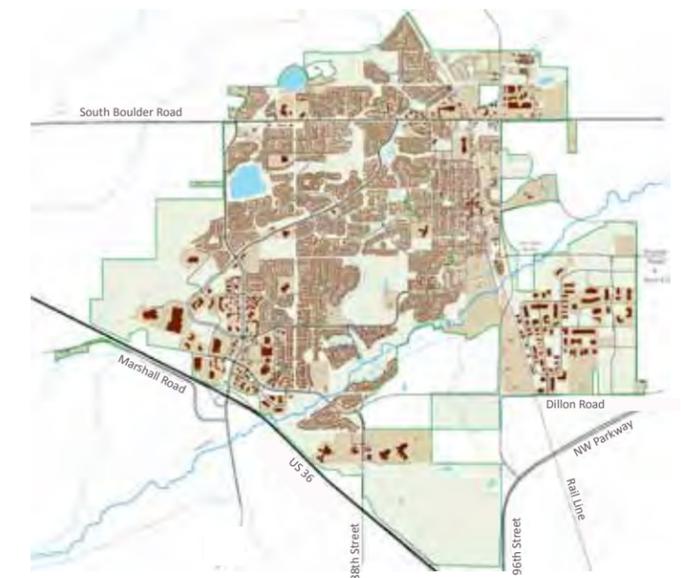
Louisville 1910



Louisville 1970



Louisville 1990



Louisville 2013



The Framework

New retailers emerged in the Louisville trade area along key regional commuting corridors, including Wal-Mart and King Soopers along US 287 and Target, Costco and Whole Foods at McCaslin Boulevard and US 36. The change in commuting patterns, the continued loss in market share, the generally built out nature of the residential areas in Louisville, and other factors have had their economic impacts on the regional retail structure of the City. Now nearly 40% of the City's sales tax revenues come from local groceries and food and beverage sales, not regional retail.

Stage 5: Maturity (What's Next?): As new development continues in neighboring jurisdictions, Louisville's vehicular traffic level of service (LOS) over the next 20 years will deteriorate from LOS C to LOS D regardless of what local development may occur in Louisville. More and more cars on Louisville roads will neither begin nor end their trips in the City. Currently, nearly 40% of all trips on Louisville streets are regional in nature without an origin or destination within Louisville. Future transportation investments in the City will be challenged to accommodate basic demands for regional traffic mobility while maintaining a LOS C and at the same time address livability and economic viability concerns internal to Louisville.

Louisville's physical expansion is near completion. Open space, City boundaries and inter-local agreements with neighboring jurisdictions limit where Louisville can annex and expand. All first generation development has been planned and entitled in Louisville except the 12 acre Alkonis property. Currently, 19% of Louisville's developable land remains vacant. However, this does not mean Louisville will not continue to evolve. Louisville's building stock will continue to age and will require improvements to remain economically viable.

Anticipated transportation projects influencing Louisville's form and character include: McCaslin Boulevard / US 36 Interchange (the Divergent Diamond Interchange and Bus Rapid Transit Station), HWY 42 redesign, and the Regional Transportation District's (RTD) Northwest Rail Corridor. Future Louisville transportation investments are prioritized toward transit and a more bal-

anced (multimodal) system. Correspondingly, Louisville growth trends for the future have shifted away from vehicular-scaled design toward a more pedestrian scaled design; from community expansion to community reinvestment, refurbishment, and redevelopment, as second and third generation development occurs in Louisville.

The construction of the managed lanes along US 36 and the Divergent Diamond Interchange at McCaslin Boulevard will introduce high capacity transit to Louisville. Current land patterns near the interchange and park-and-ride facility do not maximize the opportunities presented by the US 36 Bus Rapid Transit System.

The City's current transportation policies and regulations reflect a community focus on vehicular movement and not a more balanced multimodal transportation system. The policies support transportation actions which continue to expand street capacity and are not consistent with the realities of a community that is landlocked and experiencing second and third generation growth.

The City's current transportation regulations are aligned with regional mobility concerns and are designed to accommodate vehicular traffic, roadway capacity, and safety features for higher speeds. These policies are in direct conflict with the City's Vision Statement and many of the City's Core Community Values. Louisville's transportation priorities need to be aligned with multimodal transportation, roadway efficiency, property access, and safety features to create a balanced transportation system.

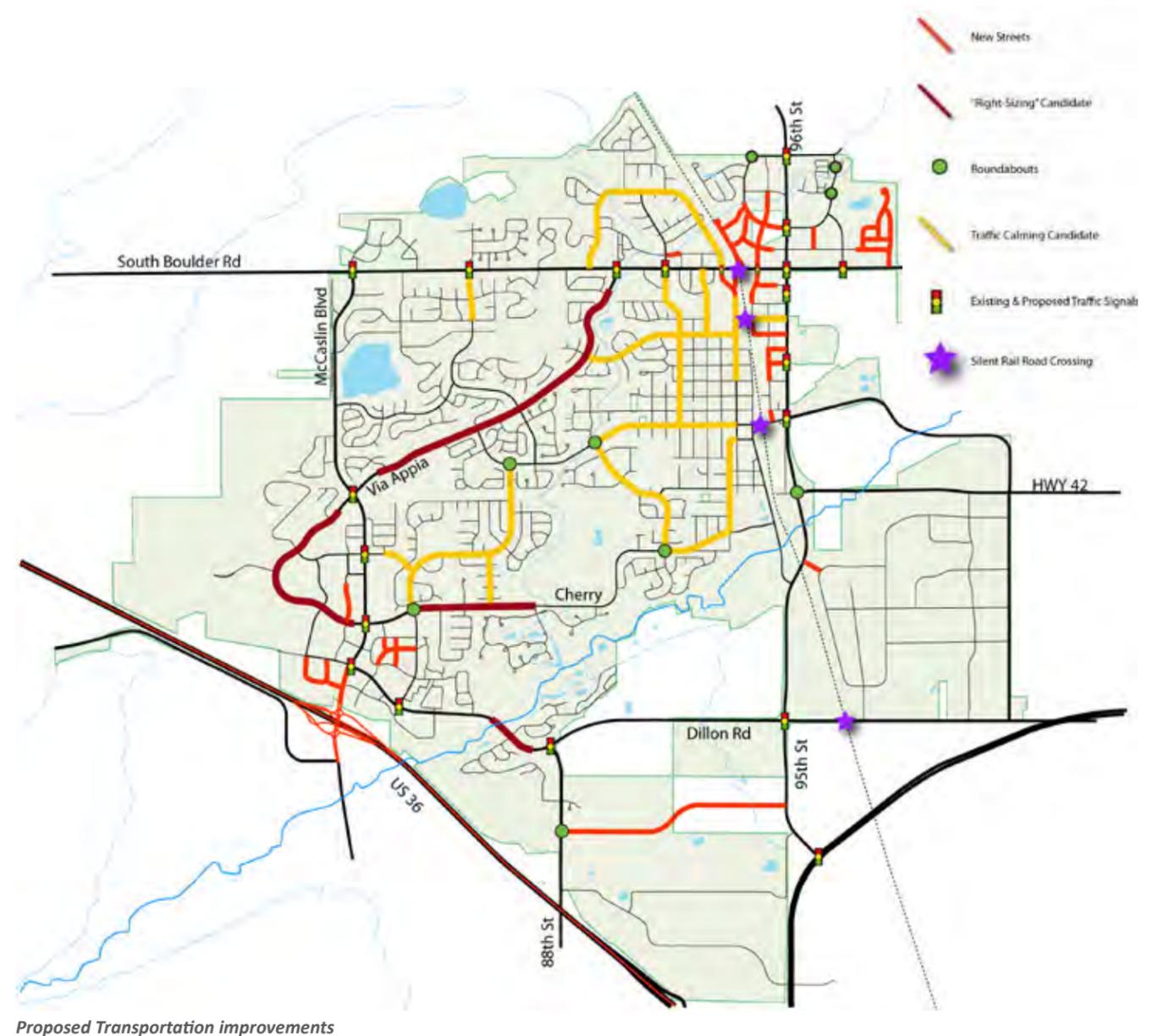
Analysis and Recommendations

Using the traffic model developed from the Denver Regional Council of Governments (DRCOG) 20 year forecasts, staff analyzed the transportation impacts associated with the endorsed development scenario. A goal of this Comprehensive Plan is to maintain vehicle LOS C unless to maintain LOS C it would be necessary to widen the street or make other capacity modifications in a way that would conflict with these desired small town transportation qualities:

- Pedestrians of all ages and abilities should be able to safely and comfortably walk along, or across a street, arterial corridor, or intersection, as well as wait for public transit.
- Bicyclists of all ages and abilities should be able to safely and comfortably ride along, or across a street, arterial corridor, or intersection.
- All streets, arterial corridors and intersections

are designed and function to be compatible with the City's desired character zone identified in the Framework.

- Streets, arterial corridors and intersections do not negatively affect the adjacent neighborhoods, historic assets, natural resources, or emergency responses.



The Framework

Regional cut-through traffic projected by the DRCOG’s model in the year 2035 causes traffic volumes in Louisville to exceed LOS C standards, regardless of what local development may occur in Louisville.

Based on these criteria, the majority of the City’s streets have the capacity to accommodate the 20 year forecasted traffic volumes for the preferred Framework at LOS C. However, several of the City’s arterials will operate at LOS D. It is important to note the anticipated regional cut-through traffic in the year 2035 causes traffic volumes on the arterials to exceed LOS C standards, regardless of any additional development in Louisville. Staff believes that the required vehicle capacity modifications necessary to maintain LOS C conflict with Louisville’s small town transportation quality expectations.

Several significant observations have emerged from the transportation analysis and community outreach efforts of the Comprehensive Plan when compared to the City’s Vision Statement and Core Community Values.

20 year Forecasts - With the approval of the Divergent Diamond Interchange at the McCaslin Boulevard and US 36 interchange, all Louisville streets are expected to meet the anticipated regional traffic forecasts and maintain an overall Level of Service (LOS) D.

PRINCIPLE TMA-1. The City of Louisville is committed to creating a context-sensitive, multimodal transportation and trail system which integrates land use, transportation, and recreational considerations and enables vehicles, transit, bicycles, and pedestrians of all ages and abilities to move in ways that contribute to the economic prosperity, public health and exceptional quality of life of Louisville

Policy TMA-1.1: New streets are needed as properties experience second-and third-generation redevelopment. The long-term transportation strategy for the City should focus on local street network enhancements balanced with neighborhood traffic calming, improving the connectivity and livability of the City’s arterial network.

Policy TMA-1.2: Corridor Master Plans and Preliminary Engineering Designs are needed for Hwy 42/96th Street; McCaslin Boulevard; South Boulder Road; and Dillon Road.

The purpose of these multimodal corridor plans is to outline a plan of action and specific strategies which ensure mobility and access for individuals within a broad range of ages and abilities on all City arterials by providing safe, convenient, and efficient multimodal transportation infrastructure. The Corridor Master Plans and 30% Designs shall meet existing and future needs, support the implementation of adopted community plans, and reflect and support the anticipated and expected development character of the areas they are traversing. Each Corridor Master Plan and 30% Design shall:

- Balance regional mobility and community livability,
- Develop partnerships to work cooperatively with all stakeholders served by the corridor;
- Provide a supportive transportation system that enables the Community’s Land Use Vision;
- Consider and balance the impacts upon natural, social and cultural resources;
- Provide safe and convenient facilities for a broad range of users and multiple modes of travel;
- Accommodate future regional transit plans;
- Promote regional trail connectivity;
- Design sustainable solutions; and,
- Develop creative, cost-effective and implementable solutions.

Policy TMA-1.3: The Louisville street network has excess capacity on a few of its arterial streets. Via Appia, Centennial Parkway, Cherry Street (between Dahlia and Heritage Park), and Dillon Road (between 88th Street and Club Circle) are candidates for “right sizing”. Right sizing candidates are roadways where the expected volume of traffic does not warrant the size of the street and the capacity of the street could be reduced and still meet expected traffic levels of service.

Benefits of right sizing include: traffic safety, pedestrian and bicycle accommodation, neighborhood continuity, and reduction in long-term maintenance costs to the City.

Challenges to right sizing include a reduction in mobility, a motorist’s ability to freely maneuver along a corridor, and if done improperly, slower emergency response times.

This recommendation simply identifies these four road segments as candidates for right sizing and recommends a more detailed corridor analysis be conducted to evaluate peak hour traffic conditions and specific pedestrian and bicycle utilization rates along with crash histories for each corridor. The timing of these corridor studies should be aligned with the City’s capital improvement program and reconstruction schedule of each roadway.

Policy TMA-1.4: Three roundabouts operate in the City of Louisville; one in the Steel Ranch Community and two in the North End Community. This Comprehensive Plan identifies the potential for a number of additional roundabouts throughout Louisville.

Roundabouts are preferred traffic control devices based on multiple opportunities to improve safety, operational efficiency, and community aesthetics. The intent of the candidate roundabout program in Louisville is to identify opportunities for more detailed analysis and the possibility of introducing roundabouts to promote a safer and more balanced transportation system. The timing of these roundabout studies and their possible implementation should be aligned with the City’s neighborhood planning initiatives and the reconstruction schedule in the Capital Improvement Program for candidate intersections. The benefits of roundabout intersections include:

- Traffic Safety
- Operational Performance
- Traffic Calming
- Pedestrian Safety
- Aesthetics
- Land Use Transitions

- Ongoing Operations and Maintenance
- Environmental Factors

Policy TMA-1.5: The transportation analysis identified traffic calming candidate streets throughout Louisville. A number of streets were identified as traffic calming candidates where residential homes “fronted” high volume roadways which carry more than reasonable neighborhood traffic volumes (1,000 vehicles per day). The purpose of this classification is not to reduce the capacity of the street, but to develop physical measures which reduce the speeds at which motorists are traveling along these streets in order to make them traverse the neighborhoods at safe speeds. Physical measures can include narrowing streets or changing street geometrics, among other things. This recommendation identifies these streets as candidates for traffic calming and recommends a more detailed neighborhood traffic plan be created to evaluate real conditions, rather than modeled conditions. The timing of these neighborhood traffic plans should be aligned with the City’s Capital Improvement Program and repaving schedule of each neighborhood, concurrent with the development of recommended Neighborhood Plans.

Policy TMA-1.6: Transit service to Louisville can and should be improved. Louisville supports the Regional Transportation District’s (RTD) FasTrack Program. Louisville’s land use strategies are tied to the implementation of the Bus Rapid Transit Corridor along US 36 and the implementation of the Northwest Rail Corridor with a commuter rail station serving Downtown Louisville.

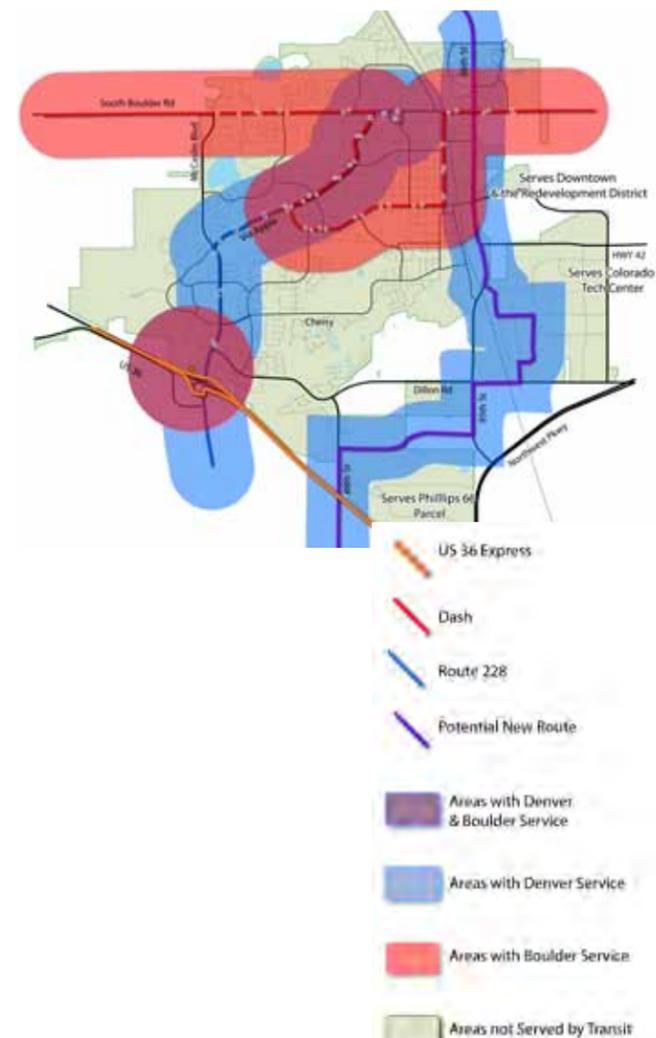
Additionally, there are two key components to local bus transit service within Louisville: *coverage* and *frequency*. Coverage refers to what portions of the City have local transit service. Frequency refers to how often the areas which have local transit service are served by transit. Louisville needs improvements in both aspects of RTD’s local transit service.

Currently, the entire southeastern portion of the City has no local transit service, including Avista Hospital, the Colorado Technology Center, Monarch Campus and the Phillips 66 property. All are critical employment areas



to the City and the entire metro region. The City should work with its neighboring jurisdictions and RTD to provide transit service along HWY 42/96th Street between Lafayette and Broomfield and introduce transit service to Avista Hospital, the Colorado Technology Center, the Monarch Campus, and, as development occurs, the Phillips 66 property.

Policy TMA-1.7: Walkability is a key ingredient to livable cities and neighborhoods. Great cities and neighborhoods all feature street level experiences that invite and stimulate pedestrian and bicycling activities. Walkability



Proposed Transit Service Improvements

enhances public safety, fosters personal interactions, improves public health, and increases economic vitality.

Louisville has an excellent recreation trail network and generally a high quality walking environment on its City streets. The intent of this Comprehensive Plan is to establish a transportation policy which raises the bar and better integrates the City's recreational trail network with City's street network. This interconnection will help create a more balanced transportation system that serves the entire City and is designed for all users of all ages and ability levels.

Policy TMA-1.8: Louisville has four at-grade crossings of the Burlington Northern Santa Fe (BNSF) Rail line. Three of the crossings: Main Street, Griffith Street and South Boulder Road are located within, or immediately adjacent to established residential neighborhoods. The fourth is located at Dillon Road near the Colorado Technology Center and proposed relocation of the St. Louis Catholic Church and School.

Federal Railroad Administration regulations require locomotive horns be sounded for 15-20 seconds before entering all public at-grade crossings, but not more than one-quarter mile in advance. This federal requirement preempts any state or local laws regarding the use of train horns at public crossings, unless certain improvements are made to the crossings.

The noise level of the horns negatively impacts the quality of life for residents and employees living and working near the rail corridor. It is a recommendation for the City of Louisville to work with its neighboring jurisdictions and the BNSF to create safe Federal Railroad Administration qualifying upgrades to all four rail crossings in the City. The timing of these investments was tied to FasTrack's Northwest Rail Corridor improvements. However, because of the uncertainty of the Northwest Rail Project, the City of Louisville should continue to advance implementation of the four crossings improvements necessary for a City-wide Quiet Zone in a strategy separate from the Northwest Rail Study.

PRINCIPLE TMA-2. The City of Louisville should develop and implement area-specific and City-wide transportation plans through an open and collaborative process to achieve the principles and policies outlined above.

Policy TMA-2.1: The Planning and Building Safety Department, Public Works Department and the Parks and Recreation Department shall collaboratively generate multimodal transportation plans for the residential neighborhoods and commercial areas of the City. At a minimum, this work shall include:

- a. Safe Routes to School
- b. Parking Management
- c. Pedestrian Circulation
- d. Bicycle Circulation
- e. Vehicular Circulation and Neighborhood Traffic Calming

Policy TMA-2.2: The Planning and Building Safety Department, Public Works Department and the Parks and Recreation Department shall collaboratively generate multimodal transportation corridor plans for HWY 42/96th Street; McCaslin Boulevard; South Boulder Road; and Dillon Road which shall include:

- a. Long-Term Land Use Vision and Urban Design Assessment
- b. Near-term and Long-term multimodal transportation performance evaluation
- c. Parking
- d. Transit Circulation and pedestrian access
- e. Pedestrian and bicycle crossings

Policy TMA-2.3: The Planning and Building Safety Department, Public Works Department and the Parks and Recreation Department shall generate a City-wide multimodal Transportation Master Plan that incorporates and consolidates the findings of each neighborhood, commercial area, and corridor plan. The plan shall include:

- a. Traffic Management and Traffic Calming Program
- b. Pedestrian Master Plan
- c. Bicycle Master Plan

- d. Transit Service Plan
- e. Primary Corridor Plan
- f. Transportation Demand Management

Policy TMA-2.4: The Departments of Planning and Building Safety, Public Works and Parks and Recreation will review and update the current design and construction standards including Resolution 9, Series 1994 (Roadway Construction and Design Standards); and LMC Chapter 12 – Streets and Sidewalks; Chapter 16.16 – Design Standards; and Chapter 17.14 – Mixed Use Zone District.

The review and update will ensure they reflect the best design standards and guidelines to provide flexibility for context-sensitive design. The roadways will be designed within the context of the neighborhood and corridors, recognizing all streets are different. The user, mobility, and land use needs will be balanced and consistent with the context sensitive multimodal transportation policy stated above.

The Framework

CULTURAL HERITAGE (CH)

The Cultural Heritage of Louisville consists of the built environment augmented by the stories of those who have lived here. The social history gives life and meaning to buildings that could otherwise not speak, and to the people associated with these structures that provide a tangible link to the past. The principles and policies below will ensure the Cultural Heritage of Louisville is protected and celebrated, in accordance with the Vision Statement and Core Community Values.

PRINCIPLE CH-1. The City should support and encourage the voluntary preservation of historic structures through its policies and actions.

Policy CH-1.1: The City should create a Preservation Master Plan to define a period of significance and identify resources and guide the City's Historic Preservation Program and the use of Historic Preservation Funds.

Policy CH-1.2: Area and Neighborhood Plans should incorporate historic preservation elements, where appropriate.

Policy CH-1.3: The City's Design Standards and Guidelines, particularly the Downtown Design Handbook, should be regularly evaluated and updated if necessary to incorporate best practices in historic preservation.

PRINCIPLE CH-2. Preservation efforts should contribute to a sustainable community.

Policy CH-2.1: The City should highlight preservation projects for their sustainable benefits, expand partnerships with sustainability organizations and programs, and include preservation considerations as it develops new sustainability policies and regulations.

Policy CH-2.2: The City should promote economic sustainability through historic preservation, including:

- Promote Louisville as a destination for visitors interested in cultural and historic attractions.
- Coordinate preservation efforts with other

programs designed to support local businesses. Promote adaptive reuse of historic properties. Work with economic development partners to include historic resources in redevelopment policies and economic development plans.

Policy CH-2.3: The City should promote environmental sustainability through historic preservation, including:

- Expand partnerships with sustainability organizations and programs .
- Create energy efficiency standards to fit historic resources.
- Highlight green building practices through various City programs.

Policy CH-2.4: The City should work with affordable housing organizations to utilize historic resources.

PRINCIPLE CH-3. City policies should encourage a livable community with a strong sense of history.

Policy CH-3.1: The City should evaluate the programmatic needs of the existing Museum to meet museum standards for allocation of resources by developing a Historical; Museum Campus Master Plan.

Policy CH-3.2: The City should consider creating a Historic Park where buildings slated for demolition can be moved and used as interpretive education to showcase Louisville's mining and agricultural heritage.

Policy CH-3.3: The City should develop procedures for identifying, preserving and protecting archaeological resources.

PRINCIPLE CH-4. The City should provide effective public outreach regarding Cultural Heritage issues.

Policy CH-4.1: The City should provide educational programs such as a rehabilitation skill-building program for local trade workers.

Policy CH-4.2: The City should stage regular outreach events with community organizations that may become

future partners in historic preservation.

Policy CH-4.3: The City should promote public awareness and understanding of the city's cultural and social history through programs such as an interactive map which provides hyperlinks to social histories of historic properties.

Policy CH-4.4: The City should encourage public participation in the preservation program.

Policy CH-4.5: The City should develop policies that provide clear guidance to the public for the treatment of locally designated historic resources.

Policy CH-4.6: The City should monitor the preservation program on an on-going basis to assure that it maintains a high level of performance and implement an annual program review that includes Certified Local Government programming.

PRINCIPLE CH-5. The City should ensure fiscally-sound best management practices for City historic resources.

Policy CH-5.1: The City should establish minimum maintenance requirements for landmark properties.

Policy CH 5.2: The City should ensure the policies and extents of the grant and demolition review programs match the community's goals with respect to aging structures outside the traditional historic core.

Policy CH-5.3: The City should create an effective and efficient process which guides the voluntary nomination and designation of historic resources and should establish a user-friendly system for the voluntary designation of individual landmarks and districts.

Policy CH-5.4: The City should work with past grant recipients to learn from past experiences.



The Framework



Miners on Acme Mine coal car, 1917



Mine rescuers, Acme Mine, circa 1920s



Federal troops camped near Louisville during mine strike violence, 1914



J.J. Steinbaugh's blacksmith shop, Front Street, circa 1890s



Louisville Grain Elevator, 1916



Catholic women preparing chicken dinners to raise money for St. Louis Church, early 1940s

The Framework

PARKS, RECREATION, OPEN SPACE, AND TRAILS (PROST)

Louisville’s open space and recreational amenities are among the most highly valued features of the City. These include the City’s recreation center, parks, fields, pools, trails, and open spaces as well as services such as classes, leagues, and senior services. These amenities contribute greatly to the quality of life in Louisville and steps should be taken to ensure they continue to do so.

In 2012, the City adopted a Parks, Recreation, Open Space, and Trails Master Plan (PROST Plan) that defined goals and objectives for Louisville’s parks and recreational amenities.

The PROST Plan made recommendations for maintaining and improving the high level of service enjoyed by Louisville residents and those recommendations, along with the entire PROST Plan, are hereby adopted by this Comprehensive Plan. In summary, the principles and policies identified in the PROST Plan and adopted here are as follows:

PRINCIPLE PROST-1. Improve trail connections to promote healthy and enjoyable alternative transportation and opportunities for active recreation

Policy PROST-1.1: Enhance the trail user experience through improved wayfinding and additional safety and comfort features.

Policy PROST-1.2: Improve safety, accessibility, and continuity for the trails within Louisville.

Policy PROST-1.3: Continue to provide connections from Louisville’s trails to regional trails and trails provided by neighboring agencies.

PRINCIPLE PROST-2. Maintain existing high levels of service for parks, open space, and trails as Louisville matures and evolves.

Policy PROST-2.1: Ensure that Levels of Service are appropriate and equitable now and in the future across the entire city so that all residents have equitable access to services.

PRINCIPLE PROST-3. Ensure a Service Delivery Model that remains responsive and relevant to City residents’ leisure behaviors, interests, and needs.

Policy PROST-3.1: Address emerging recreation and leisure trends and changing population characteristics including the aging population and current increasing demand for pre-school age programming.

Policy PROST-3.2: Respond to the 2008 citizen survey, the 2009 Comprehensive Plan, 2010 citizen survey that suggested teen activities/programming is a high unmet need.

PRINCIPLE PROST-4. Enhance programming capacity by exploring opportunities outside of City of Louisville facilities and services.

Policy PROST-4.1: Assess partnerships with local organizations and agencies to provide access to other spaces for programming.

PRINCIPLE PROST-5. Promote environmental stewardship and education.

Policy PROST-5.1: Continue to develop and incorporate environmental stewardship and education curricula to respond to community values.

PRINCIPLE PROST-6. Enhance communications and outreach efforts to increase efficiencies and effectiveness.

Policy PROST-6.1: Continue to develop and implement an enhanced, streamlined marketing, communications, and outreach plan in response to a need identified to increase efficiencies and create cost-savings.

PRINCIPLE PROST-7. Maximize intergovernmental agreements with Boulder Valley School District.

Policy PROST-7.1: Maximize partnerships with governmental agencies through adjustments to existing intergovernmental agreements (IGAs).

PRINCIPLE PROST-8. Evaluate and review the effectiveness and understanding of partnership agreements.

Policy PROST-8.1: Develop and implement a partnership policy to be used for the development of all new partnership agreements.

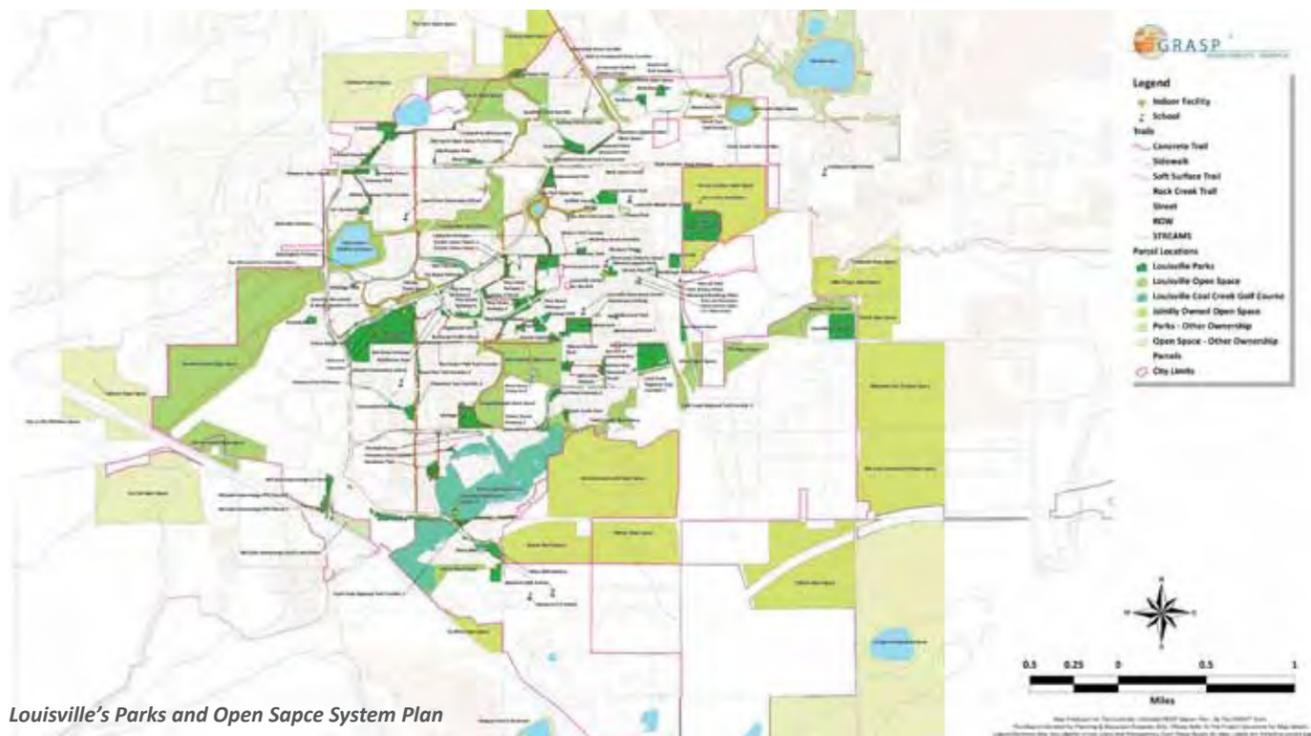
PRINCIPLE PROST-9. Define/Improve Park Maintenance Standards.

Policy PROST-9.1: Adopt general Park and Athletic Field maintenance standards.

PRINCIPLE PROST-10. Define/Improve Open Space Maintenance & Management Standards.

Facility	Quantity
Louisville Parks	306 acres
Louisville Coal Creek Golf Course	154 acres
Louisville Open Space	698 acres
Jointly Owned Open Space	1,060 acres
Open Space – Other Ownership	1,117 acres
Parks – Other Ownership	182 acres
Component	Quantity
Arboretum	1
Art Walks	2
Ball Diamonds	10
Basketball Courts	4
BMX Course	1
Bocce Courts	9
Community Gardens	1
Disc Golf Course	1
Dog Parks	2
Horseshoe Pits	4
In-Line Pk	1
Multi-Purpose Fields	11
Outdoor Fitness Court	2
Playgrounds	13
Picnic Shelters	16
Pool (outdoor)	1
Recreation Center	1
Skate Park	1
Tennis Courts	9
Volleyball Courts	2
Trail Type	Quantity (mile)
Louisville (Soft Surface)	13.23
Louisville (Paved)	15
Louisville (Sidewalks)	9.8
Other Ownership (All Surfaces)	30.4

Facility Inventory



Louisville's Parks and Open Sapce System Plan



Policy PROST-10.1: Create, review, and update Open Space Maintenance & Management Plans to provide consistency in management practices throughout the system.

PRINCIPLE PROST-11. Sustain the high level of service to which citizens have become accustomed.

Policy PROST-11.1: Identify and estimate the cost of future maintenance and operations (staffing, supplies, and services) for any newly-proposed parks, open space, trails, and indoor facilities to ensure that future development O & M is funded.

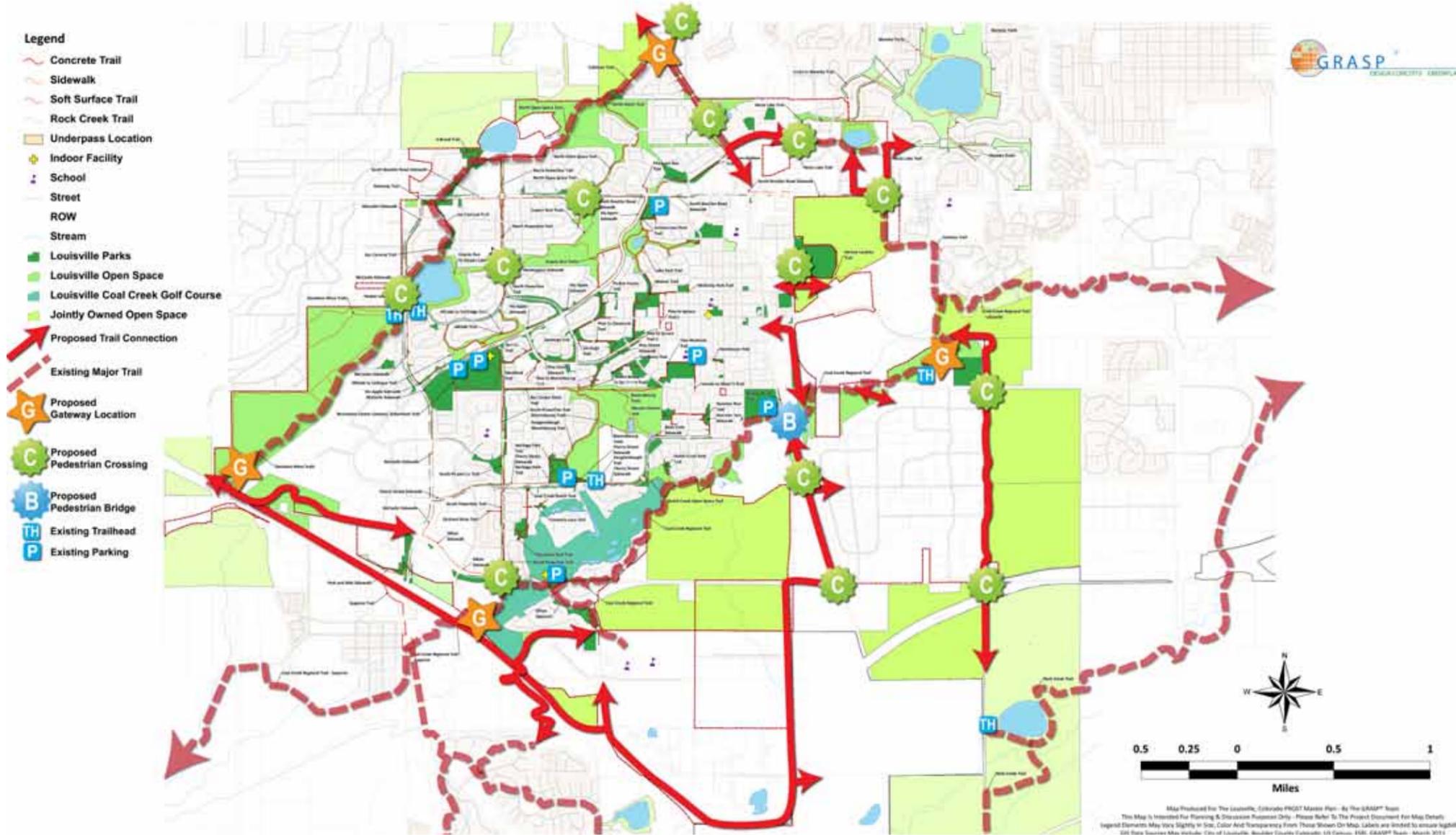
Policy PROST-11.2: Create and implement a cost recovery philosophy and policy.

PRINCIPLE PROST-12. Renovate, expand, and develop Facilities.

Policy PROST-12.1: Conduct Feasibility Studies to understand future capital and operational funding and revenue generation potential.

PRINCIPLE PROST-13. Implement 2011 Coal Creek Golf Course Strategic Plan.

Policy PROST-13.1: Improve overall maintenance and playability, and secure capital funding for repairs, replacement, and improvements.



Louisville's Regional Trails Improvement Plan

The Framework

MUNICIPAL INFRASTRUCTURE (MI)

Louisville’s municipal infrastructure includes roads (addressed in the Transportation section), raw water supply and treatment, sanitary sewers and wastewater treatment, and storm sewers and drainage. Other infrastructure not belonging to the City, but in which the City has a vital interest, include gas, electric, and telecommunications lines.

As described in the Existing Conditions chapter, raw water supply is secured for the City’s planned build out, but improvements may be needed to the water treatment plants to serve new commercial and industrial development. Improvements to the Wastewater Treatment Plant will be undertaken as needed. The City will also make improvements to the storm sewer system to improve water quality and mitigate the impacts of flooding.

PRINCIPLE MI-1. The City should provide adequate public facilities, water, sewer and related services to meet the demand of existing and future residents and commercial and industrial growth.

Policy MI-1.1: Through the use of water tap fees for new development, the City should ensure that water acquisitions will supply adequate water to meet the needs of the community.

Policy MI-1.2: The City’s water quality standards and treatment practices should continue to maintain a high level of health protection for its residents.

Policy MI-1.3: The City should ensure that its storm drainage and wastewater treatment system is adequate to meet the demands of existing and planned development.

Policy MI-1.4: The City should continue to require the dedication of water rights or the payment of a water resource fee in lieu of dedication from newly annexed property.

PRINCIPLE MI-2. The City should take measures to en-

sure development fees provide adequate improvements necessary to serve new development.

Policy MI-2.1: The City should develop and utilize long-range plans for determining infrastructure requirements to meet the demand of planned growth.

Policy MI-2.2: The City should continue to assess impact fees on new development requiring development to pay its calculated share of new public facilities and infrastructure.

Policy MI-2.3: The City should coordinate with other service providers on development requests to ensure that necessary services not provided by the City should be made available for planned new development and redevelopment.

Policy MI-2.4: Development patterns should be planned with the consideration of the alignment and location of existing and future public facilities and infrastructure.

Policy MI-2.5: Future development and redevelopment should be coordinated with all utilities to ensure that development is buffered to the full extent necessary from the existing locations, as well as future expansion of high pressure natural gas pipeline systems and overhead transmission lines and associated infrastructure.

Policy MI-2.6: All new developments should dedicate to the City required right-of-ways and install designated public improvements per approved design standards.

Principle MI-3. The City should continue to make improvements to reduce the impacts of potential flooding on property owners.

Policy MI-3.1: The City should continue to participate in the Federal Emergency Management Agency (FEMA) Community Rating System to decrease the flood danger and reduce the cost of flood insurance for property owners.

Policy MI-3.2: The City should work with FEMA and the Urban Drainage and Flood Control District to define the

floodplain in the Empire Road area and consider pursuing a letter of map change in partnership with private property owners to remove the area from the floodplain.

Policy MI-3.3: The City should support appropriate requests for letters of map change brought by private property owners.

Policy MI-3.4: The City should continue to follow the Louisville/Boulder County Outfall System Plan and work with neighboring jurisdictions, partner agencies, and property owners to make improvements to the storm sewer system, particularly with respect to Downtown Louisville.

Policy MI-3.5: The City should continue to work with and support property owners and developers on maintaining existing and new drainageways to maintain drainage capacity.

PRINCIPLE MI-4. The City should take steps to ensure an adequate long-term water supply for the City in the face of droughts and changes to the regional climate.

Policy MI-4.1: The City should complete a water conservation plan that will encompass Comprehensive Plan updates and climate impacts with up-to-date raw water needs.

Policy MI-4.2: The City should adopt revised Drought Management Practices, including changing the drought surcharge from mandatory to discretionary and adding discussion surrounding water restrictions as a tool.

Policy MI-4.3: The City should continue to work with other area municipalities on water supply and delivery strategies and communications.

ENERGY (E)

The City of Louisville recognizes that protection and conservation of its local and regional environmental resources is important to City residents. Residential and commercial buildings account for nearly half of the elec-

tricity and natural gas consumed in Colorado. Building codes and policy initiatives play a critical role in ensuring that energy efficiency technologies are supported in the marketplace, and provide multiple benefits to homeowners, renters, building owners and tenants, and society at large through reduced energy demand, energy cost savings, and reduced carbon emissions. Policies and procedures should be examined with input from all affected parties to lessen energy consumption, waste generation, water, air, and light pollution impacts to our community. The City should also continue strive to promote wise use of energy resources in its own municipal operations.

PRINCIPLE E-1. The City should efficiently use energy resources and continually strive to conserve energy where practical.

Policy E-1.1: The City should pursue cost effective measures to reduce its dependency on non-renewable energy sources by pursuing the use of renewable energy sources for residents and businesses as well as for its municipal operations.

Policy E-1.2: The City should encourage building designs that maximize the use of natural light and thus diminish the need for energy consuming supplemental lighting.

Policy E-1.3: The City should encourage the use of energy-efficient lighting, appliances, and other devices in new development, redevelopment and in municipal operations.

Policy E-1.4: The City should encourage the use of landscaping that assists energy savings by the use of buffers and admittance of solar access in the winter and shade in the summer.

Policy E-1.5: The City should encourage renewable forms of energy in new development and redevelopment.

Policy E-1.6: The City should encourage and pursue opportunities for wind or solar energy for on-farm electrical needs on Parks & Recreation and Open Space-



owned agricultural land.

PRINCIPLE E-2. The City should increase its internal purchase of renewable energy and expand opportunities for renewable energy where practical.

PRINCIPLE E-3. The City should promote increased energy efficiency in residential and commercial properties.

Policy E-3.1: Increase outreach and education efforts with local energy efficiency contractors, designers, home and business owners.

Policy E-3.2: Work with partner agencies to offer free and subsidized weatherization services to qualifying residents.

Policy E-3.3: Strive to remain current with the following model building codes from the International Code Council: International Energy Conservation Code, International Green Construction Code.

Policy E-3.4: The City should establish community-wide energy consumption baseline statistics to inform future conversations regarding City energy policies.

COMMUNITY SERVICES (CS)

Community services include schools, libraries, police and fire services, solid waste / recycling / composting services, and health services. While not all of these services are provided directly by the City of Louisville, the Vision Statement and Core Community Values have indicated that they are very important. These principles and policies will ensure that the City supports community services to the fullest extent possible.

Schools

The City of Louisville is served by three elementary schools, the Louisville Middle School, and the K-12 Monarch campus. The following table shows 2012 enrollments and projected enrollments based on build-out of the Framework Plan. Louisville enrollment has been broken out from total enrollment to reflect what portion of the total enrollment is made up of Louisville students.

As the Boulder Valley School District (BVSD) practices an open enrollment policy, the enrollment numbers reflect that approximately 20% to 30% of the total enrollment at the elementary level are comprised of students that open enroll from outside the City of Louisville.

School	October 1 Count 2012				Future % Louisville	Capacity Surplus (Deficit)
	Program Capacity	Louisville Enrollment*	Resident Students*	Total Enrollment		
Fireside El.	576	372	372	449	82.9%	372
Coal Creek El.	555	406	483	453	89.6%	415
Louisville El.	603	481	556	554	86.8%	600
Monarch K-5	427	403	367	403	100.0%	493
Louisville M.S.	691	449	490	632	71.0%	512
Monarch 6-8	506	412	346	412	100.0%	488
Monarch H.S.	1833	1293	1971	1576	82.0%	1475
Total	5191	3816	4585	4479	85.2%	4407

* includes students open-enrolled from other Louisville schools

**number of students residing in the attendance area

note: high school includes students from additional feeder schools in Superior

Source: Boulder Valley School District

* Note: Louisville enrollment for Monarch was not determined as the attendance area includes Superior and Louisville.

** Future surplus/deficit based on 2007-2008 program capacity with future enrollment potential based on the Framework Plan.

Louisville public schools reflect a strong connection to the neighborhoods within their respective attendance area and enjoy a high level of parent involvement. As education is a defining attribute of the community, the City will continue to cooperate with BVSD to maintain an excellent school system.

PRINCIPLE CS-1. City of Louisville should actively coordinate land use efforts with the Boulder Valley School District and promote excellence in education.

Policy CS-1.1: The City should ensure that land use and housing policies of the City complement the mission statement of the BVSD.

Policy CS-1.2: The City should promote joint planning activities with BVSD to ensure that new facilities are appropriately located, are provided in a timely manner and meet the needs of extracurricular and community use.

Policy CS-1.3: The City should continue to work closely with the BVSD to provide program capacity to meet Louisville and District needs.

Policy CS-1.4: The City should continue to refer appro-

appropriate proposed residential development applications to the Boulder Valley School District for review and comment and consider the estimated student yield of new residential neighborhoods during the development review process.

Policy CS-1.5: The City should encourage BVSD and school principals to become involved in the planning process as the City continues to develop and redevelop in areas that will affect the school district.

Policy CS-1.6: The City should encourage new developments to provide Safe Routes to School to ensure the safety of Louisville students as they commute to and from school.

Library Services

PRINCIPLE CS-2. Excellence in education and access to educational opportunities should be a key feature of life in Louisville for residents of all ages.

Policy CS-2.1: Library facilities, services, and programs should meet the existing and future library needs of all Louisville residents. The Library should:

- Provide a community gathering place for learning, entertainment, and the exchange of ideas for residents of all ages;
- Provide its citizens with exemplary service, quality print and non-print collections, and access to electronic resources using the latest in proven Technology tools;
- Support the acquisition of pre-literacy skills for Louisville's youngest citizens and encourage literacy for all residents in the digital age;
- Support and encourage an atmosphere of intellectual curiosity and continuing education within the Louisville community through the ongoing enhancement and promotion of the Library's services and programs;
- Strengthen Louisville's longstanding tradition of educational excellence through continued collaboration with local schools and other educational agencies.

Policy CS-2.2: Management should be consistent with the Library's policies as adopted by the Board of Trustees, the Library's goals and objectives as delineated in its Strategic Plan, and the City's Home Rule Charter and Louisville Municipal Code.

Policy CS-2.3: The City should collaborate with other area municipalities so the Library can pursue consortial agreements to ensure cost-effective services and operation.

Police and Fire Services

PRINCIPLE CS-3. The City should promote the health and safety of the community.

Policy CS-3.1: The City should remain committed to maintaining its police force level of service to ensure the safety of the community.

Policy CS-3.2: The City should support crime prevention through environmental design.

Policy CS-3.3: The City should continue to support a Fire Protection District to ensure preservation of life and property through fire prevention, fire suppression, hazardous materials response and emergency medical services support. The City, together with the Louisville Fire Protection District, should encourage the use and cost effectiveness of fire sprinklers in protecting life and property.

Health Services

Policy CS-3.4: The City should coordinate with the Boulder County Health Department and Avista Hospital to ensure that public health services are available to residents of all ages.

Policy CS-3.5: The City should encourage programs or projects that promote healthy eating and active living.

Solid Waste Services

PRINCIPLE CS-4. Promote and implement waste-reduction and recycling programs.

Policy CS-4.1: The City should work with governmental,



The Framework

private and not-for-profit agencies to develop regional approaches to solid waste reduction and management.

Policy CS-4.2: The City should continue its efforts to reduce waste generation from its municipal operations and explore methods for additional reduction. The City should consider the purchase of supplies with recycled content when feasible.

Policy CS-4.3: In its own operations, the City should consider the environmental and economic costs, risks, benefits and impact from a life-cycle perspective when making, planning, contracting, purchasing and operating decisions.

Policy CS-4.4: The City should continue to promote public education related to the value, methods and techniques of recycling, resource recovery and waste reduction.

Policy CS-4.5: The City should promote diversion from the landfill of construction and demolition refuse.

Civic Events

PRINCIPLE CS-5: The City should promote citywide community and civic events

Policy CS-5.1: The City should continue to support events such as live music, fairs, parades, ice skating, etc. These events are important to the economic and social welfare of our community.

Policy CS-5.2: The City should promote community activities in other areas of the city, such as McCaslin Urban Center and Highway 42/South Boulder Road Urban Center. Activities in these areas cohesively connects them with the rest of the community.

Arts and Culture

PRINCIPLE CS-6: The City promotes the public and private advancement of the arts and culture to strengthen the quality of life and small town character of Louisville by encouraging the development of a City-wide Arts and Cultural Master Plan aimed at integrating the arts, culture and humanities with urban design, economic

development, education and other community development initiatives.

Policy CS-6.1: The Community-wide Arts and Culture Master Plan should include the following components:

- Economic Vitality and the Arts - Preserve and share the Louisville's unique setting, character, history, arts and culture by identifying partnerships, resources and attractions that respect the needs and desires of Louisville residents.
- Facility Evaluation and Development - Respond to the growing desire for cultural facilities by identifying short and long-term facility needs and priorities, and recommending public and private methods to meet those needs.
- Public Art and Community Design - Create a stimulating visual environment through the public and private artworks programs, and create a greater understanding and appreciation of art and artists through community dialogue, education and involvement.
- History and Heritage - Work with the Louisville Historical Commission to develop a greater understanding of our heritage and assess the City's facilities in which that history is preserved, interpreted, and shared.
- Humanities - Foster the spirit of community in which the richness of human experience is explored and nurtured through ongoing analysis and exchange of ideas about the relation to self, others and the natural world.
- Local Artists - Encourage local support for a creative and economic environment that allows artists to continue to live and work in and for the community, and for themselves.
- Marketing and Communications - Identify marketing and communication systems to promote the arts and culture through public dialogue, media and education.
- Art and Culture Education - Demonstrate commitment to quality arts and culture education and lifelong learning by advocating for inclusion of the arts and culture in our schools and in community settings.

- City Board and Commission Support - Advance the community's understanding of local zoology and botany with the Horticulture and Forestry Advisory Board.
- Financial Resources - Encourage the fiscal soundness of Louisville Cultural Council by evaluating and recommending improvements to its capacity to maintain effective public, private and earned income funding.

Policy CS-6.2: The appropriate City Departments and the Louisville Cultural Council (LCC), as the principal advisory board to the Louisville City Council related to the arts, shall serve as the primary voice for the development of the Arts and Culture Master Plan.

Policy CS-6.3: The appropriate City Departments and the LCC shall provide an inclusive public forum for discussion of issues and ideas affecting the development of a City-wide Arts and Culture Master Plan.

ECONOMIC DEVELOPMENT (ED) AND FISCAL HEALTH (FH)

Economic Development

Given Louisville's central location along the US 36 Corridor, between Broomfield and Boulder, the community is strategically located to capture its share of the region's business growth. The level of investment that actually occurs within the community will correlate to the City's commitment to its Vision and Core Community Values as expressed in this Comprehensive Plan Update, supportive policies, creative financial solutions and removal of barriers. Barriers to the development of the concepts presented within this document fall within five principal categories – organizational, physical, market, regulatory and financial. Strategies for the removal of these barriers will be critical to the ultimate implementation of the Comprehensive Plan.

Encouraging strategic investment in an environment that contains an appropriate mix of land uses and creates a unique sense of place is the central approach for targeting investment in key areas within the City. This premise assumes concentrating resources in the key

commercial, retail, and employment centers in the City that will have a positive economic ripple effect throughout the entire City. In this way, the City of Louisville, as a public partner, can effectively leverage public investment efforts to overcome barriers and achieve desired outcomes. The economic future of the City will depend on how effectively these leveraged efforts are implemented.

It is also important to note the key role residential development plays in attracting new businesses and retaining existing businesses in the community. A diverse housing base is a prominent criterion businesses use to evaluate a community. The ability of a wide range of employees to live and work in close proximity increases business efficiency, provides a higher quality of life for employees, and discourages companies to relocate their business outside of the community. This relationship between residential diversity, availability and business growth should continue to be fostered in future economic development efforts.

PRINCIPLE ED-1. The City should retain and expand existing businesses and create an environment where new businesses can grow.

Policy ED-1.1: The City should work to maintain a business friendly environment, where services to new and existing businesses are delivered in a timely and efficient manner.

Policy ED-1.2: The City should encourage employment centers to provide goods and services which will bring revenue from outside of the community into the community.

Policy ED-1.3: The City should focus on primary job creation that provides job diversity, employment opportunities and increased revenue for Louisville.

Policy ED-1.4: The City should focus on efforts that will encourage existing businesses to expand and develop in Louisville.

Policy ED-1.5: The City should review requests for busi-



ness assistance based upon criteria under the Business Assistance Program.

Policy ED-1.6: The City should continue its business retention program as a means of reaching out to businesses in Louisville to specifically understand the needs of the business community.

PRINCIPLE ED-2. The City should direct growth in an economically responsible way in order to maintain high quality amenities and high service levels for residents.

Policy ED-2.1: The City should strive to achieve complementary land uses that promote an economically healthy community.

Policy ED-2.2: The City should work to maintain and improve community assets such as the educational, housing, recreational, retail and cultural opportunities that encourage local businesses to remain and expand in Louisville.

PRINCIPLE ED-3. The City should be responsive to market opportunities as they occur, and maintain and enhance the City’s competitive position to attract development that adheres to the Community Vision.

Policy ED-3.1: The City should actively compete for quality economic development opportunities.

Policy ED-3.2: The City should consider strategic public investments and partnerships to encourage, promote and recruit private investment that responds to the Community Vision and Core Community Values.

Policy ED-3.3: The City should maintain a protocol for responding, from a single point of contact, to real estate, economic and demographic information requests.

Policy ED-3.4: The City should support Chamber of Commerce and the Downtown Business Association activities directed toward economic development both financially and through staff and support services.

Policy ED-3.5: The City should fund and manage a

successful range of economic development services to respond to business development inquiries.

Policy ED-3.6: The City should support redevelopment efforts that bring diversity and income generation to aging and distressed areas within Louisville.

PRINCIPLE ED-4. The City should cooperate with surrounding communities to explore opportunities for regional solutions to economic development challenges.

Policy ED-4.1: The City should participate with public and private entities that further economic development on a regional and state level.

Policy ED-4.2: The City should evaluate the benefits of forming a regional partnership within Boulder County as a vehicle to pool resources and encourage cooperation.

Policy ED-4.3: The City should participate in regional activities that promote Louisville.

Policy ED-4.4: The City should participate in bringing state and local programs designed to encourage business growth to businesses in Louisville.

PRINCIPLE ED-5. The City should work to support and maintain the historic and cultural attributes of the Downtown Business District.

Policy ED-5.1: The City should periodically review the Downtown Framework Plan and the Downtown Design Handbook to ensure that the guidelines are applied in a manner that encourages the revitalization of existing structures, historic preservation where applicable, application of appropriate guidelines in the construction of new structures and expansion of existing buildings.

Policy ED-5.2: The City should support and promote the revitalization of existing structures that maintain the character of downtown, while providing a diverse business base.

Policy ED-5.3: The City should support a mix of uses which bring new revenues to the downtown area.

Policy ED-5.4: The City should support and promote efforts that showcase both development opportunity and quality of life in Louisville, such as the “Street Faire,” parades, the “Taste of Louisville,” shopping opportunities and other community events.

Fiscal Health

A community’s fiscal environment can be described as a “three-legged” stool, balancing nonresidential development, municipal services and amenities and residential development. The first “leg” of the stool – nonresidential development - provides the vast majority of revenues to support municipal services. Municipal services and amenities, the second “leg,” attract residents and maintain their quality of life. The third “leg” – residential development – generates the spending and employees to support nonresidential business. Fiscal sustainability of the community relies on this type of balance, which must continually be maintained, even through changing economic cycles.

Over the past two decades, the City of Louisville has been at the forefront of Boulder County communities in maintaining its fiscal health. The City recognized early on the need for revenue-generating, nonresidential development to offset the costs of providing a high level of service and community amenities to its residents. To this end, the City continues to make significant public investments to attract new businesses to retail, office and industrial developments. In 2011, a use tax was approved by voters to strengthen the tax base and offset the swings experienced from a declining retail market. The City continues to attract high-quality residential development to support business growth.

During the national recession between 2008 and 2010, sales tax revenues in Louisville declined by 6%, as large format retailers in the McCaslin and South Boulder Road Corridors have closed down.

The City’s continued fiscal challenge will be balancing its revenues and expenditures while maintaining the municipal services that its residents expect. This fiscal balance has to occur recognizing that Louisville is land

locked. Successful redevelopment and revitalization will be keys to the City’s future. However, if the desired land use pattern does not support the desired municipal level of service under the existing revenue structure, a change in the revenue structure may be required, similar to the adoption of the use tax.

Certain retail areas of the City of Louisville are depended upon to produce revenues that exceed the cost associated with providing services to them. These areas are the key producers of net positive revenues which in turn are used to provide City-wide services. The majority of the City’s sales tax revenue comes from a few key activity centers (see below). The land use mix in each of these key areas must provide positive fiscal returns to the City, and certain areas must provide exceedingly strong fiscal benefits to the City under the current City tax structure.

- 1. The McCaslin Boulevard and US Highway 36 Interchange** - The McCaslin Boulevard and US Highway 36 Interchange Area generates approximately 33% percent of the City of Louisville’s sales tax revenue. These revenues are due in large part to regional retail operations located in close proximity to McCaslin Boulevard and the Highway 36 interchange. Future land use scenarios should ensure that this area continues to provide strong fiscal benefits to the City by capitalizing on improvements in infrastructure and adapting to market trends.
- 2. The South Boulder Road and Highway 42 area** - In contrast to McCaslin Boulevard’s Regional Retailers, the South Boulder Road and Highway 42 intersection is a Community Retail center serving a smaller trade area. Although sales tax revenue generated in this area is not as high as the McCaslin Boulevard area, the revenue generated in this area is crucial to the continued fiscal success of the City, and the future land use mix in this area should produce positive fiscal returns to the City.
- 3. Downtown Louisville** - Currently, about 18% percent of retail sales tax revenue in the City of Louisville comes from food and beverage sales. A large percentage of this food and beverage sales tax is generated by the restaurants and bars in Downtown Louisville. Future



The Framework

land use plans for the Downtown area must continue to provide strong positive benefits to the City by supporting the continued success of the restaurant sector while enabling a diversification into other retail sectors.

PRINCIPLE FH-1. The City should maintain fiscal balance through effective land use decisions, focused economic development efforts, encouraging a mix of residential unit types and pricing, and strategic public investments, all consistent with the community's desire for high-quality services and amenities.

Policy FH-1.1: Fiscal impacts of proposed annexation, development or redevelopment should be evaluated to determine both operational and capital cost impacts upon all service departments of the City. The City should develop and utilize a marginal cost model which assigns incremental costs to new development based on a desired level of services.

Policy FH-1.2: Annexation, development or redevelopment must have a positive impact on the City's fiscal and economic position, especially in historically retail areas. The impact of new development should be evaluated by its effect on City revenue generation, service provision, capital investments, job creation, catalytic opportunities, and quality of life.

Policy FH-1.3: Fees associated with development should be continually reviewed, and adjusted, as required to cover the cost of impacts upon the City.

Policy FH-1.4: The City should coordinate the need for capital improvements, the need to expand operating programs and services, and the need for revenue prior to the approval of new annexations and rezonings.

Policy FH-1.5: With respect to infrastructure investment for new development, the City should carefully evaluate the use of alternative financing mechanisms, including special districts and regional authorities.

Policy FH-1.6: The City's fiscal structure should consistently be evaluated to ensure it supports the desired land use pattern and community levels of service.

Policy Alignment & Implementation

The Comprehensive Plan is a vision document which sets goals and principles to help guide policy initiatives and future developments within the City of Louisville. As stated in the Introduction, the Comprehensive Plan is an advisory document that provides a conceptual framework to advance the Community's Vision Statement and Core Values. It is not a regulatory document, nor does it have the force of law.

Through the 18 month planning process, a clear Vision Statement with supporting Core Values emerged based on thoughtful community input and the premise of ensuring a vibrant, economically successful, and fiscally healthy City which adds to the quality of life of existing and future citizens.

The City of Louisville must take on the task of implementing realistic strategies to translate the Community's Vision Statement and Core Values into reality. The implementation strategy outlined below will be developed through a coordinated effort of updating the Louisville Municipal Code and funding specific initiatives through the City's annual budgeting process. This effort will continue to involve all of Louisville's stakeholder groups including but not limited to residents, property owners, business operators, Boards and Commissions of the City, and the City Council.

This Comprehensive Plan was developed with a broad, long range view for the future of the City. Successfully executing specific implementation strategies will require a focused effort drawing on the expertise of the citizenry, property and business owners, and Boards and Commissions of the City.

Since the Comprehensive Plan does not have the force of law, the City relies on other regulatory measures to implement the plan. The information presented here is designed to provide a range of actions for consideration and sound decision-making. No one step will effectively achieve the Comprehensive Plan's Vision. Rather, implementation will be dependent on a series of actions designed to capitalize on market opportunities and overcome barriers with active community involvement and coordinated regulatory updates. Key to the

successful implementation of the Comprehensive Plan will be the continued identification of actions and an implementation approach tailored to the unique issues identified in the Framework and supporting Principles and Policies. The following is an overview of the various types of strategies that will be used to implement the Vision Statement, Core Community Values, and Framework of this Comprehensive Plan.

Small Area Plans and Neighborhood Plans

The Comprehensive Plan takes a broad and expansive look at the City and cannot focus on the specific details or development rights of a particular property or parcel. For example, the Comprehensive Plan may state that increased pedestrian connectivity is desired in a certain area of the City, but it does not elaborate on the width of a sidewalk, or the exact location of a street crossing. Similarly, the Comprehensive Plan's Framework may describe development goals of a specific character zone within the City, but it cannot identify a specific development performance measure for a specific property.

To attain the level of detail necessary to advance the Community's vision outlined in the Framework, specific small area plans, or neighborhood plans, are needed to ensure the expectations outlined in the Comprehensive Plan are met on individual properties. These area planning efforts can focus in on certain portions of the City, and examine the specific property information necessary to implement the vision and specific principles and policies outlined in the Comprehensive Plan. Small Area Plans and Neighborhood Plans, both must be used to help implement the Vision Statement, Core Community Values and Framework.

Louisville Municipal Code Amendments

The Louisville Municipal Code (LMC) is the primary regulatory tool the City has at its disposal to implement the principles and policies outlined in the Comprehensive Plan's Framework. The LMC has the force of law and is the regulatory tool utilized to dictate how the City will conduct business with regards to Revenue and Finance, Parks and Open Space, Public Safety, and Land Use, to name only a few areas. Chapters 15 (Buildings), 16 (Subdivisions) and 17 (Zoning) of the LMC regulate the

use, character, and form of the built environment in the City. Many of the principles and policies outlined in the Framework require city ordinances adopted through properly noticed public hearings to modify or create additional sections to Chapters 15, 16 and 17 of the LMC.

The City's Operating and Capital Improvement Budget

Many of the principles and policies outlined in the Framework Plan require the dedication of financial resources to be successfully implemented. The City of Louisville updates its budget annually, and it is during this budgeting process that new funding can be dedicated to implement the Comprehensive Plan's Vision Statement, Core Community Values and Framework.

The City's operating budget includes funds for the day-to-day functioning of the City and the ongoing provision of services to the citizenry. Operating budget items include things like snow removal, police services, and operation of the recreational center. To implement the Framework, new funds may need to be dedicated or reallocated through the annual operating budget process.

The Capital Improvement Program (CIP) is dedicated to the construction or acquisition of new assets. Examples of items found in the CIP include the construction of new bridges and roads, or the acquisition of new maintenance equipment. Implementation of the Framework may require the construction of new City funded infrastructure including, for example, trails, utility lines, or roads. The budgeting process will be utilized to identify Operating and Capital Improvement Budget allocations which will assist in the implementation of the Comprehensive Plan.

The Zoning Map

The Framework is a map that reflects preferred character areas by designating development patterns and development types for general geographical locations in the City. The locations shown on the Framework are illustrative, and are not intended to depict either parcel-specific locations or exact acreage for specific uses.



Policy Alignment & Implementation

The City of Louisville Zone District Map reflects a number of zone districts that govern where uses by right and uses by special review may be located. The Zoning Map of the City should correspond to the goals and policies of the Comprehensive Plan's Framework Plan to ensure that incremental development decisions reflect the Community Vision. Evaluating and amending the Zoning Map will be necessary to align zoning with the vision, values, principles, and policies outlined in the Comprehensive Plan.

Existing Zoning Agreements

Planned Community Zone Districts (PCZD) and approved General Development Plans (GDP), in particular, are a result of a contractual agreement between a property owner(s) and the City. These contracts were created in recognition of the economic and cultural advantages that will accrue to the residents of an integrated, planned community development of sufficient size to provide related areas for various housing types, retail and service activities, recreation, schools and public facilities and other multifaceted uses of land. In some instances these zoning agreements no longer reflect the vision, values, principles and policies outlined in the Comprehensive Plan, and they may need to be amended.

Section 17.72.170 of the Louisville Municipal Code (LMC) requires that the amendment process for contractual zoning plans will be subject to the same procedures, limitations and requirements by which such plans were originally approved. The City should lead in coordinating open reviews and amendments of existing zoning agreements between the City and property owners. If agreement on changes cannot be reached, the existing contractual zoning will remain in force as per the terms of the agreement.

Compliance with Intergovernmental Agreements

Parcels which are affected by an intergovernmental agreement (IGA) remain subject to the provisions and terms of the applicable IGA. The implementation of a preferred land use, which may differ from the land use recommended under the IGA, would require an amend-

ment of the applicable IGA. The Comprehensive Plan may be updated to reflect any new IGA amendments without requiring a complete City Comprehensive Plan amendment process.

POLICY ALIGNMENT

The various departments, boards, and commissions within the City of Louisville are each focused on specific areas of interest. For example, the Public Works Department's primary responsibility is the municipal infrastructure of the City, while the Open Space Advisory Board is concerned with the management and acquisition of open space properties. The goals and objectives of each

of these groups are specific to their areas of interest, and at times the priorities of one group, may be different with those of another.

The successful implementation of the Comprehensive Plan is dependent upon the alignment of the sometimes divergent policies of the various departments and citizen interests of the City.

IMPLEMENTATION ACTION ITEMS

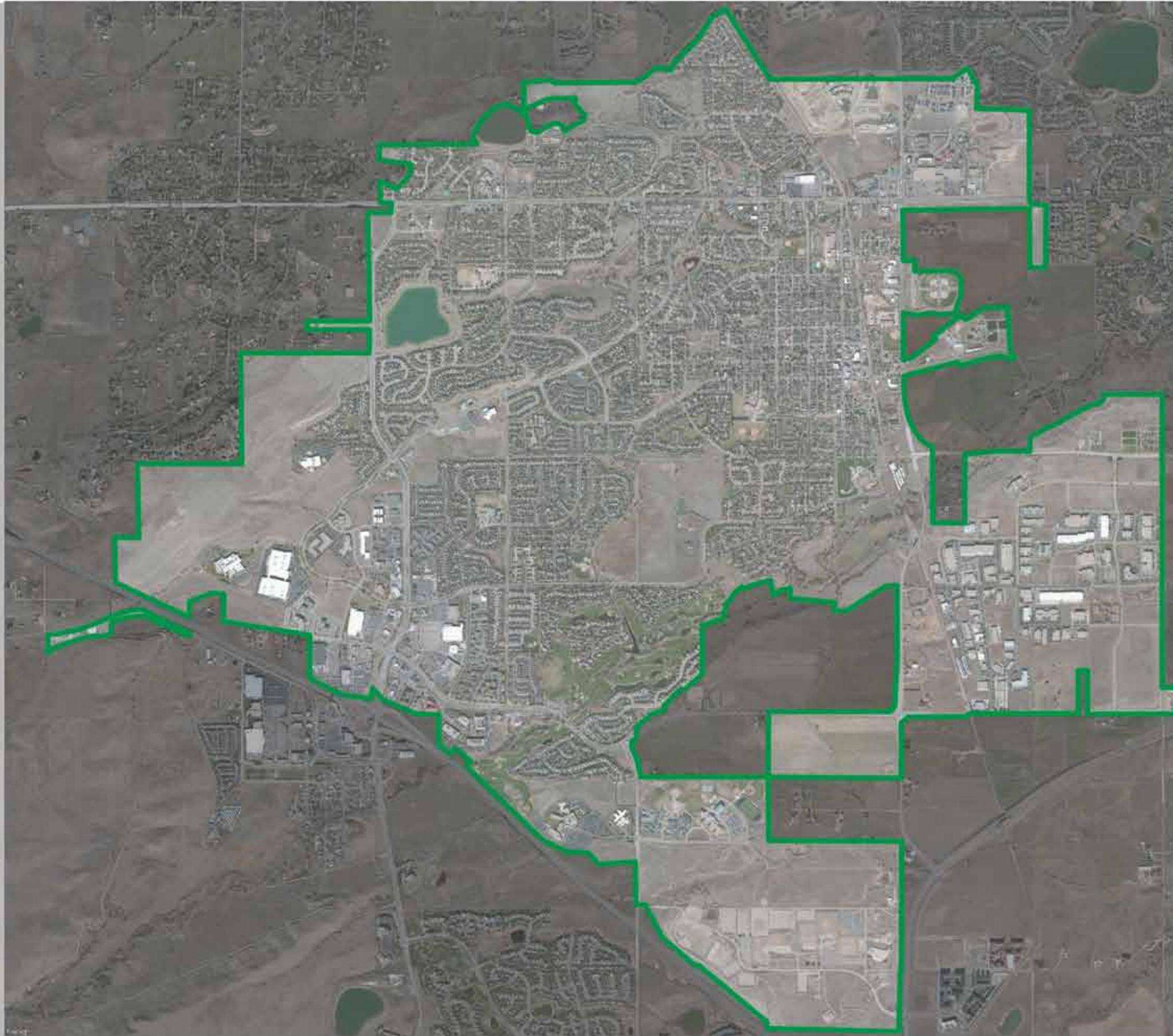
Below is a list of the important steps that should be taken to implement the goals and policies identified in this Comprehensive Plan. These actions are of the vari-

ous types previously described, and together they address every section of the Plan. The table also includes anticipated goals for the completion of each item. Note, the actual timing of actions will be determined annually by the Louisville City Council as it reviews the City's budget and priorities.

These policies alone will not effect the vision outlined in the Framework; that will require the combined efforts of the City, residents, property and business owners in Louisville.

Actions	0-3 Years	3-5 Years	5-10 Years	Actions	0-3 Years	3-5 Years	5-10 Years
Area plans				Community heritage			
McCaslin Blvd	X			Preservation Master Plan	X		
South Boulder Rd	X			Parks, recreation, open space, and trails			
Downtown	X			PROST updates		X	
Phillips 66				Rec center remodel		X	
CTC		X		Missing trail connections	X	X	X
Neighborhood plans				Municipal infrastructure			
Fireside	X			Utility rate study	X		
North Louisville	X			Water and Waste Water Master Plan	X		
Lake Park	X			Storm Water Outfall Master Plan	X		
Hillside	X			Storm drainage improvements		X	
South Louisville	X			Energy			
Davidson Mesa	X			City Wide Energy Assessment	X		
Old Town		X		Update building codes	X	X	X
Coal Creek		X		Develop city-wide energy strategy	X		
Hecla		X		Community services			
Housing				Library Strategic Plan updates		X	
Affordable housing policy	X			CPTED policy	X		
Zoning				Solid waste reduction strategy	X		
Evaluate form based code	X			Arts and Culture Master Plan	X		
Implement code changes		X		Economy and fiscal health			
Transportation				Update fiscal model	X		
Multi-modal Transportation Master Plan	X			Governance			
Coordination on US 36/FasTracks	X	X	X	Policy alignment	X	X	X





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MEMORANDUM

To: Honorable Chair and Members of the Planning Commission

From: Planning Division

Subject: Establish Official Locations for Posting of Public Notice

Date: **February 14, 2019**

State law requires that each year every municipal board or commission establish the location(s) where the notice of their public meetings will be posted. It is required the location be established at that body's first regular meeting of the year.

The City's Home Rule Charter requires that notice of City Council meetings be posted in four locations. The City Attorney and City Manager's office recommend that other boards and commissions follow the same public notice posting practice.

Consistent with that recommendation, staff is recommending the Planning Commission establish for the year 2019 the official locations for posting of Planning Commission agendas as follows:

- **The Lobby of City Hall, 749 Main Street**
- **The Louisville Public Library Bulletin Board, 951 Spruce Street**
- **The Louisville Recreation Center, 900 West Via Appia**
- **The Police / Municipal Court building, 992 Via Appia**
- **The City of Louisville website, www.louisvilleco.gov**

City Council adopted these official locations for posting of notices for public meetings at their January 8, 2019 meeting.

**RESOLUTION NO. 03,
SERIES 2019**

A RESOLUTION RECOMMENDING THAT THE FOLLOWING LOCATIONS BE ESTABLISHED AS THE OFFICIAL LOCATIONS FOR THE POSTING OF PUBLIC NOTICE OF ALL 2019 LOUISVILLE PLANNING COMMISSION MEETINGS

- **The Lobby of City Hall, 749 Main Street**
- **The Louisville Public Library Bulletin Board, 951 Spruce Street**
- **The Louisville Recreation Center, 900 West Via Appia**
- **The Police / Municipal Court building, 992 Via Appia**
- **The City of Louisville website, www.louisvilleco.gov**

WHEREAS, Senate Bill 91-33 requires that all local public bodies designate a public place or places where public notice of public meetings will be posted, with said designation being made at the first regular meeting of that body in each calendar year; and

WHEREAS, the City's Home Rule Charter requires additional locations for the posting of public notice of City Council meetings and by extension, it is the recommendation of Staff that the Planning Commission adopt the same standard for posting of public notice of their meetings; and

WHEREAS, the Planning Commission has reviewed the recommended locations for the posting of public notice and finds them to be consistent with State Statutes, Municipal Code and the Louisville Home Rule Charter.

NOW THEREFORE, BE IT RESOLVED that the Planning Commission of the City of Louisville, Colorado does hereby designate the following public places for the posting of notices for all public meetings of the Planning Commission in 2019.

- **The Lobby of City Hall, 749 Main Street**
- **The Louisville Public Library Bulletin Board, 951 Spruce Street**
- **The Louisville Recreation Center, 900 West Via Appia**
- **The Louisville Police / Municipal Court building, 992 Via Appia**
- **The City of Louisville website, www.louisvilleco.gov**

PASSED AND ADOPTED this 10th day of January, 2019.

By: _____
 , Chair
 Planning Commission

Attest: _____
 , Secretary
 Planning Commission