

## ***Planning Commission***

**March 12, 2020  
City Hall, Council Chambers  
749 Main Street  
6:30 PM**

For agenda item detail see the Staff Report and other supporting documents included in the complete meeting packet.

**Public Comment will be limited to three (3) minutes per speaker.**

1. Call to Order
2. Roll Call
3. Approval of Agenda
4. Approval of Minutes
  - a. January 9, 2020
5. Public Comment on Items Not on the Agenda
6. New Business – Public Hearing Items
  - a. **Speedy Sparkle PUD Amendment:** A request for a PUD Amendment addressing sign design and waivers for 1414 Hecla Way, 1408 Hecla Way, and 1712 Plaza Dr (Resolution 14, Series 2019)
    - i. Applicant: Speedy Sparkle Car Wash – Louisville, LLC
    - ii. Case Manager: Robert Zuccaro, Director of Planning and Building Safety
  - b. **St Louis Parish and Commercial Park GDP, Second Amendment:** A request for approval of a second amendment to the St Louis Parish and Commercial Park General Development Plan to amend allowed uses and development standards, located at the northeast corner of S. 96<sup>th</sup> Street and Dillon Road. (Resolution 2, Series 2020)
    - i. Applicant: United Properties
    - ii. Case Manager: Lisa Ritchie, Senior Planner
7. Planning Commission Comments
8. Staff Comments
9. Items Tentatively Scheduled for the regular meeting April 9, 2020:
  - Napa Auto Parts PUD Amendment

10. Adjourn

***Planning Commission  
Meeting Minutes  
January 9<sup>th</sup>, 2020  
City Hall, Council Chambers  
749 Main Street  
6:30 PM***

**Call to Order** – Chair Brauneis called the meeting to order at 6:30 PM.

**Roll Call** was taken and the following members were present:

Commission Members Present: Steve Brauneis, Chair  
Tom Rice, Vice Chair  
Jeff Moline  
Debra Williams  
Keaton Howe  
Ben Diehl  
Commission Members Absent: Dietrich Hoefner  
Staff Members Present: Rob Zuccaro, Dir. of Planning & Building  
Lisa Ritchie, Senior Planner  
Elizabeth Schettler, Senior Admin. Assistant

**APPROVAL OF AGENDA**

Williams moved and Howe seconded a motion to approve the January 9th, 2020 agenda. Motion passed unanimously by voice vote.

Moline made a motion to nominate the current officers, Chair Brauneis, Vice Chair Rice, and Secretary Williams, to continue their positions. Howe seconded. Motion passed unanimously by voice vote.

**APPROVAL OF MINUTES**

Williams noted that there was an unfinished sentence on page 13 in the staff packet and suggested that it be deleted.

Rice moved and Moline seconded a motion to approve the December 12<sup>th</sup>, 2019 minutes with Commissioner Williams' correction. Motion passed unanimously by voice vote. Howe and Diehl abstained.

**PUBLIC COMMENTS ON ITEMS NOT ON THE AGENDA**

None.

**DISCUSSION OF THE 2020 WORK PLAN**

Ritchie listed the completed work items from 2019:

- Adopted the Transportation Master Plan

- Sign Code Update
- Marijuana Regulations Update
- Parcel O Study and Recommendations

Ritchie described the upcoming work plan items. Staff hoped to present the remaining Open Space re-zoning projects to the Commission. Staff was also underway on the Design Guidelines and PUD Criteria, which would be accompanied by revised PUD and Waiver criteria. A review of the Old Town Overlay and Building Height would be reviewed together in 2020, as well. That item could involve a work session for the Commission and there would also be community comment. Ritchie listed several other items under consideration for the 2020 work plan, including group home regulations and a city-initiated rezoning process. She added that staff was working on the biennial budget and requested recommendations for projects that would require consultant work so staff could work those suggestions into the budget.

Zuccaro noted that in 2022 there would be a 10-year update to the Comprehensive Plan.

Rice asked if the update to the Comprehensive Plan was a significant undertaking.

Zuccaro replied that it was.

Williams asked if the Comprehensive Plan would be in the same year as the budget.

Zuccaro replied that the next biennial budget would include a recommended line for the Comprehensive Plan process.

Williams asked why the City picked 2022.

Rice replied that the process was started in 2012 and adopted in 2013 and the Code required an update every 10 years.

Moline asked if there were things in the 2013 Comp Plan that had not been attended to already and should be included in the next iteration.

Ritchie replied that a lot of the policies and recommendations had been implemented, but one thing that had not been implemented were the neighborhood planning efforts. She thought that the City needed to revisit whether those efforts should remain in the plan.

Zuccaro added that the City had decided not to do the neighborhood planning process since a lot of the goals of those projects were better addressed in the Transportation Master Plan (TMP). He invited the commissioners to offer their suggestions on the neighborhood planning process and its usefulness.

Rice asked for the status of the sign code.

Ritchie replied that the new sign regulations had been approved in October and had gone into effect in November. Code Enforcement was advertising that there were new regulations in place. Any new sign-related items would come under those guidelines.

Rice asked about the area plans for South Boulder and McCaslin.

Ritchie replied that she was currently working on the design guidelines for those plans. The land-use policies had been reviewed extensively under the Parcel O plan.

Zuccaro noted that the main recommendation that came out of both of the corridor plans was to update the design guidelines. He noted that there was no land-use change policy with the corridor plans, except one policy in the South Boulder Road plan, which called for no longer allowing residential SRUs.

Rice expressed interest in hearing back about the outcomes of the Commission's discussions.

Zuccaro replied that all of the character areas and districts in the Small Area Plans (SAP), which the Commission had worked on, would be reflected in the design guidelines.

Ritchie added that the Small Area Plan surveys had been informative and would apply to more areas of the city than those two small areas. She thought the height discussions would be relevant, as would the street and setback orientations.

Moline asked how people could track the ongoing City prioritization of the items in the TMP.

Zuccaro responded that there were TMP programs and projects that would need attention in the Capital Implementation Plan (CIP) and that an interdepartmental group would be formed to make recommendations to Council. In addition, the new budget would include a 6-year CIP budget. Staff would start with the TMP priorities and approach Council with next steps.

Moline appreciated that staff was taking an active role in working on the priorities of the TMP.

Zuccaro added that staff was actively working on construction design documents for implementing the Pine Street improvements that were in the TMP.

Ritchie added that the TMP was a useful tool for staff.

Brauneis observed that the work plan was more administrative than it had been in past years when it had included more technical, hands-on projects. He welcomed additional requests from Council.

Williams asked how often staff reviewed the Municipal Code.

Ritchie replied that anyone could make recommendations for amending the Code, including the Planning Commission. She noted that staff worked with Code every day and could be working on it all the time, but it was a matter of priority. The Old Town Overlay lives in the Code and would be a potential major amendment. Staff usually brought a code amendment or two before the Commission each year.

Williams asked if staff ever went through a comprehensive code review.

Ritchie replied that she would like to, but it was a matter of priority.

Zuccaro replied that it was definitely something a City could do. For example, cities consolidated parts of the Code and hired consultants to help do that work. A Comprehensive Plan Amendment in the next couple of years would be a good time to revisit the Code.

Howe noted that a big concern for citizens coming to the Planning Commission had been when developers leave large piles of dirt on construction sites. He wondered how to encourage development while avoiding those kinds of issues.

Zuccaro replied that there could be construction staging standards in the Code. He noted that the City had recently changed its administrative policy due to issues with dirt storage. The current policy was to no longer allow dirt import onto a property without PUD approval and an approved set of Civil Construction (CC) plans. The City had not always done that in the past. In addition, the approval of those CC plans would include a time limit that would require the dirt to be removed if time limits were not met.

Howe stated that if a PUD expired a developer should be required to return the land to the way it was. That would also serve as an incentive to the developer to continue to develop it.

Rice asked if the CC addressed earthwork and infrastructure.

Zuccaro confirmed.

Diehl asked if those changes were in place today.

Zuccaro replied that the changes were administrative and, if the problems continued, the next step would be to put construction staging standards in the Code.

Rice asked if those would be for existing issues or if existing developments would go through enforcement.

Zuccaro replied that the changes in administrative policy only applied to upcoming plans and that existing construction issues were a matter of enforcement.

Diehl asked what the enforcement mechanism would be for a dirt pile.

Zuccaro replied that staff would have to consult with the City Attorney's office to determine the right way to address enforcement in each situation. Options included addressing it under storage of construction material, dust, or grading and drainage.

Howe stated that he thought it would be worth a discussion to update the PUD Code and to attend to the process for expired PUDs. General agreement.

Ritchie noted that there were currently no special criteria for extensions, but there could be.

Diehl asked where those regulations would live.

Ritchie replied that they would live in the Code.

Williams noted that there was a discussion on the extension of PUDs and construction staging and asked if both of them were in the Code.

Zuccaro confirmed and noted that part of the discussion could be about how and where to address these issues. For example, some of these issues happen on non-PUD properties. He noted that these issues might not be easy to enforce, but the City's ability to do so could be improved.

Williams asked if the City had recourse to put liens on properties.

Zuccaro replied that the City could place a lien if a property owner did not pay for abatement costs.

Rice noted that there were other recourses that were not often invoked.

Williams asked if there were other committees that the Planning Commission should meet with and if the Commission should meet with Council more often.

Zuccaro replied that the Old Town Overlay revision could involve a joint meeting with the Historic Preservation Commission.

Brauneis noted that they used to have liaisons, but there wasn't a lot of live discussion. He thought that there were specific cases where it was appropriate to hear from other committees. He thought the Sustainability Advisory Board was one example, but that it would be more of a project-specific, case-by-case basis.

Williams thought that there would be some overlap with other committees on the Open Space items.

Ritchie replied that the Commission's role was to look at applications on Open Space property versus looking at policy.

Ritchie finished her presentation by updating the Commission on the status of the Strategic Planning Framework.

Moline asked if Planner Ritchie had a sense of upcoming development proposals.

Ritchie replied that there was a little bit left out at CTC and that, in general, there were larger-scale development applications and fewer smaller-scale applications.

Rice asked if the ConocoPhillips property would come on the agenda this year.

Zuccaro replied that it would likely be on the agenda during the first half of the year. He added that the Tennis Center property might also come in.

Brauneis asked who was up for the Sunshine Law review.

Ritchie replied that she would reach out to the City Clerk and see who was due for that training.

Brauneis and Ritchie advised the commissioners to avoid public meetings on applications that may come before the Commission.

Zuccaro added that there could be site visits using the City Attorney's guidelines.

Williams asked for a heads-up before the Medtronic application came forward so she could sit down with staff and the attorney to figure out conflicts of interest.

Ritchie replied that there was a list of current developments on the City website. Staff would also send out a spreadsheet with all active applications.

Zuccaro added that staff and the City Attorney could work with commissioners on conflicts of interest.

Zuccaro replied that they should not get into more detail about specific applications.

Brauneis asked for other questions on the 2020 Work Plan. Seeing none, he asked for staff comments.

#### **STAFF COMMENTS**

- Open Government & Ethics Pamphlet – 2020 Edition

Ritchie asked the commissioners to take a look at the pamphlet.

- Public Notice Posting Locations (Resolution No. 1, Series 2020)

Rice moved to approve Resolution 1, Series 2020. Howe seconded. Resolution approved unanimously by voice vote.

- 2020 Meeting Dates

#### **ITEMS TENTATIVELY SCHEDULED FOR FEBRUARY 13, 2020**

- St Louis Parish and Commercial Park GDP 2<sup>nd</sup> Amendment

- Dinner with City Council on February 25<sup>th</sup>

Howe noted that he would not be able to attend the February meeting.

Brauneis stated that he would not be in attendance in March.

**Adjourn: Howe** moved to adjourn. **Williams** seconded. Adjourned at 7:14 PM.

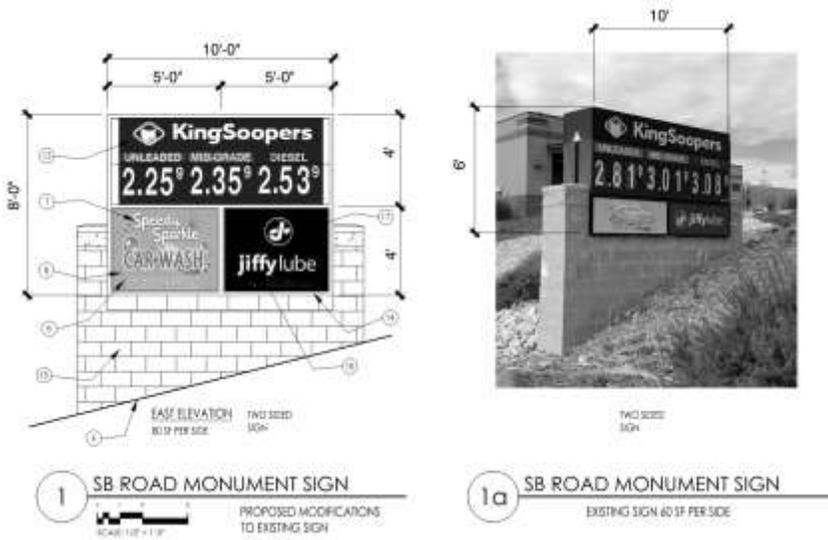
<b>ITEM:</b>	PUD-0222-2019; 1414 Hecla Way; 1408 Hecla Way and 1712 Plaza Drive - Planned Unit Development Amendment Addressing Sign Design Waivers
<b>PLANNER:</b>	Rob Zuccaro, AICP, Planning and Building Safety Director
<b>OWNER:</b>	Car Wash 2, LLC
<b>REPRESENTATIVE:</b>	Chip Weincek CW Associates, PLLC 672 W. Pine Street Louisville, CO 80027
<b>EXISTING ZONING:</b>	Planned Community Zone District – Commercial (PCZD-C)
<b>LOCATION:</b>	1414 Hecla Way; 1408 Hecla Way; and 1712 Plaza Drive (Lot 6, Louisville Plaza Filing 2 and Lots 1 and 2 Louisville Plaza Filing 3)
<b>TOTAL SITE AREA:</b>	90,682 Square Feet
<b>RESOLUTION:</b>	Approval of Resolution 14, Series 2019, a resolution recommending approval to City Council

**SUMMARY**

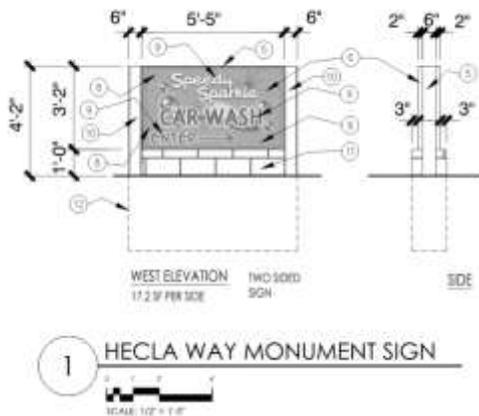


The applicant, Speedy Sparkle Car Wash – Louisville, LLC, requests approval of an amended Planned Unit Development (PUD) to modify the sign allowance for their property at 1414 Hecla Way and modify the joint monument sign facing South Boulder Road that also serves 1408 Hecla Way (Jiffy Lube) and 1712 Plaza Drive (King Soopers Fueling Center) (see Attachment 2 for application materials). More specifically, the proposal includes the following:

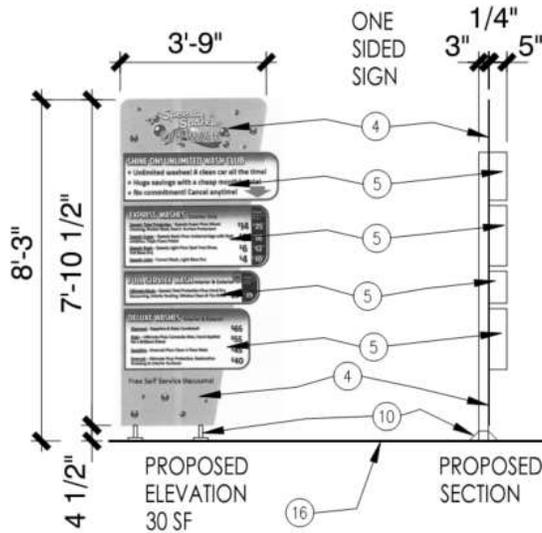
- South Boulder Road Monument Sign. Modification to the existing shared monument sign by increasing the overall height of the sign by 2 ft. and allocating that additional height to the bottom two tenant panels. This increases the sign area from 60 sq. ft. to 80 sq. ft. Each bottom sign panel will be increased from 10 sq. ft. to 20 sq. ft.



- Hecla Way Monument Sign. This replaces the design of a previously approved monument sign facing Hecla Way that was never constructed. The proposed has a masonry base and steel beam frame on both sides. The sign is 4 ft., 2 in. tall and has a sign area of 17.1 sq. ft.



- Menu Board Signs. Two menu board sign on the north side of the building. These signs have already been installed. The signs are on a short dual-pole mounted based, are 8'-3" tall and 27 sq. ft. in sign area.



- Canopy Sign. There is an existing wall-mounted sign extending over a canopy on the south side of the building. This PUD plans do not note this existing sign. The sign includes individual, 24 in. internally illuminated letters, is 14 ft. wide and has a sign area of 28 sq. ft.

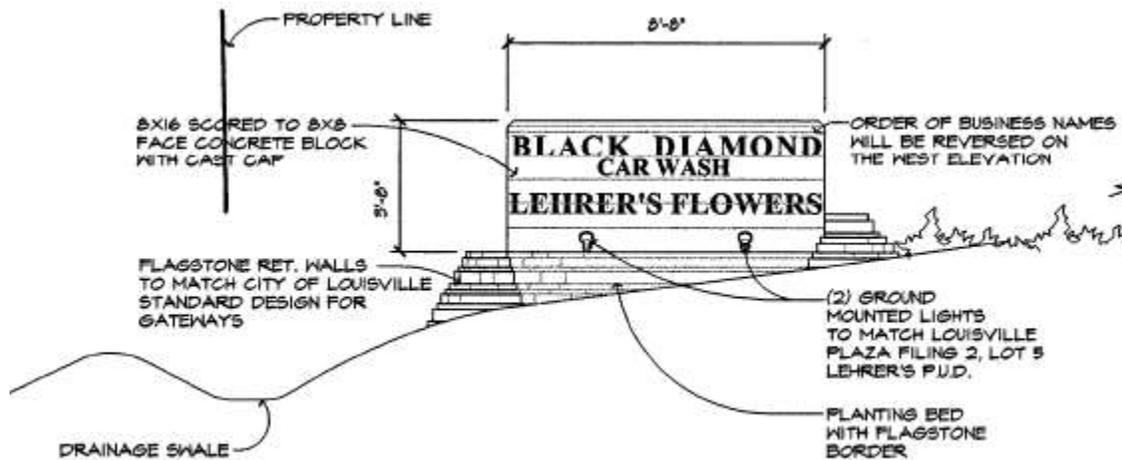


- Sign Illumination. The applicant proposes the following standards for illumination:
  - South Boulder Road Monument Sign – Opaque panels with translucent letters and graphics. No time limits on illumination.
  - Hecla Way Monument Sign – Opaque panel with translucent letters and graphics. Illumination limited to business hours.
  - Menu Board Signs – Translucent panels. Illumination limited to business hours.
  - Canopy Sign – Internally lit channel letters. Illumination times not noted on plans.

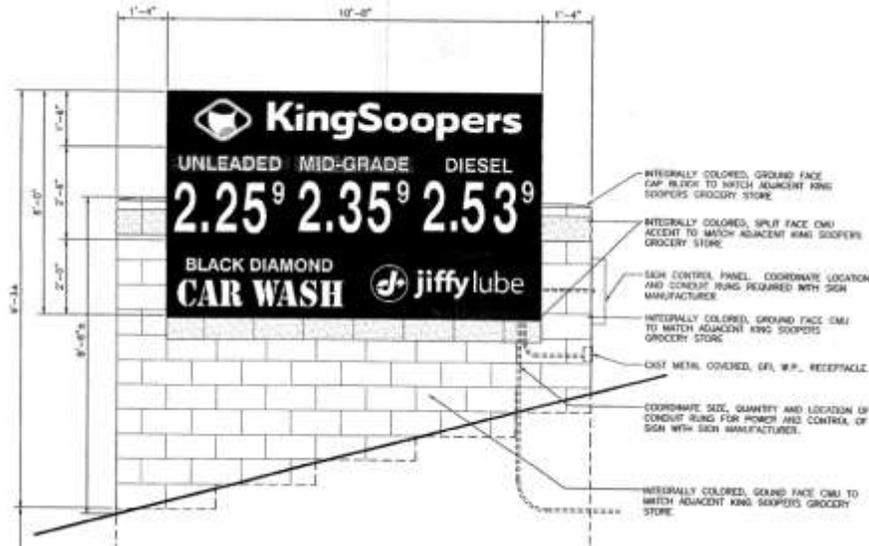
## BACKGROUND:

The City has approved several previous PUDs for the subject properties addressing signage. These are summarized below:

- Black Diamond Car Wash and Lehrer Flowers PUDs, 2000. The City approved both PUDs in 2000 and established a joint monument sign facing South Boulder Road for the Black Diamond Car Wash and Lehrer's Flowers. The Lehrer's Flowers property was later split into two lots that are now the 1408 Hecla (Jiffy Lube) and 1712 Plaza Drive (King Soopers Fueling Station). The sign included two panels for each business. The PUDs also allowed separate monument signs for both the car wash and Lehrer's Flowers facing Hecla Way. The PUD does not show the existing car wash wall sign on the south side of the building. At the time, the staff report noted that the two applicants "have worked together to develop a sign program that minimizes the impact of signs in this suburban-rural setting of the City while recognizing the needs of the businesses." The subject properties are located directly across the street from the Harney Lastoka Open Space adding to the rural context of the area.



- Lehrer's Flowers and Jiffy Lube PUD, 2005. This PUD, and accompanying plat, split the Lehrer's Flowers lot to allow the Jiffy Lube development. Wall signs for each building were approved and a note was included stating:  
*An existing monument sign is currently located on the east property line at the SE corner of the Site. This monument sign will be shared between Black Diamond Car Wash, Lehrer's Flowers and Jiffy Lube. A revised drawing will be submitted at a later date.*
- King Soopers Fueling Center PUD, 2010. This PUD allowed the redevelopment of the Lehrer's Flowers property to the King Soopers Fueling Center and included sign allowances for that property as well as modifications to the shared monument sign facing South Boulder Road. This PUD outlines the currently allowed sign design for the shared sign.



**PLANNING COMMISSION REVIEWS – JULY 11, 2019 & OCTOBER 10, 2019:**

The Planning Commission reviewed two previous versions of this proposal on July 11<sup>th</sup> and October 10<sup>th</sup> of 2019. The Commission continued the request after each meeting for the applicant to address concerns over the proposal. The minutes of each meeting are attached.

**ANALYSIS:**

Sign design is subject to LMC Chapter 17.24 and Chapter 7 of the Commercial Development Design Standards and Guidelines (CDDSG). The CDDSG includes “standards” that must be met and “guidelines” that are preferred design elements. Any proposal that does not comply with LMC Chapter 17.24 or a CDDSG “standard” must receive a waiver through the Planned Unit Development (PUD) process. Although a new sign code has been adopted since this hearing, the original application took place prior to adoption and the applicant has requested this application to be reviewed under the code in existence at the time of application.

The Goal statement from the CDDSG for signs is the following:

*Signs should be consistent with project and overall development design but should be subordinate to architectural and landscape elements. Signs serve to identify, inform, direct, regulate and interpret. Each commercial building or group of commercial buildings should have a consistent and comprehensive sign program from project identification at the street through individual tenant suite identity. Placement, scale, and readability should be considered in developing a sign package*

Waivers needed for current proposal:

- Number of Signs Allowed. CDDSG Sec. 7.2.B.1) states: “One monument sign is allowed per free standing building.” The applicant’s proposal includes two stand-alone signs for Speedy Sparkle, including the joint sign facing South Boulder Road and a stand-alone sign facing Hecla Way.
- Monument Sign Size. CDDSG Sec. 7.2.C.1) states: “Monument signs shall not exceed 60 square feet per sign face in retail zones....” The proposed South Boulder Road monument sign is 80 sq. ft., exceeding the maximum size by 20 sq. ft.

- Sign illumination. CDDSG Sec. 7.4.E. states: “When using an internally illuminated sign cabinet, only that portion of the sign face dedicated to the trademark or characters may be translucent. The balance of the sign face shall be opaque.” Although both monument signs meet this requirement, the menu board signs are translucent and do not meet this standard.

The criteria to waive any of these above requirements is found in LMC Sec. 17.28.110, which states “requirements may be waived or modified through the approval process of the planned unit development if the spirit and intent of the development plan criteria contained in Sec. 17.28.120 are met and...that the modification or waiver is warranted by the design and amenities incorporated into the development plan.”

#### PUD Waiver Criteria

Staff finds that having a joint monument sign that reasonably exceeds the maximum allowed sign area located on South Boulder Road provides an improved design over separate monument signs for each business, which would add to “sign clutter.” Sign clutter is a concept of having too many signs located together in close proximity, leading to ineffective wayfinding, distracting (and thus unsafe) signage, and signage that detracts from a quality built environment. The sign panels provided on the lower part of the sign are proportional to the development and consistent in size with other joint monument signs in the City. Staff finds this sign design will also provide improved visibility for all three businesses, while maintaining the “suburban-rural” setting this site due to its close proximity to protected open space and agricultural lands.

Staff finds that the Hecla Way monument sign provides appropriate design elements to match the architecture and site design on the property. This includes the use of a masonry base to match the retaining wall along South Boulder Road and the metal beam sign frame to mimic architectural elements of the car wash building. This sign will aid in business wayfinding for vehicles entering from Hecla Way. Potential glare impacts on the adjacent neighborhood will be limited by a restriction on the sign illumination only being allowed during business hours.

Staff finds that the use of translucent panels on the menu board signs is an acceptable waiver since these signs are internal to the site and the illumination will have limited impact on surrounding properties due to their location. Potential glare impacts on the adjacent neighborhood will also be limited by a restriction on the sign illumination only being allowed during business hours.

#### Staff Conditions

If the Commission recommends approval, Staff requests Commission adoption of the following conditions:

- Add a call-out and note to the site plan on Sheet A1.0 noting the location of the existing canopy sign and stating: “Existing canopy sign. Sign has area of 28 sq. ft., is 14 ft. wide and copy height of 2 ft.”
- Remove Note No. 5 from Sheets A2.1 and A2.2 that states that staff designed the South Boulder Road monument sign. City staff does not provide sign design for applicants. Staff’s analysis from the last hearing included an image to demonstrate joint monument sign alternatives, which has been adopted by the applicant, but does not constitute staff designing the sign on behalf of the applicant. A proposed design must come from the applicant.

**PUBLIC COMMENTS:**

Public comments received by staff are included as Attachment 10. These comments include those received prior to the previous hearings and additional comment received after those hearings.

**STAFF RECOMMENDATION:**

Staff recommends approval of Resolution 14, Series 2019, recommending approval of the application to City Council with the following conditions:

1. Prior to the City Council hearing, the applicant shall revise the PUD to add call-out and note to the site plan on Sheet A1.0 noting the location of the existing canopy sign and stating: "Existing canopy sign. Sign includes internally illuminated channel letters, has area of 28 sq. ft., is 14 ft. wide and copy height of 2 ft."
2. Prior to the City Council hearing, the applicant shall revise the PUD to remove Note No. 5 from Sheets A2.1 and A2.2 that states that staff designed the South Boulder Road monument sign.

**ATTACHMENTS:**

1. Resolution No.14, Series 2019
2. Application Materials
3. Black Diamond Carwash PUD
4. Lehrer's Flowers PUD
5. Lehrer's Flowers and Jiffy Lube PUD
6. King Soopers Fueling Center PUD
7. July 11, 2019 Planning Commission Minutes
8. October 10, 2019 Planning Commission Minutes
9. Chapter 7, CDDSG – Sign Regulations
10. Public Comments

**RESOLUTION NO. 14  
SERIES 2019**

**A RESOLUTION RECOMMENDING CONDITIONAL APPROVAL OF A REQUEST FOR A PLANNED UNIT DEVELOPMENT AMENDMENT ADDRESSING SIGN DESIGN WAIVERS FOR 1414 HECLA WAY; 1408 HECLA WAY; AND 1712 PLAZA DRIVE (LOT 6, LOUISVILLE PLAZA FILING 2 AND LOTS 1 AND 2 LOUISVILLE PLAZA FILING 3)**

**WHEREAS**, there has been submitted to the Louisville Planning Commission an application for approval of a request for a Planned Unit Development Amendment to allow design changes to the signs on the subject properties that required waivers from Louisville Municipal Code (LMC) Chapter 17.24 and the Commercial Development Design Standards and Guidelines (CDDSG); and

**WHEREAS**, the City Staff has reviewed the information submitted and found that the application complies with applicable PUD waiver criteria in LMC Sec. 17.28.110 as described in the Louisville Planning Commission Staff Report dated March 12, 2020; and

**WHEREAS**, the Planning Commission has considered the application at a duly noticed public hearing on March 12, 2020, where evidence and testimony were entered into the record, including the findings in the Louisville Planning Commission Staff Report dated March 12, 2020.

**NOW THEREFORE, BE IT RESOLVED** that the Planning Commission of the City of Louisville, Colorado does hereby recommend approval of a request for a Planned Unit Development Amendment to allow changes to the signage that include waivers from the adopted City standards for properties at 1414 Hecla Way; 1408 Hecla Way and 1712 Plaza Drive, with the following conditions:

1. Prior to the City Council hearing, the applicant shall revise the PUD to add call-out and note to the site plan on Sheet A1.0 noting the location of the existing canopy sign and stating: "Existing canopy sign. Sign includes internally illuminated channel letters, has area of 28 sq. ft., is 14 ft. wide and copy height of 2 ft."
2. Prior to the City Council hearing, the applicant shall revise the PUD to remove Note No. 5 from Sheets A2.1 and A2.2 that states that staff designed the South Boulder Road monument sign.

**PASSED AND ADOPTED** this 12<sup>th</sup> day of March, 2020.

By: \_\_\_\_\_  
Steve Brauneis, Chairperson  
Planning Commission

Attest: \_\_\_\_\_  
Debra Williams, Secretary  
Planning Commission

# CWA

23 December 2019

Mr. Rob Zuccaro  
Director of Planning & Building Safety  
749 Main Street  
Louisville, CO 80027

RE: Speedy Sparkle Car Wash  
PUD Amendment #3 – Signage  
CWA #18021

Dear Mr. Zuccaro,

This is the response to the last Planning Commission hearing on 10/10/2019 and further discussions with you on this amended signage proposal.

Please reference the attached revised drawings, revision dated 12/23/2019, that reflect the current modifications, as you have requested and as listed below:

1. Existing South Boulder Road Monument Sign – reference sheet A1.0 and details 2 & 2a on sheet A2.1 which reflect the following:
  - The existing Speedy Sparkle Car Wash and Jiffy Lube sign panels will be increased in size from 10 SF to 20 SF.
  - The Speedy Sparkle separate individual monument sign on their property has been eliminated.
  - Sign lighting clarified – Illumination allowed on at all times.
  - Existing sign area to be increased from 60 SF to 80 SF.
  - Transparent and opaque sign surfaces are clarified for Speedy Sparkle sign panel. Sign letters and logo areas are translucent and sign green trademark background made opaque.
  - King Soopers sign panel will remain as existing.
  - Jiffy Lube sign panel will be designed per Jiffy Lube.
  
2. Revised Speedy Sparkle Hecla Way Sign – reference sheet A2.2 revised as follows:
  - \$4 bubble removed.
  - Square Footage at 17.2 sf.
  - Steel I-beam detail provided, along with the concrete block base, aiding the city code three-sided design element.
  - Transparent and opaque sign surfaces are clarified. Sign letters and logo areas are translucent and sign green trademark background made opaque.
  - Sign lighting clarified – Illumination off at end of the business day.
  - Concrete block base (CMU) added to reflect the existing retaining wall materials.

- 1414 Address on base has been removed.
  - “Free Vacuums” changed to “Entry” with graphic directional arrow.
  - Lowered in height from 5’-2” to 4’-2”.
3. Menu Signs – reference sheet A2.1 detail 2 and 2a.
- No revisions requested or required to the existing built signs.
  - Menu graphic areas are translucent and illuminated.
  - Menu background area is opaque and unlit.
  - Sign lighting clarified – Illumination off at end of the business day.

The King Soopers and Jiffy Lube support letters for this Speedy Sparkle Car Wash PUD Amendment #3 – Signage proposal are underway.

Reference the 200+ customer support signatures that agree with the need for these proposed signs.

Per your request you have asked the applicant to comment on waivers for (4) items as listed below:

**1. Number of signs Allowed:**

CDDSG allows:

7.2 Sign Number and Area:

The existing South Boulder Monument sign meets the original approved PUD intent with your request to expand the existing south boulder monument sign from the existing 60 SF to 80 SF and eliminate the applicants proposed separate speedy sparkle monument sign.

This PUD amendment provides for the same number of signs as the original approved PUD. Your waiver request is met by increasing the existing South Boulder Monument sign from 60 SF to 80 SF as you require.

**2. Sign illumination:**

CDDSG allows:

7.4 Sign Illumination:

The Speedy Hecla Way monument sign exceed this requirement, as the opaque area is a portion of the trademark logo design. The South Boulder monument sign is per your design. Reference the drawings for clarification. No waivers needed.

**3. Sign Cabinet:**

CDDSG allows:

The current proposal meets the City/CDDSG requirements– no waivers needed.

**4. Sign Materials:**

CDDSG allows:

The current proposal exceeds the CDDSG/City requirements, providing the three-sided architecture sign elements with the steel i-beams on two sides and the colored concrete block base that matches the existing retaining wall concrete block.

**Menu Signs:** – (Not addressed in the CDDSG or City Code, no waivers needed.)

Per your request you have asked the applicant to address the following PUD Waiver Criteria – Waiver Warranted by Design and Amenities:

**1. Sign Clutter:**

This PUD Amendment #3 has been adjusted to reflect your request for no allowance of an individual Speedy Sparkle south boulder monument sign on their property. Current design for the expanded existing South Boulder Monument sign reflects your design for no sign clutter. No waiver required.

**2. Proportionality of the sign area to the development, the lot area and lot frontage:**

King Soopers Fueling Station has approval for two large monument signs (40 sf & 29 sf = 69 sf monument sign area) with less lot area and less lot frontage than Speedy Sparkle?

- Property Size: Speedy = 45,687 sf, King Soopers= 19,236 sf
- Lot Frontage: Speedy = 294.75', King Soopers = 229.5'

Speedy Sparkle's current sign proposal = 37.2 sf monument sign area for both the South Boulder monument sign and the Hecla Way directional monument sign. No waiver required

**3. Quality of Sign Materials and Design:**

The current proposal meets the CDDSG/City requirements per your design and requests - No waiver required.

**4. Visibility needs for the sign:**

- Speedy Sparkle Car Wash's application meets your design of the signs as you have requested.

This revised Speedy Sparkle Car Wash PUD Amendment #3 – Signage proposal with your required modifications, is consistent with the overall development design and complements the architectural, landscape elements and the existing signs.

The applicant believes we meet your request per the existing approved PUD, City Code and the CDDSG (Commercial Development Design Standards and Guidelines).

We believe this current PUD Amendment #3 – Signage modifications meets all your design directions and city codes. We request approval by the City staff and Planning Commission for this re-submitted Speedy Sparkle Car Wash PUD Amendment #3 – Signage.

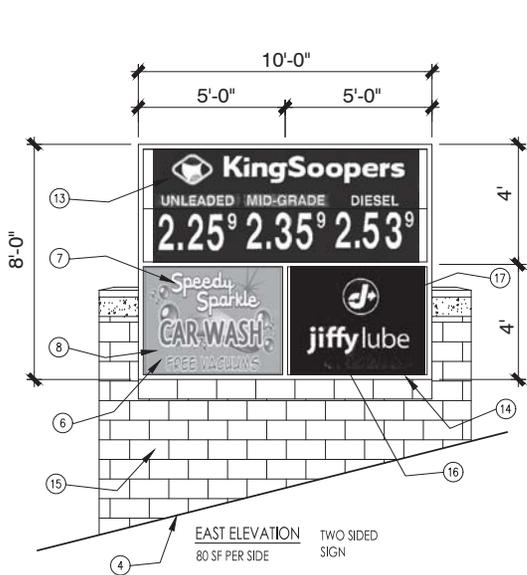
Sincerely,

Robert Kearney  
Owner  
Speedy Sparkle Car Wash - Louisville

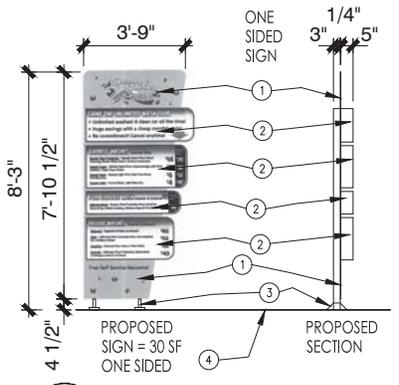
Chip Weincek, AIA LEED AP  
Principal Architect/Planner  
CWA

Attachments

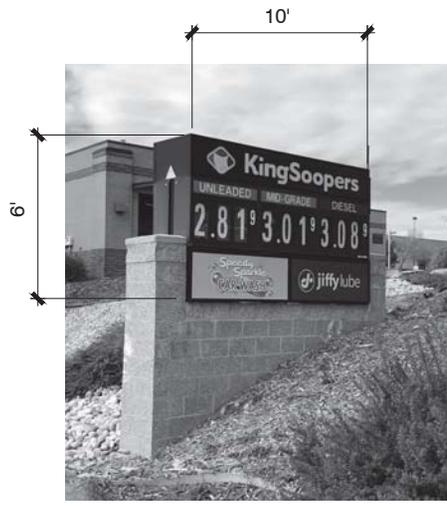




**1 SB ROAD MONUMENT SIGN**  
SCALE: 1/2" = 1'-0"  
PROPOSED MODIFICATIONS TO EXISTING SIGN



**2 DRIVE-UP MENU SIGN**  
SCALE: 1/2" = 1'-0"



**1a SB ROAD MONUMENT SIGN**  
EXISTING SIGN 60 SF PER SIDE



**2a DRIVE-UP MENU SIGNS**

**KEYNOTES:**

- ① METAL PLATE WITH FULL COVER PRINTED GRAPHICS, NON-ILLUMINATED.
- ② METAL SIGN BOX - WITH EXISTING TRANSPARENT ILLUMINATED SIGN PANEL.
- ③ METAL BASE PLATE.
- ④ GRADE.
- ⑤ 10' ALUMINUM CONSTRUCTION PAINTED CABINET.
- ⑥ ACRYLIC FACE WITH FULL SPEEDY SPARKLE LOGO DIGITALLY PRINTED GRAPHICS WITH LED INTERNAL CABINET LIGHTS.
- ⑦ SPEEDY SPARKLE CAR WASH FREE VACUUMS AND GRAPHICS ARE TRANSPARENT.
- ⑧ SPEEDY SPARKLE SIGN BACKGROUND AREA TO BE OPAQUE.
- ⑨ SPEEDY SPARKLE CAR WASH ENTER AND GRAPHICS ARE TRANSPARENT.
- ⑩ WALK'S STEEL-BEAM SUPPORTS PAINTED TO MATCH STEEL MEMBERS ON BUILDING.
- ⑪ CONCRETE MASONRY UNIT (CMU) TO MATCH EXISTING CMU RETAINING WALL.
- ⑫ FOUNDATION.
- ⑬ EXISTING METAL SIGN BOX FOR THE KING SOOPERS SIGN TO REMAIN - RELOCATE ON TOP OF THE NEW SIGN BOX BELOW. ANY NEW SIGN PANELS MUST MEET THE CITY OPACITY STANDARD.
- ⑭ NEW SIGN BOX FOR THE SPEEDY SPARKLE AND JIFFY LUBE NEW SIGNS TO MATCH THE EXISTING METAL SIGN BOX.
- ⑮ EXISTING CONCRETE MASONRY UNITS (CMU) BASE TO REMAIN.
- ⑯ PANEL AREA FOR JIFFY LUBE - SIGN DESIGN, LAYOUT AND PANEL CONSTRUCTION PER JIFFY LUBE.
- ⑰ ANY NEW JIFFY LUBE SIGN PANEL MUST MEET THE CITY OPACITY STANDARD.

**NOTES:**

1. SIGN MANUFACTURE WILL BE RESPONSIBLE FOR FINAL SIGN PANEL DESIGN, CONSTRUCTION DOCUMENTS AND SIGN BUILDING PERMIT.
2. THE MENU SIGNS AND HECLA WAY MONUMENT SIGN WILL HAVE THEIR LIGHTS TURNED OFF AT THE END OF BUSINESS.
3. THE SOUTH BOULDER MONUMENT SIGN LIGHTS WILL BE ALLOWED TO BE ON AT ALL TIMES.
4. MENU SIGNS AND HECLA WAY MONUMENT SIGNAGE PROVIDED BY OWNER. SIGN DESIGN BY SCHLOSSER SIGNS, INC. LOVELAND, COLORADO.
5. SOUTH BOULDER MONUMENT SIGN MODIFICATION DESIGN BY ROBERT ZUCCARO, AICP PLANNING AND BUILDING SAFETY DIRECTOR - CITY OF LOUISVILLE, COLORADO.

No	Date	Revisions
1	4-19	RJD Annex #1 Modification
2	8-21-19	RJD Annex #1 Modification
3	8-23-19	RJD Annex #1 Modification
4	11-25-19	RJD Annex #1 Modification
5	12-16-19	RJD Annex #1 Modification
6	12-22-19	RJD Annex #1 Modification
7	1-14-20	RJD Annex #1 Modification
8	2-20-20	RJD Annex #1 Modification

THIS IS A PRELIMINARY DESIGN. THE DESIGN AND CONSTRUCTION OF THIS PROJECT IS THE RESPONSIBILITY OF THE ARCHITECT AND NOT THE RESPONSIBILITY OF ANY MANUFACTURER OR SUPPLIER OF ANY OF THE ABOVE MENTIONED ITEMS. THE ARCHITECT AND CONSULTANTS SHALL NOT BE RESPONSIBLE FOR ANY COMPROMISES.

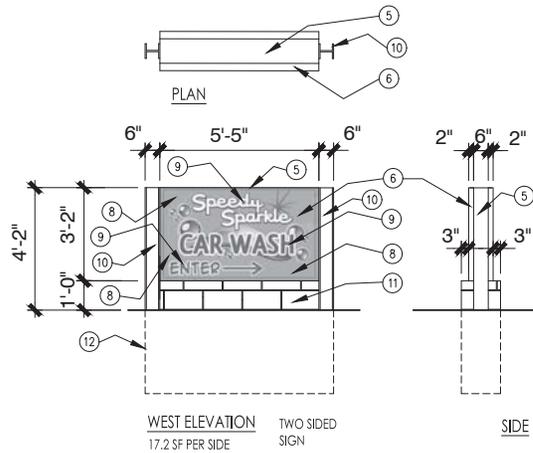
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8	2-20-20	RJD Annex #1 Modification

**SPEEDY SPARKLE CAR WASH**  
1414 Hecla Way  
Louisville, Colorado 80027  
SIGNAGE

Date:	11-26-18
CWA #	18021
Drawn:	CWA
Checked:	CW
Phase:	PUD AMENDMENT

Sheet:  
**A2.1**



**1 HECLA WAY MONUMENT SIGN**

0 1' 2' 4'

SCALE: 1/2" = 1'-0"

**KEYNOTES:**

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- ④ GRADE.
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- ⑧ SPEEDY SPARKLE SIGN BACKGROUND AREA TO BE OPAQUE.
- ⑨ SPEEDY SPARKLE, CAR WASH, ENTER AND GRAPHICS ARE TRANSPARENT.
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NOTES: 1. This drawing is the property of CWA Associates, PLLC. It shall not be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic, mechanical, photocopying, recording, or by any information storage and retrieval system, without the prior written permission of CWA Associates, PLLC (CWA).  
 2. These drawings shall be prepared and executed in accordance with the standards and specifications set forth in the contract documents. The contractor shall be responsible for the accuracy of any information provided to the architect and for the accuracy of any drawings prepared by the contractor. The contractor shall be responsible for any and all errors and omissions and shall be liable for any and all consequences thereof.

No	Date	Revisions
1	4-4-19	PUD Amm. #1 Modification
2	8-21-19	PUD Amm. #1 Modification
3	8-22-19	PUD Amm. #1 Modification
4	8-22-19	PUD Amm. #1 Modification
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7	12-23-19	PUD Amm. #1 Modification
8	1-14-20	PUD Amm. #1 Modification
9	1-20-20	PUD Amm. #1 Modification

**SPEEDY SPARKLE CAR WASH**  
 1414 Hecla Way  
 Louisville, Colorado 80027  
**SIGNAGE**

Date:	1-15-19
CWA #	18021
Drawn:	CWA
Checked:	CW
Phase:	PUD AMENDMENT

Sheet:  
**A2.2**



## **203 Signatures in Support of Speedy Sparkle Car Wash Signage**

For a few hours on several days between July 28 and August 7, my daughter and I asked our customers if they supported additional signage for Speedy Sparkle Car Wash.

The overwhelming result was 203 signatures supporting our request. The support requested:

**"I support Speedy Sparkle Car Wash's sign request with the City of Louisville as shown on the attached sheet.**

**The request is for 50% as much signage as King Soopers Fuel Station currently has.**

**I believe local small businesses require adequate signage to serve our community and prosper, just like big businesses require adequate signage.**

**Small businesses should receive fair treatment in their signage requests."**

A scan of the laminated sheet is attached.

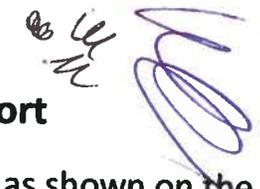
The response was dramatic a mere handful of customers declined to sign the sheet, usually due to time constraints. This response is a clear indication that consumers doing business in Louisville appreciate adequate signage. Some commented that Google or another app did not do a great job of helping them navigate directly to the car wash's entrance on Hecla Way. Others noted South Boulder Road as requiring better signage and still others noted uncertainty as to how to enter the car wash facility.

This appreciation of how signage helps was reflected in the research done by the City on the upcoming sign code update...consumers appreciate good signage and agree that bigger signs are helpful. No comments were made regarding too many signs or sign clutter.

We are providing these signatures and the address information to the City for sharing with the staff, Planning Commission and City Council. We respectfully request that these signature pages NOT be made public or posted on the internet as part of public disclosure documents. This request is to protect the privacy and security of our customers and their participation in the signing.

The point is that a small sample of our customers over a handful of days brought very strong support to signs, sign sizes and our designs that are dwarfed by the very large number and size of those King Soopers has on its tiny gas station plot. The support is for signs larger than our amended application includes. We submit this as evidence of need, support and interest in the community.

## Speedy Sparkle Car Wash – Louisville Signage Support



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Date	Print Name	Street Address	City	Signature
7/28	Julia Moritz	649 Raulin Way	80026	[Signature]
7/28	TOOD THOMAS	1417 WILCOX	80303	[Signature]
7/28	JOE HIRRELSON	547 E 19th St	80027	[Signature]
7/28	[Signature]	2885 E MIDWAY	80027	[Signature]
7/28	Ryann Rice	472 King	80027	[Signature]
7/28	Robin Galloway	928 Treece	80027	[Signature]
7/28	James Richards	5624 Isplia	80002	[Signature]
7/28	Nicole Bryson	567 August Lane	80027	[Signature]
7/28	Mat Campbell	435 Stratford	80026	[Signature]
7/28	[Signature]	1545 HELLAWAY	80027	[Signature]
7/28	Gary Onofrese	1356 Snowberry	80027	[Signature]
7/28	Wade F Goyke	1175 Atlantic	80026	[Signature]
7/28	Randy Parker	908 S 30th Way	80026	[Signature]
7/28	JUDITH BOKER	"	80026	[Signature]
7/28	[Signature]	495 Union at Cr. 20 City Ave	80026	[Signature]
7/28	Michelle Higgins	1848 Pioneer	80026	[Signature]

Signature Sheet Number \_\_\_\_\_



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Date	Print Name	Street Address	City	Signature
7/28/19	Anya Williams	470 Stammme Ln	Lafayette	<i>Anya Williams</i>
7/28	Brian Murphy	930 Homer	Lafayette	<i>Brian Murphy</i>
7/28	MARTIN KELLY	1202 KENNEDY	LOUISVILLE	<i>Martin Kelly</i>
7/28	Melissa Fryer	2345 Deanna	"	<i>Melissa Fryer</i>
7/28	Lisa Poundbrush	138 S. Washington Ave	Louisville	<i>Lisa Poundbrush</i>
7/28	LINDA HUNTER	542 HAYDEN	LAFAYETTE	<i>Linda Hunter</i>
7/28	Lupe Lomeli	1330 Cimarron	Lafayette	<i>Lupe Lomeli</i>
7/28	CHAS COPPA	420 Blueview	"	<i>Chas Coppa</i>
7/28	JESUS MARR	11990 e Southbark	Lafayette	<i>JESUS MARR</i>
7/28	Aimee Miller	2974 Shoreline	Lafayette	<i>Aimee Miller</i>
7/28	Mark King	303-346 4516		<i>Mark King</i>
7/28	John	6120 203 442 3333	Baller	<i>John</i>
7/28	Matthew King	1194 Ridgeway	Bloomfield	<i>Matthew King</i>
7/28	Mike	1115 Archway Dr	Louisville	<i>Mike</i>
7/28	COBEN CALIANO	480 MAD CT.	LOUISVILLE	<i>Coben Caliano</i>
7/28	Carol House	2034 Eisenhower	Louisville	<i>Carol House</i>

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Date	Print Name	Street Address	City	Signature
7/28	Bill Jankins	1776 Steel St #313	Louisville, CO	<i>[Signature]</i>
9/29	Betty Luna	473 Pheasant Run	Louisville, CO	<i>[Signature]</i>
7/28	Mary Alford	1642 Anchor	Lafayette	<i>[Signature]</i>
7/28	Jim Latta		Ernie	<i>[Signature]</i>
7/28	Linda Rosmann		Ernie	<i>[Signature]</i>
7/28	Meredith Daws	1509 Madison Ct	LSVC	<i>[Signature]</i>
	Eric Davila	1403 Garfield Ave	Louisville	<i>[Signature]</i>
7/28	Brad Herbst	2538 Sweetwater	Lafayette	<i>[Signature]</i>
7/28	Charles Ellis	1979 Powell	Lafayette	<i>[Signature]</i>
7/28	Ardan Peltz	855 Dillard	Louisville	<i>[Signature]</i>
7/29	Laura Weert	1130 W Enclave	Louisville	<i>[Signature]</i>
7/29	Mary Mahira	Mary Mahira	Brighton	<i>[Signature]</i>
7/29	Amy Tabora	202 W Cleveland	Lafayette	<i>[Signature]</i>
7/29	Janice Keefer	1450 Lincoln	Louisville	<i>[Signature]</i>
7/29	Tamara Badner	2355 Hillsdale	Boulder	<i>[Signature]</i>
7/29	Kelli Ohm	1055 Corners for Art	Louisville	<i>[Signature]</i>

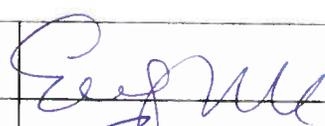
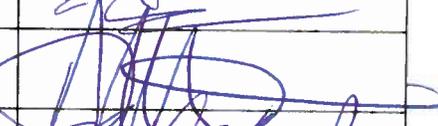
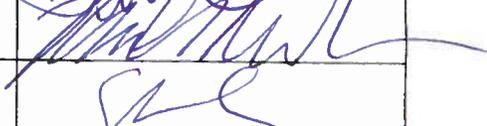
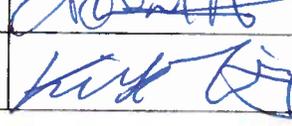
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Date	Print Name	Street Address	City	Signature
7/29	Emily Marshall		Louisville,	
7/29	<del>_____</del>		Bowling	<del>_____</del>
7/29	Helen Towler		Louisville	
7/29	Mark Van Horn		Lafayette	
7/29	Steve Smith		Bowling	
7/29	Walter Thomas		Erie	
7/29	Wendy Hartman		Erie	
7/29	Corey Goss		Lafayette	
7/29	Andrew Kain		Bowling	
7/29	Will Scherer		Louisville	
7/29	Daniel Skeen		Loveland	
7/29	Ivan Ramirez		Louisville	
7/29	Melanie Fang		Louisville	
7/29	Denise Bruder		Superior	
7/29	Roland Voss		Louisville	
7/29	KIRK WITTK		LSUL	

## Speedy Sparkle Car Wash – Louisville Signage Support

*cc*  
*cc*

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Date	Print Name	Street Address	City	Signature
07/18	Nade Sitchant	296 S Lark	Louisville	<i>[Signature]</i>
7/29	Ally Wilton	1225 Apollo Dr	Lafayette	<i>[Signature]</i>
7/29	Lisa McKellan	415 Sunnyside	Boulder	<i>[Signature]</i>
7/29	C.P. AMARAL	16252 WYWAY Red Mountain	Broomfield	<i>[Signature]</i>
7/29	Jill Ayde	4260 Harbour Dr Boulder, CO	Boulder	<i>[Signature]</i>
7/29	MILAN VIKARI	13593 VIA VARRA, Broomfield, CO	Broomfield	<i>[Signature]</i>
7/29	Shobahn Amber	2100 S ROCK CREEK DR #7-102, Superior, 80079	Superior	<i>[Signature]</i>
7/29	Caitlyn Hubbell	380 Pleasant Run Louisville, CO	Louisville	<i>[Signature]</i>
7/29	Walt M. Ha	CAF Agency		<i>[Signature]</i>
7/29	Amber Cunningham	1155 Cannon St	Louisville	<i>[Signature]</i>
7/29	Jonathan Chongoly	3100 S. Prairie	Lakewood	<i>[Signature]</i>
7/29	Ken Moss	6230 Fairview Indian	Fredonia	<i>[Signature]</i>
7/29	JANE VOHS	2335 Sunnyside Denver	Denver	<i>[Signature]</i>
7/29	Mustafa Elm		Denver	<i>[Signature]</i>
7/29	<i>[Signature]</i>	WINDMILL	CAF	<i>[Signature]</i>
7/29	Sean Rutledge	2432 Greenlane	Loveland	<i>[Signature]</i>

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Date	Print Name	Street Address	City	Signature
7/28	Spencer Bellwig	1009 Alton	Erte	
7/28	Jonathan Cr.	Sir galahad	Lafayette	
7/28	Michael Wengryn	2980 Hawk Ct	Lafayette	
7/28	Treddie Wengryn	2980 Hawk Ct	Lafayette	
7/28	Tim Richards	7505 LAFAYETTE	Lafayette	
7/28	John Beary	289 Phasant	Loell	
8/3	Sarah Weber	2738 BIRN HORN CIR	Lafayette	
8/3	RS Jim	240 SKYLINE	Lafayette	
8/3	Grace Cuff	1760 Steel	Louisville	
8/3	Joe Spirka	126 S Fillmore	Louisville	
8-3	Kevin Wagner	4363 Cicely	Imasdown	
8-3	Joseph Campa	516 E. Ganson	Lafayette	
8/3	Timber Ablich	86 E Beresed	Lafayette	
8/3	Yongil Kim	1112 Steel	Louisville	
8/3	John Fox	1315 Jackson	"	
8/3	Brian Warren-Jung	17576 Da Veria	Brownsville	

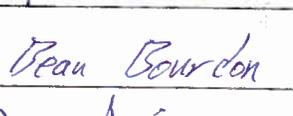
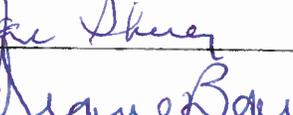
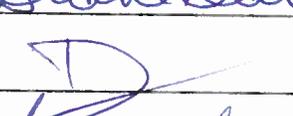
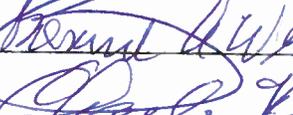
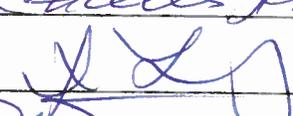
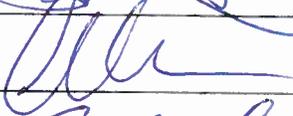
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Date	Print Name	Street Address	City	Signature
8/30	Josh Mac	1060 Willow	Louisville	
7/30	JEFF GOODING	786 Mendota Dr	Lafayette	
7/30	John P. Macdonald	728 Macdonald	Lafayette	
7/30	BSH	7416 P. Place	Berlin	
7/30	Ben M	4205 Strathmore Ln #106	Lafayette	Ben Bourdon
7/30	Jim Shirey	224 Fairfield	Louisville	
7/30	DIANE BARNWELL	2551 N. Franklin AVE	Louisville	
4/30	BTJ Smith	1430 Argyle Way	Lafayette	
7/30	Bonnie A. Wilcox Ky	1108 Delfino Dr.	"	
7-30	Charles Krantz	704 Johnson	Louisville	
7-30	Susie Lenny	2077 Eagle Ave	Suspension <sup>LO</sup>	
7-30	M. Lewis	514 Hopfree	Louisville	
7-30	Steve Wick	6110 Talia	Louisville	
7-30	Chris	385 Rainbowl Ln	Lafayette	
7/30	15101 Elmore Sue	P.O. Box 517	Lafayette	
7-30	Sue Macdonald	1730 S. Kestrel	Louisville	

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Small businesses should receive fair treatment in their signage requests.

Date	Print Name	Street Address	City	Signature
07/30/18	Shivang	1146n franklin	Louisville	Shivang Patel
7/30	Bethany	1905 Chakiss Dr	Lafayette	Bethany King
7/30	Lynne Saul	1545 Heela Way	Louisville	Lynne Saul
7/30	Plm Murr	492 Dahlia Way	Louisville	Plm Murr
7/31	Jill Sommer		Lafayette	Jill Sommer
7/31	Walt Cunningham	4385 Sunlap	Boulder	Walt Cunningham
7/31	Adam Heerden	310 Bates Ave	Boulder	Adam Heerden
7/31	Cindy Wise	2294 Englewood	Lafayette	Cindy Wise
7/31	WILLIAM RYAN	516 GRISWOLD	Louisville	William Ryan
7-31	Chantel Bon	1801 Lydia	Lafayette	Chantel Bon
8/1	Gretchen Heine	302 Condon	Lafayette	Gretchen Heine
8/1	Stephanie Texera		Boulder	Stephanie Texera
8/1	Noel Brindley		Superior	Noel Brindley
8/1	Marcie Escamilla	500 Eisenhower Dr. 80057	Louisville	Marcie Escamilla
8/1	Jennica Peterson	325 Buchanan St.	Louisville	Jennica Peterson
8/1	Erin King	11725 Dunwoody Rd	Boulder	Erin King

## Speedy Sparkle Car Wash – Louisville Signage Support

I support Speedy Sparkle Car Wash's sign request with the City of Louisville as shown on the attached sheet.

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Date	Print Name	Street Address	City	Signature
8.1.19	Leanna Eakins	7630 Spring Dr.	Boulder	
8/1/19	Mackenzie Cooper	645 Viridian Dr.	Lafayette	
8/1/19	Mady Sari	993 E Moorhen Cir	Boulder	
8/1/19	Linda Strange	680 Colorado Ave	Loveland	
8/1/19	Aaron Wilfong	330 County Rd	Louisville	
8/1/19	Carolyn Yates	3100 Broadway Blvd 103	Boulder	
8/1/19	Valerie Whittle	365 Corner St	Louisville	
8/1/19	Derek Lariakette	349 Jasper Park	Lafayette	
8/1/19	Kathy Maloney	548 W. Cactus	Louisville	
8/3/19	Morgan McBride	11775 Wadsworth Blvd apt 10309	Broomfield	
8/3	Shel Coony	1223 Centaur Circle	Lafayette	
8/3	Pete Ricupero	1140 Hillside Ln Louisville	Louisville	
8/3	Joe Bartmess	300 Wilco Circle	Louisville	
8/3	Sabrina Bakeschius	733 Marine St Boulder Co	Boulder	
8/3	Elaine Feeley	2848 Shadyside Rd Lafayette Co	Lafayette	
8/3	Les Not	1407 Imperial Dr Bellevue NE	Orem	

## Speedy Sparkle Car Wash – Louisville Signage Support

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Date	Print Name	Street Address	City	Signature
8/3	Carol Hammer	103 High Country	Laf.	Carol Hammer
8/3	Remington Stebbins	1035 S. Boulder	Louisville	Remington Stebbins
8/3	Jill Linsley	714 N. Southpark Louisville, CO	Cornisville	Jill Linsley
8/3	LUS CHAVEZ	255 ANIMON AVE	Lafayette	LUS Chavez
8/3	Steve Savino	184 W. Elm St.	Louisville	Steve Savino
8/3	Mary Cheney	4811 Redtop Ct., Longmont, CO		Mary Cheney
8/3	<del>Christina</del>	1111 N. Main	Lafayette	<del>Christina</del>
8/3	Jerry Gosh	1923 Pioneer	Lafayette	Jerry Gosh
8/3	Lynne Scheriff	3413 17th	Boulder	Lynne Scheriff
8/7	Danokhiles	507 W. Geneva St.	Lafayette	Danokhiles
8/7	Z B	8543 W. Fox Rd	Boulder	Z B
8/7	Roger Etzinger	670 W Locust	Louis	Roger Etzinger
8/7	Dave Holsclaw		Lafayette	Dave Holsclaw
8/7	Alex Kampet	223 Summit Cir	Lafayette	Alex Kampet
8/7	Tim King	3471 Esplanade	Need	Tim King
8/7	Ryan Budon	1505 HOLLAWAY	LOUISVILLE	Ryan Budon

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Date	Print Name	Street Address	City	Signature
8/3	Sean Quinlan	141 S Washington	Louisville	<i>[Signature]</i>
8/3	Rene Seena	1350 Cimarron	LAFAYETTE	<i>[Signature]</i>
8/3	Franz Wenzel	765 Burgundy	Boulder	<i>[Signature]</i>
8/3	Madeline Ruiz	2128 Hecla Dr	Louisville	<i>[Signature]</i>
8/3	Paul Herrin	6936 Baseline	Louisville	<i>[Signature]</i>
8/3	Gary Feltham	300 Center	Superior	GARY FELTHAM
8/3	MT Maetz		Boulder	<i>[Signature]</i>
8/3	Debbie Siegel	1209 James Cr.	Lafayette	<i>[Signature]</i>
8/3	Judy Thompson		Louisville	<i>[Signature]</i>
8/3	<i>[Signature]</i>	11635 <i>[Signature]</i>	<i>[Signature]</i>	<i>[Signature]</i>
8/3	Duane Harrison	1145 Achilles Cir	Lafayette	<i>[Signature]</i>
8/3	Greg Lamb	11206 Colony Cir	Broomfield	<i>[Signature]</i>
8/3	Anna Royal	1000 Milo	Lafayette	<i>[Signature]</i>
8/3	Laura C Fox	2907 Shadow Creek Dr	Boulder	<i>[Signature]</i>
8/3	Brian M Tan	2019 LASALLE	Superior, CO	<i>[Signature]</i>
8/3	Nina Heeren	968 Milo Circle Unit B	Lafayette	N. Heeren

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Date	Print Name	Street Address	City	Signature
8/3	marlene		Louisville	<i>Marlene</i>
8/3	Fabiola		Louisville	Fabiola
8/3	David		Lafayette	<i>David</i>
8/3	Brayton Maine		Louisville	<i>Brayton Maine</i>
8/3	Shawna		Lafayette	<i>Shawna</i>
8/3	Lisa		Louisville	<i>Lisa</i>
8/3	Gwen Brandenburg		Lafayette	<i>Gwen Brandenburg</i>
8/3	Charles		Louisville	<i>Charles</i>
8/3	Margaret Lopez		Lafayette	<i>Margaret Lopez</i>
8/3	<i>[Signature]</i>		Lafayette	<i>[Signature]</i>
8/3	<i>[Signature]</i>		Lafayette	<i>[Signature]</i>
8/3	Timothy		Louisville	<i>Timothy</i>
8/3	Jason Kohler		Lafayette	<i>Jason Kohler</i>
8/3	Jesus Perez		Lafayette	<i>Jesus Perez</i>
8/3	Austin Brattin		Brambleton	<i>Austin Brattin</i>
8/3	Eric Ellison		Louisville	<i>Eric Ellison</i>

## Speedy Sparkle Car Wash – Louisville Signage Support

I support Speedy Sparkle Car Wash's sign request with the City of Louisville as shown on the attached sheet.

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Date	Print Name	Street Address	City	Signature
8/7	Patti Wade	1057 W. Century Dr	Louisville	<i>Patti Wade</i>
8/7	Betsy Greenwood		Lafayette	<i>Betsy Greenwood</i>
8/7	John C. Bae	608 Mills St	Lafayette	<i>John C. Bae</i>
8/7	Amanda Martinez	518 W. South Boulder Rd	Louisville	<i>Amanda Martinez</i>
8/7	Joe Rudolph	616 Marine St Boulder CO	Boulder	<i>Joe Rudolph</i>
8/7	KIP LAWRAJ	1245 JAMES CIR	LAFFAYETTE	<i>KIP LAWRAJ</i>
8/7	Ercel Bluff	309 WAREKA DR LAFFAYETTE		<i>Ercel Bluff</i>
8/7	Evan House		Arvada	<i>Evan House</i>
8/7	Kiki Ryland	539 Sawtooth Point	Lafayette	<i>Kiki Ryland</i>
8/7	Devon Flynn		Lafayette	<i>Devon Flynn</i>
8/7	Brooke Anderson		Broomfield	<i>Brooke Anderson</i>



Speedy Sparkle Car Wash - Louisville, LLC  
1414 Hecla Way  
Louisville, CO 80027  
303-666-6696

Business Address:  
549 N. 4<sup>th</sup> Street  
Berthoud, CO 80513

January 22, 2020

Jennifer Welch  
Jiffy Lube/Griffin Companies  
Sent by email to: Jenifer@Griffco.com

RE: PUD Signage Conflict Resolution – PUD Amendment

Dear Jennifer:

We are continuing the process with the City of Louisville to amend the Speedy Sparkle PUD. This amendment will resolve the PUD signage conflict between the original Black Diamond PUD and the Kroger/Jiffy Lube PUD.

This PUD Amendment version attached provides for our sign and your sign to each be increased in size to 4 feet tall by 5 feet wide. The size of each of our signs goes from 10 square feet to 20 square feet. King Soopers will retain its current sign at 40 square feet in size. The City wants the authorization and agreement of all three property owners and for us to sign the final PUD amendment (Signature block to be revised at City direction).

This amendment will also provide official City approval of our existing menu signs and approve a new entrance sign at the North end of our property

Once approved, so long as we both desire to expand our space on the existing monument sign, we can split the cost of increasing the size.

I have attached the PUD Amendment #3 sheets showing these changes.

I will really appreciate it if you can indicate your support for this amendment by signing this letter. Thank you for your interest and assistance. My office number is shown above, my cell phone is 303-902-9100 and my email is [robert@coloansonline.com](mailto:robert@coloansonline.com).

Sincerely,

Robert E Kearney  
Principal

Griffin Companies (Jiffy Lube Louisville)

By: 

Its: Vice President



We recycle our water



Speedy Sparkle Car Wash - Louisville, LLC  
1414 Hecla Way  
Louisville, CO 80027  
303-666-6696

---

Business Address:  
549 N. 4<sup>th</sup> Street  
Berthoud, CO 80513  
970-532-4243

January 17, 2020

Dave Seagraves  
King Soopers  
65 Tejon St  
Denver, CO 80223

EMAIL to [Dave.seagraves@kroger.com](mailto:Dave.seagraves@kroger.com)

RE: PUD Signage Conflict Resolution – PUD Amendment

Dear Mr. Seagraves:

We are continuing the process with the City of Louisville to amend the Speedy Sparkle PUD. This Amendment #3 to our PUD does not affect your PUD or signage rights. PUD Amendment #3 will only affect the car wash property and the size of the car wash and oil change cabinets below the King Soopers sign on the existing South Boulder Road Monument sign.

The changes to the PUD Amendment which is shown as the “12/23/19 Planning Commission Resubmittal” (I have no idea why it is called that, since it was not submitted on that date!).

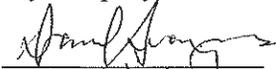
You may return this letter with a signature indicating your agreement to the amendment and authorization to proceed.

Thank you for your interest and assistance. My office number is shown above, my cell phone is 303-902-9100 and my email is [robert@coloansonline.com](mailto:robert@coloansonline.com).

Sincerely,

Robert E Kearney  
Principal

Dillon Companies, LLC, a Kansas limited liability company

By:   
Name: David Seagraves  
Its: 2/26/2020



We recycle our water  
40

**LAND USE APPLICATION**

**CASE NO.** \_\_\_\_\_

**APPLICANT INFORMATION**

Firm: Speedy Sparkle Car Wash - Louisville, LLC \_\_\_\_\_  
 Contact: Robert Kearney  
 Address: 1414 Hecla Way  
 Louisville, CO 80027  
 Mailing Address: 549 N 4th Street  
 Berthoud, CO 80513  
 Telephone: (970) 532-4243  
 Fax: (970) 532-3603  
 Email: robert@coloansonline.com

**OWNER INFORMATION**

Firm: Car Wash 2, LLC  
 Contact: Robert Kearney  
 Address: 549 N. 4th Street  
 Berthoud, CO 80513  
 Mailing Address: 549 N. 4th Street  
 Berthoud, CO 80513  
 Telephone: (970) 532-4243  
 Fax: (970) 532-3603  
 Email: robert@coloansonline.com

**REPRESENTATIVE INFORMATION**

Firm: CW Associates, PLLC  
 Contact: Chip Weincek  
 Address: 672 W. Pine Street  
 Louisville, CO 80027  
 Mailing Address: P.O. Box 271033  
LOUISVILLE, CO 80027  
 Telephone: (303) 666-8941  
 Fax: \_\_\_\_\_  
 Email: chip@cwa-architect.com

**PROPERTY INFORMATION**

Common Address: 1414 Hecla Way  
 Legal Description: Lot 6 Blk \_\_\_\_\_  
 Subdivision Louisville Plaza Filing #2  
 Area: 44,026 Sq. Ft.

**TYPE (S) OF APPLICATION**

- Annexation
- Zoning
- Preliminary Subdivision Plat
- Final Subdivision Plat
- Minor Subdivision Plat
- Preliminary Planned Unit Development (PUD)
- Final PUD
- Amended PUD
- Administrative PUD Amendment
- Special Review Use (SRU)
- SRU Amendment
- SRU Administrative Review
- Temporary Use Permit: \_\_\_\_\_
- CMRS Facility: \_\_\_\_\_
- Other: (easement / right-of-way, floodplain; variance; vested right; 1041 permit; oil / gas production permit)

**PROJECT INFORMATION**

Summary: -See Attached-  
 Amendment to correct and update signage.  
 For site and neighboring properties.

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

\_\_\_\_\_

Current zoning: PUD Proposed zoning: PUD

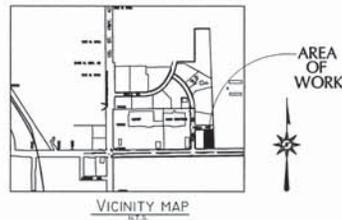
**SIGNATURES & DATE**

Applicant: Robert E. Kearney, Manager  
 Print: Robert E. Kearney, Manager  
 Owner: Robert E. Kearney, Manager  
 Print: Robert E. Kearney, Manager  
 Representative: \_\_\_\_\_  
 Print: \_\_\_\_\_

**CITY STAFF USE ONLY**

- Fee paid: \_\_\_\_\_
- Check number: \_\_\_\_\_
- Date Received: \_\_\_\_\_

# BLACK DIAMOND CARWASH FINAL PLANNED UNIT DEVELOPMENT LOT 6, LOUISVILLE PLAZA FILING NO. 2 COUNTY OF BOULDER STATE OF COLORADO



## PUD DEVELOPMENT GUIDELINES

BLACK DIAMOND CARWASH, LOT 6, LOUISVILLE PLAZA, FILING NO. 2

ZONING DISTRICT	FCZD/P-C (COMMERCIAL)
MIN SETBACK: FROM ARTERIAL STREET	50 FEET BUILDING SETBACK 28 FEET PARKING SETBACK
MIN SETBACK: COLLECTOR STREET	20 FEET BUILDING SETBACK 15 FEET PARKING SETBACK
MIN SETBACK: FROM INTERNAL BOUNDARY	10 FEET BUILDING & PARKING SETBACK
MAXIMUM ALLOWABLE HEIGHT	35 FEET (PER CDDSG) 35 FEET TO TOP OF MECHANICAL SCREENS (PER CDDSG)
PROPOSED BUILDING HEIGHT:	35 FEET ABOVE GRADE (TO PEAK OF SLOPED ROOF)
TOTAL LOT AREA	49,601 SQ FT

**SITE SUMMARY:**

LOT SUMMARY:	49,601 SQ FT
SITE AREA	23,807 SF + 625M
BLDG. PROG. & DRIVEWAY	16,790 SF + 375M
OPEN SPACE	

**PARKING SUMMARY:**

RETAIL: 2,276 SF (45,1000 SF)	10 SPACES
OFFICE: 124 SF (14,000 SF)	3 SPACES
PARKING REQUIRED:	13 SPACES
PARKING PROVIDED:	15 SPACES + 2 LOADING

**BUILDING SUMMARY:**

FIRST LEVEL RETAIL	2,276 SQ FT
SECOND LEVEL OFFICE	124 SQ FT
TOTAL RETAIL/OFFICE	2,400 SQ FT
CAR WASH	3,516 SQ FT
TOTAL:	6,440 SQ FT

PROPOSED BUILDING USE: FULL SERVICE CAR WASH

## SIGNATURE BLOCKS

**CITY COUNCIL CERTIFICATE**  
Approved this 15th day of September, 2002, by the City Council of the City of Louisville, Colorado.  
Resolution No. 55 Series 2002  
Mayor: Tom Davis City Clerk: Stephanie

**PLANNING COMMISSION CERTIFICATE**  
Recommended approval this 15th day of July, 2002, by the Planning Commission of the City of Louisville, Colorado.  
Resolving Series 2002  
Chairperson: [Signature] Secretary: [Signature]

**CLERK AND RECORDER'S CERTIFICATE**  
(COUNTY OF BOULDER STATE OF COLORADO)  
I hereby certify that this instrument was filed in my office at 2:00 o'clock P.M. this 15th day of September, 2002, and is recorded in Plan File 082-E-248, Folio 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 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399, 400, 401, 402, 403, 404, 405, 406, 407, 408, 409, 410, 411, 412, 413, 414, 415, 416, 417, 418, 419, 420, 421, 422, 423, 424, 425, 426, 427, 428, 429, 430, 431, 432, 433, 434, 435, 436, 437, 438, 439, 440, 441, 442, 443, 444, 445, 446, 447, 448, 449, 450, 451, 452, 453, 454, 455, 456, 457, 458, 459, 460, 461, 462, 463, 464, 465, 466, 467, 468, 469, 470, 471, 472, 473, 474, 475, 476, 477, 478, 479, 480, 481, 482, 483, 484, 485, 486, 487, 488, 489, 490, 491, 492, 493, 494, 495, 496, 497, 498, 499, 500, 501, 502, 503, 504, 505, 506, 507, 508, 509, 510, 511, 512, 513, 514, 515, 516, 517, 518, 519, 520, 521, 522, 523, 524, 525, 526, 527, 528, 529, 530, 531, 532, 533, 534, 535, 536, 537, 538, 539, 540, 541, 542, 543, 544, 545, 546, 547, 548, 549, 550, 551, 552, 553, 554, 555, 556, 557, 558, 559, 560, 561, 562, 563, 564, 565, 566, 567, 568, 569, 570, 571, 572, 573, 574, 575, 576, 577, 578, 579, 580, 581, 582, 583, 584, 585, 586, 587, 588, 589, 590, 591, 592, 593, 594, 595, 596, 597, 598, 599, 600, 601, 602, 603, 604, 605, 606, 607, 608, 609, 610, 611, 612, 613, 614, 615, 616, 617, 618, 619, 620, 621, 622, 623, 624, 625, 626, 627, 628, 629, 630, 631, 632, 633, 634, 635, 636, 637, 638, 639, 640, 641, 642, 643, 644, 645, 646, 647, 648, 649, 650, 651, 652, 653, 654, 655, 656, 657, 658, 659, 660, 661, 662, 663, 664, 665, 666, 667, 668, 669, 670, 671, 672, 673, 674, 675, 676, 677, 678, 679, 680, 681, 682, 683, 684, 685, 686, 687, 688, 689, 690, 691, 692, 693, 694, 695, 696, 697, 698, 699, 700, 701, 702, 703, 704, 705, 706, 707, 708, 709, 710, 711, 712, 713, 714, 715, 716, 717, 718, 719, 720, 721, 722, 723, 724, 725, 726, 727, 728, 729, 730, 731, 732, 733, 734, 735, 736, 737, 738, 739, 740, 741, 742, 743, 744, 745, 746, 747, 748, 749, 750, 751, 752, 753, 754, 755, 756, 757, 758, 759, 760, 761, 762, 763, 764, 765, 766, 767, 768, 769, 770, 771, 772, 773, 774, 775, 776, 777, 778, 779, 780, 781, 782, 783, 784, 785, 786, 787, 788, 789, 790, 791, 792, 793, 794, 795, 796, 797, 798, 799, 800, 801, 802, 803, 804, 805, 806, 807, 808, 809, 810, 811, 812, 813, 814, 815, 816, 817, 818, 819, 820, 821, 822, 823, 824, 825, 826, 827, 828, 829, 830, 831, 832, 833, 834, 835, 836, 837, 838, 839, 840, 841, 842, 843, 844, 845, 846, 847, 848, 849, 850, 851, 852, 853, 854, 855, 856, 857, 858, 859, 860, 861, 862, 863, 864, 865, 866, 867, 868, 869, 870, 871, 872, 873, 874, 875, 876, 877, 878, 879, 880, 881, 882, 883, 884, 885, 886, 887, 888, 889, 890, 891, 892, 893, 894, 895, 896, 897, 898, 899, 900, 901, 902, 903, 904, 905, 906, 907, 908, 909, 910, 911, 912, 913, 914, 915, 916, 917, 918, 919, 920, 921, 922, 923, 924, 925, 926, 927, 928, 929, 930, 931, 932, 933, 934, 935, 936, 937, 938, 939, 940, 941, 942, 943, 944, 945, 946, 947, 948, 949, 950, 951, 952, 953, 954, 955, 956, 957, 958, 959, 960, 961, 962, 963, 964, 965, 966, 967, 968, 969, 970, 971, 972, 973, 974, 975, 976, 977, 978, 979, 980, 981, 982, 983, 984, 985, 986, 987, 988, 989, 990, 991, 992, 993, 994, 995, 996, 997, 998, 999, 1000.

**OWNERSHIP SIGNATURE BLOCKS:**  
BY SIGNING THIS PUD, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH IN THIS PUD.  
Witness our hands and seals this 11th day of October, 2002.  
RACHEL S. GERRY  
Owner: [Signature]  
Notary: [Signature]

**NOTES**

1. Improvement plans shall be approved by the Public Works Department prior to construction of public improvements.
2. The property owner shall install and maintain the landscape located within the right-of-way adjacent to their lot.
3. Deciduous trees shall not be planted within 5 feet and coniferous trees shall not be planted within 10 feet of a public utility or sidewalk.
4. Site lighting shall be provided by means of pole-mounted fixtures and building mounted fixtures. Building and pole-mounted lighting shall not exceed 24' in height. Fixture design, spacing and shielding shall be provided to cutoff effect of lighting at adjoining properties exclusive of shared access easements. Locations of pole-mounted fixtures shall be as shown on sheet 2.
5. All trash enclosures shall be constructed of materials to match the building architecture, per details on sheet.
6. Outdoor sales, storage or displays other than the temporary storage of vehicles being serviced on site for not more than 72 hours, is prohibited.
7. No building permits shall be issued until Hecia Key is completed with curb, gutter and first lift of asphalt. The Subdivision Agreement stipulates that no certificates of occupancy will be issued until all Subdivider obligations are met.
8. Ingress-egress easement to be vacated by separate instrument.
9. Window signage shall be prohibited.
10. At such time the parcel abutting Lot 6 to the east is improved the slope along the east lot line shall be modified. The City may at its discretion require additional landscape materials be provided in this area consistent with applicable regulations at such time this requirement is triggered.
11. Off-site trees along the east lot line shall not be disturbed by activities occurring on or related to lot 6.
12. The native grasses shall be drilled at 50 PLS.
13. Noise levels shall comply with current State statutes for noise regulations in a commercial area or current applicable municipal regulations.
14. All planting beds should be mulched with wood or decorative rock to stabilize soils, control erosion, and conserve water use.
15. The City shall not be responsible for damage to, maintenance of, or repair of any private improvements affected by City activities related to the public sanitary sewer easement (to be delineated by separate instrument) traversing the southern portion of the site.
16. The canopy downlight shall not hang below the bottom plane of the canopy. The parking lot and building mounted luminaires shall be of a fully cut-off design.
17. Temporary banners/signage are not permitted.

## SHEET INDEX

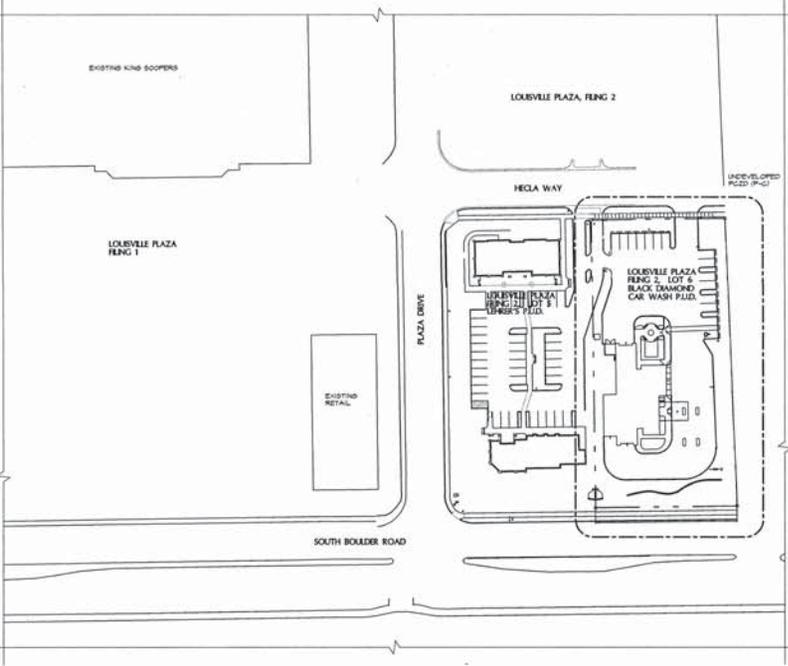
**PUD SUBMITTAL:**

- 1 VICINITY MAP, VICINITY MAP, DEVELOPMENT GUIDELINES, NOTES, INDEX SIGNATURE BLOCKS
- 2 PUD PLAN, LANDSCAPE PLAN, LANDSCAPE LEGEND, SIGN ELEVATION
- 3 ELEVATIONS
- 4 ELEVATIONS
- C1 GRADING & DRAINAGE PLAN
- C2 MASTER UTILITY PLAN

**ATTACHMENTS**

PHOTOMETRICS & LUMINAIRE SCHEDULE

## 2 PERSPECTIVE



## 1 IMMEDIATE VICINITY PUD PLAN

## 2 ROOF PLAN



## NOTES

1. Improvement plans shall be approved by the Public Works Department prior to construction of public improvements.
2. The property owner shall install and maintain the landscape located within the right-of-way adjacent to their lot.
3. Deciduous trees shall not be planted within 5 feet and coniferous trees shall not be planted within 10 feet of a public utility or sidewalk.
4. Site lighting shall be provided by means of pole-mounted fixtures and building mounted fixtures. Building and pole-mounted lighting shall not exceed 24' in height. Fixture design, spacing and shielding shall be provided to cutoff effect of lighting at adjoining properties exclusive of shared access easements. Locations of pole-mounted fixtures shall be as shown on sheet 2.
5. All trash enclosures shall be constructed of materials to match the building architecture, per details on sheet.
6. Outdoor sales, storage or displays other than the temporary storage of vehicles being serviced on site for not more than 72 hours, is prohibited.
7. No building permits shall be issued until Hecia Key is completed with curb, gutter and first lift of asphalt. The Subdivision Agreement stipulates that no certificates of occupancy will be issued until all Subdivider obligations are met.
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16. The canopy downlight shall not hang below the bottom plane of the canopy. The parking lot and building mounted luminaires shall be of a fully cut-off design.
17. Temporary banners/signage are not permitted.



801 MAIN STREET  
SUITE 300  
LOUISVILLE, CO 80027  
VOICE: 303.473.9304  
FAX: 303.473.9318

BLACK DIAMOND CAR WASH  
FINAL PLANNED UNIT DEVELOPMENT  
LOUISVILLE PLAZA SUBDIVISION, FILING 2  
LOUISVILLE, COLORADO

PROJECT # 9963  
DATE: 20 APRIL 2000  
DRAWN BY: SLB/DF  
CHECKED BY: JH  
REVISIONS:  
FINAL P.C. 4 JMS 00  
FINAL P.C. 5 JMS 00  
FINAL P.C. 24 JMS 00  
FINAL P.C. 25 JMS 00

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Hartronc Fauri Architects, P.C.

VICINITY PLAN  
GUIDELINES  
NOTES, INDEX  
SIGNATURE  
BLOCKS

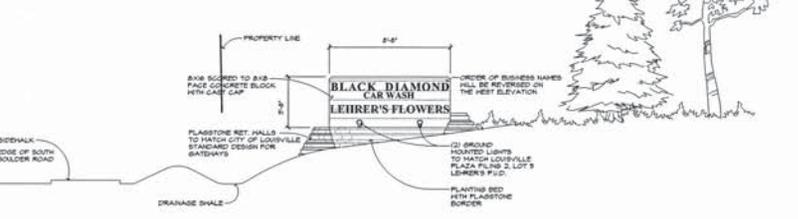
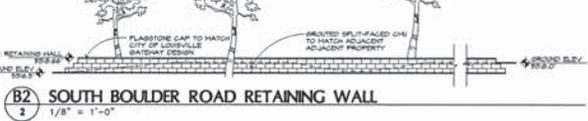
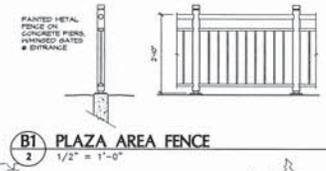
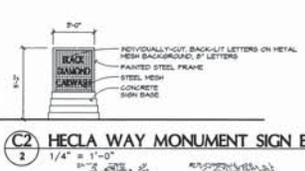
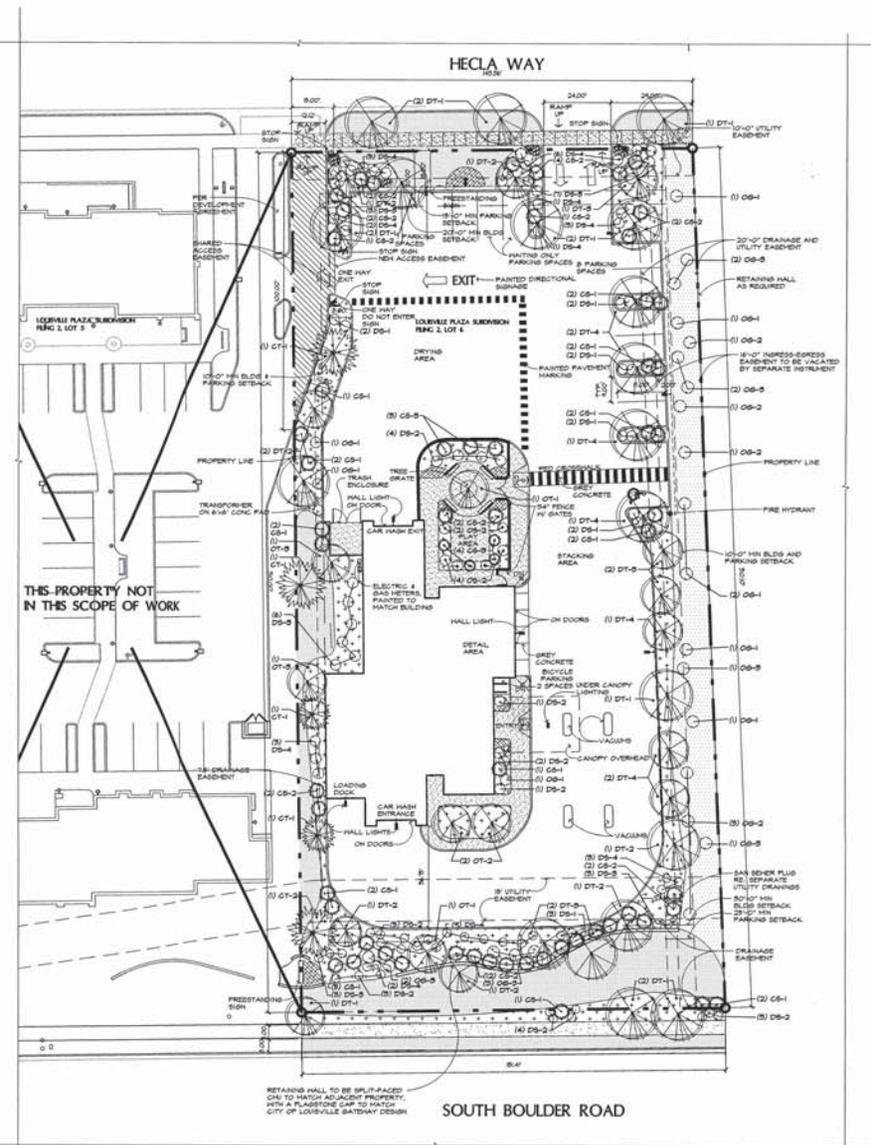
Sheet  
1  
1 of 1 Sheets

LANDSCAPE SCHEDULE - SHRUBS					
QTY	TAG	SIZE	COMMON NAME	SCIENTIFIC NAME	NOTES
<b>DECIDUOUS SHRUBS</b>					
16	DS-1	3 GAL	ROCK COTONEASTER	COTONEASTER HORIZONTALIS	
15	DS-2	3 GAL	SNOWHOUND SPIRAEA	SPIRAEA HIPPURICA 'SNOWHOUND'	
11	DS-3	3 GAL	REDTIPS DOGWOOD	CORNUS STOLONIFERA	
45	DS-4	3 GAL	FORSYTHIA INTERMEDIA	CARYOPTERIS INGANA	
<b>CONIFEROUS SHRUBS</b>					
15	CS-1	3 GAL	CREEPING HOLLYGATE	MAHONIA REPENS	
51	CS-2	3 GAL	PIRE THORN	PYRACANTHA COCCINEA	
7	CS-3	3 GAL	BUFFALO JUNIPER	JUNIPERUS SABINA 'BUFFALO'	
<b>CONIFEROUS TREES</b>					
1	CT-1	3 GAL	BLUE OATGRASS	HELICTOTRICHON SCHEPHERDIANS	
1	CT-2	3 GAL	FEATHER REEDGRASS	CALAMAGROSTIS ACUTIFLORA STRICTA	
1	CT-3	3 GAL	JAPANESE BLOODGRASS	IMPERATA CYLINDRICA 'RED BARKO'	

LANDSCAPE SCHEDULE - TREES					
QTY	TAG	SIZE	COMMON NAME	SCIENTIFIC NAME	NOTES
<b>DECIDUOUS TREES</b>					
11	DT-1	5" CALIPER	RICHMOND LINDEN	TILIA BUCKLORA 'REDHOND'	
6	DT-2	5" CALIPER	RED MAPLE	ACER RUBRUM	
9	DT-3	5" CALIPER	WERS-GUTLEAF	ACER SACCHARINUM 'HER'	
7	DT-4	5" CALIPER	WESTERN HACKBERRY	CELTIS OCCIDENTALIS	
<b>CONIFEROUS TREES</b>					
4	DT-5	6" TALL	WHITE FIR	ABIES CONCOLOR	
1	DT-6	6" TALL	GOLDRADO BLUE SPRUCE	PICEA PENSILVANA 'BLAU'	
<b>ORNAMENTAL TREES</b>					
2	OT-1	2" CALIPER	BRADFORD PEAR	PIRUS CALLERYANA 'BRADFORD'	
2	OT-2	2" CALIPER	CORALBURST CRABAPPLE	MALUS CORALBURST	
2	OT-3	2" CALIPER	AMAR MAPLE	ACER GINNALA	

LANDSCAPE SCHEDULE	
	Planting bed - 2" gravel/3" mulch/6" topsoil
	Ground cover - Vinca minor, Parthenocissus
	Soil/Turf areas - 10% Peat/90% Bluegrass/10% Bluegrass/10% Native Grasses/10% Native Grasses/10% Native Grasses/10% Native Grasses
	Decorative shrub - 3 GAL
	Ornamental tree - 2" Caliper
	Coniferous tree - 2" Caliper
	Landscape border

PERENNIAL LIST	
COMMON NAME	
*PERENNIAL LIST IS INCLUSIVE BUT NOT LIMITED TO THE LIST BELOW	
DAFFODIL	PHLOX
TULIP	SUNFLOWER
PERIWINKLE	THISTLE
LILY OF THE VALLEY	GOLDENROD
ROCKY MOUNTAIN COLUMBINE	ASTER
DAISY	HEM
FLAX	SNAPDRAGON
IRIS	BURGLASS
POPPY'S	LAUNDRESS
IRIS'S	CRABGRASS
SADE	NARCISSUS
PEONY	
CORALBELLS	
VERDENA	
YARROW	
DAILY	
EVENING PRIMROSE	



**HARTRONFT FAURI ARCHITECTS**  
Planning - Architecture - Interiors

801 MAIN STREET  
SUITE 111  
LOUISVILLE, CO 80202  
PHONE: 303.473.9184  
FAX: 303.473.9114

**BLACK DIAMOND CAR WASH  
FINAL PLANNED UNIT DEVELOPMENT**  
LOUISVILLE PLAZA SUBDIVISION, FLING 2  
LOUISVILLE, COLORADO

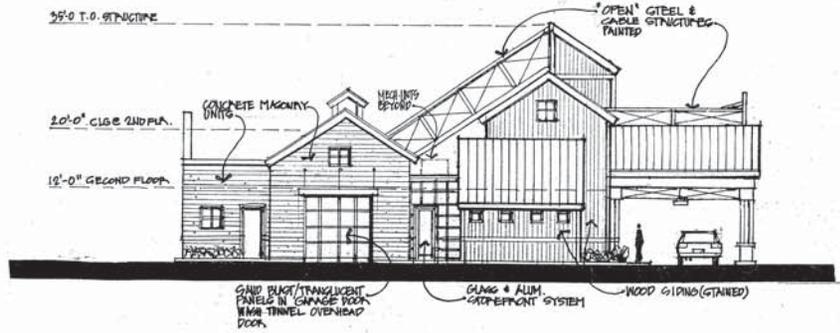
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PROJECT # 9963  
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DRAWN BY: SLB/SF  
CHECKED BY: JH  
REVISIONS:  
FINAL P.C. 5 LINE 00  
FINAL P.C. 24 JULY 00  
FINAL P.L.A.R. 8 OCT 00

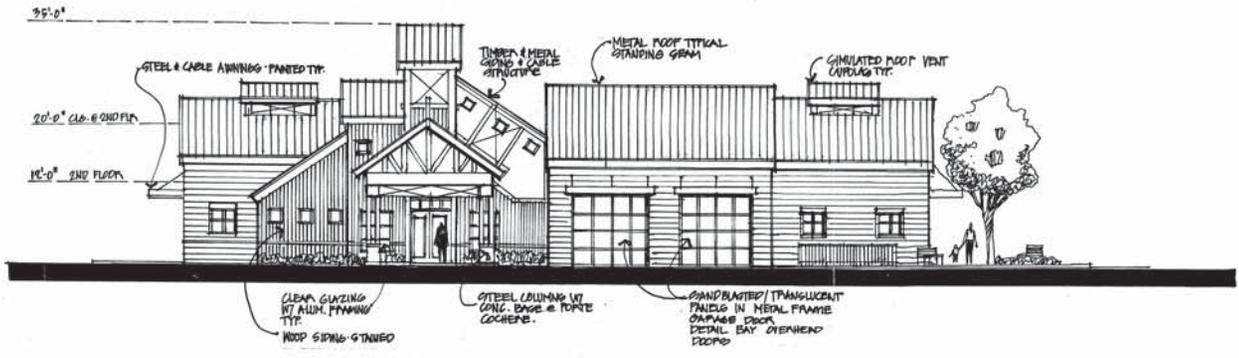
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LANDSCAPE  
LEGEND  
SIGNAGE

Sheet  
**2**  
2 of 7 Sheets

1/25/06/47-0001\_5479963/00000111.DWG • 09/09/2000 • 07:46 • 8.87/6.87



**B1** SOUTH ELEVATION  
3  
1/8" = 1'-0"



**A1** EAST ELEVATION  
3  
1/8" = 1'-0"

**HARTRON FAURI ARCHITECTS**  
Planning • Architecture • Interiors

801 MAIN STREET  
SUITE 300  
LOUISVILLE, CO 80027  
VOICE 303.673.9304  
FAX 303.673.9319

**BLACK DIAMOND CAR WASH**

PLAN  
P.D. SUBMITTAL

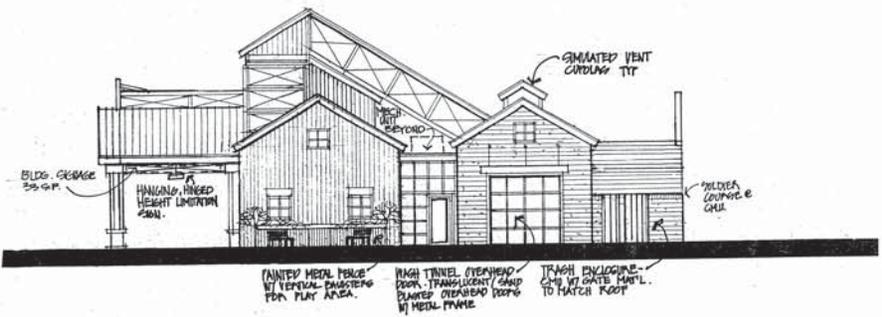
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DATE: 20 APRIL 00  
DRAWN BY: JEP  
CHECKED BY: EDH  
REVISIONS: 01-JUNE 00  
02-JULY 00  
03-24-JULY 00  
04-MAY 2004 EDH

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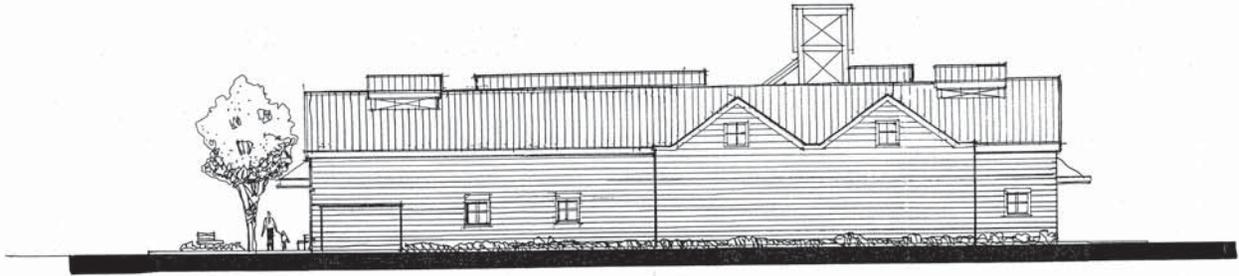
ELEVATIONS

Sheet  
**3**  
5 of 7 Sheets

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**B1** NORTH ELEVATION  
1/8" = 1'-0"



**A1** WEST ELEVATION  
1/8" = 1'-0"

**HARTRONFT FAURI ARCHITECTS**  
Planning • Architecture • Interiors

801 MAIN STREET  
SUITE 300  
LOUISVILLE, CO 80027  
VOICE: 303.473.9334  
FAX: 303.473.9334

BLACK DIAMOND  
CAR WASH

FINAL  
P.D. SUBMITTAL

PROJECT # 9963  
DATE: 20 APRIL 00  
DRAWN BY: HPF  
CHECKED BY: JPH  
REVISIONS: 04 LINE 00  
PC: 09-JULY 00  
CC: 04-JUNE 00  
BYLINE: 04-01-00

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ELEVATIONS

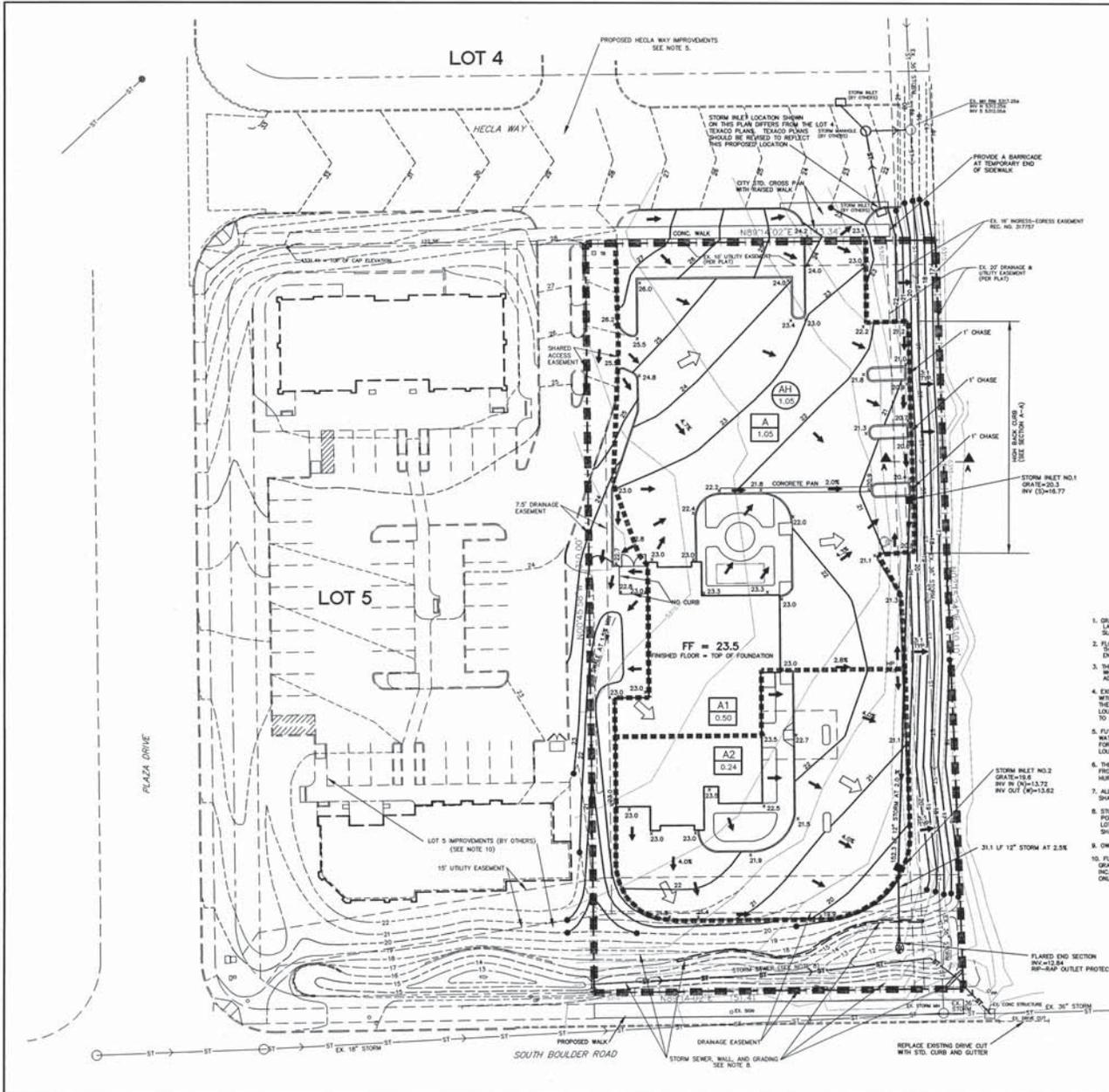
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4 of 7 Sheets

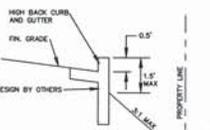
PLANNED DEVELOPMENT • PINK, CALIFORNIA  
REGISTERED PROFESSIONAL ENGINEER  
1530 50th Street • Boulder, Colorado 80503  
303.444.3051

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### LEGEND

- 21.7--- EXISTING CONTOUR
- - - - 25 - - - - PROPOSED CONTOUR (DESIGNED BY OTHERS)
- - - - 25 - - - - PROPOSED CONTOUR
- - - - x 25.8 - - - - PROPOSED SPOT ELEVATION
- HISTORIC SHEET FLOW
- PROPOSED FLOW DIRECTION
- - - - - DRAINAGE BASIN BOUNDARY (HISTORIC)
- - - - - PROPOSED BASIN BOUNDARY
- (A) 23.5 BASIN DESIGNATION (HISTORIC)
- (A) 23.5 BASIN DESIGNATION (PROPOSED)
- ST --- EXISTING STORM SEWER W/MANHOLE
- - - - ST - - - - PROPOSED STORM SEWER W/MANHOLE (DESIGNED BY OTHERS)
- - - - ST - - - - PROPOSED STORM SEWER W/MANHOLE
- FF FINISHED FLOOR ELEVATION (NOMINAL, SEE ARCH. DWGS.)



### SECTION A-A

### NOTES

1. GRADE AWAY FROM BUILDINGS A MINIMUM OF A 1/2" INCH SLOPE IN THE FIRST 10 FEET IN LANDSCAPE AREAS AND AT A MINIMUM 2% SLOPE IN THE FIRST 10 FEET IN IMPERVIOUS AREAS. SLOPES IN LANDSCAPE AREAS SHALL NOT EXCEED 4% UNLESS OTHERWISE NOTED.
2. FLOOD INFORMATION: THE FEMA FLOOD INSURANCE RATE MAP PANEL 0805300270 DATED JUNE 1995 INDICATES THAT THE ENTIRE SITE IS OUTSIDE OF ANY MAPPED 100-YEAR FLOODPLAIN.
3. THE BENCHMARK FOR THIS PLAN IS THE TOP OF THE CAP ON THE NORTHWEST CORNER OF LOT 5. THE BENCHMARK ON THIS PLAN IS DESIGNATED ELEVATION 23.5. REFER TO THE DESIGN PLANS FOR LOT 5 ADD 3.50 TO ALL PROPOSED SPOT ELEVATIONS AND PROPOSED CONTOUR LABELS.
4. EXISTING TOPOGRAPHY SHOWN ON THESE PLANS IS BASED ON THE "LAND SURVEY PLAN WITH TOP OF LOT 6, LOUISVILLE PLAZA FILING NO. 2" BY ORETEL BARRELL & CO., DATED 1/23/00. THE DATUM OF THE TOPOGRAPHY HAS BEEN ADJUSTED FROM THE ORIGINAL PLAN TO REFLECT CITY OF LOUISVILLE DATUM. THE EXISTING BENCHMARK SHOWN ON THIS PLAN SHALL BE FIELD VERIFIED PRIOR TO CONSTRUCTION AND ANY DISCREPANCIES SHALL BE REPORTED TO THE OWNER.
5. FUTURE IMPROVEMENTS FOR HECLA WAY SHOWN ON THESE PLANS INCLUDING CURB AND GUTTER, WATERWAYS, STORM SEWERS, INLETS, AND FINISHED GRADES ARE FROM THE SITE CONSTRUCTION PLANS FOR TEXADO EXPRESS LANE BY TOP FIVE INC. DATED 12-20-99 AND APPROVED BY THE CITY OF LOUISVILLE ON 3-8-00.
6. THE SANITARY SEWER MAIN AT THE SOUTHERN END OF THE SITE SHOWN ON THESE PLANS IS FROM THE SANITARY SEWER PLAN FOR LEDNER'S FLOWERS, LOT 5, LOUISVILLE PLAZA PLNG 2 BY HURST & ASSOCIATES, INC. DATED JUNE 5, 2000.
7. ALL EXISTING CONCRETE ADJACENT TO THE PERIMETER OF THE SITE THAT IS SETTLED OR OTHERWISE DAMAGED SHALL BE REMOVED AND REPLACED AS PART OF THIS PROJECT, AT THE DEVELOPER'S EXPENSE.
8. STORM SEWER IMPROVEMENTS, RETAINING WALL AND FUTURE GRADING SHOWN IN THE SOUTHERN PORTION OF LOT 6 HAS BEEN DESIGNED BY HURST & ASSOCIATES, INC. REFER TO THE DESIGN PLANS FOR LOT 6 BY HURST & ASSOCIATES FOR DESIGN INFORMATION IN THIS AREA. THE WATER QUALITY SHALE SHALL BE MAINTAINED AS REQUIRED (SEE DRAINAGE REPORT).
9. OWNER MUST ENSURE COMPACTION OF SOIL OF ASTM D698 AT TRENCHES AND SUBGRADES WITHIN PUBLIC EASEMENTS.
10. FUTURE IMPROVEMENTS FOR LOT 5 SHOWN ON THESE PLANS ARE FROM THE WASTEWATER DRAINAGE AND GRADING PLAN FOR LEDNER'S FLOWERS, LOT 5, LOUISVILLE PLAZA, PLNG 2, BY HURST & ASSOCIATES, INC. LOT 5 GRADING AND IMPROVEMENTS ARE SHOWN FOR COORDINATION PURPOSES ONLY. REFER TO HURST & ASSOCIATES, INC. PLANS FOR ANY LOT 5 IMPROVEMENTS.



## GRADING AND DRAINAGE PLAN BLACK DIAMOND CAR WASH LOT 6, FILING 2, LOUISVILLE PLAZA LOUISVILLE, COLORADO

		SCOTT, COOK & ASSOCIATES, INC. consulting engineers • surveyors 1530 50th Street • Boulder, Colorado 80503 303.444.3051	
Designed by	RGB	Date	4/14/00
Drawn by	RGB	Scale	1"=20'
Checked by	WHM	Survey	Site
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		Sheet	C1
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		Project No.	00280

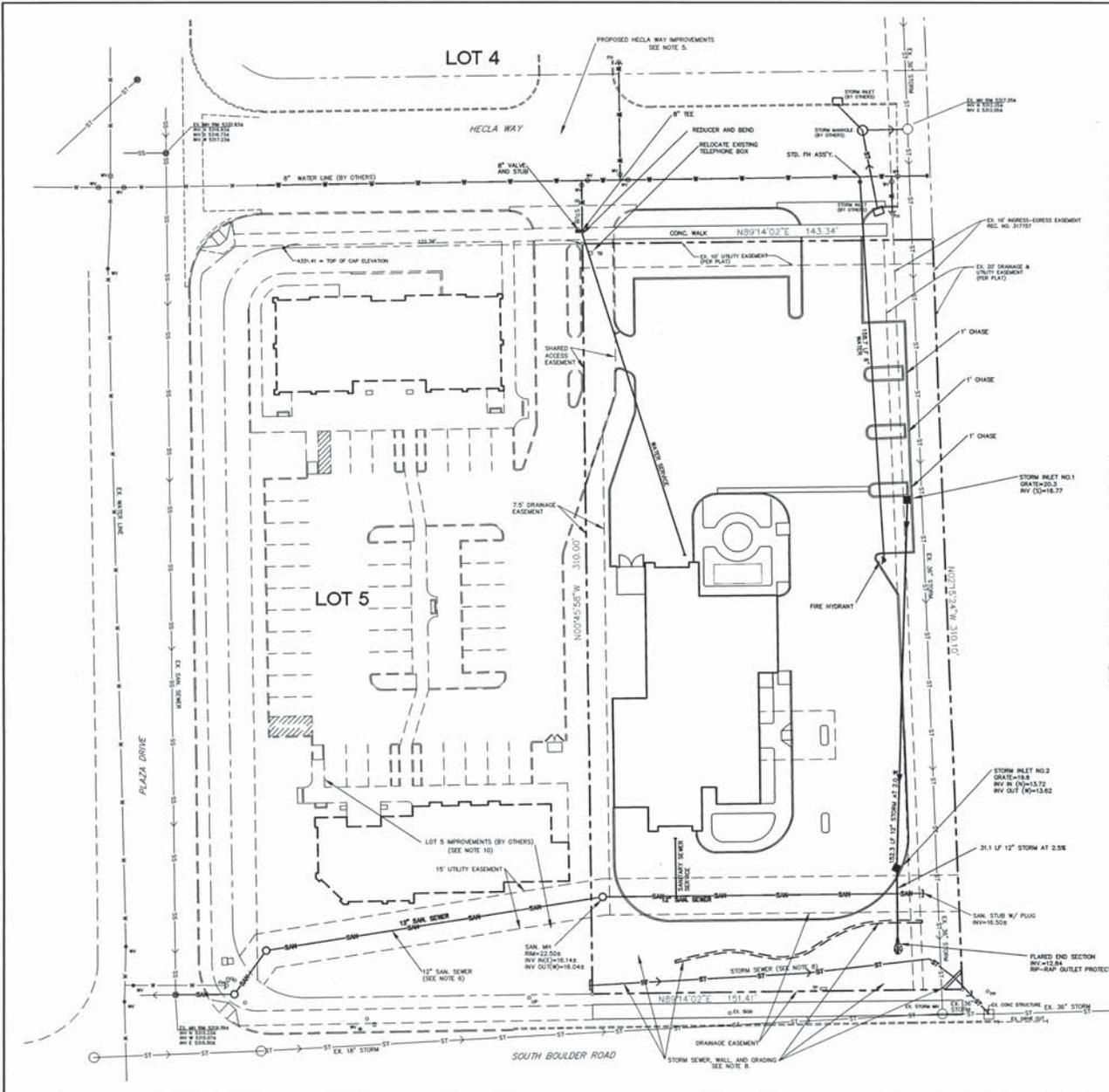
LOT 6, LOUISVILLE PLAZA  
 FILING # 2  
 SEPT. 2000

DRAWING NUMBER

DRAWING NUMBER

Black Diamond Car Wash  
 6 of 7

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**LEGEND**

- ⊕ EXISTING POWER POLE
- ⊕ EXISTING TELEPHONE PEDESTAL
- EXISTING SANITARY SEWER W/MANHOLE
- S—S— PROPOSED SANITARY SEWER W/MANHOLE (DESIGNED BY OTHERS)
- W—W— EXISTING WATER W/FIRE HYDRANT
- H—H— PROPOSED WATER W/FIRE HYDRANT (BY OTHERS)
- H—H— PROPOSED WATER W/FIRE HYDRANT
- ST—○— EXISTING STORM SEWER W/MANHOLE
- ST—○— PROPOSED STORM SEWER W/MANHOLE AND INLET (DESIGNED BY OTHERS)
- ST—○— PROPOSED STORM SEWER W/MANHOLE
- PROPOSED DRAIN INLET
- ⊕ EXISTING WATER VALVE
- ⊕ PROPOSED WATER VALVE
- C.O. PROPOSED CLEAN-OUT

**NOTES**

1. ALL CITY UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF LOUISVILLE STANDARD SPECIFICATIONS.
2. FINAL WATER, FIRE AND GAS SEWER TAPS AND SERVICE LINE SIZES SHALL BE DETERMINED AT THE TIME OF BUILDING PERMIT APPLICATION.
3. ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE DONE IN A WAY SO AS TO MINIMIZE DISRUPTION IN SERVICE TO EXISTING USERS.
4. THE LOCATION OF EXISTING UTILITIES AND FUTURE UTILITIES BY OTHERS SHOWN HEREON ARE BASED ON FIELD SURVEYS, DESIGN PLANS AND OTHER INFORMATION PROVIDED BY OTHERS WHICH MAY INCLUDE THE UTILITY OWNER OR UTILITY LOCATING SERVICES. SCOTT, COX & ASSOCIATES, INC. IS NOT RESPONSIBLE FOR UTILITY INFORMATION PROVIDED BY OTHERS. SCOTT, COX & ASSOCIATES, INC. RECOMMENDS THAT THE LOCATION OF THE UTILITIES BE FIELD VERIFIED PRIOR TO ANY DIGGING ON OR ADJACENT TO THE SUBJECT PROPERTY.
5. FUTURE IMPROVEMENTS FOR HECLA WAY SHOWN ON THESE PLANS INCLUDING CURB AND GUTTER, WATERLESS STORM SEWER, INLETS, AND FINISHED GRADES ARE FROM THE SITE CONSTRUCTION PLANS FOR TEXACO EXPRESS LUBE BY TYP FIVE INC. DATED 12-10-99 AND APPROVED BY THE CITY OF LOUISVILLE ON 3-9-00.
6. THE SANITARY SEWER MAIN AT THE SOUTHERN END OF THE SITE SHOWN ON THESE PLANS IS FROM THE SANITARY SEWER PLAN FOR LEONER'S FLOWERS, LOT 5, LOUISVILLE PLAZA FILING 2 BY HURST & ASSOCIATES, INC. DATED JUNE 5, 2000.
7. NO DECIDUOUS TREES SHALL BE PLANTED WITHIN 5 FEET AND CONIFEROUS TREES WITHIN 10' OF A PUBLIC UTILITY OR SIDEWALK.
8. STORM SEWER IMPROVEMENTS, RETAINING WALL AND FUTURE GRADING SHOWN IN THE SOUTHERN PORTION OF LOT 6 HAS BEEN DESIGNED BY HURST & ASSOCIATES, INC. REFER TO THE DESIGN PLANS FOR LOT 6 BY HURST & ASSOCIATES FOR DESIGN INFORMATION IN THIS AREA.
9. STORM SEWER PIPE SHALL BE CLASS B ROP.
10. FUTURE IMPROVEMENTS FOR LOT 5 SHOWN ON THESE PLANS ARE FROM THE MASTER DRAINAGE AND GRADING PLAN FOR LEONER'S FLOWERS, LOT 5, LOUISVILLE PLAZA, FILING 2, BY HURST & ASSOCIATES, INC. LOT 5 GRADING AND IMPROVEMENTS ARE SHOWN FOR COORDINATION PURPOSES ONLY. REFER TO HURST & ASSOCIATES, INC. PLANS FOR ANY LOT 5 IMPROVEMENTS.



MASTER UTILITY PLAN  
 BLACK DIAMOND CAR WASH  
 LOT 6, FILING 2,  
 LOUISVILLE PLAZA  
 LOUISVILLE, COLORADO

<b>SCOTT, COX &amp; ASSOCIATES, INC.</b> consulting engineers • Surveyors 1330 15th Street • Boulder, Colorado 80503 (303) 441-3001						
Designed by	RCB	Date	4/14/00	Scale	1"=20'	
Drawn by	RCB	Version	1	Survey	6/2/00	
Checked by	WM	City Comments		Date	6/2/00	
					Sheet	C2
					Drawing no.	00280
					Project no.	00280

**Arm-Mounted/Canopy Die-Cast Cutoff, Drop/Flat Lens**  
Use for parking garages, convenience stores, hotels and walkways.

Example: KAC 400M FP 120 SFD09

Catalog Number	FP	120	SFD09
Designation	Distribution	Voltage	Mounting
1000W KAC 100S	SPK	208	SPKR
1500W KAC 150S	DPF	240	SPKR
2000W KAC 200S	DPF	240	SPKR
3000W KAC 300S	DPF	240	SPKR
4000W KAC 400S	DPF	240	SPKR
5000W KAC 500S	DPF	240	SPKR

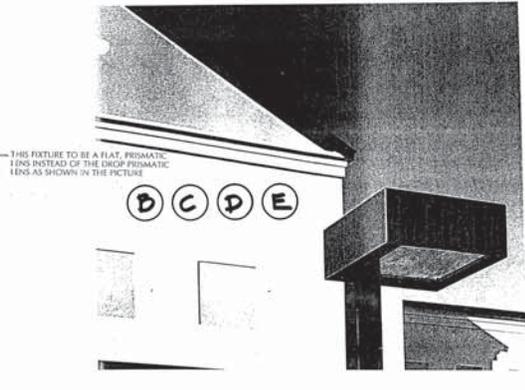
**Options/Accessories**  
For options and accessories, see page 258-259.

**STANDARD PACKAGING**  
Items are packed in one carton.  
40, 100, 200, 300, 400, 500  
Items are packed in one carton.  
Accessories are packed separately.

**Area Lighting**  
**KAC KACM**  
**Contour™ Series**



**Features**  
Housing: Rugged, die-cast, soft corner aluminum housing with 0.12" nominal wall thickness. Die-cast aluminum door frame has prismatic, impact-resistant, tempered glass, drop dish acrylic lens or drop dish polycarbonate lens. Door frame is fully gasketed with one-piece tubular silicone. KAC utilizes extruded 2" soft corner mounting arm. KACM is surface mounted utilizing four 0.5" mounting holes and one G3/8 wire entry hole (KACM mounting hardware not included). Dark bronze polymer powder finish is standard. Other architectural colors available.  
Optic: Reflector is optical-quality aluminum that works in tandem with a light-diffusing prismatic lens.  
Ballast/Electrical System: Ballast is high-resistance, high-power factor (150W HPF) or high-resistance, high-power factor constant-energy, auto-recovery (175-400W MH & HPF).  
Socket: Mogul base porcelain socket, 40V pole, rated. Fashion-oriented socket standard on all metal fixture units.  
Landscape: Use fixed for wet locations. Ceiling, mount on concrete, steel or aluminum. Not for use in dwellings. Listed and labeled to comply with Canadian Standards (candel only).  
Notes: See page 258-259.



**ARS-SQ-18 AND ARS-SQ-22**

**ARS-SQ LAMP SPECIFICATIONS**  
All items supplied with luminaires have a metal base and are non-vented. Available long-throwing configurations.

ARS-SQ-18	ARS-SQ-22
175 MH E29	400 MH E29
200 MH E29	1000 MH E29
400 MH E18	400 MH E18
100 MH E29/27	1000 MH E29/27
200 MH E18	400 MH E18
400 MH E18	400 MH E18

**DIMENSIONS**

ARS-SQ-18	ARS-SQ-22
Ø	Ø
A	A
B	B
C	C
D	D

**Control Glare Reduce Wasted Light**  
**Control Light**  
**Rugged Die-Cast Aluminum**

**TECHNICAL INFORMATION**

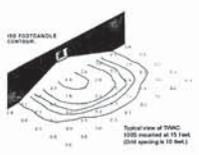
The TWAC provides this performance:  
• True cutoff lighting  
• Pleasant, highly uniform lighting  
• Voltage capability up to 100 watts MH and 100W, or 42 watt instant on fluorescent  
• Contour Series aesthetics and quality construction (EPK rated)

Traditional Mini-Reflectors  
TWAC Cutoff Mini-Reflectors

Traditional security lighting fixtures can throw half their light either into the sky (wasteful and inefficient), or straight into the neighbors' windows.

**Footcandle Correction for Mounting Height:**  
Actual mounting height: 8' 12' 14' 16'  
Correction factor: 0.25 0.20 0.16 0.10  
Multiply the values on the photometric curves by these factors to calculate light levels for different mounting heights.

**Footcandle Correction for Wattage:**  
Watts: 42 54 70 100  
Correction factor: 0.36 0.30 0.25 0.18  
Multiply the values on the photometric curves by these factors to calculate light levels for different lamps and wattages.



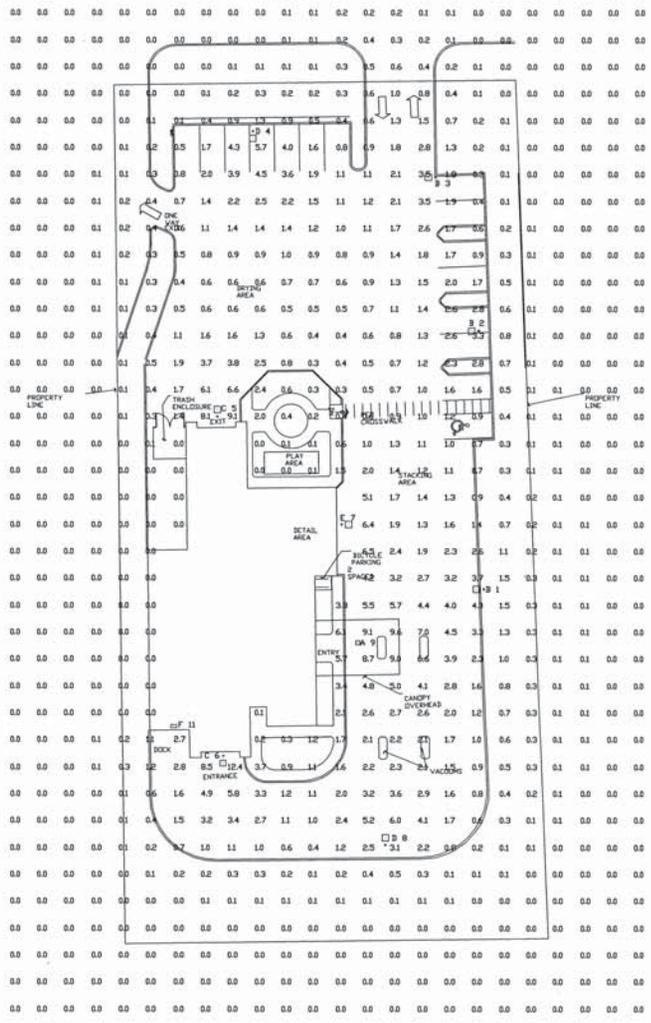
**LUMINAIRE SCHEDULE**

Symbol	Label	Qty	Catalog Number	Description	Lamp	File	Lumens	LLF
□	A	1	KAC 200M DP	CONTOUR SERIES SOFT SQUARE LUMINAIRE	200 MH/175 E29	91221/MS	20000	0.72
□	B	3	ARSQ1820M-05	AREA LIGHT 18- HYPERFORMED TYPE B (CUT-LENS) B SHELL	200W CLEAR METAL HALIDE- E29	1025M/MS	20000	0.72
□	C	2	ARSQ1817M-0P	AREA LIGHT 18- PRISMED FAB REF-LUMEN LENS	175W CLEAR SUPER METAL HALIDE- E29	1817M/MS	13200	0.72
□	D	2	ARSQ22M-0P-05	AREA LIGHT 22- PRISMED FORWARD THROUGH-CL LENS-B SHELL	200W CLEAR METAL HALIDE- E29	1025M/MS	20000	0.72
□	E	1	ARSQ1817M-05	AREA LIGHT 18- HYPERFORMED TYPE B REF-CL LUMEN	175W CLEAR SUPER METAL HALIDE- E29	1817M/MS	13200	0.72
□	F	1	TWAC 25W NEW LED/BLK TAPE SWY	WALL MOUNTED AREA LIGHT W/CUTOFF LENS	ONE 25 WATT CLEAR LED-17 METAL HALIDE, HORIZONTAL POSE	00402/MS	5000	0.72

**LUMINAIRE LOCATIONS**

No.	Label	X	Y	Z	Mt	Orientation	Tilt	X	Y	Z
1	B	494.0	228.0	24.0	24.0	270.0	0.0	491.3	225.0	0.0
2	B	493.0	321.0	24.0	24.0	297.0	0.0	490.3	320.0	0.0
3	B	476.0	378.0	24.0	24.0	288.0	0.0	473.3	377.0	0.0
4	D	408.0	388.0	24.0	24.0	179.4	0.0	405.0	383.0	0.0
5	C	388.0	298.0	14.0	14.0	0.0	0.0	385.0	291.0	0.0
6	C	388.0	183.0	14.0	14.0	180.0	0.0	385.0	180.0	0.0
7	E	412.0	348.0	14.0	14.0	90.0	0.0	444.7	240.0	0.0
8	D	408.0	190.0	24.0	24.0	1.0	0.0	405.1	182.0	0.0
9	A	440.0	225.0	24.0	24.0	0.0	0.0	440.0	225.0	0.0
11	F	380.0	174.0	15.0	15.0	180.0	0.0	380.0	174.0	0.0

**HECLA WAY**



**PHOTOMETRIC PLAN**  
1" = 20'

**SOUTH BOULDER ROAD**

**HARTRONFT FAURI ARCHITECTS**  
Flowing - Architecture - Interiors  
801 MAIN STREET  
SUITE 300  
LOUISVILLE, CO 80027  
PHONE: 303.673.9344  
FAX: 303.673.9319

**BLACK DIAMOND CAR WASH**  
**FINAL PLANNED UNIT DEVELOPMENT**  
LOUISVILLE PLAZA SUBDIVISION, FILING 2  
LOUISVILLE, COLORADO

FINAL PUD SUBMITTAL

PROJECT # 9963  
DATE: 20 APRIL 00  
DRAWN BY: SLA  
CHECKED BY: JH  
REVISIONS:  
1 JULY 00 RC  
2 JULY 00 RC  
3 JULY 00 RC  
FINAL UTILITY & DIST. 00

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**PHOTOMETRIC PLAN LUMINAIRE SCHEDULE**

Sheet  
**7**  
7 of 7 Sheets

# LEHRER'S FLOWERS PLANNED UNIT DEVELOPMENT

FOR  
LOT 5, LOUISVILLE PLAZA FILING NO. 2  
CITY OF LOUISVILLE, BOULDER COUNTY, COLORADO

## SIGNATURE BLOCKS

**CITY COUNCIL CERTIFICATE**  
APPROVED THIS 5<sup>th</sup> DAY OF September, 2000 BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO. RESOLUTION NO. 54 SERIES 2000

*Tom Dandrea* MAYOR  
*Genevieve* CITY CLERK

**PLANNING COMMISSION CERTIFICATE**  
RECOMMENDED APPROVAL THIS 11<sup>th</sup> DAY OF July, 2000 BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO. RESOLUTION NO. 24 SERIES 2000

*Myra H* CHAIRPERSON  
*John Stalder* SECRETARY

**CLERK AND RECORDER CERTIFICATE**  
(COUNTY OF BOULDER STATE OF COLORADO)

I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT 8:30 O'CLOCK P. M. THIS 6<sup>th</sup> DAY OF February, 2001, AND IS RECORDED IN PLAN FILE NO. E-12 FOR 250 PAID FILE NO. 216618 RECEIPTS 1/25/01

*Charlotte Houston* RECORDER  
*Scottie J. Mulcahy* DEPUTY

**OWNERSHIP SIGNATURE BLOCK**  
BY SIGNING THIS PUD, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH IN THIS PUD.

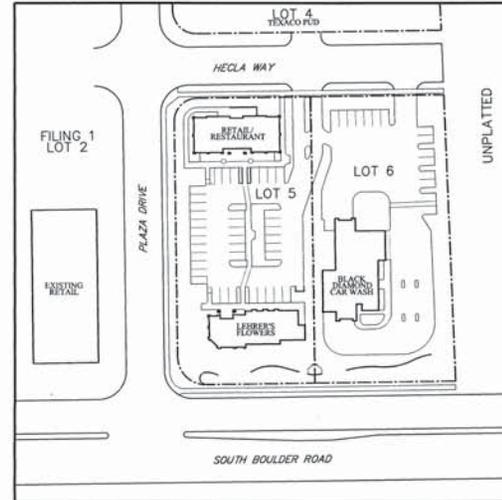
Witness our hands and seals this 17<sup>th</sup> day of October, 2000.

*Lehrer's Flowers* OWNER  
*David Byerly* OWNER

*Christy Obidly* NOTARY  
My Commission expires 11/25/2002




LOCATOR MAP



VICINITY MAP  
1" = 60'

## DRAWING INDEX

- A1 SITE PLAN
- A2 LEHRER'S FLOWERS ELEVATIONS
- A3 RETAIL/RESTAURANT ELEVATIONS
- A4 DETAILS/SIGN CRITERIA
- L1 LANDSCAPE PLAN
- C1 MASTER DRAINAGE AND GRADING PLAN
- C2 MASTER UTILITIES PLAN
- E1 PHOTOMETRIC SITE PLAN LIGHT FIXTURES

**PROPERTY OWNER**  
SOUTH BOULDER ROAD CORP.  
BOB BRISNEHAN, PRESIDENT  
913 MAIN STREET  
LOUISVILLE, CO 80027  
303-666-5005

**APPLICANT/TENANT/  
BUILDING OWNER**  
LEHRER'S FLOWERS  
DAVID BYERLY, PRESIDENT  
3191 WEST 38TH AVENUE  
DENVER, CO 80211  
303-433-6892

**GENERAL CONTRACTOR**  
WILLIAMS CONSTRUCTION CORPORATION  
965 S. KIPLING PARKWAY  
LAKEWOOD, CO 80226  
303-989-9009

**ARCHITECTS**  
ROBERT CHISHOLM ARCHITECTS, INC.  
1108 WASHINGTON AVENUE  
GOLDEN, CO 80401  
303-277-9390

**LANDSCAPE ARCHITECTS**  
ARNOLD ASSOCIATES  
5414 ALLISON STREET  
ARVADA, CO 80002  
303-423-1968

**CIVIL ENGINEERS**  
HURST AND ASSOCIATES, INC.  
4999 PEARL EAST CIRCLE, SUITE 106  
BOULDER, CO 80301  
303-449-9105

**ELECTRICAL ENGINEERS**  
MICHAEL J. HUTCHINSON ASSOCIATES, INC.  
1737 CENTRAL STREET  
DENVER, CO 80211  
303-480-5144

DRAWING NUMBER: LEHRER'S FLOWERS PLANNED UNIT DEVELOPMENT - LOT 5, LSVL, PLZ, FILING #2  
DRAWING NUMBER: LEHRER'S FLOWERS PLANNED UNIT DEVELOPMENT - LOT 5, LSVL, PLZ, FILING #2  
DRAWING NUMBER: LEHRER'S FLOWERS PLANNED UNIT DEVELOPMENT - LOT 5, LSVL, PLZ, FILING #2  
LEHRER'S FLOWERS  
LOT 5, LSVL, PLZ, FILING #2  
SEPT. 2000 1 OF 9







### LEHRER'S FLOWERS

TYPICAL WALL MOUNTED SIGN 1/4" = 1'-0"  
 NOTE: STYLE OF LETTERS MAY DIFFER FROM THAT SHOWN AND MAY VARY FROM TENANT TO TENANT

#### WALL MOUNTED SIGN SCHEDULE

- A MAXIMUM SIZE: 1'-6" HIGH x 19'-0" LONG  
 AREA: 28.5 S.F.  
 QUANTITY: 1
- B MAXIMUM SIZE: 1'-6" HIGH x 22'-0" LONG  
 AREA: 33 S.F.  
 QUANTITY: 1
- C MAXIMUM SIZE: 1'-6" HIGH x 20'-0" LONG  
 AREA: 30 S.F.  
 QUANTITY: 5
- D MAXIMUM SIZE: 1'-6" HIGH x 19'-0" LONG  
 AREA: 28.5 S.F.  
 QUANTITY: 1
- E MAXIMUM SIZE: 1'-6" HIGH x 26'-0" LONG  
 AREA: 39 S.F.  
 QUANTITY: 1

#### WALL MOUNTED SIGN NOTES

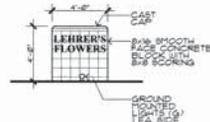
1. TEMPORARY BANNERS WILL NOT BE ALLOWED.
2. NO WINDOW SIGNS SHALL BE VISIBLE FROM S. BOULDER RD.
3. GRAPHICAL LOGOS ARE LIMITED TO 20% OF THE SIGN AREA.

#### WALL MOUNTED SIGN SPECIFICATIONS

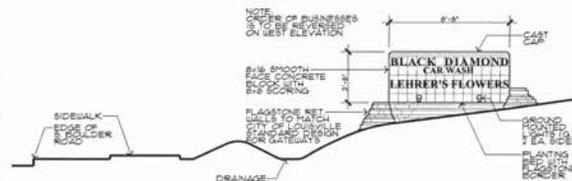
INDIVIDUAL "CHANNEL" LETTERS  
 MIN. 20GA. METAL RETURNS AND BACKS  
 5" DEPTH  
 INTERNAL NEON ILLUMINATION  
 TRANSLUCENT ACRYLIC FACES

WALL MOUNTED SIGNS

NOTE: STYLE OF LETTERS MAY DIFFER FROM THAT SHOWN.



TYP. ELEVATION HECLA WAY SIGN 1/4" = 1'-0"



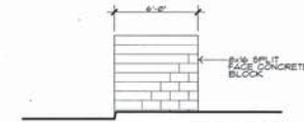
EAST ELEVATION S. BOULDER ROAD SIGN 1/4" = 1'-0"

#### MONUMENT SIGN SPECIFICATIONS

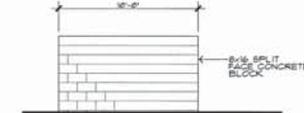
SOUTH BOULDER ROAD SIGN AREA: 32 S.F.  
 HECLA WAY SIGN AREA: 16 S.F.

LETTERS:  
 INDIVIDUAL "CHANNEL" LETTERS  
 MIN. 20GA. METAL FRONTS, BACKS, AND RETURNS  
 7" DEPTH  
 EXTERNAL ILLUMINATION

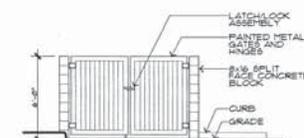
MONUMENT SIGNS



TYPICAL SIDE



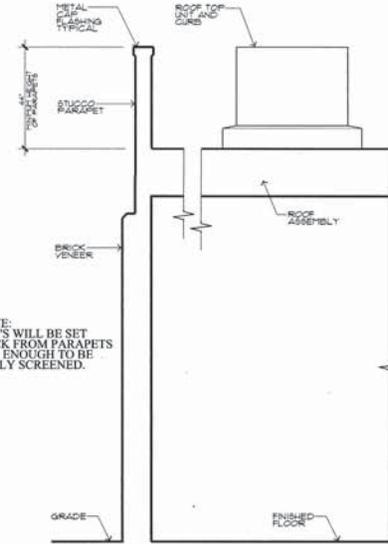
REAR



FRONT

### DUMPSTER ENCLOSURE

1/4" = 1'-0"



NOTE: RTU'S WILL BE SET BACK FROM PARAPETS FAR ENOUGH TO BE FULLY SCREENED.

A4 RTU SCREENING

1/2" = 1'-0"

ROBERT CHISHOLM ARCHITECTS  
 1 - 1 - 0 - 8 WASHINGTON AVE  
 GOLDEN CO 80401  
 TEL 303 277 8990  
 FAX 303 277 0987  
 E.M. CHISHOLM  
 USWEST NET

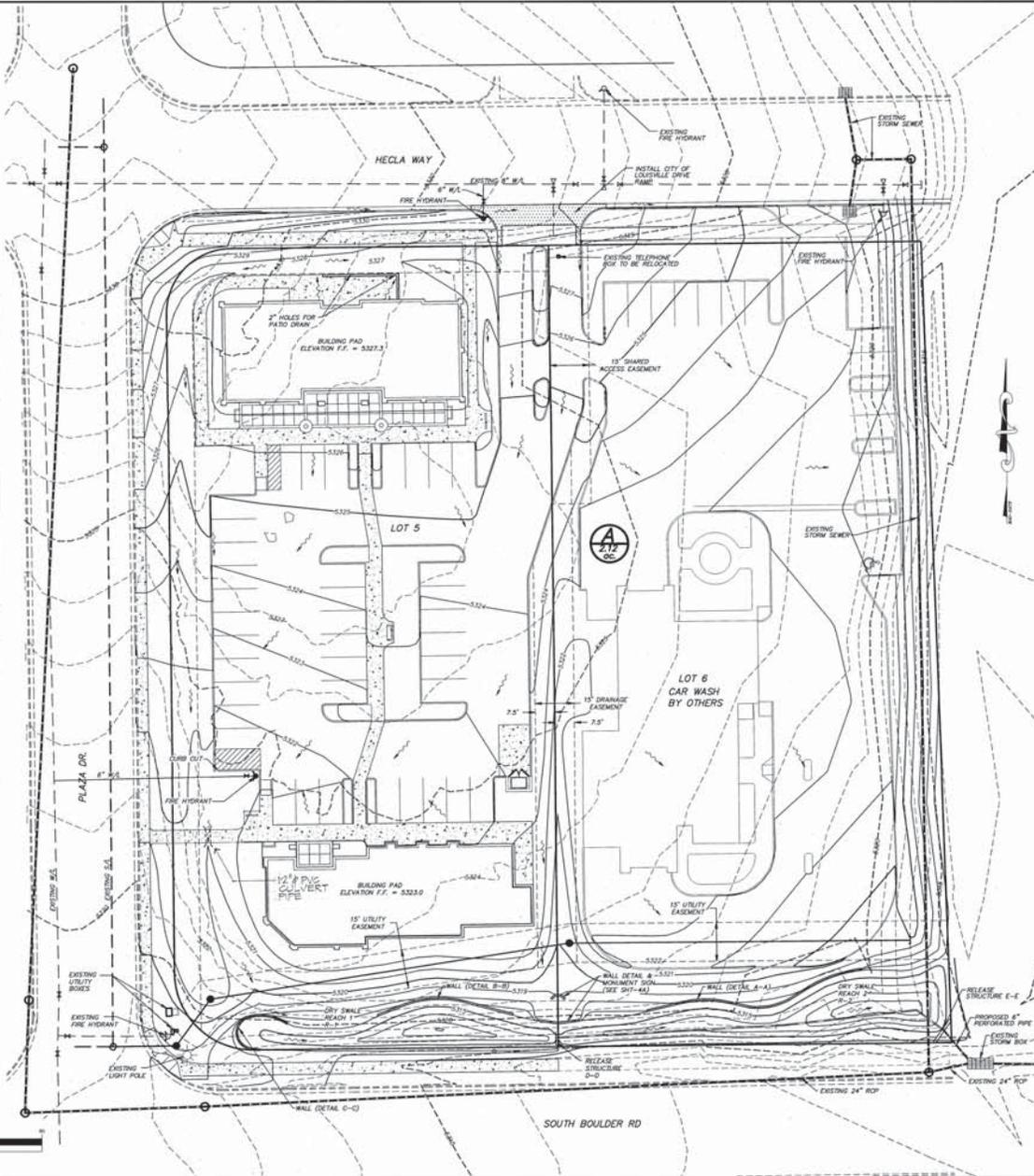
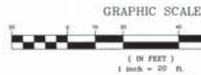
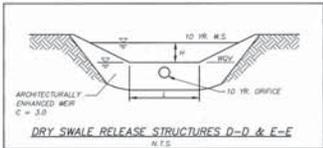
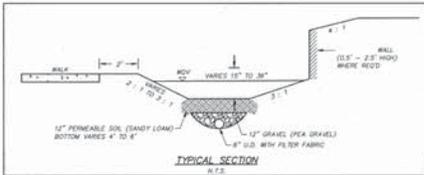
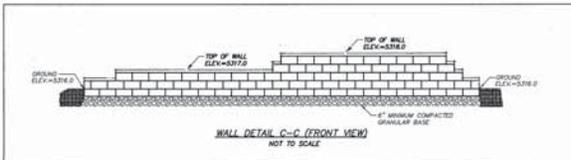
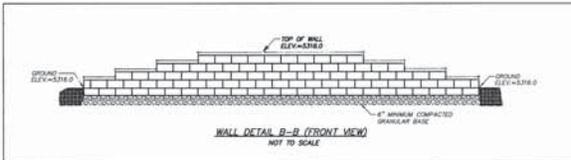
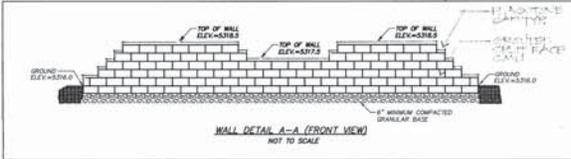
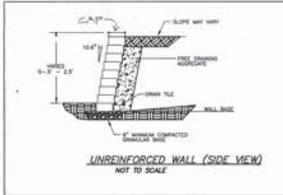
LEHRER'S FLOWERS  
 LOT 5, LOUISVILLE PLAZA FILING 2  
 LOUISVILLE, COLORADO

DETAILS  
 SIGN CRITERIA  
 SEP. 20, 2000  
 SCALE NOTED

A4

**LEGEND**

- Manhole
- Existing Manhole
- 5' Type 78' Inlet
- ▨ 10' Type 78' Inlet
- Storm Pipe
- - - Existing Storm Pipe
- ~ Existing Contours
- ~ Proposed Contours
- · - · - Basin Boundary
- Flow Arrow
- ⊙ Basin Designation



**LEHRER'S FLOWERS & ASSOCIATES, INC.**  
 CONSULTING ENGINEER  
 1000 14TH AVENUE, SUITE 100  
 BOULDER, COLORADO 80502  
 PHONE: 303.440.1100  
 FAX: 303.440.1101

**LEHRER'S FLOWERS  
 LOT 5, LOUISVILLE PLAZA FILING 2  
 LOUISVILLE, COLORADO**

SEP. 20, 2000  
 MASTER DRAINAGE & GRADING PLAN  
 C1

DRAWING NUMBER  
 DRAWING NUMBER  
 DRAWING NUMBER  
 LEHRER'S FLOWERS  
 LOT 5, LEVEL PLZ. FILING # 2  
 5 OF 9

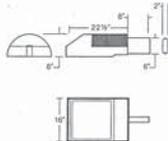


**POLE LIGHT (P)**

KIM LIGHTING AR ARCHETYPE

DESCRIPTION: ONE PIECE DIE-CAST ALUMINUM WITH POWDER COAT PAINT.  
 CLEAR FLAT TEMPERED GLASS IN ONE-PIECE DIE-CAST ALUMINUM LENS FRAME.  
 MOUNTING: ALUMINUM SUPPORT ARM WITH INTERNAL DRAW BOLTS. 2" SQUARE BRONZE POLE ON CONCRETE BASE (SEE DETAIL)  
 LAMP: ONE 250W METAL HALIDE  
 COLOR: DARK BRONZE

QUANTITY: 5

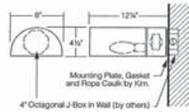


**WALL MOUNTED FIXTURE (W)**

KIM LIGHTING SW1 SITE WALLFORMS

DESCRIPTION: ONE PIECE CAST ALUMINUM WITH POWDER COAT PAINT.  
 CLEAR FLAT TEMPERED GLASS.  
 MOUNTING: CONCEALED BOLT ATTACHMENT TO WALL.  
 LAMP: ONE 50W METAL HALIDE  
 COLOR: DARK BRONZE

QUANTITY: 12



**GROUND MOUNTED FIXTURE (G)**

KIM LIGHTING EL205 SQUARE HOOD

DESCRIPTION: DIE-CAST ALUMINUM WITH POWDER COAT PAINT.  
 CONVEX CLEAR TEMPERED GLASS.  
 MOUNTING: PORTABLE SPEAR MOUNT.  
 LAMP: ONE 50W PAR 50 HALOGEN.  
 COLOR: BLACK

QUANTITY: 4

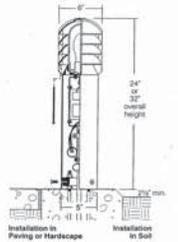


**BOLLARD (B)**

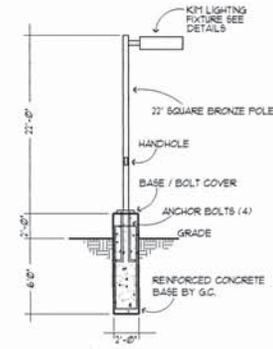
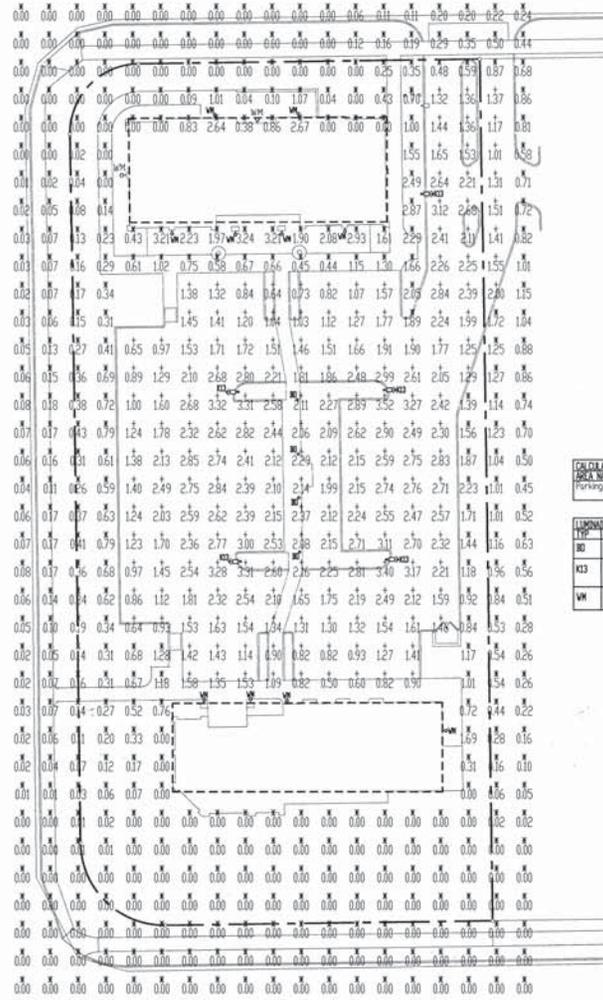
KIM LIGHTING CB COMPACT BOLLARD

DESCRIPTION: INTERLOCKING DIE-CAST ALUMINUM SECTIONS WITH CONCEALED STAINLESS STEEL SCREWS. POWDER COAT PAINT.  
 MOUNTING: ANCHOR BOLTS IMBEDDED IN A POURED-IN-PLACE CONCRETE FOOTING.  
 LAMP: ONE 50W METAL HALIDE  
 COLOR: DARK BRONZE

QUANTITY: 4



**LIGHT FIXTURE DETAILS**



**POLE BASE DETAIL**  
NOT TO SCALE

CALCULATION SUMMARY:			
AREA NAME	DIMENSIONS	GRID MARK	AVG. FOOT C.
Parking	100' x 100'	100' x 100'	0.3

LUMINAIRE SCHEDULE							
ID	SYMBOL	DESCRIPTION	LAMP	LUMENS	MOUNTING/HALLAST	HT.	QTY
B0	○	Kim EL205/SPEAR/BL/7' 0" LAMP	(1) 50W/MEH	3500	STD	0.78	4
B3	□	Kim LIGHTING 250W/III (1) 2" 483/250W/III	(1) MS-250/MEH	23000	22" 505 Z concrete	8.68	5
WN	◊	Progress P5641-3L	(1) SFP4820	1050	N/A	0.74	10



**PHOTOMETRIC SITE PLAN**  
1" = 20'

ROBERT CHISHOLM ARCHITECTS

1 · 1 · 0 · 8  
 WASHINGTON AVE  
 GOLDEN CO 80401  
 PH. 303 277 9390  
 FAX 303 277 0967  
 E-MAIL CHISHOLM@USWEST.NET

LEHRER'S FLOWERS  
 LOT 5, LEVEL, PLZ., FILING # 2  
 LOUISVILLE, COLORADO

DRAWING NUMBER

LEHRER'S FLOWERS  
 LOT 5, LEVEL, PLZ., FILING # 2  
 8 OF 9

DRAWING NUMBER

PHOTOMETRIC SITE PLAN  
 LIGHT FIXTURE DETAILS

SCALE NOTED

**E1**



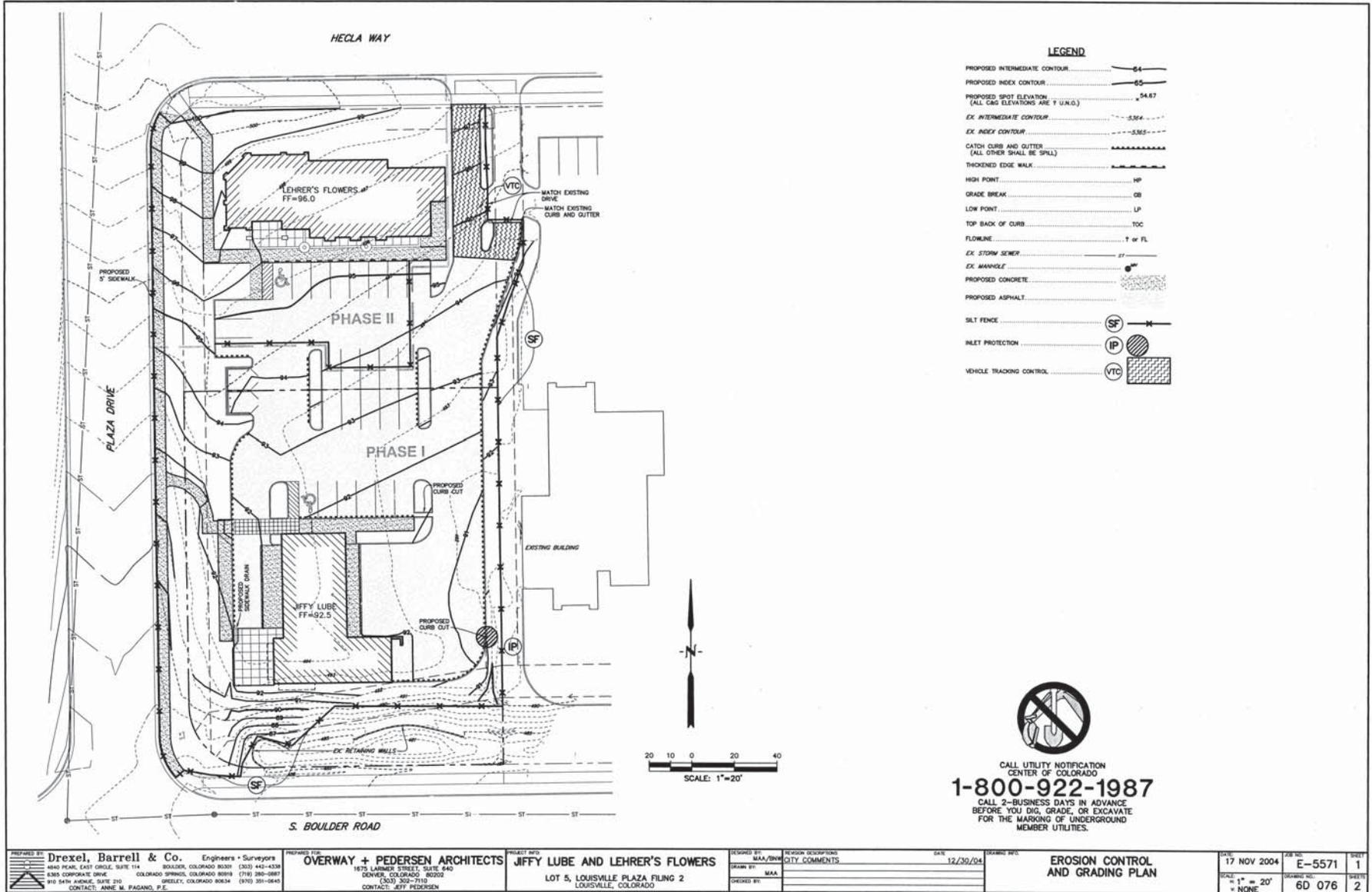


Lot 5  
Louisville Plaza  
Filing 2

DRAWING NUMBER

DRAWING NUMBER

Jiffy Lube &  
Lehrer's Flowers



CALL UTILITY NOTIFICATION  
CENTER OF COLORADO  
**1-800-922-1987**  
CALL 7-BUSINESS DAYS IN ADVANCE  
BEFORE YOU DIG, GRADE, OR EXCAVATE  
FOR THE MARKING OF UNDERGROUND  
MEMBER UTILITIES.

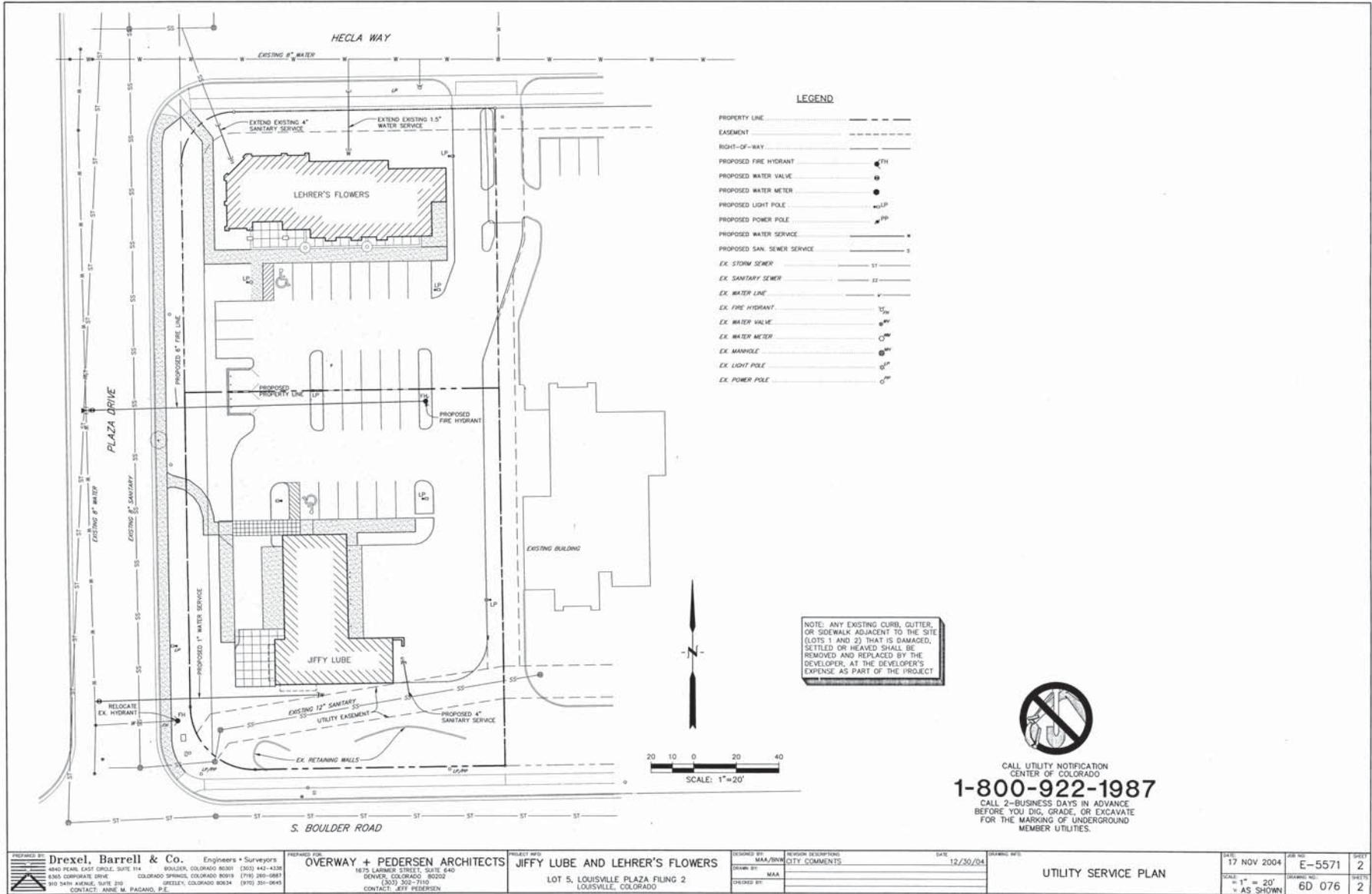
PREPARED BY: <b>Drexel, Barrell &amp; Co.</b> Engineers • Surveyors 4840 PEAR, EAST CIRCLE, SUITE 114 BOULDER, COLORADO 80501 (303) 442-4338 4340 CORPORATE DRIVE COLORADO SPRING, COLORADO 80918 (719) 260-0887 710 54TH AVENUE, SUITE 210 GREELEY, COLORADO 80634 (970) 351-0845 CONTACT: ANNE M. PAGANO, P.E.	PROJECT FOR: <b>OVERWAY + PEDERSEN ARCHITECTS</b> 1675 LARNER STREET, SUITE 640 DENVER, COLORADO 80202 CONTACT: JEFF PEDERSEN	PROJECT INFO: <b>JIFFY LUBE AND LEHRER'S FLOWERS</b> LOT 5, LOUISVILLE PLAZA FILING 2 LOUISVILLE, COLORADO	DESIGNED BY: MAA/REV/C CHECKED BY: MAA	REVIEW OCCUPATION: CITY COMMENTS:	DATE: 12/30/04	DRAWING NO.: <b>EROSION CONTROL AND GRADING PLAN</b>	DATE: 17 NOV 2004 SCALE: 1" = 20' = NONE	JOB NO.: E-5571 SHEET: 60 076 TOTAL SHEETS: 2
			DRAWN BY: MAA	DATE:	SHEET NO.:			

Lot 5  
Louisville Plaza  
Filing 2

DRAWING NUMBER

DRAWING NUMBER

Jiffy Lube &  
Lehrer's Flowers



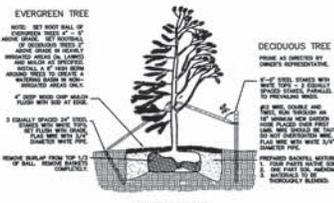
**NOTES:**

- THE CONTRACTOR IS TO FURNISH AND INSTALL PLANT MATERIALS AS NOTES. ALL PLANT MATERIAL MUST EQUAL OR EXCEED THE STANDARDS CURRENTLY REQUIRED BY THE AMERICAN ASSOCIATION OF NURSERYMEN. ALL MATERIALS SHALL BE NO. 1 GRADE, NURSERY GROWN STOCK. COLLECTED MATERIALS SHALL NOT BE USED UNLESS APPROVED BY THE OWNER'S REPRESENTATIVE.
- THE CONTRACTOR IS TO STAKE THE LAYOUT OF THE PLANT MATERIALS PRIOR TO PLANTING AND HAVE SUCH LAYOUT APPROVED BY THE OWNER'S REPRESENTATIVE. THE CONTRACTOR SHALL VERIFY FIELD CONDITIONS AND NOTIFY THE OWNER'S REPRESENTATIVE OF ANY DISCREPANCIES PRIOR TO PROCEEDING WITH THE WORK.
- CONTRACTOR TO REPAIR OR REPLACE ANY SOIL, MULCH OR SITE IMPROVEMENTS DISTURBED OR DAMAGED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR TO PROVIDE FOR PROTECTION OF ALL UTILITIES AND APPEARANCES PRIVATE OR PUBLIC PROPERTY, AND PUBLIC SAFETY FOR THE DURATION OF THE CONTRACT AND SHALL REPAIR OR REPLACE ANY DAMAGES TO SAME WITHOUT ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL SECURE AND PAY ALL APPLICABLE TAXES, PERMITS, FEES AND COSTS IN CONNECTION WITH THE WORK UNDER THE CONTRACT.
- CONTRACTOR SHALL KEEP THE PREMISES AND PUBLIC RIGHTS OF WAY, CLEAN AND ORDERLY ON DAILY BASIS AND SHALL DISPOSE OF ALL WASTE AND DEBRIS AT AN APPROVED LOCATION OFF-SITE PRIOR TO FINAL ACCEPTANCE.
- CONTRACTOR TO MAINTAIN ALL PLANTINGS, MULCHES, GRADES, SURFACES AND IRRIGATION SYSTEM COMPONENTS, INSTALLED UNDER THIS CONTRACT UNTIL FINAL ACCEPTANCE AND TURNOVER TO OWNER.
- ALL PLANT MATERIAL SHALL BE GUARANTEED FOR ONE FULL YEAR. ANY PLANT MATERIAL THAT IS IN QUESTIONABLE CONDITION SHALL RECEIVE AN EXTENDED GUARANTEE OF THE OWNER'S REPRESENTATIVE. ALL GUARANTEES ON PLANTS ARE 100% INCLUDING MATERIALS, LABOR AND COSTS BASED UPON A ONE-TIME REPLACEMENT.
- PRIOR TO ANY PLANTING OPERATIONS, ALL PLANTING AREAS SHALL BE PREPARED WITH 3 CUBIC YARDS/1,000 SQUARE FEET SCREENED WELL ROTTED AND COMPOSTED MANURE FORTIFIED WITH A MINIMUM OF 4" DEEP INTO THE SOIL PROFILE. CONTRACTOR SHALL PROVIDE DELIVERY/JOB TICKETS VERIFYING QUANTITY AND TYPE OF MATERIAL UTILIZED IN THE WORK FOR THE OWNER'S REPRESENTATIVE'S REVIEW AND APPROVAL.
- ALL TREES AND SHRUBS ARE TO BE PLANTED AFTER ROUGH GRADING AND PRIOR TO PLACING OF SOIL COVER MATERIAL. ALL FRESH GRADES OF SURFACES AND MATERIALS SHALL BE ONE (1") INCH BELOW SURROUNDING HANDSCAPE FINISH GRADES (I.E. WALKWAYS, EDGINGS, CURBS, ETC.)
- SOIL SHALL BE A BLEND OF A VARIETIES OF NITROGEN BLUEGRASS AND FESCUE APPROVED BY THE OWNER'S REPRESENTATIVE. THE SOIL SHALL BE LAD ON A FIRM PREPARED BED WITH NIGHT SHIMS AND NO VOIDS BETWEEN STRIPS. AREAS SHALL BE SETTLED AND FINE GRADED TO A SMOOTH, UNIFORM, FIRM SURFACE LEAVING NO DEPRESSIONS WHERE WATER MAY COLLECT (NO 10'). ALL DEBRIS, VEGETATION, OR ROCKS ECT. OVER 1" IN DIAMETER SHALL BE REMOVED BY THE CONTRACTOR AND PROPERLY DISPOSED OF OFF-SITE.
- PLANTING BED STEEL EDGE TO BE RYONSON STEEL EDGE, 1/8"x4"x10", OR EQUAL, AS APPROVED BY THE OWNER'S REPRESENTATIVE.
- ALL PLANTING BEDS TO BE MULCHED WITH CEDAR WOOD CHIP MULCH OVER GEOTEXTILE FABRIC AS SPECIFIED ON THE PLAN. SUBMIT SAMPLE TO OWNER'S REPRESENTATIVE FOR APPROVAL. MULCH ALL PLANTING BEDS AT A MINIMUM DEPTH OF FOUR (4") INCHES.
- ALL TREES PLANTED IN LAWN SHALL BE MULCHED WITH CEDAR WOOD CHIP MULCH TO A DEPTH OF FOUR (4") INCHES. CONTRACTOR TO SUBMIT SAMPLE FOR OWNER'S REPRESENTATIVE. RING TO BE 4" DIAMETER.
- PLACE GEOTEXTILE FABRIC (MIRAFI #140-S OR EQUAL) UNDER ALL MULCH IN ALL PLANTING BEDS.
- ALL PLANT AREAS SHALL BE WATERED BY AN AUTOMATIC IRRIGATION SYSTEM.
- ALL HEADS TO BE ADJUSTED TO PREVENT OVERTSPRAY ONTO BUILDINGS AND WALKWAYS.
- BACKFLOW PREVENTION DEVICE SHALL BE IN ACCORDANCE WITH CITY OR COUNTY SPECIFICATIONS.
- ALL PLANTED TREES TO MAINTAIN 10' MINIMUM CLEARANCE FROM SEWER AND WATER LINES, 4' FROM GAS LINES. ALL PLANTED SHRUBS TO MAINTAIN 4' MINIMUM CLEARANCE FROM SEWER, WATER AND GAS LINES. (TO PREVENT CONFLICTS WITH EXISTING UTILITIES, STREET TREE QUANTITIES AND LOCATIONS HAVE BEEN LIMITED TO THE AREAS SHOWN ON THE PLAN.) SEED MIX SHALL BE AS INDICATED IN TABLE. BROADCAST SEED IN TWO EQUAL APPLICATIONS, IN PERPENDICULAR DIRECTIONS. LIGHTLY RAKE AFTER SEED IS BROADCAST IN A DIRECTION PERPENDICULAR TO THE SLOPE OF THE LAND. HYDROSEAL SEED AREAS AT A RATE OF 2,000 LBS./ACRE.

**PLANT SCHEDULE:**

ABB.	CITY	COMMON NAME	WATER USE/ACC.	MAINT. REQUIRE.	MATURE SPREAD	SIZE
<b>DECIDUOUS SHADE TREES</b>						
ACPL	3	FARGUES MAPLE	(0)	(3)	45'-50'	2-1/2" BAB
CEOC	4	WESTERN HAZELBERRY	(M-1)	(3)	45'-50'	2-1/2" BAB
GLSK	4	SKYLINE LOGST	(3)	(3)	25'	2-1/2" BAB
GLSH	4	SHADOMASTER LOGST	(3)	(3)	25'	2-1/2" BAB
TCO	4	GREENSPICE LINEN	(0)	(3)	25'-30'	2-1/2" BAB
QUMA	3	BURR OAK	(3)	(3)	30'-40'	2-1/2" BAB
<b>ORNAMENTAL TREES</b>						
ORR	1	SHORNLESS COOKSPUR HAWTHORN	(M-1)	(3)	25'-30'	2" BAB
ORPH	4	WINDSOR HAZELBERRY	(M-1)	(3)	15'-20'	2" BAB
PRPA	1	MARSHY TREE	(M-1)	(3)	20'	2" BAB
PRV	3	CANADA RED CHERRYHONEY	(M-1)	(3)	15'-20'	6" CLUMP BAB
MASS	2	SPRING SHAW CRAB	(M-1)	(0)	15'	2" BAB
<b>EVERGREEN TREES</b>						
PEL	2	PIGON PINE	(3)	(3)	10'-15'	6" BAB
PEL	3	VANDERWOLF'S PYRAMID PINE	(3)	(3)	10'	6" BAB
<b>DECIDUOUS SHRUBS</b>						
CHNA	28	DWARF BLUE RABBITBRUSH	(3)	(0)	1'-4"	#5 Cont.
FEAT	20	BESMAN SAGE	(3)	(0)	4'-6"	#5 Cont.
PRPB	19	ORIENTAL WESTERN SMOKE CHERRY	(3)	(0)	4'-6"	#5 Cont.
RHTR	19	THREE LEAF SUMAC	(3)	(0)	6'-8"	#5 Cont.
RHTY	5	STAGHORN SUMAC	(3)	(0)	6'	#5 Cont.
RAL	17	ALPINE CURSANT	(1-0)	(0)	3'-6"	#5 Cont.
COHO	13	ROCK GEMSEATER	(M-1)	(0)	3'-6"	#5 Cont.
ROME	8	DOUBLE RED SHRUB ROSE	(1-0)	(0)	3'-4"	#5 Cont.
SYDH	58	HARCOCK CORALBERRY	(M-1)	(3)	3'-6"	#5 Cont.
POFR	12	SUTTER'S GOLD POTENTILLA	(M-1)	(0)	2'-3"	#5 Cont.
HODU	14	HALDORIC DUNN	(M-1)	(3)	3'-6"	#5 Cont.
<b>GRASSES</b>						
CAOC	13	Catanagrisis quadrifida 'Var. Fortner'	(3)		2'-3"	1 GAL.
BOOR	167	FEATHER REED GRASS	(3)		1'-2"	1 GAL.
<b>ESSENTIALS</b>						
LAWN	81	ENGLISH LAWN			12"-18"	1 GAL.
EDR	88	MATHE BLANKET FLOWER			18"-24"	1 GAL.
ESPU	60	RUBY STAR CONFDOMER			18"-24"	1 GAL.

\*\*PLANT CHOSEN FROM THE LOUISVILLE COMMERCIAL DESIGN GUIDELINES RECOMMENDED PLANT LIST



**2 TREE PLANTING DETAIL**  
1/2" TO 3/4" TO 1"

**3 SHRUB PLANTING DETAIL**  
1/2" TO 3/4" TO 1"

**TABLE 1: REQUIRED LANDSCAPE AREA CALCULATIONS**

NET SITE AREA	54,451 SF
REQUIRED LANDSCAPE AREA (X30)	16,335 SF
PROVIDED LANDSCAPING PERIMETER / OTHER PARKING LOT	25,208 SF / 709 SF
TOTAL PROVIDED LANDSCAPING	25,917 SF (48%)

**TABLE 2: PERIMETER PLANTING CALCULATIONS**

LENGTH L.F.	REQUIRED PLANTINGS	PROVIDED PLANTINGS
STREET EDGE NORTH	4 TREES & 32 SHRUBS	4 TREES & 19 SHRUBS
STREET EDGE SOUTH	4 TREES & 32 SHRUBS	8 TREES, 48 SHRUBS & 102 GRASSES
STREET EDGE WEST	17 TREES & 85 SHRUBS	12 TREES & 41 SHRUBS
EAST BUFFER	9 TREES	11 TREES & 22 SHRUBS
TOTAL	34 TREES & 149 SHRUBS	33 TREES, 130 SHRUBS & 102 GRASSES

**TABLE 3: BUILDING PERIMETER PLANTING CALCULATIONS**

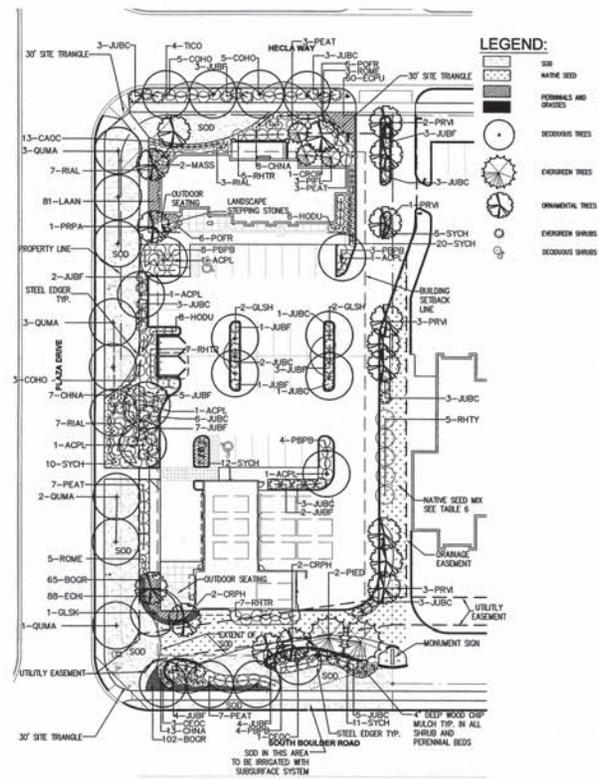
BUILDING	TOTAL PERIMETER (L.F.)	REQUIRED PLANTINGS (25%)	PROVIDED PLANTING
LEHRER'S	274 L.F.	69 L.F.	191 L.F. (70%)
JIFFY LUBE	264 L.F.	66 L.F.	123 L.F. (47%)

**TABLE 4: PARKING LOT AND SCREENING PLANTING CALCULATIONS**

PARKING	33 SPACES	REQUIRED PLANTINGS (1 TREE PER 6 SPACES = 5 TREES)	PROVIDED PLANTINGS (8 TREES & 53 SHRUBS)
DUMPSTER	DOUBLE	PER REVIEW	12 SHRUBS

**TABLE 5: AREA CALCULATION TOTALS**

AREA DESCRIPTION	REQUIRED PLANTINGS	PROVIDED PLANTINGS
PARKING AND SCREENING	5 TREES	8 TREES & 65 SHRUBS
PERIMETER	34 TREES & 149 SHRUBS	33 TREES, 130 SHRUBS & 102 GRASSES
BUILDING PERIMETER	PER REVIEW	9 TREES, 89 SHRUBS, 76 GRASSES & 229 PERENNIALS
TOTALS	50 TREES, 284 SHRUBS, 180 GRASSES & 229 PERENNIALS	



**1 LANDSCAPE PLAN**  
1/1.0 SCALE: 1" = 30'-0"

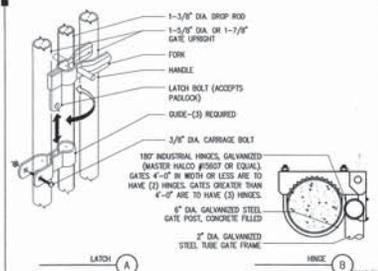
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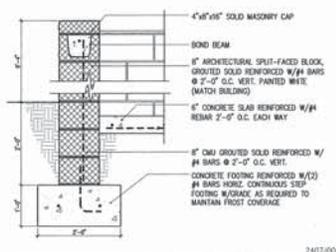
**O + P**  
DESIGN CONCEPTS  
Landscape Architects  
28 North Public Road, Suite 209 • Lafayette, Colorado 80026  
(303) 844-0201 • FAX (303) 844-0203 • concept@ocdesign.com

**FINAL PLS & DEV PLAN** - SAME SCALE CONDITIONS  
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NO. 03.000 2000  
DATE 28.000 2000  
2410  
**L1.0**  
**LANDSCAPE PLAN**

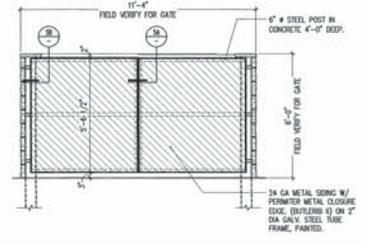
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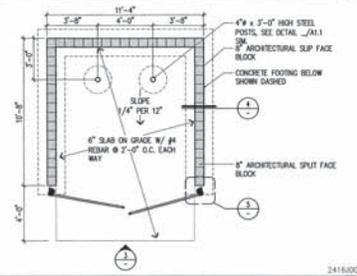
INDUSTRIAL LATCH + HINGE 1/2"=1'-0" 5



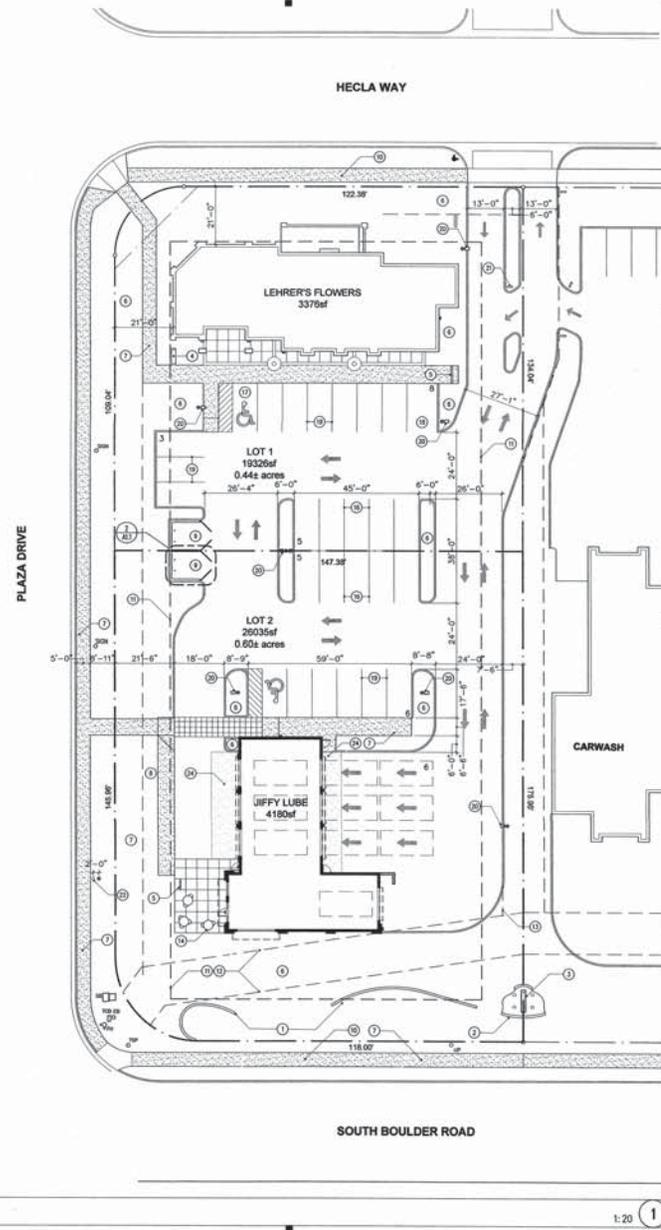
SECTION AT TRASH ENCLOSURE 3/4"=1'-0" 4



TRASH ENCLOSURE ELEVATION 3/8"=1'-0" 3



PLAN AT TRASH ENCLOSURE 1/4"=1'-0" 2



SITE PLAN 1:20 1

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- KEYNOTES:
1. GROUNDED SPLIT FACE CMU RETAINING WALL WITH FLAGSTONE CAP
  2. FLAGSTONE RETAINING WALL
  3. MONUMENT SIGN
  4. BENCH
  5. BIKE RACK
  6. LANDSCAPED AREA
  7. NEW 5'-0" SIDEWALK
  8. 6" PLANTER
  9. COMPUTER ENCLOSURE
  10. EXISTING SIDEWALK
  11. BUILDING SETBACK LINE
  12. UTILITY EASEMENT
  13. DRAINAGE EASEMENT
  14. OUTDOOR SEATING
  15. METAL CANOPY
  16. STANDARD PARKING SPACE, 9'-0"x 19'-0"
  17. ACCESSIBLE PARKING SPACE, 14'-0"x 17'-6"
  18. DELIVERY VAN PARKING SPACE, 10'-0"x 17'-8"
  19. PARKING SPACE, 9'-0"x 17'-6"
  20. 20'-0" LIGHT POLE
  21. NO LEFT TURN SIGN
  22. EXISTING STREET LIGHT, RELOCATED BY TENANT
  23. PARALLEL PARKING SPACE, 10'-0"x 22'-0"
  24. BLACK CONC. APRON 10'-0" WIDE @ FRONT/ 7'-0" AT REAR

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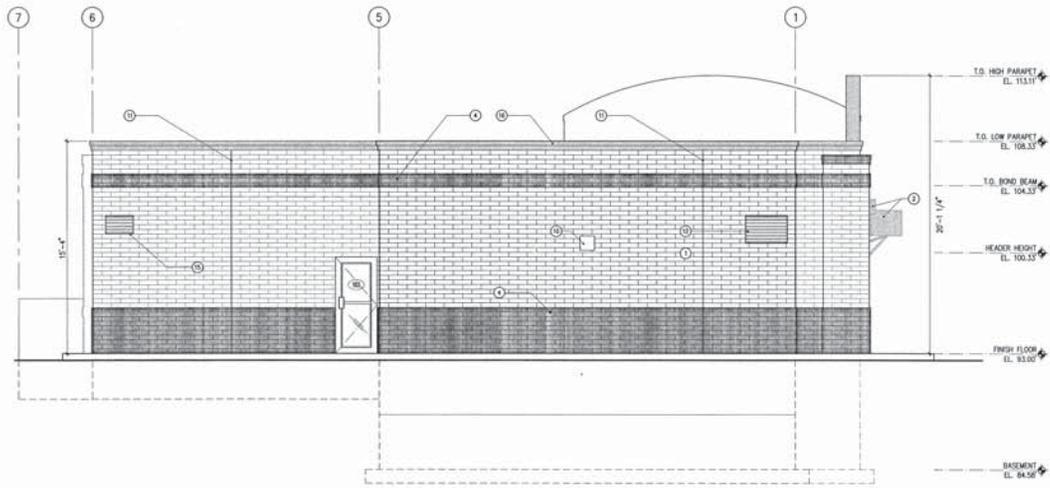
CAROLO #1 LTD.  
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REGISTERED ARCHITECT  
SINCE 1983  
STATE OF COLORADO

O + P  
OVERMYER + PEDERSEN ARCHITECTS, LLP  
NO. B-1983

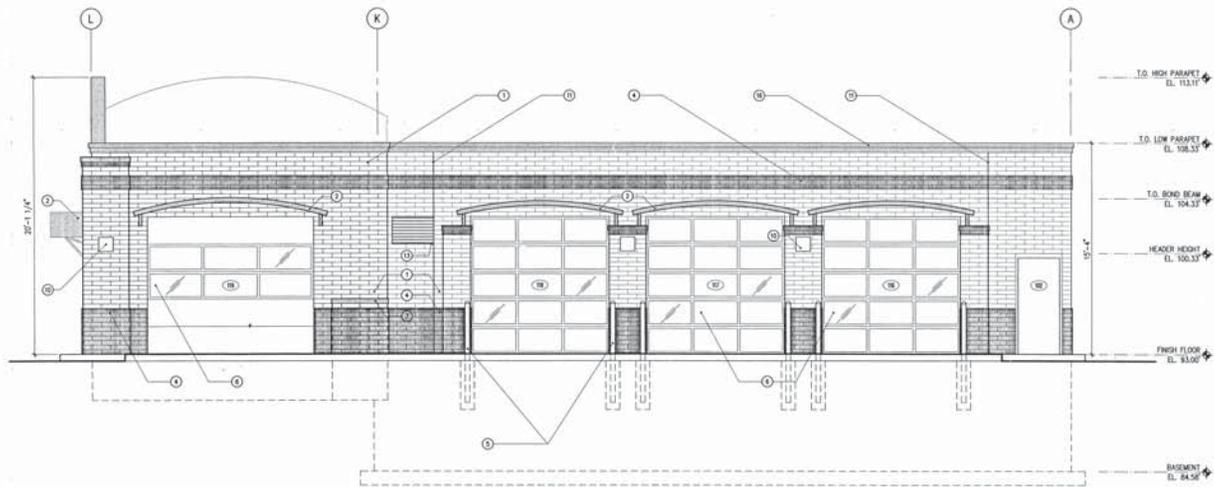
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20	2000	2000	2000	2000	2000	2000
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**SITE PLAN/  
SITE DETAILS**



NORTH ELEVATION

1/4"=1'-0" 2



EAST ELEVATION

1/4"=1'-0" 1

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KEYNOTES

1. SMOOTH-FACED ARCHITECTURAL MASONRY
2. METAL CANOPY
3. NOT USED
4. SPLIT-FACED ARCHITECTURAL MASONRY
5. STEEL ROLLS
6. ALUMINUM & TINTED GLASS SECTIONAL GLZ. DOOR
7. METAL CAP
8. 1" INSULATED, TINTED GLASS IN ALUM. SPOREMENT
9. 1" TEMP. INSULATED, TINTED GLASS IN ALUM. SPOREMENT
10. WALL MOUNTED FULL CUTOFF LIGHT FIXTURE, SEE DETAIL 2/A31
11. MASONRY CONTROL JOINTS
12. 36"x24" EXHAUST LOUVER
13. 36"x24" MAKE UP AIR LOUVER
14. 24"x18" EXHAUST LOUVER
15. 24"x18" MAKE UP AIR LOUVER
16. DOUBLE-STEPPED METAL CAP

GENERAL NOTE:

ALL MECHANICAL EQUIPMENT IS LOCATED WITHIN THE BUILDING. NO ROOFTOP EQUIPMENT EXISTS.

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FINAL PLOT & DEV. PLAN PLAN COMM. CONDITIONS  
PROJ. 17-0004 PLAN COMMENTS 21 FEB 2005  
NO. 2416 PLAN COMMENTS  
2416 PLAN COMM. COMMENTS 28 JAN 2005

**JIFFY LUBE  
EXTERIOR ELEVATIONS**

A2.1

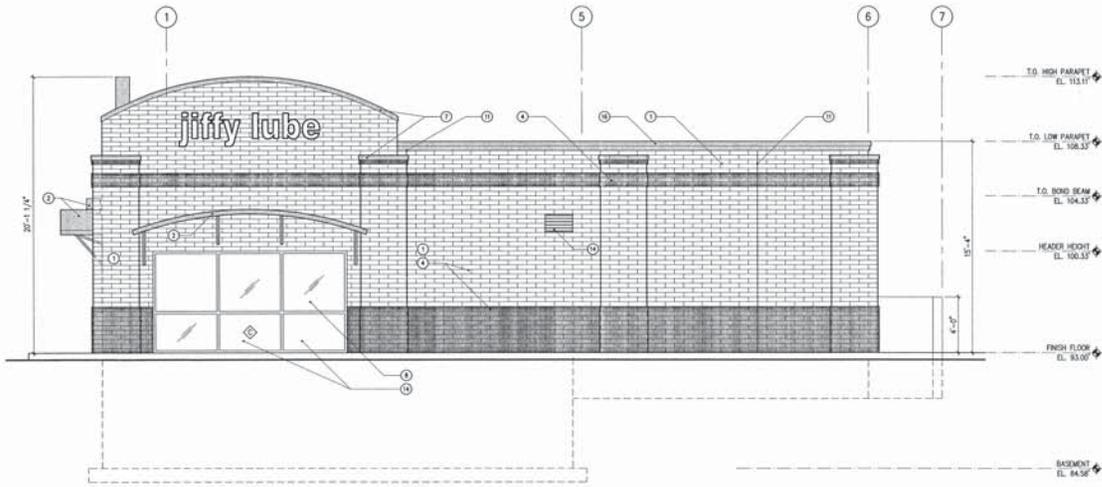
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DRAWING NUMBER

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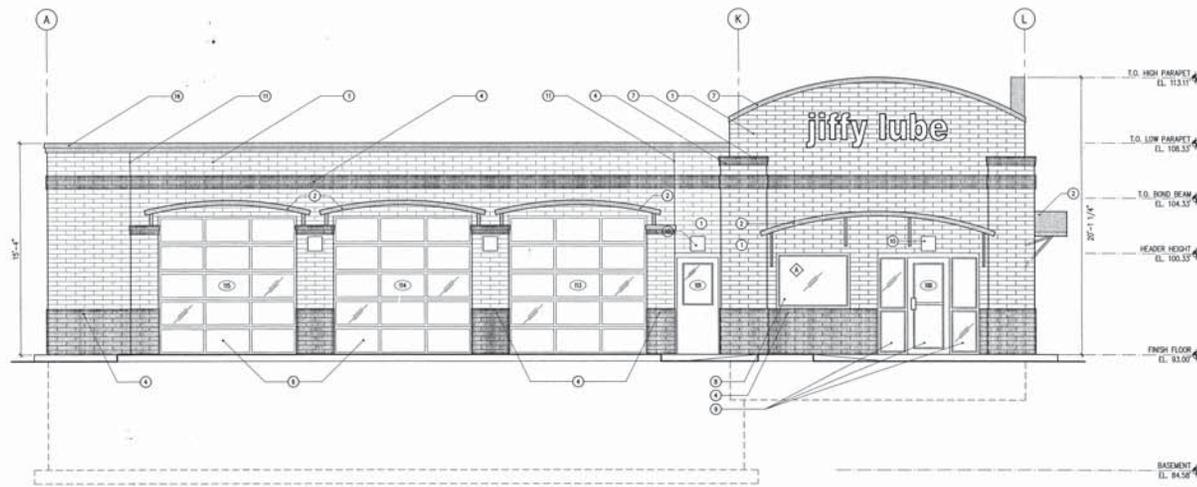
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SOUTH ELEVATION

1/4"=1'-0" 2



WEST ELEVATION

1/4"=1'-0" 1

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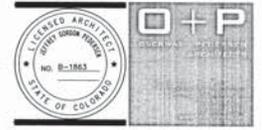
KEYNOTES

1. SMOOTH-FACED ARCHITECTURAL MASONRY
2. METAL CANOPY
3. NOT USED
4. SPIGOT-FACED ARCHITECTURAL MASONRY
5. STEEL BOLLARDS
6. ALUMINUM & TINTED GLASS SECTIONAL O.H. DOOR
7. METAL CAP
8. 1" INSULATED, TINTED GLASS IN ALUM. STOREFRONT
9. 1" TEMP. INSULATED, TINTED GLASS IN ALUM. STOREFRONT
10. WALL MOUNTED FULL CUTOFF LIGHT FIXTURE, SEE DETAIL 3/1A/11
11. MASONRY CONTROL JOINTS
12. 36"x24" EXHAUST LOUVER
13. 36"x24" MAKE UP AIR LOUVER
14. 24"x18" EXHAUST LOUVER
15. 24"x18" MAKE UP AIR LOUVER
16. DOUBLE-STEPPED METAL CAP

GENERAL NOTE:  
ALL MECHANICAL EQUIPMENT IS LOCATED WITHIN THE BUILDING, NO ROOFTOP EQUIPMENT EXISTS

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17. NOV. 2004	12. FEB. 2005
NO. 2416	28. JAN. 2005

**JIFFY LUBE  
EXTERIOR ELEVATIONS**

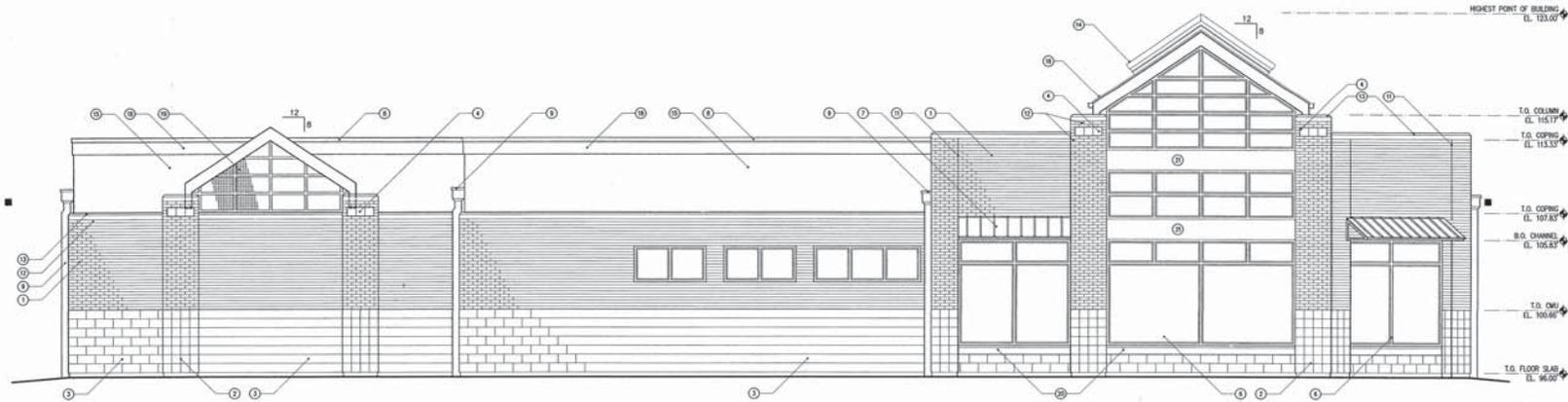
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KEYNOTES

1. STANDARD SIZE BRICK VENER
2. BAIS SMOOTH FACE CMU VENER WITH SCORING PATTERN
3. BAIS SPLIT FACE CMU VENER
4. BAIS CMU VENER AGENTS
5. STEEL CHANNELS, PAINTED
6. ALUMINUM STOREFRONT
7. FINISHED METAL ROOFING
8. FINISHED ALUMINUM COPING
9. SCUPPER & DOWNPOUT
10. WALL MOUNTED FULL GLOSS LIGHT FIXTURE, SEE DETAIL 4/A31
11. MASONRY CONTROL JOINTS
12. ACCENT BRICK BAND
13. CAST CONCRETE COPING
14. FINISHED ALUMINUM ROOF SCUPPER
15. EXT. INSULATION & FINISH SYSTEM (EIFS)
16. PROTECTED STANDING SEAM METAL ROOFING OVER PAINTED STEEL SUB-FRAME
17. BRICKING SIGNAGE
18. 1" DEEP RAISED BAND
19. PERFORATED METAL SCREEN, PAINTED
20. CAST CONCRETE SILL
21. STEEL RUBEL, PAINTED
22. AUTOMATIC SLIDING DOOR
23. 3'-4" HIGH BRICK WALL SPANNING BETWEEN COLUMNS



NORTH ELEVATION

1/4"=1'-0" 2



EAST ELEVATION

1/4"=1'-0" 1

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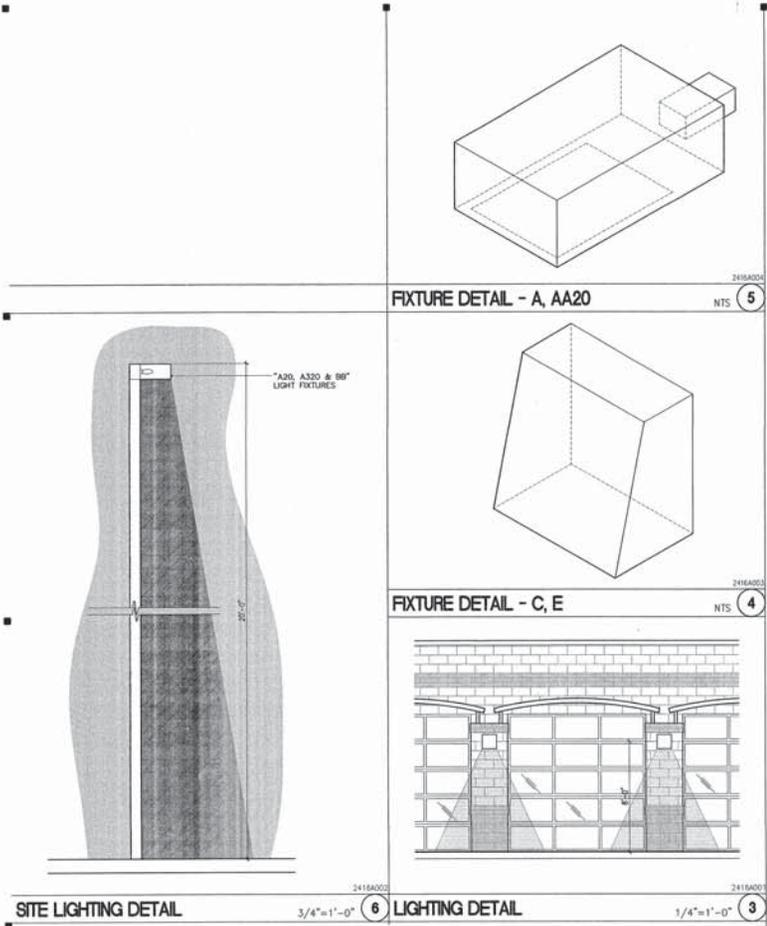
TRAC	TRAC & DEV	PLAN	PLAN COND	CONDITION
PROJ.	11-2000-2004			21 FEB 2005
NO.	2416			28 JAN 2005
				28 JAN 2005

**LEHRER'S**  
**EXTERIOR ELEVATIONS**

A2.3



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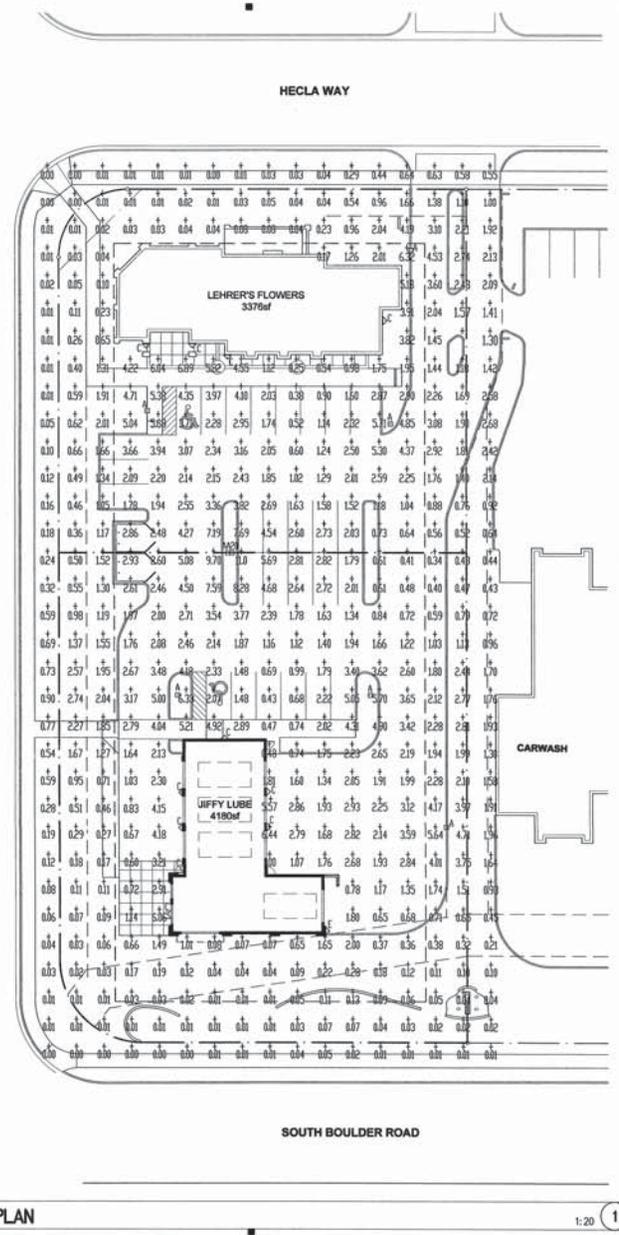
SITE LIGHTING DETAIL 3/4"=1'-0"

LIGHTING DETAIL 1/4"=1'-0"

CALCULATION SUMMARY										
Area Name	Dimensions	Grid / Type	# Pts	Space	Group	Ave	Max	Min	Max / Min	Ave / Min
R1 Exterior	175.00 x 345.00 Feet	Pavement / H-H	497	10.00	(*)	1.6	11.0	0.0	N/A	N/A
FIXTURE SCHEDULE										
Qty	Label	Lumens	Watts	Lamp	Mounting Ballast	LLF	Description			
6	A	23000	250	(1) M-250	24'-0" S.S.S Flush Concrete	0.65	Spaulding CM-H25-H4-F (1) M254 CM-M25-H4-F			
1	AA20	23000	250	(2) M250	24'-0" S.S.S Flush Concrete	0.65	Spaulding CM-H25-H4-F (2) CM74 CM-H25-H4-F			
11	C	5900	70	(1) M57 MVR70UMED	Surface Mount	0.70	Teron Lighting, HN-70MH-MTH (1) HOCC HN-70MH-MTH			
1	E	3450	50	(1) M57 MVR50UMED	Surface Mount	0.70	Teron Lighting, HN-50MH-MTH (1) HOCC HN-50MH-MTH			

PHOTOMETRIC DATA

PHOTOMETRIC PLAN



1:20

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STATE OF COLORADO

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OVERWAY + PEDERSEN ARCHITECTS, LLP  
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TITLE: PHOTO MTR PLAN	PLAN: COMB CORRECTONS
PROJ: 1500 PULASKI AVENUE	DATE: 11.18.2005
NO. 1500 PULASKI AVENUE	
2416 PLAN CORRECTONS	
	28 JAN 2006

PHOTOMETRIC PLAN  
LIGHTING DETAILS

A3.1

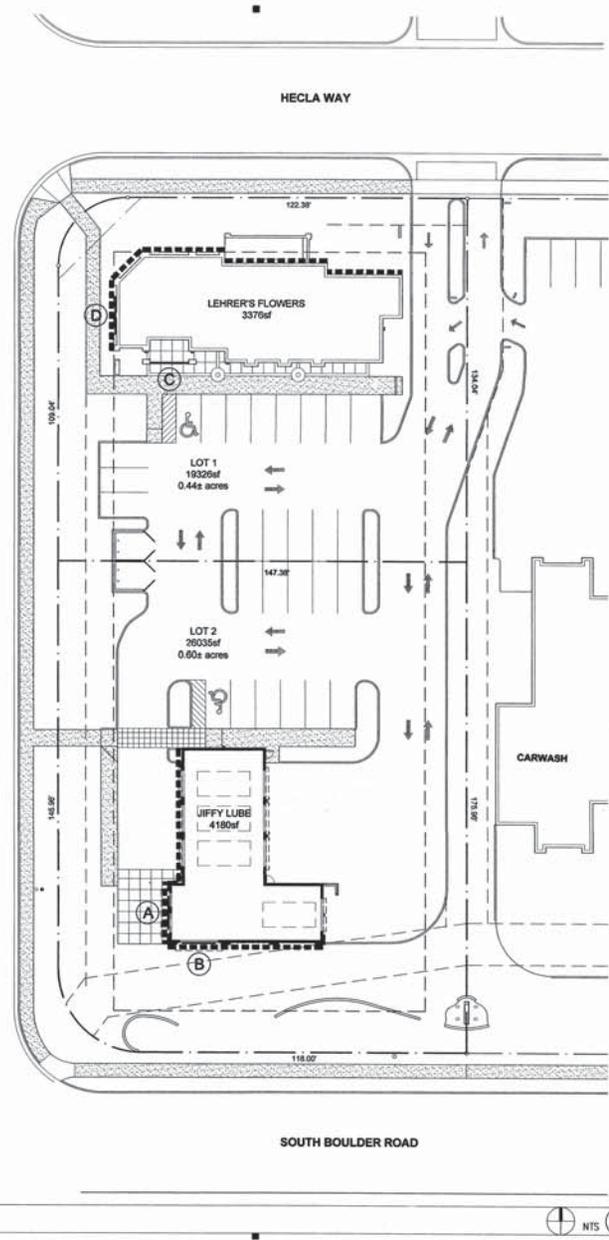
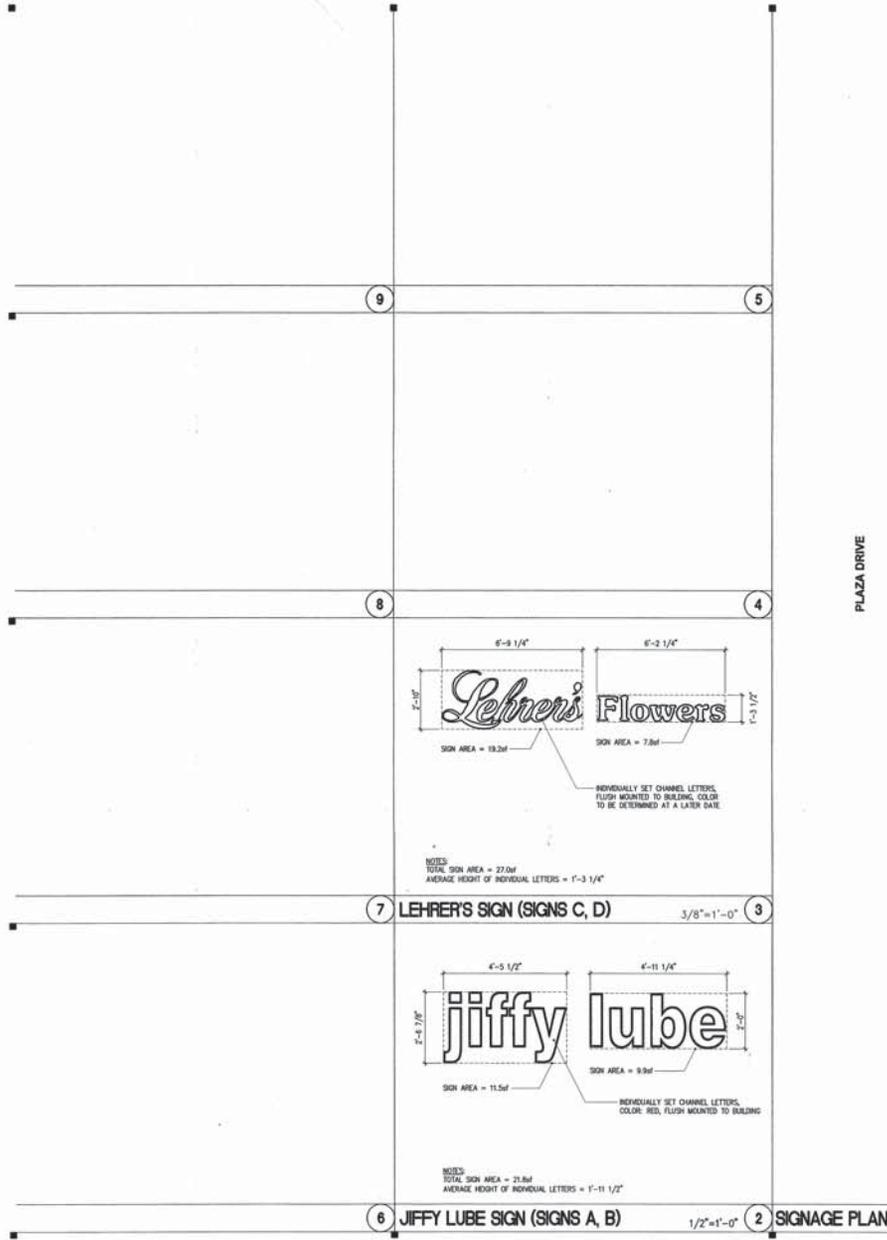
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**SIGN AREA CALCULATIONS:**

THICK DASHED LINE DESIGNATES BUILDING FRONTAGE FOR AREA CALCULATIONS.

JIFFY LUBE BUILDING FRONTAGE IS 127 LINEAL FEET.

SIGN A: 21.8sf  
SIGN B: 21.8sf  
TOTAL SIGN AREA: 43.6sf  
TOTAL SIGN AREA ALLOWED\*: 127.0sf  
SIGN AREA ALLOWED\*

LEHRER'S BUILDING FRONTAGE IS 128 LINEAL FEET.

SIGN C: 27.0sf  
SIGN D: 27.0sf  
TOTAL SIGN AREA: 54.0sf  
TOTAL SIGN AREA ALLOWED\*: 128.0sf  
SIGN AREA ALLOWED\*

\* PER LOUISVILLE DESIGN GUIDELINES SECTION 7.2.

AN EXISTING MONUMENT SIGN IS CURRENTLY LOCATED ON THE EAST PROPERTY LINE AT THE SE CORNER OF THE SITE. THIS MONUMENT SIGN WILL BE SHARED BETWEEN BLACK DRAMING CAR WASH, LEHRER'S FLOWERS AND JIFFY LUBE. A REVISED DRAWING WILL BE SUBMITTED AT A LATER DATE.

**jiffy lube**  
and  
**Lehrer's Flowers**  
S BOULDER RD AT PLAZA DR  
LOUISVILLE, CO

CARCOLO #1 LTD.  
9111 JOLLYVILLE ROAD, SUITE 105  
AUSTIN, TX 78759

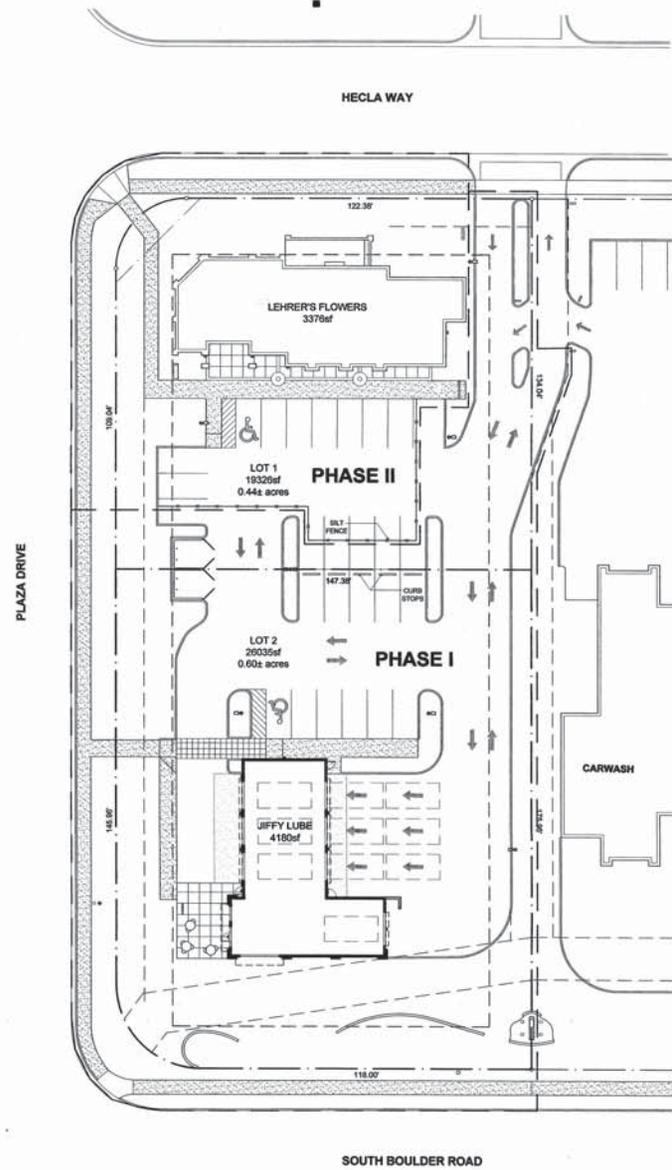


PROJ#	DATE	BY	REVISION
2416	28 JAN 2005		COMMENTS

**SIGNAGE PLAN/  
DETAILS**

A3.2

M.L.L. Louisville\_2485(P10)PhasingA33



This drawing is the property of Overway + Pedersen Architects, LLP and is not to be reproduced or copied in whole or in part. It shall only be used for the project and site specifically identified herein and shall not be used on any other project.

- PHASING NOTES:
1. EXTENT OF AREA TO BE IMPROVED WITH JIFFY LUBE IS DESCRIBED AS PHASE I.
  2. EXTENT OF AREA TO BE IMPROVED WITH LEHRER'S FLOWERS IS DESCRIBED AS PHASE II.
  3. ADD CURB STOPS & SILT FENCE AS SHOWN WITH PHASE I WORK.
  4. RESEED WITH NATIVE SEED ANY AREAS DISTURBED WITHIN PHASE I. TEMPORARILY IRRIGATE IF NECESSARY.

**jiffy lube**  
and  
*Lehrer's Flowers*  
S BOULDER RD AT PLAZA DR  
LOUISVILLE, CO

CARCOLO #1 LTD.  
9111 JOLLYVILLE ROAD, SUITE 106  
AUSTIN, TX 78759



TRAC PLAN & DEV. PLAN	TRAC ZONING CONFORMANCE
NO. 2416	NO. 2416
DATE: 23 JAN 2005	DATE: 21 FEB 2005

**PHASING PLAN**

PHASING PLAN

1:20 1

A33

Louisville Plaza  
Filing No. 3, Lot 1  
PUD Amendment

DRAWING NUMBER

DRAWING NUMBER

King Soopers  
1712 Plaza Dr.



# PUD PLAN AMENDMENT LOT 1 OF LOUISVILLE PLAZA, FILING NO. 3 KING SOOPERS FUELING CENTER #13 HECLA WAY & PLAZA DRIVE LOUISVILLE, COLORADO

**PROPERTY OWNER**

DILLON COMPANIES INC. A KANSAS CORPORATION  
KING SOOPERS (DIVISION OF KROGER)  
65 TELON STREET  
DENVER, COLORADO 80223  
TEL. (303) 715-4500  
FAX. (303) 778-3349

ATTN: DREW WAROT  
EMAIL: drewwarot@kingsoopers.com

**APPLICANT**

KING SOOPERS (DIVISION OF KROGER)  
65 TELON STREET  
DENVER, COLORADO 80223  
TEL. (303) 715-4500  
FAX. (303) 778-3349

ATTN: DREW WAROT  
EMAIL: drewwarot@kingsoopers.com

**ENGINEERING\ARCHITECT\PLANNING CONSULTANT**

GALLOWAY & CO, INC.  
5300 DTC PARKWAY, SUITE 100  
GREENWOOD VILLAGE, COLORADO 80111  
TEL. (303) 770-8884  
FAX. (303) 770-3636

ATTN: MATTHEW DUHAME P.E., P.L.S.  
EMAIL: matt@duhamegalloway.com

**LEGAL DESCRIPTION:**

LOT 1, LOUISVILLE PLAZA FILING NO. 3 LOCATED IN THE SW1/4 OF SECTION 4, T15, R69W OF THE 6TH P.M., CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO

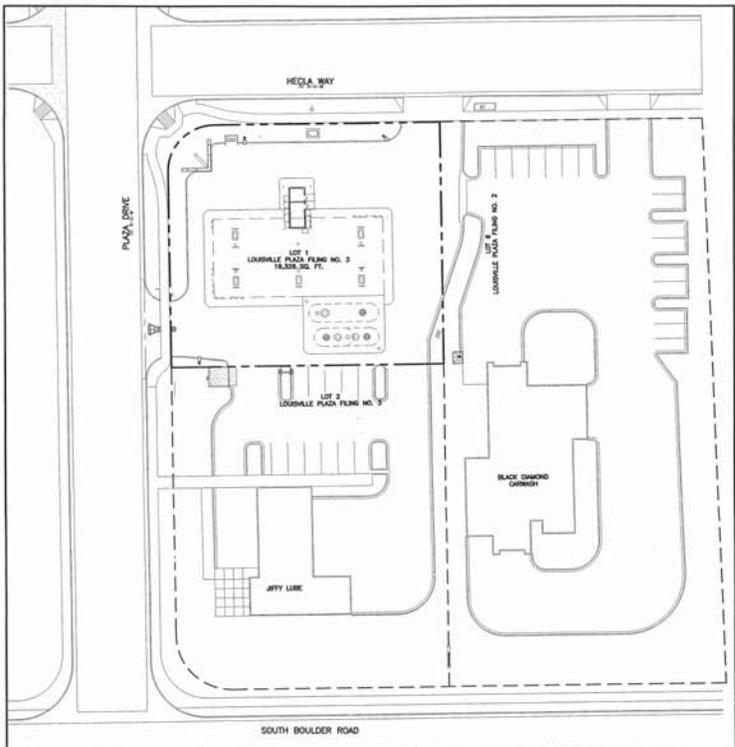
**ADDRESS:**

1712 PLAZA DRIVE, LOUISVILLE COLORADO 80027

**DATE OF PREPARATION**

INITIAL: AUGUST 12, 2010  
REVISED: SEPTEMBER 29, 2010  
REVISED: FEBRUARY 28, 2011

SITE DATA:	PROPOSED WITHIN DISTURBED AREA:
USE:	AUTOMOTIVE REFUELING STATION
EXISTING ZONING:	P-C
LOT FRONTAGE:	PLAZA DRIVE 107.1' HECLA WAY 122.4'
BUILDING AREA:	4,136 SQ. FT. (CANOPY & KIOSK) 21.4%
	KIOSK 180 SQ. FT.
	CANOPY 3,956 SQ. FT.
PAVED AREA:	12,936 SQ. FT. 66.9%
LANDSCAPING:	2,254 SQ. FT. 11.7%
LOT AREA:	19,326 SQ. FT. 100.0%
BUILDING HEIGHT:	KIOSK 11'-0"
	CANOPY 19'-0"
CANOPY STREET ROW SETBACK:	23.75 PLAZA DRIVE 34.4' HECLA WAY
CANOPY REAR LOT SETBACKS:	SOUTH 37.8" EAST 31.6"
PUMP ISLANDS:	5
CURB CUT ACCESS:	1 EXISTING ACCESS POINT, 1 NEW ACCESS POINT



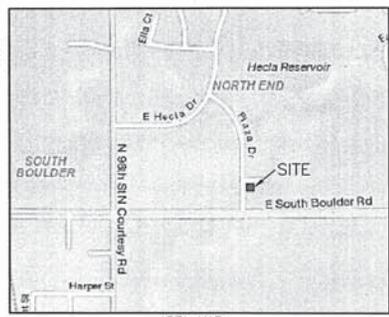
**SITE MAP**  
SCALE 1" = 30'

**OWNERSHIP SIGNATURE BLOCK**  
By signing this PUD Plan Amendment the owner acknowledges and accepts all the requirements & intent set forth herein. Witness our hands and seals this 28<sup>th</sup> day of March, 2011.

*Bessie D. Angier*  
For and on the behalf of the Dillon Companies, Inc. a Kansas Corporation

*Susan R. McKnight*  
NOTARY PUBLIC  
SEAL

MY COMMISSION EXPIRES 4-6-2014



**AREA MAP**  
SCALE 1" = 500'

**SHEET INDEX**

SHEET	SHEET TITLE
1 OF 7	COVER SHEET
1 OF 1	ALTA SURVEY
2 OF 7	OVERALL SITE PLAN
3 OF 7	SITE AND UTILITY PLAN
4 OF 7	GRADING AND EROSION CONTROL PLAN
5 OF 7	LANDSCAPE PLAN
6 OF 7	CANOPY AND KIOSK ELEVATIONS
7 OF 7	PHOTOMETRIC PLAN

**PLANNING COMMISSION CERTIFICATE**  
RECOMMENDED APPROVAL THIS 14<sup>th</sup> DAY OF October 20, 10, BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO.  
RESOLUTION NO. 26 SERIES 2010  
CHAIRMAN: *Jay*

**CITY COUNCIL CERTIFICATE**  
APPROVED THIS 19<sup>th</sup> DAY OF October 2010 BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO.  
RESOLUTION NO. 62 SERIES 2010  
MAYOR: *Walt* CITY CLERK: *Suzanne*

**CLERK AND RECORDER CERTIFICATE**  
(COUNTY OF BOULDER STATE OF COLORADO)  
I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT 3:13 O'CLOCK P.M. THIS 28<sup>th</sup> DAY OF July 2011, AND IS RECORDED IN PLAN FILE # 271, FEB 27, PAID \$71.  
FILM NO. \_\_\_\_\_ RECEPTION # 1629405  
CLERK & RECORDER: *Hilary Hall* DEPUTY: *Andrew*

NO.	REVISION	DATE
1	PER CITY COMMENTS	8/28/10
2	UPDATED PER CONSTRUCTION DOCUMENTS	2/28/11

**Galloway**  
Galloway & Co, Inc.  
5300 DTC Parkway, Suite 100  
Greenwood Village, CO 80111  
303.770.8884

**KING SOOPERS FUEL FACILITY CANOPY AND KIOSK SITE #13**  
HECLA WAY & PLAZA DRIVE  
LOUISVILLE, COLORADO

Project No: 457000013  
Sheet Title: AS NOTED  
Designed By: MFS  
Drawn By: MFS  
Date: 4/20/11  
Disk File: 45713\_P-01-Cover

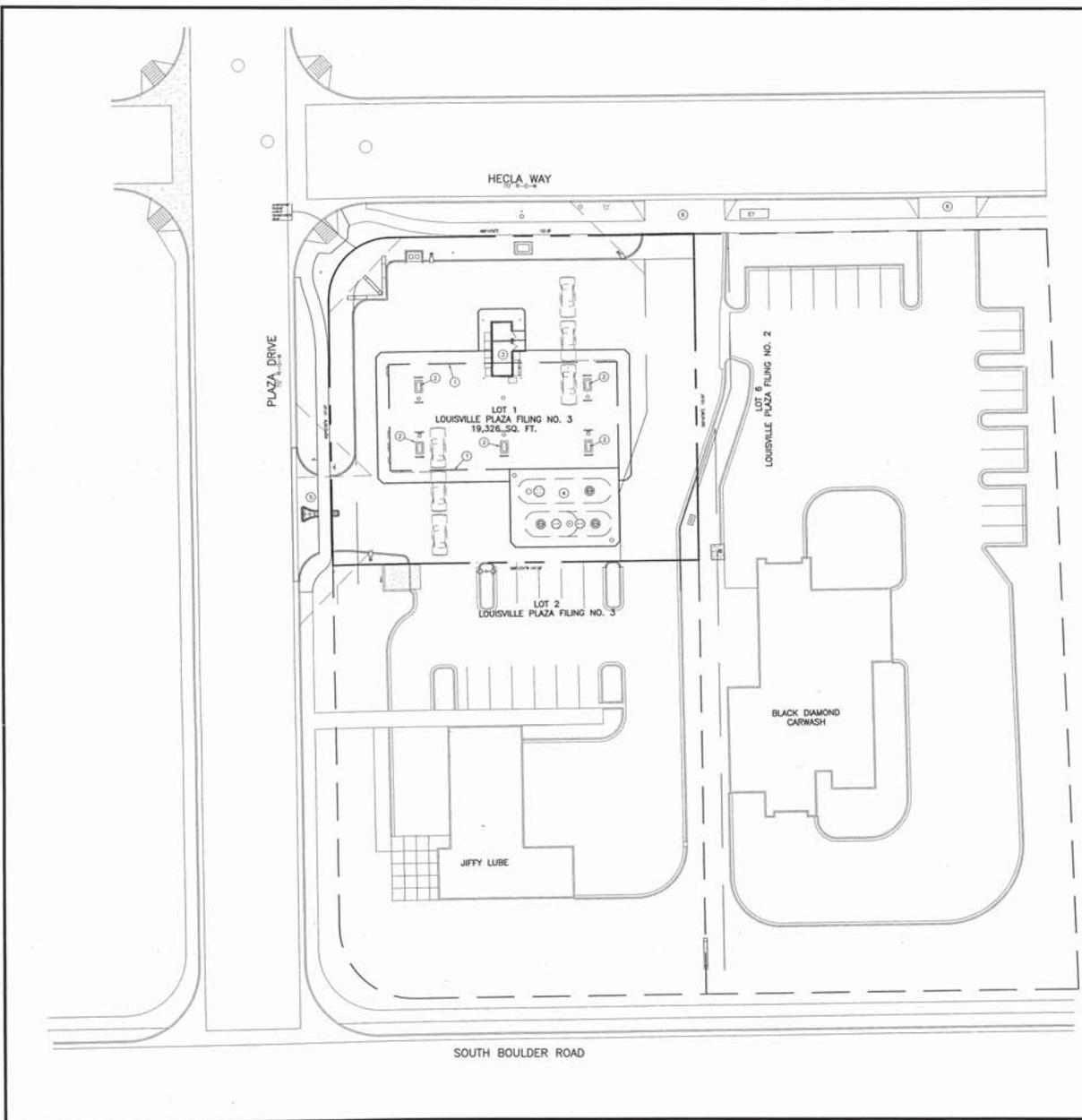
**COVER SHEET**

Louisville Plaza,  
Filing No. 3, Lot 1  
PUD Amendment

DRAWING NUMBER

DRAWING NUMBER

King Soopers  
1712 Plaza Dr.



- SITE LEGEND**
- PROPERTY BOUNDARY LINE
  - ADJACENT PROPERTY BOUNDARY LINE
  - SECTION LINE
  - CENTER LINE
  - EASEMENT BOUNDARY LINE
  - ◻ DRIVE DIRECTION
  - ◻ PARKING COUNT
  - ◻ SITE LIGHTING
  - ◻ WALL PACK LIGHT
  - ◻ STREET LIGHT
  - ◻ EXISTING TRAFFIC HOLE
  - ◻ EXISTING FIRE HYDRANT
  - ◻ PROPOSED FIRE HYDRANT
  - ◻ EXISTING MANHOLE COVER
  - ◻ PROPOSED MANHOLE COVER
  - ◻ UTILITY PEDISTAL
  - ◻ EXISTING INLET
  - ◻ PROPOSED INLET
  - EXISTING FENCE TO REMAIN
  - EXISTING RETAINING WALL
  - EXISTING TO BE REMOVED
  - PROPOSED NEW
  - PROPOSED RETAINING WALL

- SCHEDULE:**
- ① PROPOSED 43'-0" WIDE, 82'-0" LONG CANOPY (3,856 SF)
  - ② PROPOSED 21'-0" x 5'-0" ISLAND W/WASTE PRODUCT DISPENSER (170 SF)
  - ③ PROPOSED 8'-2" x 21'-6" KIOSK (WITH BATHROOM 180 SF)
  - ④ PROPOSED ONE 20,000 GALLON SINGLE COMPARTMENT UNDERGROUND DOUBLE WALL STORAGE TANK (CONCRETE) AND ONE 10,000 GALLON TWO COMPARTMENT UNDERGROUND DOUBLE WALL STORAGE TANK (30,000 GALLON PREMIUM AND 10,000 GALLON DIESEL)
  - ⑤ PROPOSED RIGHT W/HEIGHT OUT 30' CURB CUT
  - ⑥ EXISTING CURB CUT

CONTRACTOR TO REFER TO GEOTECHNICAL REPORT FOR PAVING, SUBGRADE AND CONSTRUCTION RECOMMENDATIONS. SEE GRADING PLAN FOR DETAILED INFORMATION ON FINAL GRADES AND PAVING.

CONTRACTOR TO REFER TO THE LANDSCAPE PLANS AND TREE PROTECTION NOTES FOR LOCATION OF EXISTING TREES, TREE PRESERVATION DETAILS, STORAGE OF EQUIPMENT UNDER TREES, AND PRUNING.

NO.	REVISION	DATE
1	PER CITY COMMENTS	8/29/10
2	UPDATED PER CONSTRUCTION DOCUMENTS	2/27/11

**Galloway**  
ENGINEERS ARCHITECTS  
3300 UTC Parkway, Suite 100  
Boulder, Colorado 80501  
303.440.8800  
www.gallowayinc.com

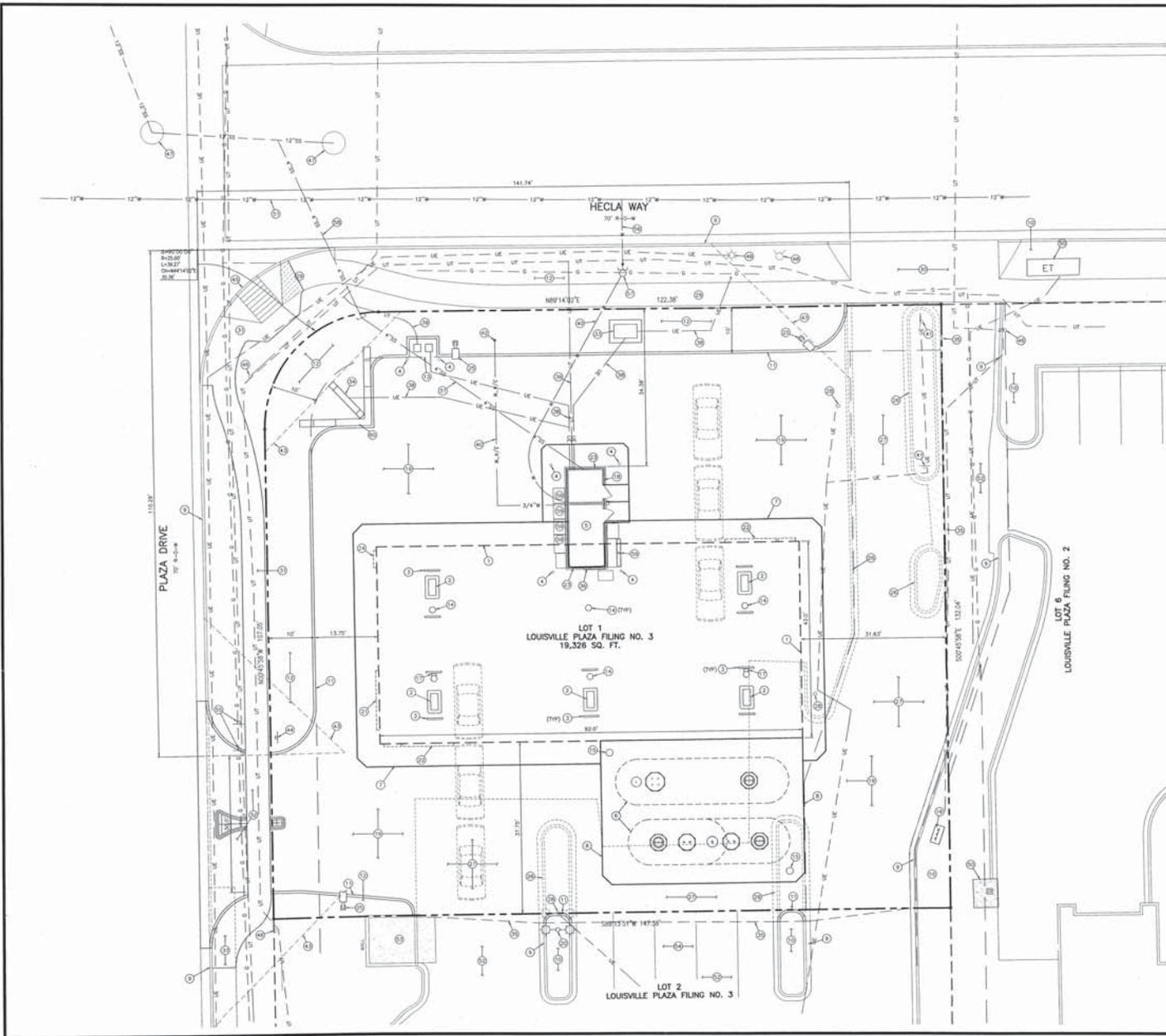
**ETITE**  
Interior, Exterior, and  
Site Planning  
1712 Plaza Drive  
Boulder, CO 80501  
303.440.8800

**KING SOOPERS  
FUEL FACILITY  
CANOPY AND KIOSK  
STORE # 13**  
HECLA WAY & PLAZA DRIVE  
LOUISVILLE, COLORADO

Project No: KSF-13  
Sheet: Scale: FULL  
Designer: JF  
Drawn: MF  
Date: AUGUST 10, 2010  
Disk File: KSF-13\_P-03-0000

**OVERALL SITE PLAN**

2 OF 7



**SITE LEGEND**

- PROPERTY BOUNDARY LINE
- - - - - ADJACENT PROPERTY BOUNDARY LINE
- SECTION LINE
- CENTER LINE
- EASEMENT BOUNDARY LINE
- TRAFFIC DIRECTION
- PARKING COUNT
- SITE LIGHTING
- WALL PEEK LIGHT
- STREET LIGHT
- EXISTING TRAFFIC POLE
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- EXISTING MANHOLE COVER
- PROPOSED MANHOLE COVER
- UTILITY PEDESTAL
- EXISTING INLET
- PROPOSED INLET
- EXISTING FENCE TO REMAIN
- EXISTING TO REMAIN
- EXISTING RETAINING WALL
- EXISTING TO BE REMOVED
- PROPOSED NEW
- PROPOSED RETAINING WALL

- SCHEDULE:**
- (1) PROPOSED 43'-0" WIDE, 92'-0" LONG CANOPY (3,856 SF)
  - (2) PROPOSED 3'-0" x 5'-0" ISLAND W/MULTIPLE PRODUCT DISPENSER (TYP. 6)
  - (3) PROPOSED ANCHORED U-SHAPED BOLLARD (TYP. 10)
  - (4) PROPOSED 6" PIPE BOLLARD (TYP. 6)
  - (5) PROPOSED 8'-2" x 21'-4" KIOSK (WITH BATHROOM 180 SF)
  - (6) PROPOSED ONE 20,000 GALLON SINGLE COMPARTMENT UNDERGROUND DOUBLE WALL STORAGE TANK (ANCHORED) AND ONE 10,000 GALLON TWO COMPARTMENT UNDERGROUND DOUBLE WALL STORAGE TANK (10,000 GALLON PREMIUM AND 10,000 GALLON DIESEL)
  - (7) PROPOSED 4" THICK REINFORCED CONCRETE SLAB UNDER CANOPY WITH 2" CHAMFER CORNERS
  - (8) PROPOSED 4" THICK REINFORCED CONCRETE SLAB OVER FUEL STORAGE TANKS
  - (9) EXISTING CURB AND GUTTER TO REMAIN
  - (10) EXISTING LANDSCAPE ISLANDS/AREA TO REMAIN
  - (11) PROPOSED 6" CURB AND 1" GUTTER
  - (12) PROPOSED LANDSCAPE ISLANDS/AREA
  - (13) PROPOSED PUBLIC AIR STAND AND PUBLIC PAY PHONE ON 6" x 4" CONCRETE PAD BOTTOM OF PAD WILL BE FLUSH WITH DRIVE
  - (14) PROPOSED CANOPY COLUMN (TYP. 4)
  - (15) PROPOSED TANK OBSERVATION WELLS
  - (16) PROPOSED 2" BLACK STEEL VENT BRIDES
  - (17) PROPOSED OVERFILL ALARM STRIKE AND ACKNOWLEDGMENT SWITCH ON CANOPY COLUMN
  - (18) PROPOSED EMERGENCY SHUT-OFF SWITCH MOUNTED ON KIOSK WITH DISTINCTLY LABELED SIGN READING "EMERGENCY FUEL SHUTDOWN DEVICE"
  - (19) PROPOSED 4" CONCRETE PAVEMENT
  - (20) PROPOSED RELOCATED SITE LIGHT (EXISTING UNDERGROUND ELECTRICAL SHALL BE RE-ROUTED TO NEW LOCATION)
  - (21) ILLUMINATED 3-FUEL PRODUCT PRICE SIGNAGE (45.5 SQ. FT.)
  - (22) ILLUMINATED 1,000 AND KING SOOPERS SIGNAGE (18.1 SQ. FT.)
  - (23) NON-ILLUMINATED KIOSK LOGO SIGNAGE (5.0 SQ. FT.)
  - (24) ILLUMINATED CANOPY LOGO SIGNAGE
  - (25) PROPOSED SITE LIGHT
  - (26) EXISTING CONCRETE CURB AND GUTTER TO BE REMOVED
  - (27) EXISTING PAVING TO BE REMOVED
  - (28) EXISTING SITE LIGHT TO BE REMOVED
  - (29) EXISTING CONCRETE SIDEWALK TO REMAIN
  - (30) EXISTING CURB CUT TO REMAIN
  - (31) PROPOSED CONCRETE SIDEWALK
  - (32) PROPOSED CURB CUT (CONSTRUCT PER CITY OF LOUISVILLE STANDARDS)
  - (33) PROPOSED PAD MOUNTED TRANSFORMER
  - (34) PROPOSED MOUNTMENT SIGN, SIGN FACE MINIMUM 10'-0" FROM RIGHT-OF-WAY LINE, REFER TO MOUNTMENT SIGN SCHEDULE, OR ARCHITECTURAL SIGNAGE
  - (35) PROPOSED ASPHALT SARGOUT LINE
  - (36) PROPOSED PAY WINDOW
  - (37) PROPOSED 4" SDN 30 SANITARY SERVICE TO KIOSK
  - (38) PROPOSED UNDERGROUND ELECTRICAL, CONSTRUCTION TO VERIFY POINT OF SERVICE WITH UTILITY COMPANY PRIOR TO CONSTRUCTION
  - (39) PROPOSED UNDERGROUND TELEPHONE
  - (40) PROPOSED 3/4" WATER LINE
  - (41) EXISTING SIGN TO BE REMOVED
  - (42) PROPOSED FROST FREE TANK HYDRANT
  - (43) 30' SITE TRIANGLE
  - (44) PROPOSED 81-1 STOP SIGN AND NO LEFT TURN SIGN MOUNTED ON SAME POST
  - (45) EXISTING HANDICAP RAMP
  - (46) EXISTING TELEPHONE PEDESTAL
  - (47) EXISTING SANITARY SEWER MANHOLE
  - (48) EXISTING FIRE HYDRANT
  - (49) EXISTING STREET LIGHT
  - (50) EXISTING ELECTRICAL TRANSFORMER
  - (51) EXISTING WATER LINE
  - (52) EXISTING ASPHALT PAVEMENT TO REMAIN
  - (53) EXISTING TRASH ENCLOSURE
  - (54) EXISTING PARKING SIGNS TO REMAIN
  - (55) EXISTING 43'-76" (LEFT LANE MUST TURN LEFT) TO BE RELOCATED
  - (56) EXISTING 1.5" SERVICE TAP TO REMAIN
  - (57) PROPOSED 3/4" WATER METER AND FIT PER CITY OF LOUISVILLE CRITERIA
  - (58) EXISTING SANITARY SEWER TILES INTO PROPERTY
  - (59) OUTSIDE SOGA POP, ICE, AND OIL VENDOR CAGES, ETC.
  - (60) PROPOSED W/6 DENSITY WALL

NO.	REVISION	DATE
1	PER CITY COMMENTS	8/27/10
2	UPDATED PER CONSTRUCTION DOCUMENTS	2/28/11

**Galloway**  
 ENGINEERING  
 30070 30th Ave  
 Suite 100  
 Louisville, CO 80027

**ET**  
 ENGINEERING  
 1712 Plaza Dr.  
 Louisville, CO 80027

**KING SOOPERS FUEL FACILITY CANOPY AND KIOSK STORE # 8**  
 HECLA WAY & PLAZA DRIVE  
 LOUISVILLE, COLORADO

Project No: KSF13  
 Sheet Scale: FULL  
 Designed By: MFD  
 Drawn By: JWB  
 Date: AUGUST 10, 2010  
 Disk File: KSF13\_P-03-02a

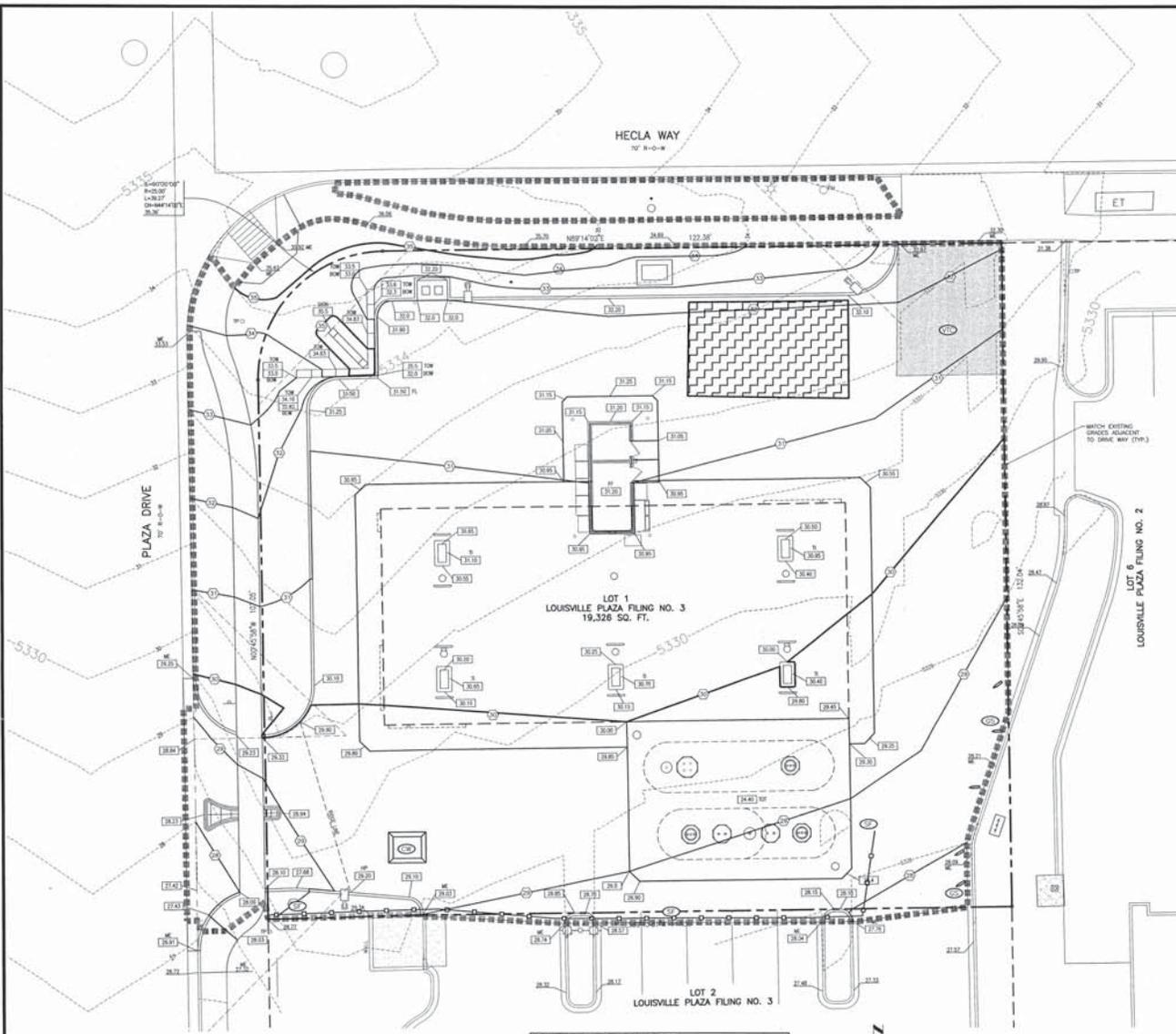
**SITE AND UTILITY PLAN**

Louisville Plaza,  
Filing No. 3, Lot 1  
PUD Amendment

DRAWING NUMBER

DRAWING NUMBER

King Soopers  
1712 Plaza Dr.



**GRADING LEGEND**

- EXISTING CONTOUR
- - - PROPOSED CONTOUR
- PROPOSED CONTOUR (BY OTHERS)
- +14.00 EXISTING SPOT ELEVATION
- [24.3] PROPOSED SPOT ELEVATION
- [24.3] PROPOSED TOP OF CURB ELEVATION
- [24.3] PROPOSED FLOORLINE ELEVATION
- FF FINISHED FLOOR
- TT TOP OF TANKS
- T TOP OF ISLAND
- TOD TOP OF DRIVE
- TC TOP OF CURB
- TG TOP OF SIDEWALK
- FL FLOORLINE
- HP HIGH POINT
- LP LOW POINT
- ME MATCH EXISTING
- ME SHEET ELEVATION
- 23271123 EXISTING STORM SEWER
- 23271122 PROPOSED STORM SEWER

**EROSION CONTROL LEGEND**

- EXISTING CONTOUR
- - - PROPOSED CONTOUR
- EXISTING CONTOUR
- PROPOSED CONTOUR
- SOIL SWALE
- SWALE ARROW
- VEHICLE TRACKING CONTROL
- SILT FENCE
- DRAGL SOCK IN GUTTER
- CONCRETE WASHOUT AREA
- (CONTRACTOR STAGING AREA APPROXIMATE)

**GENERAL NOTES**

1. REQUIREMENTS SHOWN ON SITE PLAN SHALL GOVERN DIMENSIONS AND SPECIFICATIONS SHOWING OTHERWISE NOTWITHSTANDING. CONTRACTOR TO POINT OUT ANY DISCREPANCIES TO THE OWNER PRIOR TO BID.
2. NO WORK IS TO BEGIN UNTIL ALL PERMITS HAVE BEEN OBTAINED.
3. CONTRACTOR TO PROVIDE ALL EQUIPMENT AND PERSONNEL REQUIRED FOR FINAL CHECKOUT OF ALL FACILITIES BY OWNER'S REPRESENTATIVE.
4. ENTIRE INSTALLATION SHALL MEET ALL APPLICABLE CODES.
5. VERIFY ALL DIMENSIONS AND CONDITIONS ON SITE.
6. ALL LANDSCAPING AREAS SHALL BE ORDERED TO WITHIN 0.10' WITH COMPANY APPROVED TOP SOIL TO A MINIMUM 4" DEPTH.
7. FINAL GRADES ARE SUBJECT TO MINOR CHANGE BY COMPANY REPRESENTATIVE. NO GRADE CHANGES IN EXCESS OF 1.00' WITHOUT COMPANY APPROVAL.
8. ANY FILL MATERIAL REQUIRED TO BRING THE SITE TO GRADE SHALL BE CLEAN FILL DIRT APPROVED BY GEOTECHNICAL ENGINEER AND SHALL BE PLACED FIRST FOR GEOTECHNICAL RECOMMENDATIONS.
9. SET PROPERTY CORNER PINS IN CONCRETE. IF PROPERTY CORNERS ARE DESTROYED BY CONTRACTOR, THE CONTRACTOR SHALL BEAR THE EXPENSE OF RELOCATING CORNERS BY A REGISTERED SURVEYOR.
10. THIS DESIGN IS BASED ON A SOIL AND FOUNDATION INVESTIGATION. CONTRACTOR SHALL FOLLOW ALL GEOTECHNICAL RECOMMENDATIONS. A COPY OF THIS REPORT IS AVAILABLE FOR INSPECTION BY THE CONTRACTOR.
11. ALL UTILITY CONDITIONS, FOOTING FOUNDATIONS SHALL BE INSTALLED BY GENERAL CONTRACTOR UNLESS OTHERWISE SPECIFICALLY EXCLUDED IN NOTES.
12. GENERAL CONTRACTOR SHALL REMOVE, UNLOAD, STORE AND UNLOAD ALL EQUIPMENT FURNISHED BY THE OWNER AND SHIPPED TO THE SITE. SEE SPECIFICATIONS FOR EQUIPMENT FURNISHED BY OWNER.
13. LANDSCAPING AND IRRIGATION SYSTEM SHALL BE INSTALLED BY GENERAL CONTRACTOR. SEE LANDSCAPE AND IRRIGATION PLANS FOR DETAILS.
14. GENERAL CONTRACTOR TO PERFORM GENERAL YARD AND BUILDING CLEAN-UP AT COMPLETION OF WORK.
15. SEE THE SITE SURVEY FOR SURVEY INFORMATION. CONTRACTOR'S CONSTRUCTION SURVEYOR TO OBTAIN AUTOMATIC FILE PRIOR TO CONSTRUCTION STARTING. CONSTRUCTION SURVEYOR SHALL OBTAIN TO ALL AGENCIES REQUIRED FOR CONSTRUCTION STARTING.
16. ALL TOP OF CURB GRADES ARE 6" HIGHER THAN FINISH GRADES SHOWN ON DRIVE UNLESS OTHERWISE NOTED. ALL SPOT ELEVATIONS SHOWN ADJACENT TO GUTTERS AND SIDEWALKS REFER TO FLOORLINE UNLESS NOTED OTHERWISE.
17. GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR MINIMIZING DEPOSITION OF DUST/SEDIMENTS INTO SURROUNDING PUBLIC STREETS DURING CONSTRUCTION. SEE SWEEP EROSION CONTROL PLANS FOR ADDITIONAL DETAILS.
18. GENERAL CONTRACTOR SHALL PROVIDE COMPREHENSIVE TRAFFIC CONTROL PLAN WHICH SHALL BE SUBMITTED TO AND APPROVED BY THE CITY AND/OR STATE PRIOR TO ANY WORK IN THE PUBLIC ROW.
19. ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURVEY EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN APPROXIMATE LOCATION ONLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION OF ALL UTILITIES, PUBLIC OR PRIVATE, WHETHER SHOWN ON THE PLANS OR NOT, PRIOR TO CONSTRUCTION. REPORT ANY DISCREPANCIES TO THE ENGINEER PRIOR TO CONSTRUCTION.
20. WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POSITIVE OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.
21. IF DRAINAGING IS REQUIRED TO INSTALL UTILITIES, A STATE CONTRACTING DRAINAGING SHEET WITH EROSION CONTROL MEASURES IS REQUIRED IF WATER IS DISCHARGED INTO A STORM SEWER, CHANNEL, IRRIGATION DITCH OR ANY WATERS OF THE UNITED STATES.
22. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RECORDING AS-BUILT INFORMATION ON A SET OF RECORD DRAWINGS KEPT ON THE CONSTRUCTION SITE, AND AVAILABLE TO THE CITY, ITS AGENTS, OWNER OR GALLOWAY AT ALL TIMES. DIMENSIONS SHALL BE ANNOTATED ON AS-BUILT RECORD DRAWINGS. AS-BUILT DRAWINGS ARE REQUIRED PRIOR TO SUBMITTAL OF FINAL PERMITS. AS-BUILT DRAWINGS WILL BE SUPPLIED TO OWNER AND CITY PRIOR TO FINAL PERMITS.
23. ALL MINOR ELEVATION SHALL BE PROTECTED BY SIGNS OR SUPPORTED IN A MANNER REQUIRED BY OWNER OR AS REQUIRED BY STATE OR LOCAL LAWS.
24. ALL SWALE SLOPES SHALL BE 3:1 OR LESS. SIDEWALKS AND DRIVES WHERE SWALES CROSS SHALL HAVE LONGITUDINAL SLOPES 1% OR LESS AND CROSS SLOPES 3% OR LESS.
25. PUBLIC IMPROVEMENTS CONSTRUCTION FOR UTILITY AND CONCRETE WORK WITHIN RIGHT OF WAY SHALL MEET THE REQUIREMENTS OF THE CITY OF LOUISVILLE DESIGN AND CONSTRUCTION STANDARDS. CONTRACTORS WORKING WITHIN PUBLIC PROPERTY ARE REQUIRED TO OBTAIN A RIGHT OF WAY PERMIT AVAILABLE FROM THE CITY OF LOUISVILLE PUBLIC WORKS DEPARTMENT.

**EROSION CONTROL NOTES:**

1. USE DRAGL SOCKS CAN BE PLACED AT CURB HEIGHTS (CONSTRUCTION) ONLY IN LIEU OF THE GRASS FILTER. INSTALL DRAGL SOCKS SIMILAR TO THE DRAGL SOCKS OF AREA BEING DETAIL. USE DRAGL SOCKS MUST BE PLACED PERPENDICULAR TO FLOW AND COMPLETELY AROUND GRADE CHANGES AT ALL TIMES.
2. VTC VEHICLE TRACKING PAD SHOULD BE EXTENDED ACROSS THE ENTIRE ENTRANCE IF TRUCK TRAFFIC TRAVELS OFF DESIGNATED PAD.
3. IF STRIP PILES MAY BE STALLED DOWN SIMILAR TO THE WAY SILE STABILIZATION IS OCCURRING.
4. ON SEVERAL CONCRETE WASHOUT LOCATIONS ARE SHOWN HERE AS SPREADS FOR THE CONTRACTOR'S USE DURING CONSTRUCTION TRAFFIC. ONLY ONE LOCATION MAY BE USED AT A TIME.
5. CONTRACTOR TO REPLACE STRIP SILE ALLET PROTECTION AROUND SHEETS WITH CURB INLET GRATEL FILTERS ONCE PAVING OF SITE HAS STARTED.
6. VTC - VEHICLE TRACKING CONTROL SHALL BE LOCATED AS REQUIRED, AS CONSTRUCTION PROGRESSES, FOR ADEQUATE SEDIMENT CONTROL.

**BENCHMARK**  
THE BENCHMARK UTILIZED FOR THIS SURVEY IS A NGS MONUMENT Q 413, PID = KK1556, WITH A PUBLISHED ELEVATION OF 5296.02 FEET, NAVD 83 DATUM.

**CALL UTILITY NOTIFICATION**  
CENTER OF COLORADO  
1-800-922-1987  
CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, DRILL OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES.

**SOIL PREPARATION NOTE**  
SOIL PREPARATION FOR NON-LANDSCAPING AREAS SHALL BE PER RECOMMENDATIONS FROM A GEOTECHNICAL REPORT PREPARED FOR THIS SITE AS FOLLOWS:  
GEOTECHNICAL ENGINEER: [REDACTED]  
REPORT NO: [REDACTED]  
THE CONTRACTOR MUST FULLY REVIEW THIS REPORT PRIOR TO CONSTRUCTION. INFORMATION IN THE GEOTECHNICAL REPORT SUPERSEDES ANY CONFLICTING INFORMATION CONTAINED IN THE CONSTRUCTION PLANS AND SPECIFICATIONS. REFER TO GENERAL STRUCTURAL NOTES FOR SPECIFIC SOIL PREPARATION AT SITE STRUCTURES. REFER TO THE LANDSCAPE PLANS FOR SPECIFIC PLANTING AREA SOIL AMENDMENT PROCEDURES.



NO.	REVISION	DATE	BY	CHK'D BY
1	PER CITY COMMENTS	8/27/15	EW	JTB/15
2	UPDATED PER CONSTRUCTION DOCUMENTS	8/27/15	EW	JTB/15

**Galloway**  
ENGINEERS ARCHITECTS  
1500 S. W. 10TH AVE. SUITE 100  
MIAMI, FL 33135  
305.775.8888  
WWW.GALLOWAYFL.COM

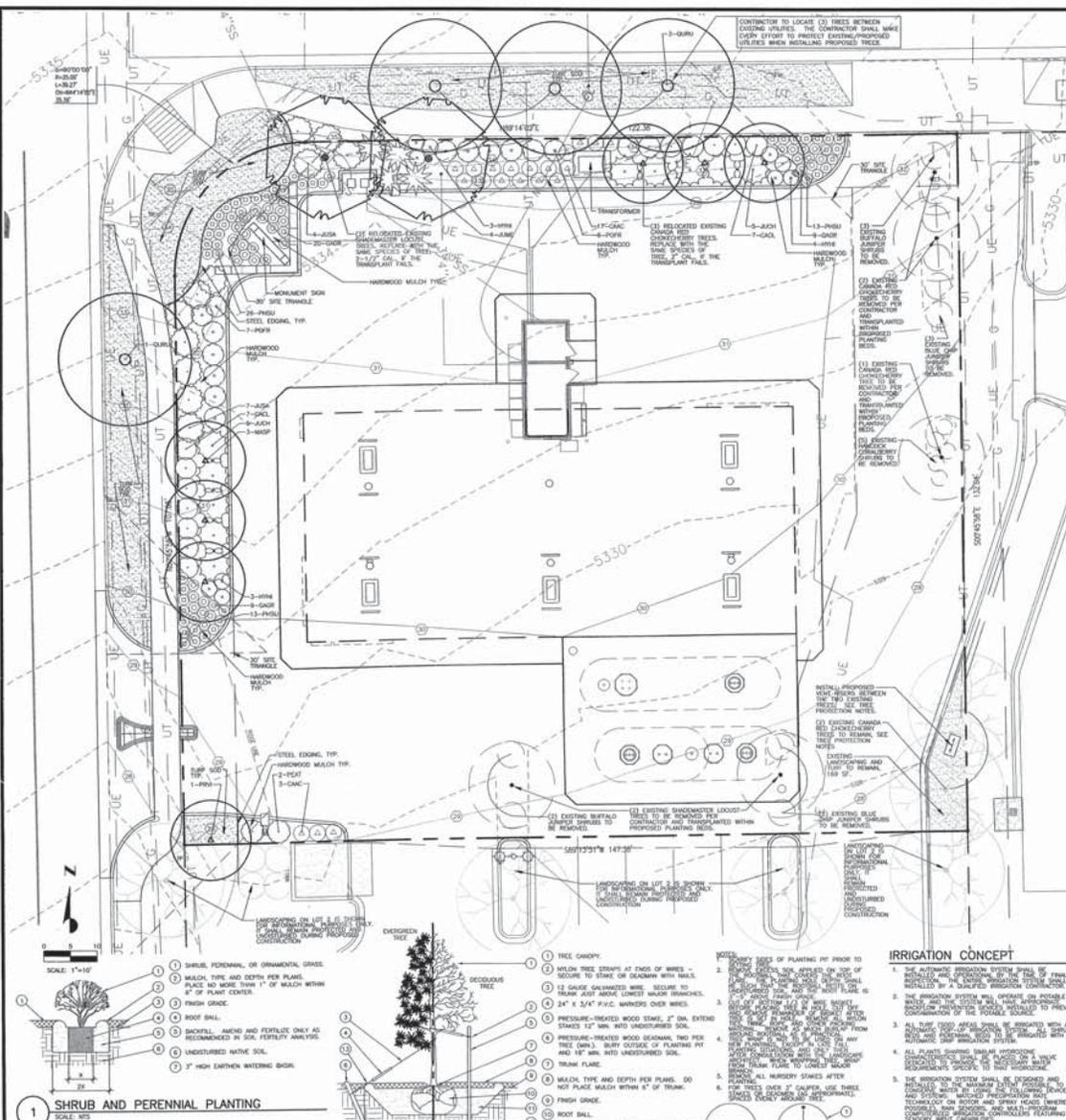
**FILE**  
100% COMPLETE  
100% PERMITS  
100% RECORDS

**KING SOOPERS**  
FUEL FACILITY  
CANOPY AND KIOSK  
STORE # 13  
HECLA WAY & PLAZA DRIVE  
LOUISVILLE, COLORADO

Project No: KSP00013  
Sheet No: 11 of 107  
Drawn By: MFD  
Checked By: TMB  
Date: AUGUST 10, 2015  
Disk File: KSP13\_PUD\_001.DWG

**GRADING AND EROSION CONTROL PLAN**

4 OF 7



**PLANTING LIST** APPROVED BY THE LOUISVILLE COMMERCIAL DESIGN GUIDELINES RECOMMENDED PLANT LIST

PLANT BOTANIC NAME	COMMON NAME	SIZE	HARDINESS	WIND RES.	HT. X SPD QUANT.
<b>DECIDUOUS SHADE TREES</b>					
QUERCUS RUBRA	NORTHERN RED OAK	2.5' CALIPER	LOW	LOW	50' X 40'
<b>DECIDUOUS CONIFER TREES</b>					
SPRUCEDALE PINE	SPRING DALE PINE	2.0' CALIPER	MEDIUM/LOW	LOW	20' X 20'
FRAXINUS VIRGINIANA 'CANADA RED'	CANADA RED DOGWOOD	2.0' CALIPER	MEDIUM/LOW	MODERATE	20' X 15'
<b>DECIDUOUS SHRUBS</b>					
CARYOPHTERIS X CLAUDONIA	BLUE MOSS SPIREA	5 GALLON	LOW	LOW	5' X 5'
HYPERICUM MOIDICUM	ST. JOHN'S WORT	5 GALLON	LOW	LOW	2' X 2'
POTENTIALIA FRUTICOSA 'GOLD STAFF'	GOLD STAFF POTENTILLA	5 GALLON	MEDIUM/LOW	MODERATE	2' X 3'
<b>PERENNIALS AND ORNAMENTAL GRASSES</b>					
CALLUNA HEATH	HEATH	1 GALLON	LOW	MODERATE	4" X 4"
CALLUNA X GARDENIA	BLANCKET FLOWER	1 GALLON	LOW	MODERATE	1.5' X 2'
PRICKER SUBULNA	CREeping PRICKER	1 GALLON	LOW	MODERATE	4" X 2'
<b>SOIL AND MULCH</b>					
100% REVEILLE SOIL OR APPROVED EQUAL	TURF SOIL	500	MODERATE		100' SF
DOUBLE SHREDED CLEAR BARK MULCH	BARK MULCH				100' SF

**PLANTING NOTES**

1. ALL LAND AT (303) 232-1991 PRIOR TO ANY EXCAVATION.
2. ALL WORK SHALL CONFORM TO APPLICABLE LOCAL CODES.
3. PLANT QUANTITIES SHOWN ON THIS PLAN ARE FOR GENERAL INFORMATION. THE PLANT QUANTITIES SHOWN ON THIS PLAN ARE FOR GENERAL INFORMATION. THE PLANT QUANTITIES SHOWN ON THIS PLAN ARE FOR GENERAL INFORMATION.
4. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.
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**MULCH NOTES**

1. ALL MULCH IS AVAILABLE THROUGH CONTRACTOR'S SUPPLIER. CONTRACTOR SHALL INSTALL LANDSCAPE MULCH AND A MINIMUM 4" THICK LAYER OF APPROVED MULCH IN ALL PLANTING AREAS. CONTRACTOR SHALL SUBMIT SAMPLES OF ALL MULCH TO THE OWNER AND LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO CONSTRUCTION. MULCH SHALL BE APPLIED TO ALL PLANTING AREAS. CONTRACTOR SHALL SUBMIT SAMPLES OF ALL MULCH TO THE OWNER AND LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO CONSTRUCTION. MULCH SHALL BE APPLIED TO ALL PLANTING AREAS.

**SOD NOTES:**

1. ALL SOD IS AVAILABLE THROUGH CONTRACTOR'S SUPPLIER. CONTRACTOR SHALL INSTALL LANDSCAPE SOD AND A MINIMUM 4" THICK LAYER OF APPROVED SOD IN ALL PLANTING AREAS. CONTRACTOR SHALL SUBMIT SAMPLES OF ALL SOD TO THE OWNER AND LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO CONSTRUCTION. SOD SHALL BE APPLIED TO ALL PLANTING AREAS.

**PERIMETER PLANTING CALCULATIONS**

PLANT	QUANTITY	SIZE	HT. X SPD	QUANT.
QUERCUS RUBRA	1	2.5' CALIPER	LOW	50' X 40'
SPRUCEDALE PINE	1	2.0' CALIPER	MEDIUM/LOW	20' X 20'
FRAXINUS VIRGINIANA 'CANADA RED'	1	2.0' CALIPER	MEDIUM/LOW	20' X 15'
CARYOPHTERIS X CLAUDONIA	1	5 GALLON	LOW	5' X 5'
HYPERICUM MOIDICUM	1	5 GALLON	LOW	2' X 2'
POTENTIALIA FRUTICOSA 'GOLD STAFF'	1	5 GALLON	MEDIUM/LOW	2' X 3'
CALLUNA HEATH	1	1 GALLON	LOW	4" X 4"
CALLUNA X GARDENIA	1	1 GALLON	LOW	1.5' X 2'
PRICKER SUBULNA	1	1 GALLON	LOW	4" X 2'
TURF SOIL	1	500	MODERATE	100' SF
BARK MULCH	1			100' SF

**TREE PROTECTION NOTES**

1. PROTECTED ZONE FOR EXISTING TREES: BEFORE BEGINNING ANY EXCAVATION OR CONSTRUCTION, THE CONTRACTOR SHALL IDENTIFY ALL EXISTING TREES AND THEIR PROTECTED ZONES. THE PROTECTED ZONE SHALL BE MAINTAINED AND PROTECTED AT ALL TIMES. THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL UTILITIES AND SHALL BE RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.
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**LANDSCAPE TABLE**

TOTAL LOT LANDSCAPE SIZE (IN SF)	2,200 SF
-10% MIN. LANDSCAPE SIZE (IN SF)	2,200 SF
-10% LANDSCAPE SIZE (IN SF)	4,135 SF

**Galloway**

**KING SOOPERS FUEL FACILITY CANOPY AND KIOSK STORE # 8**

HECLA WAY & PLAZA DRIVE LOUISVILLE, COLORADO

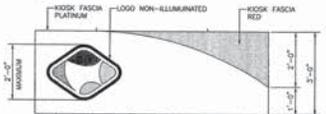
Project No.: 459713  
 Sheet Scale: 1" = 10'  
 Prepared By: JAW  
 Review By: JAW  
 Date: AUGUST 10, 2010  
 Date Plotted: AUGUST 10, 2010

**LANDSCAPE PLAN**

5 OF 7

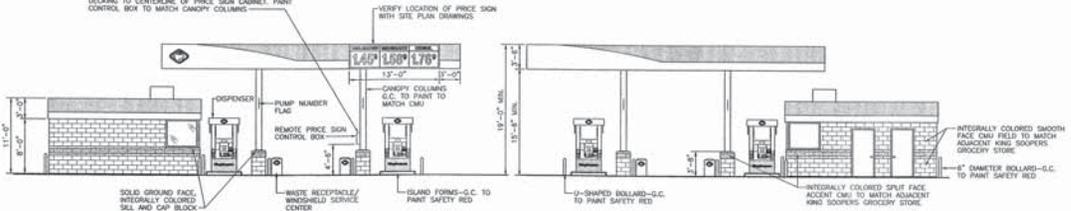


1 CANOPY GRAPHICS  
 SCALE: 1/2"=1'-0"



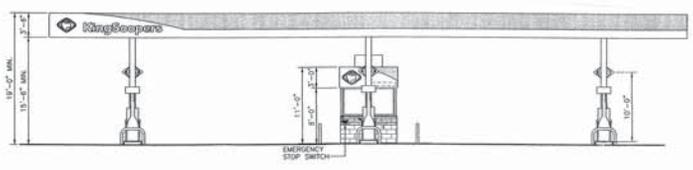
2 KIOSK GRAPHICS  
 SCALE: 1/2"=1'-0"

INSTALL REMOTE CANOPY PRICE SIGN CONTROL BOX ON COLUMN SHOWN ON SITE PLAN. INSTALL CONTROL CABLES (FURNISHED WITH SIGN) INSIDE COLUMN AND THROUGH (1) 3/2" PVC CONDUIT FROM COLUMN ABOVE BEARING TO CENTERLINE OF PRICE SIGN CHANNEL. PAINT CONTROL BOX TO MATCH CANOPY COLUMNS.

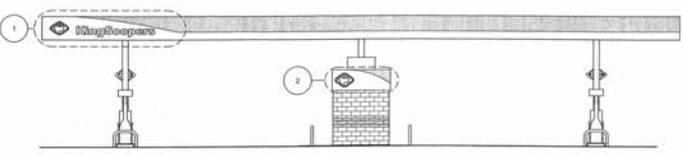


3 WEST ELEVATION  
 SCALE: 1/8"=1'-0"

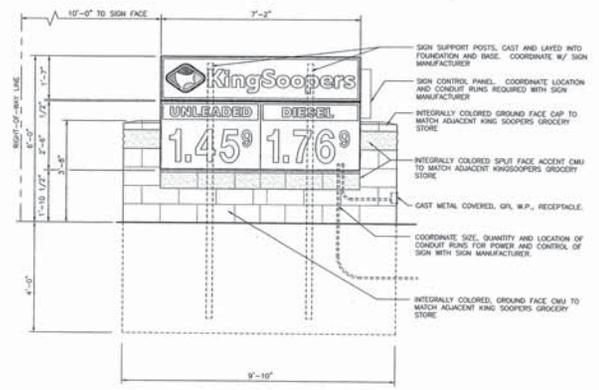
4 EAST ELEVATION  
 SCALE: 1/8"=1'-0"



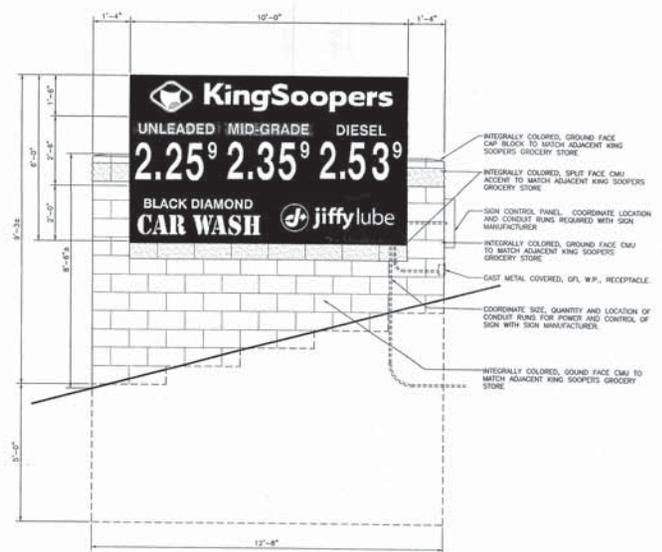
5 SOUTH ELEVATION  
 SCALE: 1/8"=1'-0"



6 NORTH ELEVATION  
 SCALE: 1/8"=1'-0"



7 FUEL MONUMENT SIGN ELEVATION  
 SCALE: 1/2"=1'-0"



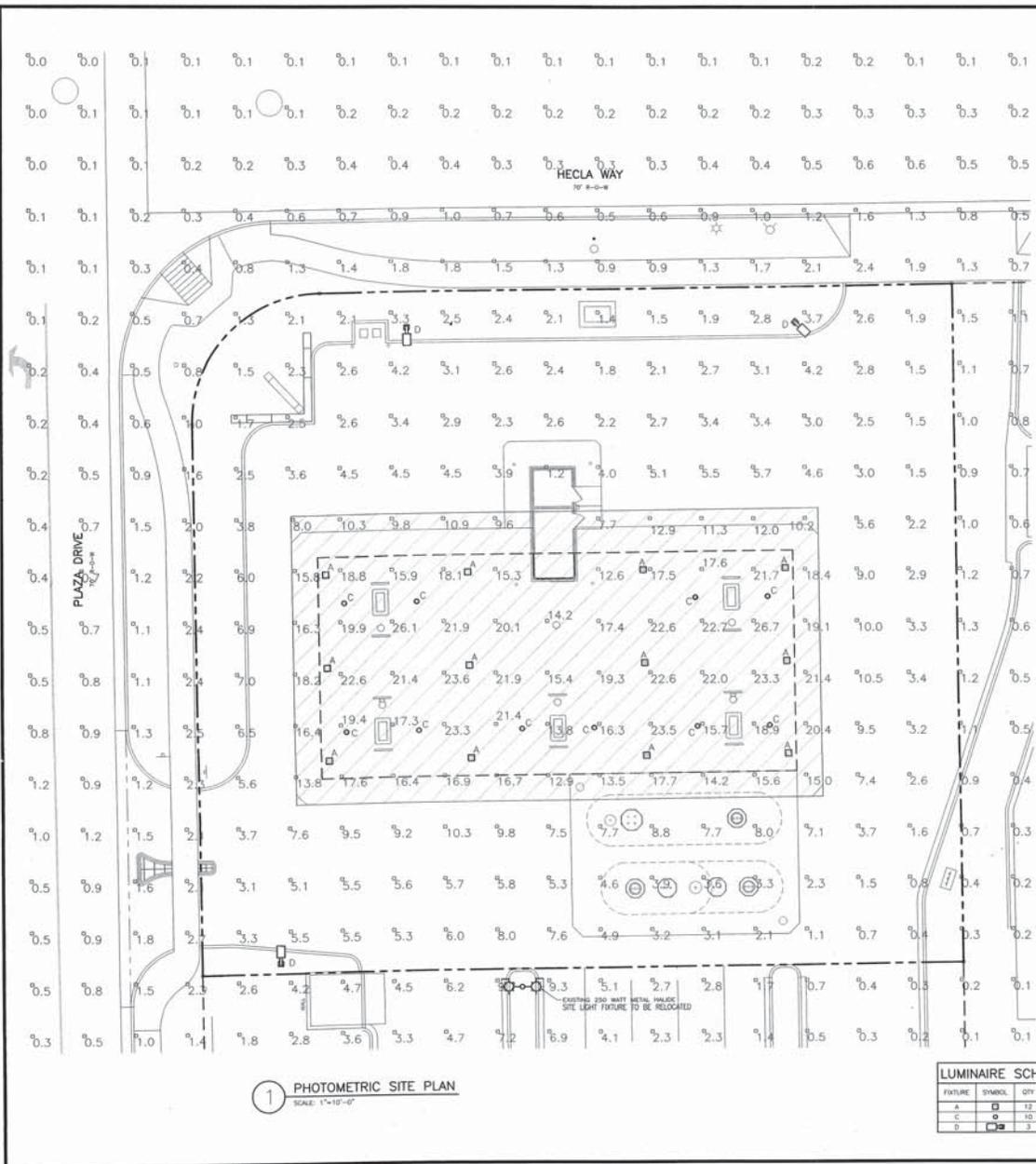
8 GROCERY MONUMENT SIGN ELEVATION  
 SCALE: 1/2"=1'-0"

PROJECT NO.	DATE
SHEET NO.	DATE
DRAWN BY	DATE
CHECKED BY	DATE
IN CHARGE	DATE
NO.	REV.
1. REV. CITY COMMENTS	

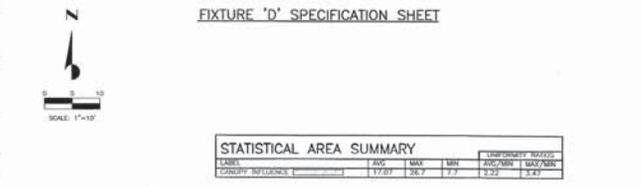
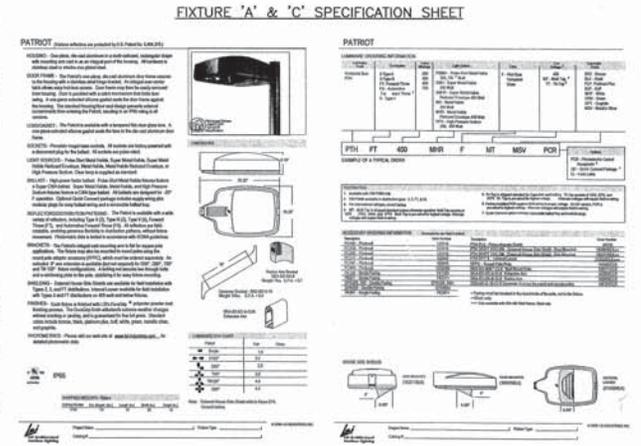
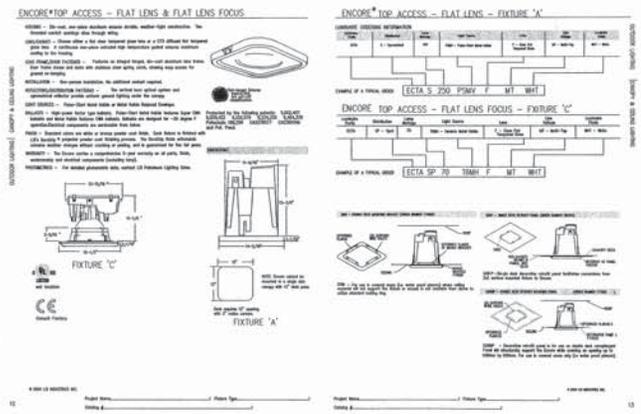
**Galloway**  
 ARCHITECTS  
 1200 17th Avenue, Suite 100  
 Broomfield, CO 80011  
 303.773.8888

**KING SOOPERS FUEL FACILITY CANOPY AND KIOSK STORE # 18**  
 HECLA HWY & PLAZA DRIVE  
 LOUISVILLE, COLORADO

Project No.	KSP000012
Client Name	FISL
Contract No.	MDC
Drawn By	DLS
Date	August 10, 2010
Scale	AS SHOWN



1 PHOTOMETRIC SITE PLAN  
SCALE: 1"=10'-0"



STATISTICAL AREA SUMMARY

FIXTURE	TYPE	FOOT CANDLE	BEAM	TEMPERATURE	RATIO
ENCORE	FLAT LENS	17.07	36°	1.77	1.34

LUMINAIRE SCHEDULE

FIXTURE	SYMBOL	QTY	POLE HEIGHT / MOUNTING HEIGHT	MOUNTING	RETAIL LUMENS	BEAM/DIAMETER AVERAGE	MANUFACTURER/CATALOG # (SEE NOTE 3 BELOW)	LAMP TYPE	WATTS
A	[Symbol]	12	15'-6"	SURFACE	23500	0.72	LSI LIGHTING, ENCORE SERIES RECTA-S-250-PSMV-F-MT-WHT	PULSE START METAL HALIDE	250
C	[Symbol]	10	15'-6"	SURFACE	6600	0.72	LSI LIGHTING, ENCORE FOCUS RECTA-SP-70-T8MBL-F-MT-WHT	CERAMIC METAL HALIDE	70
D	[Symbol]	3	24'-0"	POLE	20000	0.72	LSI LIGHTING, PATRIOT SERIES PTH-FT-200-PSMV-F-MT-WHT	PULSE START METAL HALIDE	200

REVISIONS

NO.	REVISION	DATE
1	PER CITY COMMENTS	8/29/15

DATE: 8/29/15

PROJECT: King Soopers Fuel Facility Canopy and Kiosk Store # 13

HECLA WAY & PLAZA DRIVE, LOUISVILLE, COLORADO

PHOTOMETRIC PLAN

7 OF 7

Moline moved and Howe seconded to continue the item to the August meeting.

**Speedy Sparkle PUD Amendment:** A request for approval of a Planned Unit Development Amendment to allow changes to the signage at 1414 Hecla Way. (Resolution 14, Series 2019)

- Applicant: Speedy Sparkle Car Wash – Louisville, LLC
- Case Manager: Felicity Selvoski, Planner/Historic Preservation

Rice asked for conflicts of interest. Hoefner stated that he was a customer at the Speedy Sparkle but did not think that disqualified him.

All notices met as required.

Selvoski presented the request to amend the sign code for three properties at Speedy Sparkle. The original PUD was approved in 2000 as the Black Diamond Car Wash with two monument signs. In 2010, the King Soopers Fueling Center PUD included a shared monument sign, as well. The PUD amendment included requests to modify and install a monument sign along Hecla Way, bring the installed menu signs into compliance, and build their own monument sign. This application did not include confirmation from the other two properties that they are okay with these changes.

The sign architecture is proposed for steel I-beams, a metal base cabinet, and an LED panel. The Hecla Way sign included the same steel I-beam architectural border. Staff used the CCDSG to evaluate the proposed signs. The materials are supposed to be compatible with the associated structure in terms of materials, color, and design, and staff does not feel that the signs meet this criteria. The Speedy Sparkle building does include steel I-beams, but staff did not find this to be a strong enough connection. This application would also result in three monument signs on the Speedy Sparkle property. Currently, the sign code allows for one and the original PUD allowed for two. The monument signs along South Boulder Road greatly exceed the 60 square feet. The proposed LED panel was not something permitted under the sign code. Finally, the proposed sign designs did not provide information to determine which sides were translucent and which were opaque and current sign code only allows the letters to be translucent; the sign background coloration did not match coloration elsewhere on the site; and the signs were not uniform in color, all of which is both are required in the current sign code.

Staff also addressed the draft sign code, though they did not use it to judge the application. One of the goals of the draft was to reduce sign clutter, which this application did not achieve. Electronic message centers are allowed in the draft code if there are exceptional circumstance and if they elevate the design. Staff did not feel they had enough information to determine this. This application also has signs taller than the maximum freestanding 5 feet height maximum on the existing PUD.

Staff recommends approval of the resolution, which would deny this application. The proposed PUD amendment did not meet the intent and requirements of our current design guidelines and any changes would need to be approved by the adjacent property owners that would be affected.

Rice asked if the current monument sign was at the maximum size or if it could be made bigger.

Zuccaro replied that the sign was likely built to what was allowed but he would confirm. He noted that at the time of the original PUD it made sense to combine the three signs from the three properties even though it required a waiver from the sign program.

Rice asked about the menu signs.

Selvoski replied that they were put in without going through the PUD process or a building permit and that they were not addressed under the current sign code.

Moline asked what it would take to modify the existing sign.

Selvoksi stated that it was already at the maximum size.

Zuccaro added that any change would require a PUD amendment because the sign did not currently adhere to the code.

Rice invited the applicant to make his presentation.

Robert Kearney, 549 North Fourth Street in Loveland, asked for a show of hands to who had been to the car wash and proceeded to hand out flyers. Vice Chair Rice informed Mr. Kearney that the Commission could not receive anything from the applicant at a public hearing. Kearney stated that the original PUD included all the property with the three owners. Under that PUD, the car wash was entitled to half of the joint sign on South Boulder Road. Speedy Sparkle occupied about half of the total property. The King Soopers PUD used to have a different sign requirement, but their PUD never had a signature from the car wash owner and the South Boulder Road sign is an off-premises sign to King Soopers. He did not want to diminish that sign, but they were asking for half of the signage space for Speedy Sparkle. He described that the other signs at King Soopers were larger than those at Speedy Sparkle. He stated that 54% of customers in a four-year study couldn't find signs due to being too small and customers complain that the lettering on signs was too small and that 81% of consumers appreciated LED signs. He listed other percentages to show that signage is important to businesses big and small. He and his business wanted to be treated fairly as King Soopers has been.

Chip Weincek from CWA Architecture described the history of the application, which had been started in September 2018. He believed that the proposal responded to the request to address the contextual built environment. They had had multiple meetings and revisions to their submittals and had not had much feedback from staff. Weincek proceeded to describe the application. The site plan showed that speedy sparkle was the largest property on the site. The shared signage, which was never recorded with the Speedy Sparkle property, was too small for the property. He showed that Speedy Sparkle and Jiffy Lube had 10 square feet on the shared sign each and King Soopers had 40 square feet. King Soopers also had a second monument sign for a total of 69 square feet. He showed the existing menu signs, reminding the Commission that the menu signs were not addressed in the code and the owner of Speedy Sparkle thought that that meant he could proceed to put them up. Weincek stated that the menu signs

had been very helpful for business. At first, they had requested a 12-foot sign and staff said that was too tall so they tried to lower it but thought that the 5-foot limit was too small. He showed the need for a sign on Hecla Way where customers enter the property. They were proposing to have Speedy Sparkle have its own monument sign, Jiffy Lube have 20 square feet, and King Soopers remain the same. He thought there was a good synergy of the owners in the area and they wanted to maintain that. They were also proposing to convert the flip-over numbers to LED, which is what King Soopers is doing for the fueling station. It was Weincek's understanding that staff supported turning the gas station numbers to LED. He stated that the code allows individual property owners to have up to 60 square feet of signage under the new sign code. He responded to staff's concerns about the architecture by pointing out that the architecture matched what was originally approved along Hecla and that the main architectural features of the Speedy Sparkle building was steel. He stated that the CDDSG allowed for multiple signs at multiple entries for identifying businesses. He noted that this was a critical part of the application, because the guidelines referred to signs and entries in the plural. 12 feet in a retail zone for monument signs were also allowed. Weincek showed a selection of other signs that had been approved under the current guidelines.

Weincek asked if it was possible to continue the application based on the Commission's deliberation.

Rice suggested that they proceed as normal and the applicant can request a continuance at the end if they chose.

Moline asked the applicant to walk through the incorporation of steel in the proposed signs.

Weincek showed the elements on the PowerPoint.

Moline asked about the requirement to have one sign per structure.

Weincek replied that they believed that came from Section 7.5.

Kearney added that Speedy Sparkle had an access point on Hecla Way and a curb cut between the car wash and the King Soopers fuel station.

Howe asked for clarification between the commercial and residential guidelines.

Zuccaro replied that the CDDSG applied to this property and this applicant was about being allowed to vary from the CDDSG.

Hoefner asked about the conversations that went on among Speedy Sparkle, King Soopers, and Jiffy Lube.

Kearney replied that the PUD addressed that shared sign, which was on the property line between Jiffy Lube and Speedy Sparkle. He noted that the various PUDs were in conflict. He and King Soopers were working together to try to fix this issues and as part

of that cooperation, King Soopers wanted to have the LED signs included in the amendment.

Hoefner asked how many monument signs they thought they were entitled to.

Chip replied that they thought they should be allowed one each for Speedy Sparkle, Jiffy Lube, and King Soopers. He added that they should also be allowed signs for each entry.

Hoefner asked if electronic message centers (EMCs) were allowed under the code.

Weincek replied that he understood that they were not allowed. He stated that the City needed to update its code on LED signs, because these were the future of commercial signs.

Hoefner asked what would happen if the Planning Commission granted the continuance, would the applicants be able to come to an agreement with smaller signs and no EMCs.

Weincek replied that he wanted to hear what the Commission had to say about the larger signs and the EMCs.

Kearney added that the original proposal tried to meet Director Zuccaro's goal of having fewer signs, but the signage was greater than 60 square feet. The applicants were happy with that at the time, but later they found out that the application was no longer acceptable. He believed that there was more than one way to make signage visibility happen.

Rice asked if the 2011 PUD amendment predated the applicants' ownership of the property.

Kearney stated that he had not been the owner at the time and that he could not speak to the original agreement among the property owners.

Zuccaro added that the boundary of the original PUD included all three of the properties and appeared to be validly approved by the City and recorded.

Rice replied that he wanted to make sure it was understood that there was still one PUD over the three properties.

Zuccaro replied that, as far as signage was concerned, yes.

Weincek stated that there was nothing about ownership of the current monument sign.

Rice replied that he did not think the Planning Commission could speak to property disputes. He invited members of the public to speak.

Laura Chenerock 1459 Hecla Way in Louisville, stated that she lived in the townhouse adjacent to the car wash and was a customer there. She stated that she represented

the people living in her building and other people in the area, some of whom had written to the Commission. She asked the Commission to consider the residential perspective, noting that the examples used in the presentations of other monument signs had not been near residential areas. She was concerned about the Hecla Way sign and thought it was out of balance with the residential area, especially given its size and lighting. She noted that the car wash stayed open after dark and their lights shined directly into her living room. She added that the sign might also be disruptive to wildlife given the light pollution. She appreciated the car wash's efforts in trying to blend in already, including planting trees.

Howe made a motion to include an email from the public in the record. Motion passed.

Zuccaro noted that the underlying code provided options to abandon the current sign program and go back to individual signs for each of the businesses. However, the applicants wanted to vary from the code. Staff therefore looked to the current policies on signage. Zuccaro noted one specific policy in the CDDSG, Section 7.2: "The size of the signs should be modest and provide businesses sufficient visibility and identification without becoming a dominant part of the landscape." When staff reviewed this outside of the context of what was allowed in the code, staff had to consider what worked within the context while also serving the business. Appropriate LED signs needed to be an improvement on what would be there without LED, as well, though staff was not the arbiter on what was appropriate for LED signs. Zuccaro also addressed Section 7.5 in the CDDSG about the plural of the monument signs, noting that they were allowed one monument sign per building. He explained that staff had view the Hecla Way sign in a residential context and signs in that context needed to be an improvement on the code. He concluded by stating that the Commission could approve, approve with conditions, or deny. He noted that it would be helpful for the Commission to have a discussion about their findings .

Hoefner asked for staff's perspective on the applicant's feeling that they had not provided sufficient feedback.

Zuccaro replied that staff intended to provide feedback so that applications could be ready to go before the Commission, but the applicant had to provide sufficient plans to comment on. In some cases, the applicant and staff did not agree on certain elements of the proposal, so staff was looking to the Commission to make those decisions. Zuccaro added that staff did not make designs, but they did try to provide feedback on proposals.

Hoefner asked about the allowance for one monument sign per building.

Zuccaro responded that with the existing PUD the car wash had received a waiver to have two different signs in 2010. There were other waivers for other property owners, as well. The property owners would have to abandon the PUD in order to have a monument sign on one of their road frontages. He did not think that would work sign gas stations and car washes would likely need sign frontage on South Boulder Road.

Hoefner asked about the initial iteration of the design where there were fewer, larger signs.

Zuccaro replied that the original proposal had a sign area of 120 square feet. Staff told the applicant that something over 60 square feet might work, but 120 square feet was too much.

Kearney stated that he appreciated staff's time and that the King Soopers PUD was not recorded on the car wash's property. He described the main issue as a fundamental matter of equity and following the code. King Soopers had many large signs, which the car wash did not want for themselves. They wanted to have decent signage exposure. He appreciated any direction from the Commission to work with staff to get decent signage for his one-acre property. Having increased signage would make a big difference for the business.

Rice asked for commissioner comments.

Howe appreciated Speedy Sparkle's requests and agreed with the right to have decent signage exposure. He saw the three different signs as three different matters. He thought that they were entitled to have a sign on South Boulder Road based on Section 7.2B. However, the sign that was proposed – though they were entitled to it – did not meet the guidelines as proposed due to its size and lighting. As for the menu signs, he thought that there was no issue with them since they were not referenced. As for the Hecla Way sign, Howe quoted Section 7.5, again finding that the applicant was entitled to a sign there but it had to be responsive to the “family of signs” as described in 7.5.

Hoefner stated that he was sympathetic to the fact that the existing sign was very small. He thought that the Commission needed to find a way to approve something bigger on South Boulder Road, but he did not think that the proposed sign was it. He did not think the Commission would approve an EMC and did not support it himself. He also thought the proposed sign on Hecla was too big given the residential context. He did not see any issues with the menu boards. As for the proposed materials for South Boulder, he thought that the I-beams blended in with the building. He did not think that the PUD issue was in the Commission's wheelhouse and the applicant needed to resolve that with King Soopers.

Moline stated that he wanted Speedy Sparkle to succeed and he hoped that they could find a way to make it work. He thought that staff and the Commission had spent a considerable amount of time reviewing the new sign code and had heard a lot of feedback from the community, and the proposal was in a space where the Commission was being influenced by community desires and the code that was being developed. He thought it was helpful to look toward the future code in the case of a PUD amendment.

Rice stated that in his view it was not the Commission's role to micromanage signs and some of the criticisms of this proposal were micromanaging. He saw that the applicant needed adequate signage. However, given the grouping of the signs, he thought they should be treated together and he was concerned that the signs were being treated piecemeal instead of with all the property owners. He thought it was doable to work with all the property owners at once. He believed that Louisville should be a business-friendly community with adequate signage. He stated that he thought that if there were going to be menu signs, they should be approved by the City and so they should be

addressed in the code. He noted that the Commission and the Council had not approved the new sign code and it could not be acted upon based on this application and he was therefore not prepared to approve any EMCs. He thought that a continuance was a good way to handle this so staff and the applicant could take another look at it. He was also sympathetic to the fact that it had already been a long process, so it should be a priority for staff and the applicant. He asked for a motion.

Zuccaro recommended continuing it to a date certain and suggested asking the applicant about a reasonable timeframe. The applicant agreed to a 60-day timeline.

Howe noted that there may be additional parties that might become involved in the process.

Rice added that they might need to bring a totally new application and that would require a new public hearing.

Zuccaro replied that the Commission could take no action if there should be a new application.

Chip thanked everyone for their feedback and thought that the new information would be helpful. He agreed that September 12<sup>th</sup> would be doable and that he did not want to make a new application.

Zuccaro noted that the new sign code may come into effect and that might make the process more complicated.

Rice stated that the Commission should continue it to September 12<sup>th</sup> and if any issues come up staff and the applicant could deal with that at that time.

Hoefner made a motion to continue this application consistent with the discussion tonight to September 12, 2019. Moline seconded. Voice vote. Motion carried unanimously.

5-minute recess.

**824 South Street/957 Street PUD Extension and SRU Amendment:** A request for a one-year extension to the 824 South Street/957 Main Street Planned Unit Development and an Amendment to the Special Review Use for outdoor sales of retail goods and eating and drinking establishments. (Resolution 15, Series 2019)

- Applicant: Hartronft Associates, P.C.
- Case Manager: Rob Zuccaro, Director of Planning and Building Safety

Public notice met as required.

Zuccaro presented two requests for 824 South. The applicant acquired the property in 2018 and was working with staff to make changes under administrative review, but the change in the 2016 SRU and the extension had to go through a public hearing. The applicant believed that the one-year extension would provide adequate time. To evaluate the extension, staff considered what had changed from 2016 had found that

**Planning Commission  
Meeting Minutes  
October 10<sup>th</sup>, 2019  
City Hall, Council Chambers  
749 Main Street  
6:30 PM**

**Call to Order** – Chair Brauneis called the meeting to order at 6:31 PM.

**Roll Call** was taken and the following members were present:

Commission Members Present: Steve Brauneis, Chair  
Tom Rice, Vice Chair  
Keaton Howe  
Jeff Moline  
Dietrich Hoefner  
Debra Williams

Commission Members Absent: None.

Staff Members Present: Rob Zuccaro, Dir. of Planning & Building  
Lisa Ritchie, Senior Planner  
Harry Brennan, Planner II  
Amelia Brackett Hogstad, Planning Clerk

**APPROVAL OF AGENDA**

Howe moved and Moline seconded a motion to approve the October 10<sup>th</sup>, 2019 agenda. Motion passed unanimously by voice vote.

**APPROVAL OF MINUTES**

Moline moved and Williams seconded a motion to approve the September 12<sup>th</sup>, 2019 minutes. Motion passed unanimously by voice vote.

**PUBLIC COMMENTS ON ITEMS NOT ON THE AGENDA**

None.

**NEW BUSINESS – PUBLIC HEARING ITEMS**

**Speedy Sparkle PUD Amendment:** A request for approval of a Planned Unit Development Amendment to allow sign design waivers for 1414 and 1408 Hecla Way and 1712 Plaza Drive. (Resolution 14, Series 2019)

- Applicant: Speedy Sparkle Car Wash – Louisville, LLC
- Case Manager: Rob Zuccaro, Director of Planning and Building Safety

All public notice met as required.

Zuccaro stated that the applicant had made revisions to the previous application that went before the Commission in July, making this a new application with new public notice. He explained that any modifications to a joint sign among property owners required all owners' participation. However, this application only included changes to the Speedy Sparkle property.

Zuccaro summarized the proposal and the locations of the proposed signs.

1. Existing joint monument sign: The applicant proposed a note that the Speedy Sparkle sign face be removed from this sign. Staff requested clarification about whether the sign would be left blank and interpreted the note to mean that any of the three property owners could put up a new sign panel in that location. The applicant was also proposing a new access easement to allow the other property owners to perform maintenance on the sign, which staff supported.
2. New sign along South Boulder Road: The applicant's notation measured the sign at 9 feet and staff measured it as 9 ½ feet, which included the proposed oval panel. It was 47.5 square feet and contained three sign panels, according to the way staff measures signs based on the CCDSG. Staff was waiting for confirmation about whether the sign would be transparent or opaque.
3. New sign along Hecla Way: Staff counted four panels, for a total of 5'8" tall and 26 square feet. The proposed sign was internally illuminated and staff assumed that it was translucent, though they were still waiting for confirmation.
4. Menu board signs: These signs were already installed. The current guidelines did not address menu boards and menu boards had a varied record of approval. Staff felt they needed to be addressed on this PUD, since similar signs had been addressed on other PUDs.
5. There is also a sign, 14-feet wide and 24 inches tall, already on the building that received a building permit but was not included on the PUD.

Zuccaro explained the history of the PUDs since it informed staff's recommendation. In 2000, there was an agreement to have a joint monument sign between Black Diamond Car Wash and Lehrer's Flowers PUDs. In 2005, there was an amendment with a note to maintain the joint monument sign. In 2010, with the addition of the King Soopers fueling station, the PUD included a joint monument sign, as well. Through the history of the joint monument sign, the applicants had worked together to develop the joint monument sign, and, in that PUD, they addressed the "rural-suburban context" in recognition of the open space across the road. They mentioned that excessive signage would detract from that open space and they attended to that in their sign design. The sign got larger over time but the intent remained the same.

Zuccaro showed the sign mock-ups from the July 11<sup>th</sup> meeting. The design proposed an LED message board on the South Boulder monument sign that has since been removed. The applicant also brought the Hecla sign down closer to the approved sign from 2000 that was never built, in response to the Commission's note that the Hecla sign was too high for the context of the area. Zuccaro presented alternatives that staff had proposed to the applicant, which included maintaining the current monument base and joint sign and lifting the sign up, which would result in double the sign panel size for Car Wash and Jiffy Lube, and an 80 square foot sign area (which would still require a waiver.) Compared to other existing signs in the city, a 2x10 or a 4x5 panel sign would

be among the largest multi-panel signs in the city and would improve their signage space significantly, while controlling sign clutter.

Zuccaro reminded the Commission that if the application met code, they would not need a hearing for a waiver approval. The applicant was requesting waivers for the following:

- Number of signs allowed
- Sign illumination
- Sign cabinet
- Sign material
- Menu board signs

Zuccaro also presented the waiver criteria in Section 17.28.110 of the Code.

1. Sign clutter – not effective in wayfinding, distracting and unsafe, detracts from community character.
2. Proportionality of sign area to development and frontage – two signs on single frontage with no access drives was excessive.
3. Quality of sign material and design should exceed minimum standards for a waiver – should match building material, provide multiple materials and texture. Zuccaro noted that the non-conforming illumination did not meet best standards here.
4. Visibility and legibility – South Boulder Road was on the slower end of the city's commercial roadways and staff believes that staff's alternatives would still allow for visibility based on visibility studies that match size to visibility at different distances.

For the spirit and intent waiver criteria, Zuccaro noted that the most relevant criteria addressed the context of the area. To keep that “appropriate relationship to the existing area” stated in the Code, staff recommended improving the existing joint monument sign.

Staff finds the proposal does not meet the PUD waiver criteria in LMC Sec. 17.28.110 and recommends adoption of Resolution 14, Series 2019 recommending denial of the proposal to City Council.

Williams asked to see the location of the never-built sign.

Zuccaro replied that it was on Hecla Way and generally close to the proposed location for the sign on Hecla.

Williams asked if the applicant could build that sign today.

Zuccaro replied that the Code did not void the previous PUD, so they could still request an extension, but they could not build it today without going through that process since the PUD had expired.

Williams asked if it was possible for the two smaller signs to be on top of the King Soopers sign, or if KS had right of refusal.

Zuccaro replied that from a city standpoint they were looking at the design of the sign and that discussion would have to be worked out among the property owners. Staff could not approve a change like that without going through a review process, since it would still require a waiver. He did not think there was a big difference from staff's analysis whether one business was on top versus another. He noted that staff does not usually propose alternatives, but with the history of the joint sign staff thought it was important to provide alternatives using the existing sign.

Rice asked if the menu signs had been included in a sign permit or a PUD.

Zuccaro replied that they had not.

Rice asked if the proposed South Boulder Road sign met the sign requirements without a waiver.

Zuccaro replied that it did.

Moline asked for clarification on the monument signs.

Zuccaro showed the two existing monument signs across the properties and noted that the CCDSG allowed one monument sign per building. Usually that applied if each lot was getting their signs independently, but once you have a joint sign for multiple properties you typically do not get an additional monument sign for each property.

Howe asked if staff had tried to contact the entity that controls the sign.

Zuccaro replied that there were three property owners that used the sign and that it was located on two properties. The applicant did provide authorization letters from the other owners in the beginning, but with the new design staff was still waiting for updated authorization letters. Staff has not reached out to them directly.

Williams asked if the South Boulder Road sign could be moved to the eastern edge.

Zuccaro replied that Commissioner Williams could ask the applicant if they were interested in that option. There was a retaining wall in that location and a sign there could be possible.

Howe asked for clarification on the easement around the property.

Zuccaro replied that the applicant was proposed a 10-foot easement for the sign. To staff's knowledge there is no easement to that effect currently.

Brauneis asked for the applicant presentation.

Robert Kearney, owner of the Speedy Sparkle Car Wash, stated that the car wash PUD originally allowed for one half of the original joint sign. The other half of the joint sign was meant to be for the other property. King Soopers subsequently did a PUD for their property that changed the shared sign. That sign was not recorded on the car wash PUD and the former owner had not signed off on it. In that process, the car wash went

from half of the sign to 1/6<sup>th</sup>. He characterized what happened as a taking of Speedy Sparkle's right to an adequate sign.

Kearney described two ways to solve the problem. First, all three property owners could agree to change the current sign and do PUD amendments for all the properties. That would mean a bigger joint sign. Alternatively, the applicant could amend the car wash PUD only and build their own monument sign on their property, as proposed in July. The applicant wanted to correct the PUD amendment, obtain equitable signage, and obtain adequate signage. He noted that the proposed sign on South Boulder Road and the sign on Hecla Way were smaller than allowed.

Kearney showed a board that compared the proposed car wash signs plus the menu signs with the existing King Soopers signs, pointing out the difference in signage among the property owners. He showed the customer feedback forms from the car wash customers voicing support of their proposed signs. Customers felt it was difficult to find the car wash, even with GPS. He explained that the new application proposed a smaller sign on Hecla Way than they had proposed in July and that they had taken out the electronic message sign as requested by the Commission. He responded to Commissioner William's question, explaining that they could not mount the sign at the eastern section of the property.

Hoefner and Kearney discussed the effect of the Black Diamond history on the current Speedy Sparkle application. Hoefner wanted to know if the applicant had thought he would be entitled to more signage when he bought the property and Kearney explained that he had not thought about the signage when he bought the place, but that it was important that the King Soopers PUD was not in the public record for the Black Diamond Car Wash.

Hoefner then asked about the translucence of the sign.

Kearney replied that the Speedy Sparkle logo had a bright green background that would be translucent and the two additional panels would have translucent lettering with darker backgrounds.

Brauneis asked to enter the sign board into the record. Moline moved and Hoefner seconded. Motion approved unanimously by voice vote.

Chip Weincek, of CW Architecture at 672 West Pine Street in Louisville, showed the logo of the Speedy Sparkle Car Wash. He explained that logos and signage were a big deal for small businesses.

Kearney introduced his employees and his wife in the audience.

Weincek described the process so far starting in September 2018, summarizing the comments from the July Planning Commission meeting. He explained that staff had not focused on those comments, instead focusing on other concerns. He also felt the meetings with staff had been too short.

Williams asked if the property owner knew of the two signs, the joint sign and the one approved in 2000, when he was buying the property.

Kearney replied that he did not know of them when he bought the property. The original PUD is recorded and shows up in the chain of title. Nowhere there was the joint monument sign. He did not look at the PUD in detail until after he bought it, especially as it related to the signage, and at that time he saw that the only PUD recorded on the property showed his property having half of the joint sign. He later discovered that the King Soopers PUD from 2010 was not recorded in the chain of title. He did not think that King Soopers would agree that he should get half the sign at this point.

Weincek continued that Speedy Sparkle had inherited the PUD conflict with King Soopers. He showed the areas of the property that had a drainage ditch, explaining that there could be no signs there. They also wanted to avoid putting signage in front of the King Soopers sign. He stated that they did not care what happened with the spot on the joint monument sign and would do what the City, Jiffy Lube, or King Soopers wanted to do.

Weincek shared his interpretation of the CDDSG and City Code 17.24.110. He explained that the sign design reinforced the project architecture with the Steel I-Beams and the concrete bases. He showed the changes they made to the Hecla sign based on commissioner feedback from July. He requested input from the City on what was transparent versus opaque. He and the applicant were proposing that the green background of the sign would be transparent, as would the bubbles and the lettering on the other panels.

Weincek also showed picture of sign clutter and empty sign panels around Louisville. He did not think that multi-tenant signs applied to this situation, since those usually have one landlord. In this case, individual building monument signs with single owners were more relevant. He presented other examples of individual monument signs and stated that the proposed signs for Speedy Sparkle were reasonably sized. He added that he thought LED was the future of signage even though they had taken off the LED portion from their application after the July review. He also showed pictures of city signs that he did not feel were compliant signs and he showed examples of menu boards around Louisville.

Weincek asked the Commission to:

1. Approve sign sizes as proposed.
2. Accept applicant's application and PUD conflict resolution.
3. Accept easement.
4. Clarify the meaning of translucent and opaque signage.
5. Provide applicant with the acceptable requirements for the proposed blank sign panel on the joint monument sign.

Moline asked why the applicant did not think the sign code covered the menu signs, since it stated that all signs had to get a permit except for a few exceptions.

Weincek explained that he knew, as an architect, that you had to get a permit, but the business owner had not known that.

Moline summarized Mr. Weincek's response, saying that it sounded like the signs should have permitted but were not. He then asked if the existing monument sign was on the Speedy Sparkle property.

Weincek replied that it was on the property line between Jiffy Lube and Speedy Sparkle.

Hoefner asked for Mr. Weincek's opinion on the 2x10 panel on the existing monument sign, an alternative proposed by staff, which would provide a slightly larger square footage than what the property would have had under the 50-50 split from the previous PUD amendment.

Weincek replied that there was an original approval that split the signage 50-50 that had been changed with the King Soopers PUD.

Hoefner clarified that he meant that the area of half of the amended sign at that time was about 4x9 feet and when you split that in half you get a very similar square footage to what staff was proposing with the higher joint sign.

Weincek replied that he wanted to know how King Soopers had gotten 69 square feet out of that.

Hoefner stated that he was asking about the stated desire to have the 50% of the sign from the previous amendment.

Weincek replied that the 50/50 split was never allocated.

Hoefner asked for a yes or no response on if Mr. Weincek wanted more than the square footage the property would have had if the sign had been split as amended.

Weincek replied that the sign had never been built and therefore did not apply to today.

Hoefner responded that his understanding of Mr. Weincek's presentation was that the 2005 amendment was the relevant signage. He and Mr. Weincek continued to discuss the relevancy of the different historical amendments, King Soopers's PUD, and staff's alternative proposals.

Brauneis observed that the proposed sign and its illumination did not meet city requirements.

Kearney replied that the logo would be translucent. The \$4 bubble and the free vacuums signage were not part of the logo, so only the lettering would be translucent. He was not aware that there were not any light limitations other than translucent and opaque.

Weincek added that the applicant would like the Commission to narrow the scope of conversations with staff if staff and the applicant had to work together again.

Brauneis asked for questions of staff.

Williams asked if the current King Soopers sign was on the Speedy Sparkle PUD.

Zuccaro replied that it was approved in 2010 by City Council and he did not know if it came up in the applicant's title work. He noted that the Black Diamond Car Wash owner had not signed the PUD.

Brauneis asked if there were regulations regarding adjacency of monument signs.

Zuccaro replied that there was no minimum distance between monument signs. He added that they could not be within sight lines of intersections.

Brauneis asked if there were any requirements for vacating signs.

Zuccaro replied that blank panels usually meant there was a vacancy.

Brauneis asked about the difference between a monument sign and a wayfinding sign.

Zuccaro replied that there was a provision for on-site directional signs and the sign area was quite small, though applicants could get larger areas approved through a PUD.

Brauneis asked if directional signage was typically illuminated.

Zuccaro replied that they were usually very small signs within a site and he did not think they were typically illuminated. A larger sign for a shopping center that was also trying to be a directional sign could very well be lit.

Brauneis asked for public comment.

Michael Pao, 1817 Sweet Clover Lane, stated that he lived near Speedy Sparkle and was a patron of the business. He thought the proposed sign on Hecla Way would be out of character and detract from the area, particularly as an illuminated sign. At the same time, he felt confused about how impactful it would be in terms of directing people to Speedy Sparkle, since once you turn on Hecla, it was hard to miss.

Rice moved to include the additional emails into the record. Howe seconded. Motion passed unanimously by voice vote.

Greg Jones, 1809 Sweet Clover Lane, was a customer and a neighbor. He was also concerned about the sign on Hecla Way. He noted that there would be more businesses on Hecla and he was concerned that this would set a precedent for signs there. The menu signs were now turned off at night, but the first couple months when they were on all night was annoying. He appreciated that they turned them off. He thought the light pollution was concerning as well.

Break. Reconvened at 8:50 PM.

Brauneis asked for additional questions of staff. Seeing none, he requested a staff closing statement. None. Brauneis requested an applicant closing statement.

Kearney thanked the neighbors for their comments. He stated that the Hecla Way sign would only be illuminated during the day and South Boulder Road would be on a longer time switch. He stated that the Hecla signage was a safety issue.

Brauneis closed public hearing and requested commissioner comments and deliberation.

Howe thanked staff and the applicant for their presentations. He thought that the South Boulder Road proposed sign did meet the code if they followed the transparent/opaque division. He thought they had a right to a sign there and that better signage would improve business. He thought that the Hecla Way sign did not need a light especially in view of the neighbor's concerns and that a 5-foot sign without a light would be adequate and would not detract from the residential area.

Hoefner did not think that the proposal would benefit from a lot of detailed feedback, because he believed that the applicant needed to go back and work with staff to come back with something that better confirmed to the Code. Examples of noncomplying signs notwithstanding, the City wanted to get this one right. He was sympathetic that the applicant needed more signage, but he did not understand why staff's suggestions were inadequate in their eyes.

Moline agreed with Commissioner Hoefner. There were elements of the proposal that he did not have concerns with. Overall, he agreed with staff's concerns especially about the monument sign. He was for small businesses having appropriate signage, but the community also cared deeply about the city's visual landscape. He added that he was prepared to recommend a denial and they could make their case in front of City Council.

Williams agreed with staff in feeling that it did not meet the Code. She thought the best-case scenario would be enlarging the King Soopers shared sign as in staff's alternative proposals or putting the Speedy Sparkle sign above the King Soopers sign. She appreciated the applicant's proposal and she noted that some existing signs had been grandfathered in as the Code changed over time. The sign code existed for a reason and it would not be a good decision as a planning commissioner to go against code. She recommended denial.

Rice thought the ideal solution was to work together on a shared sign. However, that was not the proposal that was before the commission tonight. He also thought that among three property owners it would be difficult to make that happen. He had hoped that there would be a consensus proposal after the July meeting, but it seems like that did not occur. The proposal tonight got 90% of the way to addressing his concerns from the July meeting. He agreed that Speedy Sparkle did not have adequate signage for their business and the current signage was inadequate. He would like a new PUD amendment that dealt with all the signs so that one day, when the current owner sold the property, the City and the future owner did not have to go through this process again. He thought the bubbles and the letters as translucent met the Code, but having the entire sign translucent did not. He was sensitive to the neighbors near Hecla and he was not in favor of an additional monument sign there. As he read the Code, there was one sign per building, which in this case was covered by the proposed monument sign

on South Boulder Road. He agreed with Commissioner Howe that a directional sign on Hecla Way should be small and not have illumination, since it was for wayfinding and not advertising. He was more in favor of the proposal than against at this point. He wished that there was more of a consensus between staff and the applicant.

Brauneis appreciated that there was a difficult history and that it was a multi-owner sign. He agreed that the existing signage was too small. He hoped that the applicant understood that the Commission wanted to find a solution to that problem. He did not find the translucency or the concrete base on the South Boulder monument sign to be in line with the Code. He agreed with Commissioners Howe and Rice that the Hecla Way sign was a directional sign and that the residential concern was an important one. He appreciated that there were a number of examples of clutter existing in the sign, but he stated that those types of signage situations were not the goal for future signage.

Moline moved to approve Resolution 14, Series 2019, to deny the proposal. Williams seconded. Motion passed 4-2, with Commissioners Howe and Rice voting no.

**The Business Center at CTC GDP Amendment G, The Business Center at CTC Replat I Final Plat and Final PUD:** A request for an amendment to the Business Center at CTC General Development Plan, a request for a Final Plat and Final Planned United Development to allow the construction of an office building and associated site improvements at 1411 S. Arthur Avenue. (Resolution 17, Series 2019)

- Applicant: Andy Johnson, DAJ Design
- Case Manager: Lisa Ritchie, Senior Planner

Brauneis asked for conflicts of interest.

Howe disclosed that a member of the applicant team was one of his clients, but he did not have any interests in the project and did not think it would affect his judgement.

Public notice met as required.

Ritchie presented the Cantilevers proposal. The Business Center at CTC for this portion of the CTC was first approved in 1998 and was amendment for the first time in 1999. Amendment A from 1999 was in effect for these properties. Permitted uses and development standards were different for each lot. In 2013, a CTC Connectivity Study looked at options for road connections into CTC at this location. When 305 South Arthur came up in 2016, those applicants dedicated their half of the right-of-way. This proposal tonight dedicates the other half, meeting the goals in the Connectivity Study and the Transportation Master Plan.

Ritchie explained that there were three parts to the proposal: requiring both lots to develop under the CDDSG, amending permitted uses on both lots to allow industrial, office and limited commercial, and maintaining the PCZD-Industrial zoning designation. The proposal encouraged coordinated community design, accommodating more land uses that may now be viable due to the possible street connection, and it provided additional economic opportunity in the CTC. The plat has two goals: consolidate the two lots into one to allow development over the property line as well as to dedicate the other 30 feet of the right-of-way to a street connection. The replat conforms to the

requirements in the Code and is consistent with the City's comprehensive plan and Transportation Master Plan.

Ritchie presented the proposed design, which involved a U-shaped building facing 96<sup>th</sup> Street and mountain views. They were proposing underground parking to meet some of their requirement. They were also proposing multiple drainage locations through a series of ponds, rather than one large retention pond, which staff thought was a good plan. There was a Northern Colorado Water Conservancy District easement which restricted what they can and cannot do. They have indicated no concerns with this application. They indicated no issues with this proposal and staff would continue to work with them. The base would be unfinished, exposed concrete and there was a metal material but with a finished wood. It was much higher in design than typical in an IDDSG. She also showed the inside plan. There would be individually owned condo spaces with shared amenities.

Ritchie presented the 4 waivers requested.

1. Request to allow a 8'-10' parking setback on a portion where 10'0" is required.
2. Request to allow a maximum height of 40'6" where 35'0" is required.
3. Request to allow unfinished concrete and the use of metal.
4. Request for relief from the requirement for 1 tree for every 40 feet of property boundary.

Staff recommends approval of the resolution with the following condition:

1. Prior to the recordation of the PUD, the applicant shall record an easement allowing emergency access onto the property to the north.

Howe asked what the current small office space leased and unleased rates were in Louisville.

Ritchie replied that she did not know.

Moline asked about the property to the north.

Ritchie replied that they had received a mailing and there were signs on the property, but she had not heard from them.

Ritchie explained the updated landscape plan and requested that it be entered into the record. Moline moved and Rice seconded. Motion approved unanimously by voice vote.

Moline asked if the roundabout would be part of the property.

Ritchie replied that the connectivity study provided a number of different orientations, including a roundabout, but one was not proposed here.

Moline asked about funding for the roadway construction.

Ritchie replied that the City was not asking for contribution from this applicant.

Howe asked how the water pipeline easement would affect the construction of the road.

Ritchie replied that she did not know, but 96<sup>th</sup> Street was in the area of the pipeline, as well.

Howe asked if there was any precedent for roads not being able to be built due to water easements.

Ritchie replied that she could not answer that question.

Moline replied that many pipelines were long and went under many roads in Boulder County.

Brauneis asked about the no-trees-allowed request.

Ritchie replied that trees could disrupt water pipes.

Howe asked if the road would be accessed by the entire CTC.

Ritchie replied that it would be a public street that would be constructed to public street standards.

Howe asked if there had been any ideas on the impact of Highway 42.

Ritchie replied that the study addressed it and this was its recommendation. Staff would closely evaluate the impact during the planning and development for this project, including what kind of signaling or turn lanes would be needed.

Howe noted that it seemed like a narrow corridor for the road with a lot of movement, including a bridge and a railway.

Brauneis asked for further questions of staff. Seeing none, he invited the applicant to present.

Andy Johnson, DAJ Design at 922A Main Street, presented the project. He explained that the office condominiums in the proposal would be marketed for sale, not lease, though there were lease options. The site, with an entrance from Arthur Street, was one of the highest points in Louisville and faced 96<sup>th</sup> Street. The applicants wanted to dedicate a significant amount of the large property to landscaping in the form of 'outdoor rooms.' The building would be in the center of the lot and parking would be to the east of the building and the design concept took inspiration from the landscape. Inside, the plan was to use a cross-laminated wood construction, making the interior a warm and natural environment despite being a modern building. He explained that the owners are proposed to donate 13,000 square feet to the City to build out that road. He stated that the average occupancy would be 5 per office for a total of 165 occupants, though the occupancy for each condominium would vary. He passed around 3Ds models and materials samples to the commissioners.

Johnson explained that for parking, each condominium would get a space in the underground parking garage. They have about 50 spaces for bikes. He highlighted that the building had a highly efficient building envelope, heating and cooling ventilation, and

20,000 SF of roof area, and the entire subterranean parking area would be wired for car charging. He explained the multiple infiltration ponds, which allowed the parking lot to be more unique and the spaces to be used as a landscape feature. There would be bridges crossing them and places to sit alongside them. There would be 195 total parking spaces, whereas the required parking per code would be 243 spaces. Johnson noted that the CTC had a number of empty parking spaces and that the proposal's calculations accounted for visitation rates and integration of parking into the landscape.

Williams moved to introduce the samples and the 3D samples into the public record. Moline seconded. Voice vote all in favor.

Williams asked about the envisioned clientele.

Johnson replied that it was being marketed toward tech companies, wealth management firms, lawyers, investment firms, and the like. There were a number of LOIs already signed or in the works.

Rice asked about the timeline.

Johnson replied that they were hoping for a late spring start to construction.

Moline asked how the building would appear to other parts of town.

Johnson showed a view looking from 96<sup>th</sup> Street. There would be foreground between the street and the building. The building would sit on an overlook but was pushed down and nestled into the hill rather than protruding. Also, pushing the building down helped with the acoustics of the road around the building. It was, however, highly visible from 96<sup>th</sup> Street.

Howe asked if there would be any changes if the road was never built.

Johnson replied that there would be no change. The road would be a beautiful addition to the CTC, but they were aware that the road may never happen. He added that their civil engineer went the extra distance and created a grading plan that reflects the ability to do the road to make it fairly easy for the City to do it, without getting into the design of the road. And they did not want to be out of compliance if the road did go in, but if it never happened that would not be a deal-breaker.

Howe asked if the concrete base was architectural or structural.

Johnson replied that it served both roles, but the forms were high-quality architectural concrete, not a structural concrete.

Williams asked about the shelf life of a product like this and what would happen if the owners wanted to repurpose it.

Jason Collier, developer on the project, responded that each unit would be titled individually, but one person could buy up several of them and convert them into a shared office space. The modular idea also allowed them to have logical groupings

horizontally and vertically among the modules. Open floor plans, for example, have a central kitchen area and presentation area, so plumbing and other accommodations for those kinds of floor plans had been built into the plan.

Howe asked about soil types for the grading.

Johnson responded that projects always go through a soil investigation. Most of the CTC has been on a traditional foundation system and has not required a lot of extra geotechnical work.

Howe asked if the units were for purchase or for lease.

Collier replied that each module could be owned and leased differently.

Brauneis asked for staff and applicant closing statements. Seeing none, he closed the public hearing and opened up commissioner comment.

Williams stated that the design was refreshing and unique in Louisville. Her biggest concern had been the repurposing and shelf life, which Collier had addressed.

Moline noted that it was a prominent spot in Louisville and would be a neat space for that area. He appreciated staff's presentation on the waiver criteria.

Rice thanked the applicant for reaching so high and thought it was a perfect thing to put on that hillside. He agreed with staff's analysis on the waivers.

Hoefner agreed and thought it was an interesting project and the waivers were relatively minor and easy to say yes to and were more than compensating for by the other features.

Howe stated that it was one of the best architecture presentations he had heard. He noted a concern about the business model. In light of the big box boom, residents were tired of seeing vacancies and attempts to ride booms and trends had not been successful. He was optimistic but he hoped that there would not be a lot of vacancies here in the future.

Brauneis appreciated that it was on a promontory. He appreciated the level of architecture that went into the project.

Rice made a motion to approve Resolution 17, Series 2019 with the condition as stated by staff Hoefner seconded. Motion passed unanimously by roll call vote.

**The Business Center at CTC Replat J Final Plat and Final PUD:** A request for approval of a Final Plat to consolidate two lots into one, and approval of a Final Planned United Development to allow construction of a structure and associated site improvements at 1875 Taylor Ave. (Resolution 18, Series 2019) **REQUEST TO CONTINUE TO NOVEMBER 14, 2019**

- Applicant: RVP Architecture
- Case Manager: Harry Brennan, Planner II

Rice made a motion to continue to November 14<sup>th</sup>. Voice vote all in favor.

**COMMISSIONER COMMENTS**

None.

**STAFF COMMENTS**

None.

**ITEMS TENTATIVELY SCHEDULED FOR OCTOBER 10<sup>TH</sup>, 2019**

- The Business Center at CTC Replat J Final Plat and Final PUD – Continuance
- Moxie SRU
- Parcel O GDP Amendment

**Adjourn:** Rice moved to adjourn. General agreement to a second. Adjourned at 10:15 PM.



## 7. Sign Design

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### Goal:

*Signs should be consistent with project and overall development design but should be subordinate to architectural and landscape elements. Signs serve to identify, inform, direct, regulate and interpret. Each commercial building or group of commercial buildings should have a consistent and comprehensive sign program from project identification at the street through individual tenant suite identity. Placement, scale, and readability should be considered in developing a sign package.*

### 7.1 Sign Materials

#### Policy:

Design and construct signs of durable, high quality architectural materials.

#### Standards and Guidelines:

- A. The sign package must utilize materials, colors, and designs that are compatible with the associated structures. (S)
- B. Sign materials must be of proven durability. (S)

### 7.2 Sign Number and Area

#### Policy:

The size of signs should be modest and afford businesses sufficient visibility and identification without becoming a dominant part of the landscape or interfering with vehicular movement along the public streets.

#### Standards and Guidelines:

- A. Number of Signs - Commercial / Retail
  - 1) The maximum number of building-mounted signs allowed for freestanding buildings is one per individual tenant building frontage, not to exceed three signs. (S)
- B. Number of Signs - Commercial / Office
  - 1) One monument sign is permitted for each freestanding building. (S)
  - 2) Where a freestanding office building contains multiple tenants, or multiple accesses off a public right of way, an increase in the number of monument-



7.2 A & B FLUSH MOUNTED SIGNS (SEE TEXT).



7.2 B FREESTANDING SIGNS (SEE TEXT).

- signs may be permitted in the planned unit development plan process. (G)
- 3) Office building-mounted signs not exceeding 40 square feet of surface area each, and not exceeding 100 square feet total, are permitted. (S)
- C. Area of Signs: Character Heights
- 1) Monument signs shall not exceed 60 square feet per face in retail zones nor 40 square feet in office zones. (S)
  - 2) Maximum area of Commercial / Retail building-mounted signs shall be 1 square foot of sign area per linear foot of building frontage of the individual business. No individual sign shall exceed 200 square feet. (S)
  - 3) Projecting signs may not exceed 4 square feet in area. (S)
  - 4) Character heights for commercial retail and office signs shall be limited to a maximum height of 24 inches and a minimum height on monument signs of 8 inches. (S)
  - 5) Character heights may be further limited through the PUD process based on architectural compatibility and site development context. (S)
  - 6) Maximum area of commercial / office signs is as set forth in subsection B.3 above. (S)

### 7.3 Location/Placement/Visibility

#### Policy:

Signs should be located with visibility from streets and paths without conflicting with safe vehicular movement.

#### Standards and Guidelines:

- A. Signs shall be sufficiently visible from public streets so that site entrances can be readily identified by both pedestrians and persons in vehicles. (G)
- B. Locate monument signs in a planter setting within a landscaped area. (S)
- C. Locate signs a minimum of 10 feet from the right of way so as to not obstruct visibility at intersections. (S)
- D. The placement of signs on roofs is not allowed. (S)

### 7.4 Sign Illumination

#### Policy:

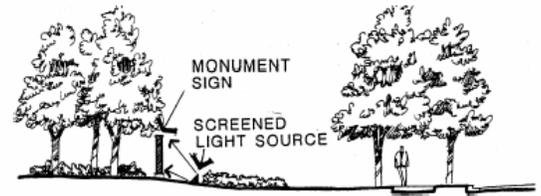
Sign illumination should complement, not overpower, the image of the building and its immediate landscaping.



7.3 B LOCATE MONUMENT SIGNS IN A PLANTER SETTING WITHIN A LANDSCAPED AREA.

## Standards and Guidelines:

- A. Neon tubing is an acceptable method of sign illumination. (G)
- B. Flashing signs are not permitted under any circumstances. (S)
- C. The use of individually-cut, back-lit character signs is strongly encouraged. (G)
- D. When external light sources are directed at the sign surface, conceal the light source from pedestrians' and motorists' "lines of sight". (S)
- E. When using an internally illuminated sign cabinet, only that portion of the sign face dedicated to the trademark or characters may be translucent. The balance of the sign face shall be opaque. (S)
- F. As used in this Chapter 7, "character" means and includes any graphic symbol used for sign text, included but not limited to letters, numbers and logos, provided that any character used for a sign must be a part of the name, service mark or trademark of the company or business.



7.4 D EXTERNAL LIGHT SOURCES DIRECTED AT SIGNS SHOULD BE CONCEALED FROM PEDESTRIANS' AND MOTORISTS' "LINES OF SIGHT".

## 7.5 Allowable Sign Types

### Policy:

The type of sign used should reinforce the urban environment of commercial developments. Signs should be designed as a "family", incorporating similar, compatible materials that reinforce the design and style of the project architecture. The following standards and guidelines apply with regard to the listed sign types.



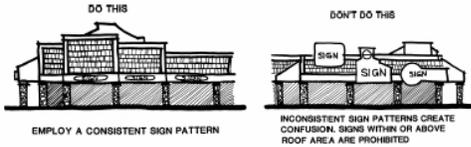
7.5 A MONUMENT SIGNS (SEE TEXT).

### Standards and Guidelines:

- A. Monument Signs
  - 1) For office zones project monument signs, if authorized, may be located at the street or primary entries to commercial developments to provide the overall project identity. (G) Such signs shall contain only the name of the project which it identifies, and shall not contain change, panels, advertising or names of individual tenants. (G)
  - 2) For retail zones individual monument signs may be located at primary entries to free-standing buildings to provide individual business identifications and building addresses. (G) Such sign shall contain only the name or trademark of the business served, and shall not contain change panels, advertising or names of individual tenants. (S)
  - 3) Affix monument signs to the ground in a continuous connection. (S)
  - 4) For multiple user projects such as shopping centers or office buildings, project identity



7.5 A 2 SMALLER SCALE MONUMENT SIGNS MAY BE LOCATED AT PRIMARY ENTRIES TO PROVIDE INDIVIDUAL BUSINESS IDENTIFICATIONS AND ADDRESSES.



7.5 B LOCATE FLUSH-MOUNTED SIGNS ON BUILDINGS AT THE FIRST FLOOR LEVEL ONLY FOR RETAIL USES.



7.5 B FLUSH-MOUNTED SIGN AT THE FIRST LEVEL



7.5 C PROJECTING SIGNS (SEE TEXT)

signage requires additional variables that must be considered. Conceptual designs for signs serving these types of facilities must be submitted to the planning department.

The total measured area of a sign shall be measured using the smallest single square or rectangle that includes the area of all writing, representation, lines, emblems or figures contained within all modules, together with any air space, material or color forming an integral part or background of the display if used to differentiate such sign from the backdrop or structure (S)

- 5) All monument signs using a sign cabinet design shall have an architectural boarder that integrates a minimum of two sides of the sign cabinet into the base. The architectural base and boarder shall be consistent with and/or compliment the building materials. (S)
  - 6) All individual tenant panels shall be of a uniform size and a minimum of 5 square feet.
  - 7) All monument signs shall be constructed of an opaque background and use a uniform color.
- B. Building-mounted Signs
- 1) Locate building-mounted signs at the first floor level only for retail uses. (S) Building-mounted signs shall identify the individual business, building or building complex by name or trademark only. (S)
  - 2) Building-mounted signs may not project more than 8 inches from the face of the building. (S)
- C. Projecting Signs
- 1) Signs that project perpendicular from a building are allowed only for multi-tenant retail and office uses within a predominantly retail center. (S)
  - 2) Projecting signs must be mounted above 7 feet from grade and may not project more than 5 feet from the wall. (S)
- D. Pole-mounted Signs
- 1) Pole-mounted signs are allowed only as traffic regulation signs or to provide appropriate directions to loading and receiving areas, visitor parking, and other areas within each development site. (S)
  - 2) Pole-mounted signs may not exceed 4 square feet in area and 6 feet in height measured from grade. (S)
- E. Flashing or moving signs are not permitted. (S)

## Rob Zuccaro

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**From:** Felicity Selvoski  
**Sent:** Tuesday, March 3, 2020 8:48 AM  
**To:** Rob Zuccaro  
**Subject:** FW: SpeedySparkle signage

-----Original Message-----

From: Trudy Turvey [mailto:trudyturveypt@gmail.com]  
Sent: Tuesday, March 3, 2020 7:52 AM  
To: Planning <planning@Louisvilleco.gov>  
Subject: SpeedySparkle signage

I live at 1483 Hecla Way, directly across from the car wash. I am writing to object to more signage on Hecla Way-

1. the signatures of course do not include ANY of the neighbors on Hecla Way; in fact most are not even in our neighborhood
2. there is no need for additional signage on Hecla Way-the existence of the car wash is quite clear without additional signage.
3. they exist within a neighborhood and additional signage only adds to the feeling of the neighborhood being a strip mall
4. Often the menu signs are left on with glaring lights into our homes. Especially if there is any intent to have these new signs be lit, I strongly object!!

Trudy Turvey  
1483 Hecla Way

**Rob Zuccaro**

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**Subject:** FW: Objection to Speedy Sparkle Signage on Hecla Way

-----Original Message-----

From: Tzvetanka Gintchin [mailto:tagintchin@yahoo.com]

Sent: Friday, October 11, 2019 5:29 PM

To: Planning Commission <PlanningCommission@louisvilleco.gov>

Subject: Objection to Speedy Sparkle Signage on Hecla Way

To Whom It May Concern:

I am writing in objection to the Speedy Sparkle Car Wash signage proposed for Hecla Way. I live in the townhome units across the car wash and ask the City of Louisville not to approve the placement of a sign on Hecla Way but approve redesign of the currently available signage on South Boulder Rd. The latter is a much better location to educate and attract customers. Adding new signs on Hecla Way will offer no advertising benefit while significantly decreasing the overall curb appeal and feel of the area.

Please I ask you to consider the negative impact such a sign will have on the overall look of the neighborhood and the current residents living in the area.

Thank you for your time and consideration.

Tzvetanka Gintchin  
1491 Hecla Way

**Rob Zuccaro**

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**Subject:** FW: RESOLUTION NO. 14 SERIES 2019

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**From:** Lazar Gintchin [<mailto:lazar.gintchin@gmail.com>]

**Sent:** Thursday, October 10, 2019 6:44 PM

**To:** Planning Commission <[PlanningCommission@louisvilleco.gov](mailto:PlanningCommission@louisvilleco.gov)>

**Subject:** RESOLUTION NO. 14 SERIES 2019

Dear Planning Commission,

Please deny this PUD/waiver application, RESOLUTION NO. 14 SERIES 2019.

Given that I live almost directly opposite from the car wash my strong preference is that they put signs on south Boulder road instead of close to our home.

Thank you,  
Lazar Gintchin

**Rob Zuccaro**

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**Subject:** FW: PUD/waiver application, RESOLUTION NO. 14 SERIES 2019.

-----Original Message-----

From: Trudy Turvey [mailto:trudyturveypt@gmail.com]

Sent: Wednesday, October 9, 2019 6:17 PM

To: Planning Commission <PlanningCommission@louisvilleco.gov>

Subject: PUD/waiver application, RESOLUTION NO. 14 SERIES 2019.

Please deny this application; the car wash has a perfectly acceptable alternative with the sign on South Boulder Road.

I am a neighbor and user of Speedy Sparkle; I do not see the need for increased signage, especially on Hecla Way. It is more than visible to all who drive by.

Trudy Turvey  
1483 Hecla Way  
Louisville, CO

## Rob Zuccaro

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**Subject:** FW: PUD-0222-2019; Speedy Sparkle Sign Waivers

**Importance:** High

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**From:** Mark Cathcart [[mailto:m\\_cathcart@yahoo.co.uk](mailto:m_cathcart@yahoo.co.uk)]  
**Sent:** Wednesday, October 9, 2019 11:14 AM  
**To:** Planning Commission <[PlanningCommission@louisvilleco.gov](mailto:PlanningCommission@louisvilleco.gov)>  
**Subject:** PUD-0222-2019; Speedy Sparkle Sign Waivers  
**Importance:** High

Dear Planning Commission,

Per staff recommendation, please deny this PUD/waiver application, RESOLUTION NO. 14 SERIES 2019.

The applicant has a perfectly acceptable alternative solution, and should work with the other businesses in this area to rebuild and update the existing monument signs to meet existing standards and the current business objectives.

I am an immediate neighbor to the applicant/speedysparkle as well as a customer; I am also a Director of the North End Master HOA, although I don't not claim to speak for all residents.

Mark Cathcart  
1763 Sweet Clover Ln, Louisville CO 80027

## Felicity Selvoski

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**From:** Lisa Ritchie  
**Sent:** Monday, July 8, 2019 2:54 PM  
**To:** Felicity Selvoski  
**Subject:** FW: SPEEDY SPARKLE PUD AMENDMENT #3 - Hearing date 7/11/19

**From:** Mark Cathcart [[mailto:m\\_cathcart@yahoo.co.uk](mailto:m_cathcart@yahoo.co.uk)]  
**Sent:** Monday, July 8, 2019 1:48 PM  
**To:** Planning Commission <[PlanningCommission@louisvilleco.gov](mailto:PlanningCommission@louisvilleco.gov)>  
**Subject:** SPEEDY SPARKLE PUD AMENDMENT #3 - Hearing date 7/11/19

Planning Commissioners,

My name is Mark Cathcart, I live at 1763 Sweet Clover Ln, Louisville. For transparency, I am a member of the Louisville Cultural Commission.

I am a regular customer at Speedy Sparkle Car Wash, I want them to be successful for both personal reasons, and for what they contribute to the city of Louisville.

I have reviewed this PUD amendment as it relates to their requested changes, both against the existing City of Louisville Sign Ordinance and the proposed, revised ordinance. While I don't have your expertise or knowledge, it seems to me this application, especially the sign proposed for Hecla Way meets neither version of the code.

The sign proposed for Hecla Way is also seriously out of character for a sign right on the transition between commercial and residential. Therefore I request you reject the proposed amendment.

++Mark.

<https://markcathcart.com/about/>

## Lisa Ritchie

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**From:** Tzvetanka Gintchin <tagintchin@yahoo.com>  
**Sent:** Thursday, July 4, 2019 9:17 AM  
**To:** Planning  
**Subject:** Re: Speedy Sparkle PUD Amendment

To Who It May Concern:

I am one of the residents of the 6 town homes across from Speedy Sparkle ad I am writing in opposition of the proposed new signage.

I ask that City of Louisville does not approve the Speedy Sparkle Signage application for the following reasons:

- It is a visual nuisance directly in the line of sight of our property and the additional homes that will be built. Such a sign will definitely decrease the aesthetic appeal of the area and negatively affect the property values of adjacent properties.
- This is an already a mixed commercial/residential space, which actually attracted me to the area, but this goes too far by placing it where proposed. An illuminated sign, especially will add unnecessary light pollution, considering the fact that we already have to deal with their current lit menu signs, overhead lights and noise.
- It is out of character and proportion for the location, given the proximity to the current and proposed residential properties.
- This may discourage future residential prospects. The new development, when it happens, will be good for everyone as it will increase property values and drive new traffic to the businesses, while a conspicuous and intrusive commercial sign will negatively affect the overall feel and look of the neighborhood and drive away potential future residents while triggering discontent in current residents.
- Finally, it will not be very effective because when the field is developed, the new buildings will block the sign. It will be much more effective if placed closer to S Boulder Road.

Thank you very much for your time and consideration.

Sincerely,

Tzvetanka Gintchin  
1491 Hecla Way

## Lisa Ritchie

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**From:** Lazar Gintchin <lazar.gintchin@gmail.com>  
**Sent:** Thursday, July 4, 2019 10:01 PM  
**To:** Planning  
**Subject:** Fw: Speedy Sparkle PUD Amendment

To Whom It May Concern:

I live on 1491 Hecla Way, Louisville CO and the purpose of this letter is to express my disagreement with the proposed new signage by Speedy Sparkle Car Wash.

My request is that City of Louisville declines the application made by Speedy Sparkle based on the following:

- Those of us who invested in a home in this neighborhood were attracted by the modern, high-class appearance which the home builder has created. Having a 12 foot tall sign would definitely change the look and feel, especially for those of us, who live directly across the street.
- I am not sure what value a sign adds, when placed in the middle between our street and S. Boulder Rd. Would it not attract more customers if placed on S. Boulder Rd?
- A brightly lit sign of this size will be very noticeable and unpleasant to look at, given that our homes, porches, and half of the balconies are facing in that direction. The carwash is noisy when operating, and it feels like too much to add a big visual on top of it.
- More residential buildings are still planned to be built, right on the East side of the carwash. I am concerned that potential buyers may be detracted from buying, resulting lowering of the property values in our neighborhood.

Thank you in advance for your consideration.

Sincerely,

Lazar Gintchin

--

Lazar Gintchin  
[lazar.gintchin@gmail.com](mailto:lazar.gintchin@gmail.com)

## Felicity Selvoski

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**From:** David Chernikoff <davidchernikoff@icloud.com>  
**Sent:** Saturday, June 29, 2019 4:02 PM  
**To:** Planning  
**Subject:** residential comment: Speedy Sparkle PUD proposed sign

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

To Whom it May Concern:

As one of the owners of a townhome at 1459 Hecla Way, which is located close to the Speedy Sparkle Car Wash at 1414 Hecla Way, I want to express my opposition to the proposed new sign in their PUD Amendment Application. The proposed sign will have a significant negative impact on the views from our building as well as those from other buildings in our neighborhood. In addition, when future structures are built on the vacant lot in accordance with current plans, these buildings will partially block the new sign that is being proposed and decrease its usefulness.

If the intention of the business owner(s) of the car wash is to attract interest from traffic on South Boulder Road, the proposed location will be of little value. The people living in the North End neighborhood will regularly drive by the car wash and will not need a sign to alert them to the existence of the car wash. While I recognize that we chose to buy a property that is located close to a business area and I accept the associated impacts on the neighborhood, this proposed sign is unnecessary and unlikely to serve its intended function. It makes much more sense for the car wash to attract business with additional signage that is closer to and clearly visible from South Boulder Road.

I'm further concerned about light pollution, which is an increasing problem throughout the Front Range. If Speedy Sparkle Car Wash is permitted to put up a new sign or to change their existing signs, I strongly oppose signage that is illuminated.

Thank you for considering this request and for doing what you can to preserve the quality of life in the North End neighborhood of Louisville.

Respectfully submitted, David Chernikoff

## Felicity Selvoski

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**From:** Rob Zuccaro  
**Sent:** Monday, July 1, 2019 8:41 AM  
**To:** trudyturveypt@gmail.com  
**Cc:** Felicity Selvoski; Lisa Ritchie  
**Subject:** RE: Objection to Signage proposal

**From:** Trudy Turvey <[trudyturveypt@gmail.com](mailto:trudyturveypt@gmail.com)>  
**Date:** June 29, 2019 at 12:41:21 PM EDT  
**To:** [CityCouncil@LouisvilleCo.gov](mailto:CityCouncil@LouisvilleCo.gov)  
**Subject:** **Objection to Signage proposal**

I live on Hecla Way. Inasmuch as this is a residential area and signage that is proposed would further destroy the ambiance (not to mention the noise levels and congestion) I oppose further signage-the car wash gets plenty of business and it is VERY visible to motorists without further making this area look like a trashy shopping center. It is not clear from the drawings (hard to discern) if this would be a neon sign-I absolutely oppose this.

Trudy Turvey

## Felicity Selvoski

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**From:** Laura Chernikoff <laurachernikoff@icloud.com>  
**Sent:** Monday, July 1, 2019 12:14 PM  
**To:** Felicity Selvoski  
**Cc:** Planning  
**Subject:** Re: Speedy Sparkle PUD Amendment - Resident Comment

Hi Felicity and the Planning Department,

Here are my revised comments based on the updated plan document:

I am a resident at 1459 Hecla Way, which is diagonally across from the Speedy Sparkle Car Wash at 1414 Hecla Way. I oppose the new 9' sign proposed in their PUD Amendment Application and I hope you will consider the perspective of the residents in my building and the North End neighborhood. The proposed Hecla Way sign is out of character and proportion for the location, given the proximity to the current and proposed residential properties. In particular, having an illuminated sign creates a light pollution nuisance for our neighborhood.

The proposed location of the new sign would be a visual nuisance every time we enter our neighborhood—not only for our 6 units, but also for the 12-unit condominium buildings nearby, and the future residences that are planned for the vacant lot. This is a residential area that is in close proximity to commercial properties. We already deal with the lit car wash menu signs, overhead lights, sound of their vacuums, traffic from their customers during work hours and employees before 7am and after 7pm every day. This proposed sign goes too far, particularly in scale and the proposed design.

The Hecla Way location does not seem effective at increasing traffic to the car wash business. There is not significant traffic heading east on Hecla Way, except the residents of the North End going to their homes. It's one thing to have signs centered around the commercial intersection at Plaza and Hecla Way, but it's another for these signs to be directly at the entrance of our neighborhood. Anyone on Hecla Way, at the King Soopers Gas Station, or Napa Tires businesses can already directly see that there is a car wash. It seems sufficient for the car wash to have the requested changes made to the signage on South Boulder Road. However, if a sign on Hecla Way is allowed, it should be much more in style with the other low-profile signs of the other businesses.

If the sign is allowed to be built, then I specifically strongly request that it not be lit or illuminated in any way as planned. Light pollution is a serious problem. In the winter hours, the car wash stays open for several hours after dark. We already have their illuminated menu signs and overhead lights shining directly at our homes in my building.

I hope you will consider the negative impact this proposal would have on the residents of this neighborhood. I worry that this unnecessary commercial encroachment would hurt our property values. This also could decrease the chances of attracting residential and commercial customers to the planned development on the vacant lot. This new development, when it happens, will be good for the neighborhood and will drive new traffic to the businesses.

Please confirm that you received this. I will also plan to attend the hearing on July 11.

Sincerely,

Laura Chernikoff

## Felicity Selvoski

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**From:** Marsha McClanahan <marshamccl@icloud.com>  
**Sent:** Monday, July 1, 2019 12:16 PM  
**To:** Felicity Selvoski  
**Cc:** Planning  
**Subject:** Re: Speedy Sparkle PUD Amendment, residential owner input

Re: Proposed additional sign for Speedy Sparkle Car Wash on Hecla Way.

I have just received notice of the amended proposal for a 9 foot high sign on Hecla Way adjacent to the entrance to my neighborhood. Both the location and the size of the sign are different than I previously understood so my previous comments can be disregarded. I am a property owner at 1459 Hecla Way.

I am against the addition of this proposed sign because it is proposed to be a lighted sign which will add additional light to the residential neighborhood, is out of proportion and very large for its parkway location and is out of character with the other signs for businesses nearby on Hecla Way. Additionally, it seems an unnecessary sign in that people going along Hecla either are already going to the carwash or are entering the neighborhood. The resident's in North End who drive regularly along Hecla Way certainly know from all the other Speedy Sparkle signs that the car wash is there and no additional signs are needed for them. If people are entering on Plaza to go to and leave the shopping center, they have already seen signs for the car wash.

Marsha McClanahan

## Felicity Selvoski

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**From:** Brady <bradymtb@gmail.com>  
**Sent:** Monday, July 1, 2019 1:31 PM  
**To:** Planning  
**Subject:** Speedy Sparkle car wash-Hecla way sign

Good afternoon,

I would like to object to a new sign on Hecla way. We already have enough light pollution coming from the car wash and gas station. This sign would directly be visible from my living room, bedroom and balcony. There are new houses planned to be built next to the car wash, any further signage for the car wash on Hecla way would reduce the value of our homes and future homes. A sign on S. Boulder rd seems reasonable.

Brady Henderson  
(970)222-9474

## Felicity Selvoski

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**From:** Tim Merkel <tim@bigcompass.com>  
**Sent:** Monday, July 1, 2019 4:21 PM  
**To:** Planning  
**Subject:** Speedy Sparkle Signage

Dear Louisville Planning,

Thank you for the opportunity to comment on the proposed signage associated with the Speedy Sparkle PUD Amendment. Unfortunately I won't be able to attend the July 11th meeting. I hope my comments here will be taken into consideration.

The proposed 6' tall monument sign on Hecla way is completely out of character for this location. A sign of this size and this much color right next to a neighborhood disrupts the transition from commercial/retail on Plaza Drive. The previously approved sign that is smaller and constructed out of metal is a nice balance between the big Napa and King Soopers gas signs adjacent to Plaza.

**I urge the planning commission to reject this plan and ask the developer to use the previously approved plans.**

Many thanks,

**Tim Merkel**  
Partner, Big Compass



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[tim@bigcompass.com](mailto:tim@bigcompass.com) | [bigcompass.com](http://bigcompass.com)  
M:303-591-4371 O:720 -328-1669

**ITEM:** ZON-0260-2019 – St. Louis Parish and Commercial Park General Development Plan, 2<sup>nd</sup> Amendment. A request for a second amendment to the St. Louis Parish General Development Plan to amend allowed uses and development standards

**PLANNER:** Lisa Ritchie, Senior Planner

**OWNER:** Archdiocese of Denver-St. Louis, Ascent Church, Adrian Games

**REPRESENTATIVE:** Alicia Rhymer, United Properties

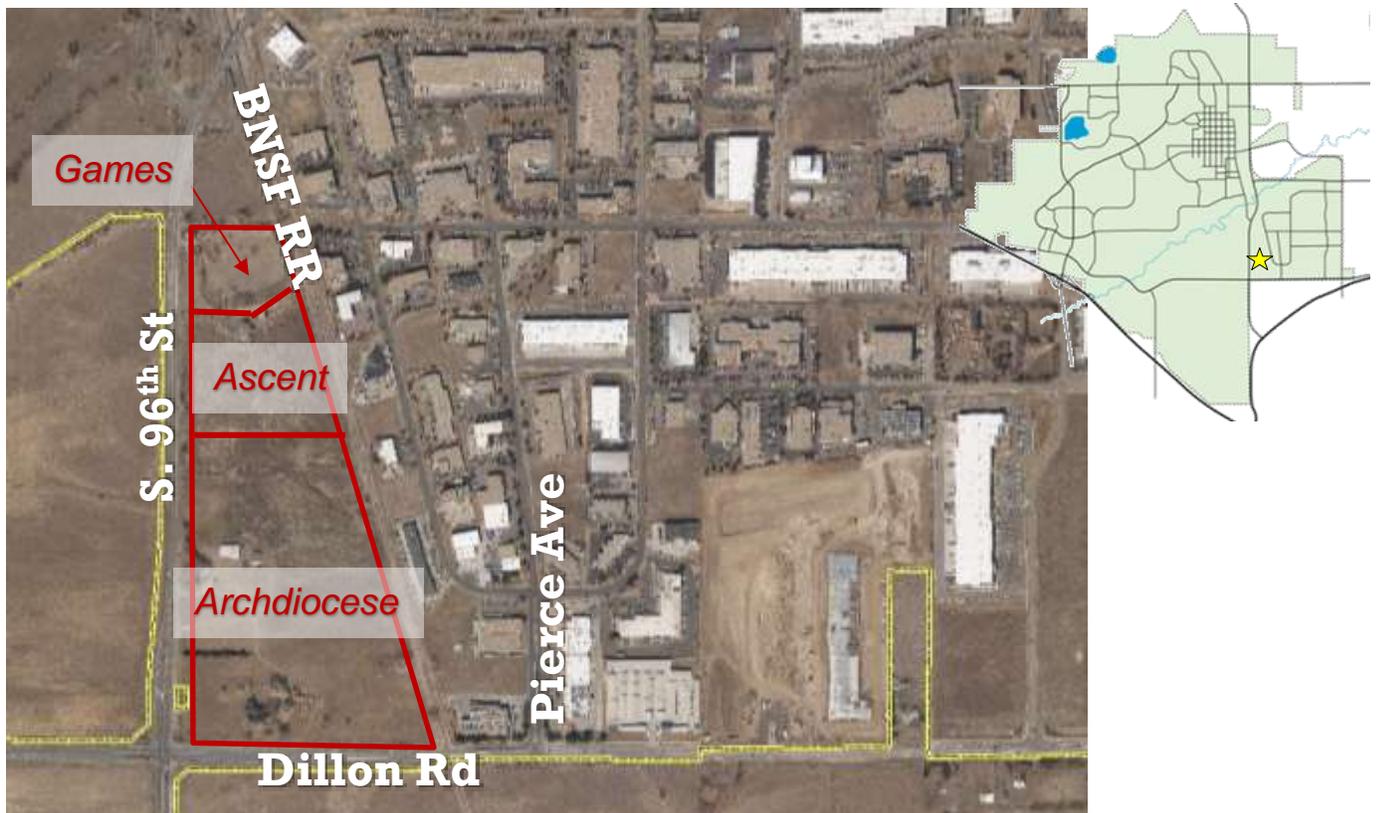
**EXISTING ZONING:** PCZD-C – Commercial

**LOCATION:** Northeast corner of S. 96<sup>th</sup> Street and Dillon Road

**TOTAL SITE AREA:** 51.6 Acres

**RESOLUTION:** Approval of Resolution No. 2, Series 2020, recommending denial of the application

**VICINITY MAP:**



## **SUMMARY:**

The applicant, United Properties, requests approval of a second amendment to the St. Louis Parish and Commercial Park General Development Plan (GDP) to amend allowed uses and development standards. Staff is recommending denial of this application due to multiple components of the request that do not support Comprehensive Plan policy, the existing intent of the GDP to provide appropriate buffer and transition to the open space to the west, and a well-coordinated planned development consistent with the intent of the Planned Community Zone District (PCZD). Some of these reasons include:

- Removal of the layering of height and density from the west to the east
- Reduction of the setback buffer dimension
- Removal of the restriction to place parking behind buildings fronting S. 96<sup>th</sup> St
- Increasing the overall allowed Floor Area Ratio (FAR) from 0.2 to 0.25, and increase allowed FAR in some development areas from 0.17 to 0.2 up to 0.275, beyond the 0.25 FAR allowed in the Comprehensive Plan.
- Changing public roads to private without multi-modal street sections consistent with Transportation Master Plan policy

Staff does not oppose the overall change in uses proposed or the overall development concept, but does oppose the change in development standards. The applicant claims these changes are needed for viable development. Staff finds that each of the claims are not substantiated as described in further detail in the analysis section below. Staff finds that the development standards work collectively to provide an appropriate rural transition between the adjacent agricultural and preservation lands. Comprehensive Plan policy and the established agreements associated with the rezoning of the subject property from Agriculture to Planned Community Zone District (PCZD) also outline the City's intent to maintain the rural transition.

## **BACKGROUND:**

The St. Louis Parish and Commercial Park property is approximately 51.6 acres in size and located northeast of the Dillon Road and S. 96<sup>th</sup> Street intersection. To the east is BNSF Rail Road right of way and the Colorado Technological Center. To the west and southwest are the Warembourg and Admor Open Spaces, which are conservation properties owned jointly by Boulder County and City of Louisville. To the south is property in unincorporated Boulder County zoned Agriculture and designated as a Preservation Area through Intergovernmental Agreements with limited residential and agricultural development. Adrian Games owns the northernmost 5.39 acre parcel, Ascent Church owns the center 13.26 acre parcel, and the Archdiocese of Denver-St. Louis owns the southernmost 32.75 acre parcel. The applicant, United Properties, is under contract to purchase the Ascent parcel.

The City separately annexed each property between the time periods of July 1996 and February 1997. Each property was zoned Agriculture when annexed. The City subsequently approved a rezoning to Planned Community Zone District (PZCD) and the St. Louis Parish and Commercial Park GDP on September 21, 2004, which included the three properties described above. Adrian Games and the Denver Archdiocese were

owners at the time of this original approval, while the center parcel was sold a number of times to different entities.

To be zoned PCZD, a property must be at least 30 acres in size and held in common ownership. The requirement for common ownership is to ensure the intent of an integrated and coordinated development. The City made an exception to the common ownership requirement, but with agreements that future development would be coordinated among the property owners in the same manner as if the properties were under single ownership.

The City approval also resulted in a significant upzoning from agriculture to commercial zoning, greatly increasing development potential on the property. As part of the agreement to allow the upzoning to PCZD zoning, the GDP established parameters that established a buffer to the open space to the west through setback, height, and density restrictions.

The original GDP divided the overall area into three distinct zones, with Zone 2 being further broken out into three subzones (Zones 2A, 2B and 2C) primarily to address height, floor area, setbacks, and site coverage limitations based on the proximity of each zone to 96<sup>th</sup> Street and the open space to the west. The structure of the Zones provides a transition of development density, maintaining a lower, more rural character adjacent to the open space lands. The GDP includes a list of permitted and Special Review uses in each Zone, which includes a mix of institutional and commercial uses.

On October 17, 2017, the City approved the first amendment to the St Louis Parish and Commercial Park GDP. This amendment allowed religious institutions as a use-by-right in Zone 2 rather than by Special Review. Following this approval, the property owners' received approval of a preliminary plat and preliminary Planned Unit Development on September 4, 2018 to establish the intent for 4 lots on the Ascent property; 2 parcels, one each on the Games and Archdiocese properties. The PUD included the construction of a 52,000 sf building and associated site improvements. Following this approval, Ascent Church made application for a final plat and final PUD to follow through with the intent of the preliminary approvals. These applications were never finalized or considered before Planning Commission and City Council.

Earlier this year, Ascent Church purchased the property at 550 S. McCaslin, the former Sam's Club property, and have abandoned their plans to pursue development in the St Louis Parish and Commercial Park GDP.

#### *Background on 2004 GDP*

The original GDP established several parameters for development based on the location and context of the area. The following is an excerpt from the [September 21, 2004 Council Communication](#) which describes the development standards and rationale.

*The 2004 GDP reflects three zones of development. The GDP specifies design and building bulk standards for each sub-zone, which creates a 'gateway' and/or 'transition' to the City of Louisville. The organization of these planning areas has been organized along 'zones of intensity or transition' rather than strictly along parcel ownership boundaries. The GDP reflects an overall Floor Area Ratio (FAR) limitation of 0.20, but may allocate a more restrictive FAR to those planning areas adjacent to a major arterial. An FAR is a measure of non-residential density; it is a ratio between gross building square footage to the gross lot area.*

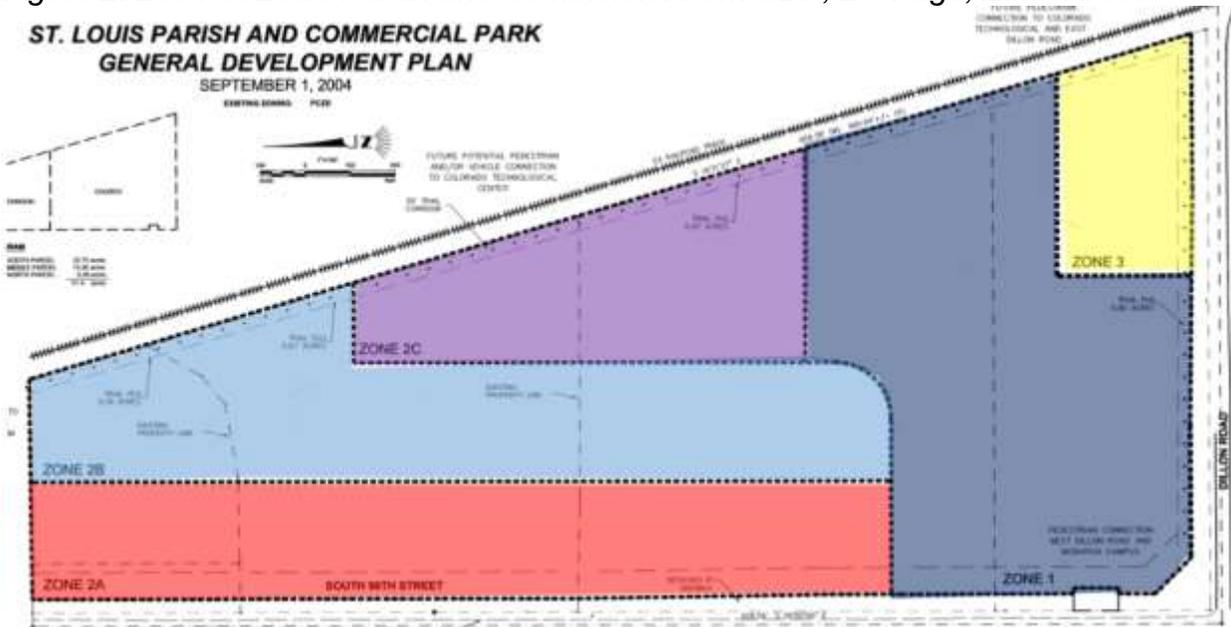
*Planning area zones have been organized in 'layers', which are generally parallel to S. 96<sup>th</sup> Street and step back to the east with areas of greater intensity. Zones II and III carry an overall FAR of 0.20, or a build out of approximately 306,531 SF. However, Zone 2A, which is adjacent and parallel to S. 96<sup>th</sup> Street has a maximum FAR of 0.17. In conjunction with a more restrictive FAR, buildings in Zone 2A are limited to one story construction, with pitched roof elements. The maximum building height in Zone 2A is 25'. The required building setback from S. 96<sup>th</sup> Street has been increased from a Commercial Development Design Standards and Guidelines (CDDSG) requirement of 30' to a minimum setback of 60'. Parking in Zone 2A is required to be placed behind, or to the east of the buildings fronting on S. 96<sup>th</sup> Street. The GDP design requirements to prohibit parking in the front setback of buildings facing S. 96<sup>th</sup> Street provides a very distinct landscape and pedestrian presentation to the adjoining arterial providing a transition between the open space to the west and the Colorado Tech Center to the east. Zones 2B, 2C, and 3 are subject to the standards of the CDDSG.*

As noted previously, the 1<sup>st</sup> Amendment approved in 2017 only revised the GDP to allow religious institutions as a use-by-right. It did not amend any of the development parameters originally established with the 2004 GDP.

Figure 1: Surrounding Open Space and Preservation Lands



Figure 2: 2004 St Louis Parish and Commercial Park GDP, 2<sup>nd</sup> Page, Zone Areas



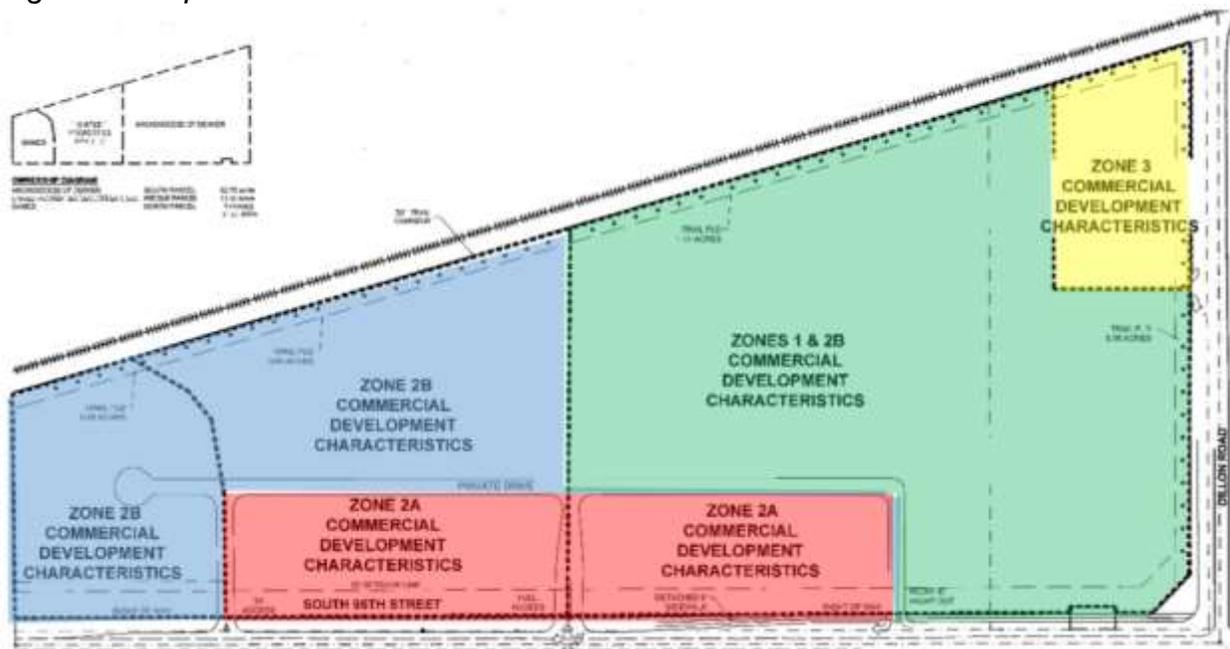
**PROPOSAL:**

The applicant requests approval of the second amendment to the St Louis Parish and Commercial Park GDP. This following list summarizes the changes requested by the applicant for this second amendment:

- Amends the use areas to align with existing property lines.
- Allows Zone 1 to develop with existing Zone 1 uses, or any use allowed in Zone 2.
- Adds light industrial uses to Zone 2 as a use-by-right.
- Adds car wash to Zone 2 as a special review use.
- Amends the development standards areas to align with current property ownership, rather than orient to allow increasing intensity from west to east from the open space.
- Revise the street network from public to private. The GDP states that cross access easements will be established at plat.
- Reduce the building setback for buildings fronting S. 96<sup>th</sup> Street from 60 feet to 55 feet.
- Allow parking between buildings and S. 96<sup>th</sup> Street with enhanced landscaping, rather than requiring it behind buildings. No definition is provided of what constitutes “enhanced landscaping.”
- Removes the requirement for sloped roofs for buildings in Zone 2A.
- Maintains the 25’ height maximum in Zone 2A for retail uses only. All other uses permitted up to 40’ height allowance in this area. This would allow the potential for 40’ tall buildings located 55’ from the road right of way, where previously this was restricted to 25’ for the property frontage except for Zone 1, which was limited to 35’ (CDDSG).
- Removes Zone 2A from the Games property and places it entirely in Zone 2B, with greater height and density allowances.
- Amends allowed heights for buildings in Zone 2B up to 40’, whereas 35’ (CDDSG) is currently permitted.
- Associates allowed FAR with property ownership, rather than Zones. This removes the FAR transition from west to east. The following table summarizes the FAR amendments:

Property	Current FAR	Proposed FAR
Games	0.17, Zone 2A 0.20, Zone 2B	0.275
United Properties	0.17, Zone 2A 0.20, Zone 2B 0.245, Zone 2C	0.25
Archdiocese, Zone 1 & 2 Parcel	No FAR, Zone 1 0.17, Zone 2A 0.20, Zone 2B 0.245, Zone 2C	No FAR, if developed with Zone 1 uses 0.25 if developed with Zone 2 uses
Archdiocese, Zone 3 Parcel	0.20	0.20
Maximum Development	425,843 sf	520,340 sf

Figure 3: Proposed GDP Zone Areas



In addition to the GDP Amendment, the applicant requests approval of an amended PCZD Agreement that updates the ownership entities and FAR allowances to match the proposed GDP Amendment. If the GDP Amendment is approved, this would be finalized prior to the City Council public hearing.

The applicant prepared a new traffic study as part of the application to reflect the additional development potential. The original study completed in 2001 anticipated a total of 7,383 average weekday trips and 2,845 average Sunday trips generated from the anticipated office, church and school, and tennis center uses. The study submitted with this application anticipates 6,248 average weekday trips and 2,036 average Sunday trips generated from light industrial, gas station, car wash, retail, and church

and school uses. While the development density is increasing with the GDP Amendment proposal, the assumptions in the traffic study include significantly more industrial use over office use, which accounts for the overall reduction in trips. Both traffic studies recommend a signal at the primary access point into the property along S. 96<sup>th</sup> Street at some point prior to build-out, and additional turn lanes and turn lane capacity. Both studies assume additional improvements will occur at the S. 96<sup>th</sup> Street and Dillon Rd intersection by others. The final required improvements would be finalized through a future phasing plan, final plat, and subdivision agreement.

**ANALYSIS:**

The GDP Amendment is subject to Section 17.72 *Planned Community Zone District* (PCZD) of the Louisville Municipal Code. Any amendments to a PCZD are subject to the same process and requirements as the initial approval. The purpose of the planned community zone district in Section 17.72.010 includes the following statements that apply to this application:

- *The purpose of the PCZD is to encourage, preserve and improve the health, safety and general welfare of the people of the city by encouraging the use of contemporary land planning principles and coordinated community design.*
- *The PCZD is created in recognition of the economic and cultural advantages that will accrue to the residents of an integrated, planned community development of sufficient size to provide related areas for various housing types, retail, service activities, recreation, schools and public facilities, and other uses of land.*

Section 17.72.030 includes the following applicability statement:

- *The PCZD may be applied only to such land as the city shall determine to be suitable for such a development.*

Staff finds that this land is not suitable for such a development with the proposed amendments in the GDP for the following reasons:

Comprehensive Plan Policy

This property is referred to as the 96<sup>th</sup> and Dillon Special District in the City's 2013 Comprehensive Plan and is designated as Rural. The language in the plan states:

*The 96<sup>th</sup> and Dillon Road Rural Special District serves as the rural gateway to the City of Louisville. The area will include a mix of commercial, institutional, and industrial uses. The uses in this special district will be separated and buffered from the surrounding roads to maintain the appearance of a rural entryway to the City.*

The Comprehensive Plan also includes a density range of up to .25 FAR for Rural designated properties, and heights up to 3 stories if clustered and located out of the public view shed and buffered by surrounding topography and open space.

Staff finds the proposed changes in to the GDP do not meet the established goals for the GDP at the time it was adopted or the current Comprehensive Plan policy in place today. Staff finds that there is substantial impact to the quality of the desired open space transition area through the following:

- Reduction of the buffer setback
- Removal of the requirement for parking behind buildings
- Removal of the requirement to transition density from west to east
- Increasing FAR densities above the maximum allowance under the Comprehensive Plan for some areas and overall increasing allowed density from that determined appropriate with the original GDP
- Increasing allowed heights in more visible areas closer to S. 96<sup>th</sup> Street,

The CTC development to the east is designated as Suburban in the Comprehensive Plan, which allows up to .5 FAR. When the CTC was established, a 55-foot conservation easement was placed along the east side which does not allow structures or parking lots. The north side of the CTC has the same 55-foot conservation easement, along with an additional 55-foot outlot that was dedicated to the City, effectively establishing a 110-foot buffer along the north side before any development may occur. These sides of the CTC also border protected open space lands.

The applicant provided a rationale for the reduction of the buffer in their narrative, and one of the reasons stated was that in order to be marketable, they need a minimum truck access area width of 130 feet and a minimum building depth of 180 feet. The applicant provided a comparison spreadsheet of similar sized buildings in the CTC, which staff finds does not demonstrate this need. There are no existing properties with truck bays with that depth, and while some buildings are at least 180 feet deep, the majority of industrial development seen in the CTC does not include buildings with this depth, and staff finds that such depths are not needed for success of a project within this market area. Standard industrial and commercial lot widths would allow the applicant to make minor adjustments to anticipated lot boundaries that would allow implementation of a 60' buffer over the proposed 55' buffer.

The application notes there will be “enhanced landscaping” in lieu of the full buffer. However, there are no design standards provided to determine what the concept of “enhanced landscaping” would include. Staff would want to confirm that any landscaping is consistent with the Rural District Comprehensive Plan policy and a consistent transition in character between the adjoining open space and agricultural properties required by the existing GDP.

Additionally, the existing GDP allowed for transitioning heights and densities with lower heights and densities abutting open space and higher heights and densities toward the east. The applicant proposes to remove these transition areas for both heights and densities. The only exception to this is a requirement for retail buildings in Zone 2A to be limited to 25 feet in height. However, this does not guarantee retail development in this zone, and office or industrial development could take place with a building height of

40 feet. The sloped roof requirement is also removed, which could also greatly increase the mass of the structure if a 25-foot tall flat roofed building or 40-foot tall flat roofed building is proposed in this zone.

The GDP Amendment also proposes changing the road sections from public to private roads and including cross sections with attached sidewalks and limited multi-modal connectivity. The City’s recently adopted Transportation Master Plan ([Link to TMP Executive Summary](#)) include the following policies on Great Streets and includes Guidelines for Walkable and Bikeable places. These policies promote enhanced infrastructure to promote walking and biking. Although there currently no transit service along 96<sup>th</sup> St., the corridor is slated for future Bus Rapid Transit service through the RTD [Northwest Area Mobility Study](#). With future enhanced transit along this corridor, robust multi-modal facilities will be especially important for first and last mile connections through this development. Staff finds that the cross sections proposed do not meet City TMP policies and would not support the anticipated transit service in the corridor.

Policy	Description
Policy 1: Great Streets	Great Streets, or complete streets, are streets that are designed and operated to be safe and accessible for all users, regardless of ability, age, or mode. This policy provides a guide for the design of new streets, improving infrastructure on existing streets, and recommends that new designs consider the surrounding context and land uses. It also provides a framework to help consider modal priorities on an individual street.
Policy 2: Guidelines for Walkable & Bikeable Places	In areas where new development or redevelopment is anticipated, the City’s policy is to facilitate design that promotes walkable and bikeable places. Elements that contribute to walking and biking include wide sidewalks, pedestrian amenities, higher intersection density, buffers for bicycles, and trail connections. The City’s design standards and guidelines should promote the development of walkable places, with a focus on corridors, including McCaslin Boulevard and South Boulder Road.

Staff finds the amendments related to uses are acceptable in terms of compliance with the Comprehensive Plan and impact to the surrounding area.

For the reasons described above, staff finds that the application does not meet the purposes of the PCZD zone district with the proposed changes and is not consistent with the City’s Comprehensive Plan or Transportation Master Plan. The transition area established in the existing GDP and supported by current Comprehensive Plan policy is not suitable for the increased development impact that would result from the reduction of the setback buffer, the allowance for parking within this area, and the removal of the height and FAR transition standards. The project lacks strong multi-modal infrastructure and does not provide coordinated planning and design within the development or with respect to the open space and agricultural lands west and south of the property.

**FISCAL IMPACT:**

Per policy, staff ran the City’s fiscal impact model under “high” and “low” scenarios, with the “low” scenario reducing several of the inputs to 80% of the “high” scenario. Under the “high” scenario, the model estimates that the 20-year fiscal impact to the City resulting from the GDP Amendment is reduced from a net positive of \$9,855,000 to \$6,395,000, a reduction of \$3,460,000. The reduction is largely due to loss of retail sales tax and office employee spending with the proposed development scenario and what was anticipated under the existing GDP. The “low” scenario shows the estimated 20-year fiscal impact to the City reduced from a net positive of \$6,499,000 to \$3,980,000, a reduction of \$2,519,000. Removing the established development standards to potentially accommodate this development concept does not fiscally benefit the city.

*High Scenario*

Revenue by Fund	SCENARIO			
	Proposed	%	Existing	%
General Fund	\$5,882	66%	\$7,576	61%
Open Spaces & Parks Fund	\$692	8%	\$1,163	9%
Lottery Fund	\$0	0%	\$0	0%
Historic Preservation Fund	\$255	3%	\$412	3%
Capital Projects Fund	\$2,114	24%	\$3,297	26%
<b>TOTAL REVENUE</b>	<b>\$8,943</b>	<b>100%</b>	<b>\$12,448</b>	<b>100%</b>
<b>Expenditures by Fund</b>				
General Fund	\$1,999	78%	\$1,962	76%
Open Spaces & Parks Fund	\$0	0%	\$15	1%
Lottery Fund	\$0	0%	\$0	0%
Historic Preservation Fund	\$0	0%	\$0	0%
Capital Projects Fund	\$549	22%	\$616	24%
<b>TOTAL EXPENDITURES</b>	<b>\$2,548</b>	<b>100%</b>	<b>\$2,593</b>	<b>100%</b>
<b>NET FISCAL RESULT BY FUND</b>				
General Fund	\$3,882		\$5,614	
Open Spaces & Parks Fund	\$692		\$1,148	
Lottery Fund	\$0		\$0	
Historic Preservation Fund	\$255		\$412	
Capital Projects Fund	\$1,565		\$2,682	
<b>NET FISCAL IMPACT</b>	<b>\$6,395</b>		<b>\$9,855</b>	

Low Scenario

Revenue by Fund	SCENARIO			
	Proposed	%	Existing	%
General Fund	\$3,608	61%	\$5,038	59%
Open Spaces & Parks Fund	\$498	8%	\$822	10%
Lottery Fund	\$0	0%	\$0	0%
Historic Preservation Fund	\$183	3%	\$291	3%
Capital Projects Fund	\$1,598	27%	\$2,389	28%
<b>TOTAL REVENUE</b>	<b>\$5,887</b>	<b>100%</b>	<b>\$8,540</b>	<b>100%</b>
<b>Expenditures by Fund</b>				
General Fund	\$1,358	71%	\$1,410	69%
Open Spaces & Parks Fund	\$0	0%	\$15	1%
Lottery Fund	\$0	0%	\$0	0%
Historic Preservation Fund	\$0	0%	\$0	0%
Capital Projects Fund	\$549	29%	\$616	30%
<b>TOTAL EXPENDITURES</b>	<b>\$1,907</b>	<b>100%</b>	<b>\$2,041</b>	<b>100%</b>
<b>NET FISCAL RESULT BY FUND</b>				
General Fund	\$2,250		\$3,627	
Open Spaces & Parks Fund	\$498		\$807	
Lottery Fund	\$0		\$0	
Historic Preservation Fund	\$183		\$291	
Capital Projects Fund	\$1,049		\$1,773	
<b>NET FISCAL IMPACT</b>	<b>\$3,980</b>		<b>\$6,499</b>	

**STAFF RECOMMENDATION:**

Staff recommends approval of Resolution 2, Series 2020, recommending denial of a request for a second amendment to the St Louis Parish and Commercial Park GDP. Conditions of approval are not included because of the large areas of difference between the application and the adopted policies of the City as it relates to the proposal.

**ATTACHMENTS:**

1. Resolution No. 2, Series 2020
2. Application Materials
3. St Louis Parish and Commercial Park GDP, 2<sup>nd</sup> Amendment, Clean
4. St Louis Parish and Commercial Park GDP, 2<sup>nd</sup> Amendment, Redline
5. Traffic Study
6. Applicant Exhibits
7. St Louis Parish and Commercial Park GDP – 2004
8. [City Council Communication](#), September 21, 2004, see page 112
9. St Louis Parish and Commercial Park GDP, 1<sup>st</sup> Amendment - 2017
10. Public Comments

**RESOLUTION NO. 2  
SERIES 2020**

**A RESOLUTION RECOMMENDING DENIAL OF A REQUEST FOR A SECOND AMENDMENT TO THE ST LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN TO AMEND ALLOWED USES AND DEVELOPMENT STANDARDS LOCATED AT THE NORTHEAST CORNER OF S. 96<sup>TH</sup> STREET AND DILLON ROAD; 1212 S. 96<sup>TH</sup> STREET, 1326 S. 96<sup>TH</sup> STREET, & 9673 DILLON ROAD**

**WHEREAS**, there has been submitted to the Louisville Planning Commission an application for approval of a request for a Second Amendment to the St Louis Parish and Commercial Park General Development Plan to amend allowed uses and development standards; and

**WHEREAS**, the City Staff has reviewed the information submitted and found that the application is not compatible with the City's Comprehensive Plan and the intent for buffer and transition from open space lands to the west; and

**WHEREAS**, the Planning Commission has considered the application at a duly noticed public hearing on March 12, 2020, where evidence and testimony were entered into the record, including the findings in the Louisville Planning Commission Staff Report dated March 12, 2020.

**NOW THEREFORE, BE IT RESOLVED** that the Planning Commission of the City of Louisville, Colorado does hereby recommend denial of a request for a Second Amendment to the St Louis Parish and Commercial Park General Development Plan to amend allowed uses and densities.

**PASSED AND ADOPTED** this 12<sup>th</sup> day of March, 2020.

By: \_\_\_\_\_  
Steve Brauneis, Chair  
Planning Commission

Attest: \_\_\_\_\_  
Debra Williams, Secretary  
Planning Commission

**LAND USE APPLICATION**

**CASE NO.** \_\_\_\_\_

**APPLICANT INFORMATION**

Firm: United Properties  
 Contact: Alicia Rhymen  
 Address: 1331 17th Street, Suite 604  
Denver, CO 80202  
 Mailing Address: Same  
 Telephone: 720.273.9841  
 Fax: \_\_\_\_\_  
 Email: alicia.rhymen@uproperties.com

**OWNER INFORMATION**

Firm: Ascent Community Church  
 Contact: Stacey Luther  
 Address: 1326 96th Avenue  
Louisville, CO 80027  
 Mailing Address: PO Box 270173  
Louisville, CO 80027  
 Telephone: 303.518.8084  
 Fax: \_\_\_\_\_  
 Email: Stacey@Ascentcc.org

**REPRESENTATIVE INFORMATION**

Firm: (EES) Entitlements & Engineering Solutions Inc.  
 Contact: Krysta Houtchens  
 Address: 501 S. Cherry Street, Suite 300  
Glendale, CO 80246  
 Mailing Address: Same  
 Telephone: 970.380.7054  
 Fax: \_\_\_\_\_  
 Email: khoutchens@ees.us.com

**PROPERTY INFORMATION**

Common Address: 1326 96th Ave  
 Legal Description: Lot \_\_\_\_\_ Blk \_\_\_\_\_  
 Subdivision TR5 - Central NBR 145-820  
 Area: 13.73 598 Sq. Ft.

**TYPE (S) OF APPLICATION**

- Annexation
- Zoning
- Preliminary Subdivision Plat
- Final Subdivision Plat
- Minor Subdivision Plat
- Preliminary Planned Unit Development (PUD)
- Final PUD
- Amended PUD
- Administrative PUD Amendment
- Special Review Use (SRU)
- SRU Amendment
- SRU Administrative Review
- Temporary Use Permit: \_\_\_\_\_
- CMRS Facility: \_\_\_\_\_
- Other: (easement / right-of-way; floodplain; variance; vested right; 1041 permit; oil / gas production permit) GDP Amendment

**PROJECT INFORMATION**

Summary: \_\_\_\_\_  
Industrial / Retail Development  
2nd GDP Amendment for  
St Louis Parish Commercial  
Park - see narrative

Current zoning: PC2D Proposed zoning: PC2D

**SIGNATURES & DATE**

Applicant: Alicia Rhymen  
 Print: Alicia Rhymen  
 Owner: Ascent Church  
 Print: Jim Candy / Ascent Church  
 Representative: \_\_\_\_\_  
 Print: \_\_\_\_\_

**CITY STAFF USE ONLY**

- Fee paid: \_\_\_\_\_
- Check number: \_\_\_\_\_
- Date Received: \_\_\_\_\_

Parcel # 15751600007 Sect. 17 Township 15 Range 69

October 17, 2019

Lisa Ritchie

Planning Department

City of

Louisville

Louisville, Colorado 80027

Dear Ms. Ritchie,

I am the property owner of 1212 S 96<sup>th</sup> Street, Louisville, CO and provide consent for the United Properties to submit an Application to the City of Louisville for an amendment to the existing General Development Plan for purpose of:

Add Industrial and car wash as an allowable use, reduce the building setback from 60 to 55 feet along 96<sup>th</sup> Street, properly align zones with property boundaries, change parking lot configuration requirements in zone 2 and 3, eliminate local road and provide private drives with cross access easements between properties and construct the 8' crushed refined trail along 96<sup>th</sup> Street to Coal Creek instead of cash in lieu for the public land deficit requirements for the overall development.

We support the proposed Second Amendment and feel it bring benefits to all three properties that will allow the developments to viably move forward. We urge the City to approve the proposed amendments to the General Development Plan that has limited and development prohibitive on all three (3) properties for many years.

Sincerely,



Adrian Games

1212 S 96<sup>th</sup> Street property owner

October 17, 2019

Lisa Ritchie  
Planning Department  
City of Louisville  
Louisville, Colorado 80027

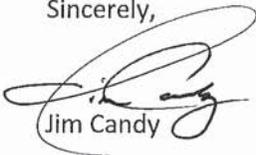
Dear Ms. Ritchie,

Ascent Community Church is the current property owner of 1326 S 96<sup>th</sup> Street, Louisville, CO. We provide consent for the United Properties to submit an Application to the City of Louisville for an amendment to the existing General Development Plan for purpose of:

Add Industrial and car wash as an allowable use, reduce the building setback from 60 to 55 feet along 96<sup>th</sup> Street, properly align zones with property boundaries, change parking lot configuration requirements in zone 2 and 3, eliminate local road and provide private drives with cross access easements between properties and construct the 8' crushed refined trail along 96<sup>th</sup> Street to Coal Creek instead of cash in lieu for the public land deficit requirements for the overall development.

We support the proposed Second Amendment and feel it bring benefits to all three properties that will allow the developments to viably move forward. We urge the City to approve the proposed amendments to the General Development Plan that has limited and development prohibitive on all three (3) properties for many years.

Sincerely,



Jim Candy

Pastor of Ascent Church Community

**St. Louis Church**  
902 Grant  
Louisville, Colo. 80027

December 19, 2019

Lisa Ritchie  
Planning Department  
City of Louisville  
Louisville, Colorado 80027

Property at 96<sup>th</sup> and Dillon

Dear Ms. Ritchie,

Saint Louis Catholic Church consents to the application filed by United Properties for an amendment to the existing General Development Plan for this property.

The existing General Development Plan has limited development opportunities, and the proposed Second Amendment is intended to make changes that will allow development to move forward.

We urge the City to approve the proposed amendments to the General Development Plan.

Sincerely,

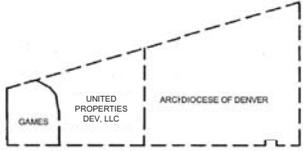


Rev. Timothy Hjelstrom

Pastor

# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN 2ND AMENDMENT

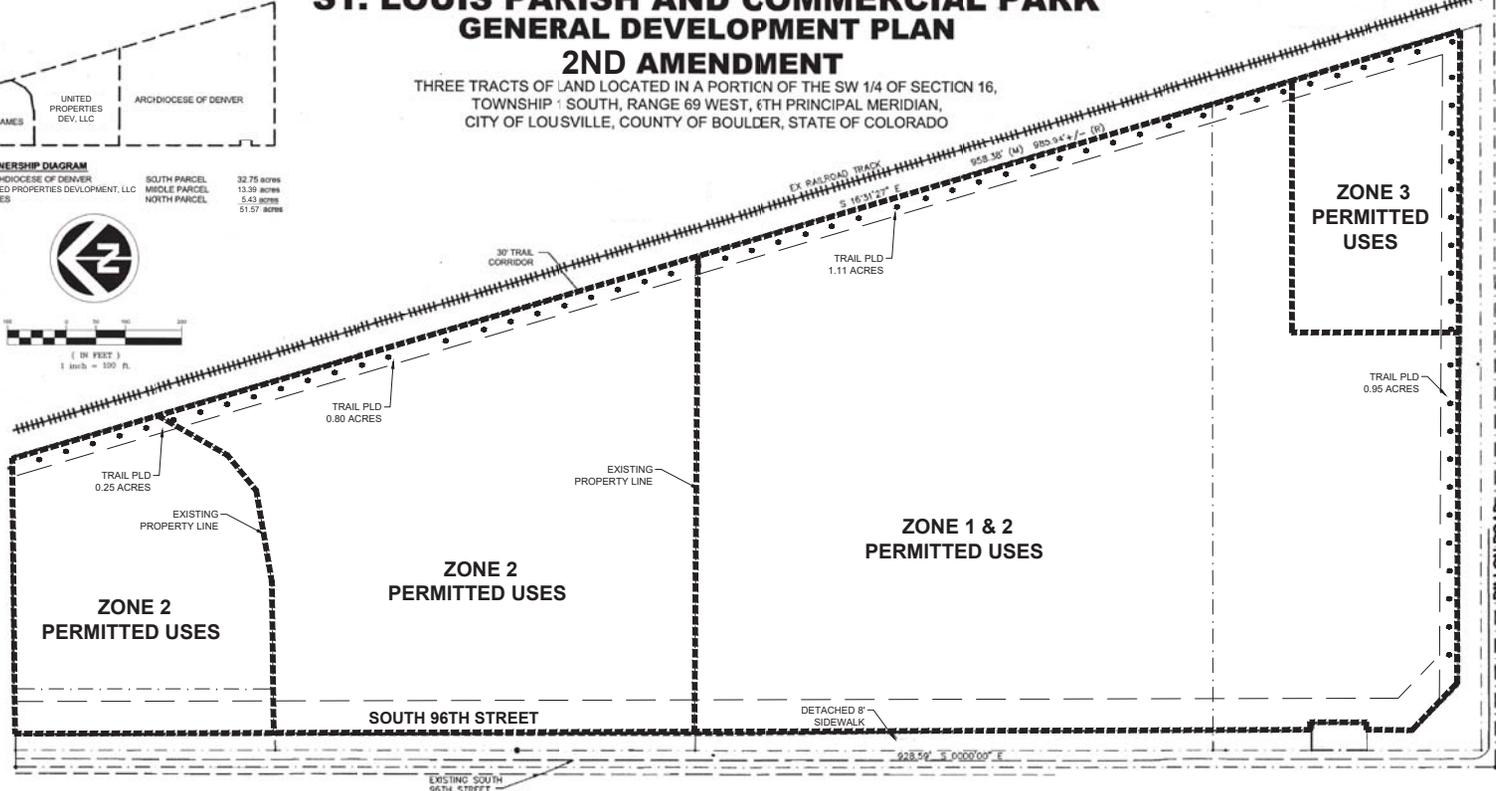
THREE TRACTS OF LAND LOCATED IN A PORTION OF THE SW 1/4 OF SECTION 16,  
TOWNSHIP 1 SOUTH, RANGE 69 WEST, 6TH PRINCIPAL MERIDIAN,  
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



**OWNERSHIP DIAGRAM**  
ARCHDIOCESE OF DENVER 32.75 acres  
UNITED PROPERTIES DEVELOPMENT, LLC 13.39 acres  
GAMES 5.43 acres  
NORTH PARCEL 51.57 acres



1 inch = 100 ft



**KEY MAP**  
HTS

**HEIGHT, YARD AND BULK REQUIREMENTS**

**ZONE ONE**

- HEIGHT TO BE MEASURED FROM FINAL FINISH GRADE.
- BUILDING SETBACKS FROM S. 96TH ARE 55 FEET. YARD AND BULK STANDARDS SHALL COMPLY WITH CITY OF LOUISVILLE ZONING REGULATIONS IN EFFECT AT TIME OF PLD.
- PARKING AMOUNT TO CONFORM WITH CITY OF LOUISVILLE REGULATIONS.

**ZONE TWO AND THREE**

- REFER TO SHEET 2 FOR ALL FAR REQUIREMENTS PER SUBAREA ZONING.
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- BUILDINGS WITHIN ZONE 2A ADJACENT TO, OR FRONTING TO SOUTH 96TH STREET SHALL NOT EXCEED TWENTY-FIVE (25) FEET IN HEIGHT FOR RETAIL USES AND ALL OTHER BUILDINGS SHALL CONFORM WITH THE CITY OF LOUISVILLE HEIGHT REGULATIONS.
- PARKING LOTS ADJACENT TO SOUTH 96TH STREET SHALL BE SHIELDED FROM SOUTH 96TH STREET USING ENHANCED LANDSCAPING TECHNIQUES SUCH THAT IT IS EFFECTIVELY BUFFERED.
- PARKING AMOUNT TO CONFORM WITH CITY OF LOUISVILLE REGULATIONS.

**SITE INFORMATION**

**OWNERSHIP**

- CATHOLIC ARCHDIOCESE OF DENVER/ST. LOUIS CATHOLIC CHURCH
- UNITED PROPERTIES DEVELOPMENT, LLC
- ADRIAN GAMES

**DEDICATIONS**

- ALL DEDICATIONS FOR SOUTH 96TH STREET AND DILLON ROAD RIGHTS-OF-WAY ARE COMPLETED PRIOR TO REZONING.
- PURSUANT TO COMPREHENSIVE PLAN POLICY, THE LAND DEDICATION REQUIRED BY THE SUBDIVISION REGULATIONS SHALL BE PRIMARILY USED FOR NEIGHBORHOOD PARKS, TRAILS LINKAGES AND BUFFERS TO SERVE THE SUBDIVISION A TRAIL LINKAGE CORRIDOR SHALL BE PROVIDED ALONG THE EAST AND SOUTH BOUNDARIES TO THE PROPERTY, AND A LANDSCAPED BUFFER SHALL BE PROVIDED ALONG SOUTH 96TH STREET. THE FORM OF DEDICATION, RESPONSIBLE FOR CONSTRUCTION AND RESPONSIBILITY FOR MAINTENANCE SHALL BE DETERMINED AT THE TIME OF SUBDIVISION.

**ACCESS MANAGEMENT**

THE ACCESS MOVEMENTS SHOWN ON THE PLD ARE SUBJECT TO CHANGE. AT ANY TIME IN THE FUTURE, IT IS DETERMINED BY THE CITY THAT CHANGE IS APPROPRIATE TO ENHANCE TRAFFIC FLOW ON ONE OR MORE SURROUNDING STREETS, OR TO MITIGATE AN UNSAFE SITUATION, UPON NOTIFICATION FROM THE CITY, THE PROPERTY OWNERS SHALL MAKE SUCH PHYSICAL CHANGES, AT THEIR COST, AS MAY BE REQUIRED BY THE CITY. EACH PRESENT AND FUTURE PROPERTY OWNER SHALL ACKNOWLEDGE IN WRITING THE FOREGOING AUTHORITY OF THE CITY. ALL ACCESS POINTS WILL BE PRIVATE. ACCESS DRIVES WITHIN THE DEVELOPMENT WILL HAVE CROSS ACCESS EASEMENTS AND MAINTENANCE AGREEMENTS. LEGAL EASEMENTS TO BE RECORDED AT PLATTING.

**DEVELOPMENT CONCEPT AND GENERAL NOTES**

- THE DEVELOPMENT CONCEPT IS TO ESTABLISH A RELIGIOUS INSTITUTION/SCHOOL CAMPUS AT THE INTERSECTION OF SOUTH 96TH STREET AND DILLON ROAD. A MIXED-USE COMMERCIAL/INDUSTRIAL DEVELOPMENT WITHIN THE CENTRAL PORTION OF THE DEVELOPMENT, WITH THE REMAINDER OF THE PARCEL(S) USED FOR COMMERCIAL PURPOSES THAT ARE NOT IN CONFLICT WITH THE PRESENCE OF RELIGIOUS INSTITUTIONS AND A SCHOOL. THE DEVELOPMENT IS INTENDED TO PROVIDE SUPPORT SERVICES TO THE INDUSTRIAL/EMPLOYMENT AREA LOCATED TO THE EAST, AND BE A TRANSITION BETWEEN THAT DEVELOPMENT AND THE OPEN SPACE TO THE WEST. A LANDSCAPE BUFFER, BUILDING HEIGHTS, FLOOR AREA RATIOS AND PARKING REQUIREMENTS SHALL ALL BE USED TO FACILITATE THE TRANSITION FROM RURAL-OPEN SPACE TO THE DEVELOPED PROPERTY.
- EXCEPT WHERE AMENDED BY THIS GENERAL DEVELOPMENT PLAN, DEVELOPMENT WILL BE SUBJECT TO THE CITY OF LOUISVILLE COMMERCIAL DEVELOPMENT DESIGN STANDARDS AND GUIDELINES (CDSDG).
- DEVELOPMENT SHALL BE COVERED BY A GENERAL DEVELOPMENT PLAN AGREEMENT, SIGNED BY ALL OWNERS, ADDRESSING THE PHASING OF DEVELOPMENT, RESPONSIBILITY FOR INFRASTRUCTURE DESIGN, CONSTRUCTION AND COST, AND ARCHITECTURAL DESIGN CRITERIA. THIS DEVELOPMENT AGREEMENT SHALL BE SUBMITTED TO, AND APPROVED BY, THE CITY OF LOUISVILLE IN CONJUNCTION WITH THE DEVELOPMENT APPLICATION AND PRELIMINARY PLAT PRIOR TO DEVELOPMENT OF ANY OF THE SUBJECT PROPERTY.
- RETAIL ESTABLISHMENTS SHALL BE LIMITED TO 70,000 SQUARE FEET.

- PERMITTED USES**
- ZONE ONE (approx. 16.2 acres)**
- RELIGIOUS INSTITUTIONS
  - SCHOOLS
  - ANCILLARY FACILITIES TYPICALLY AND COMMONLY ASSOCIATED WITH RELIGIOUS INSTITUTIONS AND SCHOOLS INCLUDING A RECTORY, ADMINISTRATIVE OFFICES, AND A CHILD CARE CENTER AS DETERMINED BY THE PLANNING COMMISSION AND CITY COUNCIL DURING THE PLD PROCESS.
- ZONE TWO (approx. 31.8 acres)**
- CONTINUAL OF THE EXISTING RESIDENTIAL USES ON THE PROPERTY.
  - RELIGIOUS INSTITUTION USE BY RIGHT.
  - ALL USES IN ZONE ONE - USE BY SPECIAL REVIEW USE
  - PROFESSIONAL, BUSINESS AND ADMINISTRATIVE OFFICES.
  - PROFESSIONAL MEDICAL OFFICES AND CLINICS.
  - FINANCIAL OFFICES AND BANKS.
  - CULTURAL FACILITIES SUCH AS MUSEUMS, THEATERS, AND ART GALLERIES - USE BY SPECIAL REVIEW USE.
  - PEDESTRIAN PLAZAS, PEDESTRIAN WAYS, INCLUSIVE OF OUTDOOR AMENITIES AS OUTDOOR ART EXHIBIT FACILITIES AND PUBLIC ART.
  - OUTDOOR SPECIALTY USES, INCLUSIVE OF SIDEWALK CAFES AND OUTDOOR MARKET PLACES. OUTDOOR FLEA MARKETS ARE AN EXCLUDED USE IN ZONE 2 AND 3.
  - INDOOR RECREATIONAL/FITNESS FACILITIES - USE BY SPECIAL REVIEW USE.
  - OUTDOOR RECREATIONAL/FITNESS FACILITIES - USE BY SPECIAL REVIEW USE.
  - OUTDOOR COMMERCIAL AMUSEMENT - USE BY SPECIAL REVIEW USE. TEMPORARY EVENTS WITH DURATIONS OF TEN DAYS OR LESS IN ONE SEASON SHALL BE PROCESSED UNDER THE APPLICABLE TEMPORARY USE REVIEW STANDARDS AND CRITERIA.
  - RESTAURANTS AND CAFES.
  - FAST FOOD SERVICES IN CONJUNCTION WITH DRIVE THROUGH SERVICE SERVICE FACILITIES - USE BY SPECIAL REVIEW USE.
  - HOSPITALS - USE BY SPECIAL REVIEW USE.
  - ANIMAL HOSPITALS AND SMALL ANIMAL CLINICS - USE BY SPECIAL REVIEW USE.
  - KENNELS FOR THE BOARDING OR BREEDING OF DOMESTIC ANIMALS OR LIVESTOCK ARE AN EXCLUDED USE IN ALL ZONES.
  - AUTO SERVICE AND FUELING STATIONS - USE BY SPECIAL REVIEW USE.
  - AUTO SALES AND AUTO BODY SHOPS ARE EXCLUDED IN ALL ZONES.
  - ASSISTED LIVING AND SKILLED NURSING FACILITIES.
  - RESIDENTIAL USES INCLUDING INDEPENDENT AND SENIOR LIVING ARE EXCLUDED.
  - CHILD CARE CENTERS - USE BY SPECIAL REVIEW USE.
  - RETAIL - PERSONAL SERVICE SHOPS.
  - CAR WASH - USE BY SPECIAL REVIEW USE.
  - RESEARCH/OFFICE AND CORPORATE USES, AND FACILITIES FOR THE MANUFACTURING, FABRICATION, PROCESSING, OR ASSEMBLY OF SCIENTIFIC OR TECHNICAL PRODUCTS, OR OTHER PRODUCTS, IF SUCH USES ARE COMPATIBLE WITH SURROUNDING AREAS.
- ZONE THREE (approx. 3.4 acres)**
- CHILD CARE CENTERS - USE BY SPECIAL REVIEW USE.
  - ALL USES PERMITTED IN ZONE ONE.
  - ASSISTED LIVING AND SKILLED NURSING FACILITIES - USE BY SPECIAL REVIEW USE.
  - RESIDENTIAL USES, INCLUDING INDEPENDENT AND SENIOR LIVING AREA EXCLUDED.

**OWNERSHIP CERTIFICATE**  
BY SIGNING THIS GDP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GDP. WITNESS OUR HANDS AND SEALS THIS \_\_\_\_ DAY OF \_\_\_\_ 202\_\_.

OWNER \_\_\_\_\_

**OWNERSHIP CERTIFICATE**  
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OWNER \_\_\_\_\_

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OWNER \_\_\_\_\_

STATE OF COLORADO  
COUNTY OF \_\_\_\_\_  
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON THIS (DATE) BY (NAME AND TITLE OF POSITION): \_\_\_\_\_

(NOTARY'S OFFICIAL SIGNATURE) \_\_\_\_\_

(COMMISSION EXPIRATION) \_\_\_\_\_

STATE OF COLORADO  
COUNTY OF \_\_\_\_\_  
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(NOTARY'S OFFICIAL SIGNATURE) \_\_\_\_\_

(COMMISSION EXPIRATION) \_\_\_\_\_

STATE OF COLORADO  
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(NOTARY'S OFFICIAL SIGNATURE) \_\_\_\_\_

(COMMISSION EXPIRATION) \_\_\_\_\_

**CITY COUNCIL CERTIFICATE**  
APPROVED THIS \_\_\_\_ DAY OF \_\_\_\_ 202\_\_ BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO.

ORDINANCE NO. \_\_\_\_\_ SERIES \_\_\_\_\_

MAYOR \_\_\_\_\_ CITY CLERK \_\_\_\_\_

**PLANNING COMMISSION CERTIFICATE**  
RECOMMENDED APPROVAL THIS \_\_\_\_ DAY OF \_\_\_\_ 202\_\_ BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO.

RESOLUTION NO. \_\_\_\_\_ SERIES \_\_\_\_\_

**BOULDER COUNTY CLERK AND RECORDER'S CERTIFICATE**  
THIS GDP WAS RECORDED IN THE OFFICE OF THE BOULDER COUNTY CLERK AND RECORDER ON \_\_\_\_ DAY OF \_\_\_\_ 202\_\_ UNDER RECEPTION NO. \_\_\_\_\_

**AMENDMENTS**

- THIS FIRST AMENDMENT ALLOWS RELIGIOUS INSTITUTIONS IN ZONE 2 AS A USE BY RIGHT.
- THE SECOND AMENDMENT REDUCES THE BUILDING SETBACK DISTANCE FROM SOUTH 96TH STREET AND CHANGES THE PARKING LOT CONFIGURATION STANDARDS IN ZONE 2 AND 3. ELIMINATES LOCAL ROAD AND PROVIDES PRIVATE DRIVES WITH CROSS ACCESS BETWEEN ELEMENTS. ALIGNS ZONES WITH PROPERTY BOUNDARY AND ADD INDUSTRIAL AND CAR WASH USES.

**ST. LOUIS PARISH AND COMMERCIAL PARK  
GENERAL DEVELOPMENT PLAN  
2ND AMENDMENT**



6	10/04/2020	RT/REB/MATT/AL	2ND GDP AMENDMENT
5	10/13/2020	RT/REB/MATT/AL	2ND GDP AMENDMENT
4	11/24/2019	RT/REB/MATT/AL	2ND GDP AMENDMENT
3	08/14/2019	RT/REB/MATT/AL	2ND SUBMITTAL
2	08/14/2019	RT/REB/MATT/AL	2ND SUBMITTAL
1	08/14/2019	RT/REB/MATT/AL	ORIGINAL ISSUE DATE

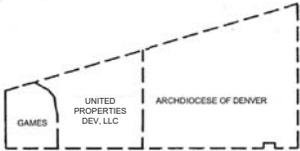
**DOCUMENT AMENDMENTS**

Project Number: 175001  
Drawn By: JDM  
Checked By: JDM  
Designed By: JDM  
Sheet Number: 1 of 2

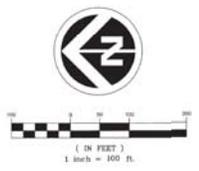
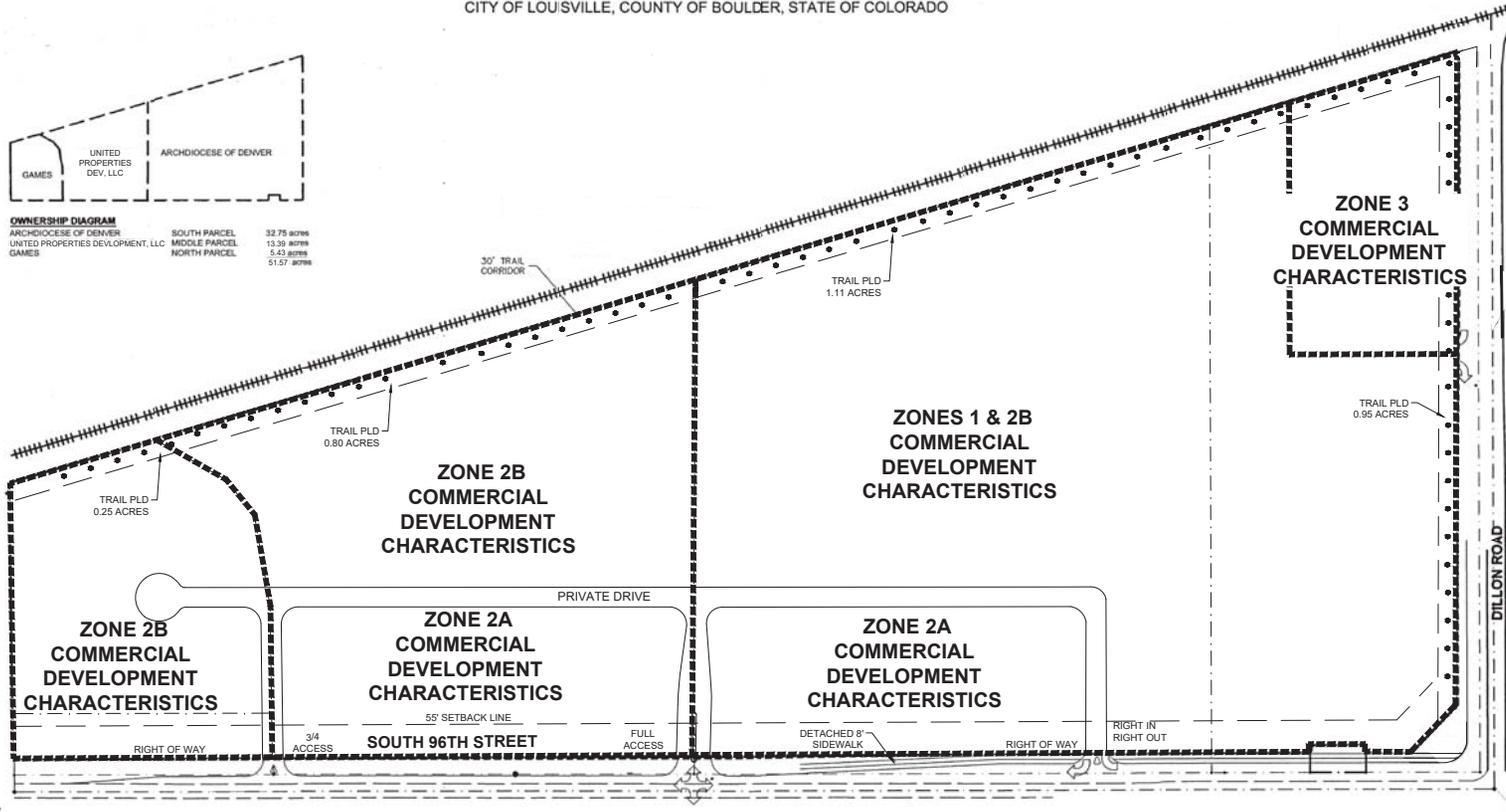
**UNITED PROPERTIES**  
1331 17TH STREET, SUITE 604  
DENVER, CO 80202  
UNPROPERTIES.COM

# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN 2ND AMENDMENT

THREE TRACTS OF LAND LOCATED IN A PORTION OF THE SW 1/4 OF SECTION 16,  
TOWNSHIP 1 SOUTH, RANGE 69 WEST, 6TH PRINCIPAL MERIDIAN,  
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



**OWNERSHIP DIAGRAM**  
ARCHDIOCESE OF DENVER 32.75 acres  
UNITED PROPERTIES DEV. LLC 13.30 acres  
GAMES 2.53 acres  
NORTH PARCEL 51.57 acres



**UNITED PROPERTIES**  
1331 17TH STREET, SUITE 604  
DENVER, CO 80202  
UNPROPERTIES.CO

**ST. LOUIS PARISH AND COMMERCIAL PARK  
GENERAL DEVELOPMENT PLAN  
2ND AMENDMENT**

**EES**  
ENTITLEMENT AND  
SURVEILLANCE  
SOLUTIONS, INC.  
501 B Cherry St, Suite 300  
Glenfield, CO 80246  
303-672-7987 www.ees.us.com

**DOCUMENT AMENDMENTS**

No.	Date	Description
1	03/02/2017	2ND SUBMITTAL
2	04/06/2017	2ND SUBMITTAL
3	06/02/2017	ORIGINAL ISSUE DATE
4	11/02/2019	1TH SUBMITTAL - 2ND GDP AMENDMENT
5	01/10/2020	1TH SUBMITTAL - 2ND GDP AMENDMENT
6	03/04/2020	1TH SUBMITTAL - 2ND GDP AMENDMENT

**COMMERCIAL DEVELOPMENT CHARACTERISTICS**

ZONE	SETBACKS:	PARKING:	HEIGHT:	ARCHITECTURE:
ZONE 2A	55' FROM 96TH STREET, PER CODE OTHERWISE	ENHANCED LANDSCAPING TO BUFFER FROM S. 96TH STREET, PER CODE OTHERWISE	25' MAXIMUM FROM FINAL FINISH GRADE (RETAIL)	SUBJECT TO PUD GUIDELINES
ZONE 2B	55' FROM 96TH STREET, PER CODE OTHERWISE	ENHANCED LANDSCAPING TO BUFFER FROM S. 96TH STREET, PER CODE OTHERWISE	40' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PUD GUIDELINES
ZONE 3	PER MUNICIPAL CODE	PER MUNICIPAL CODE	35' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PUD GUIDELINES

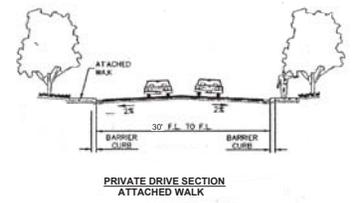
**TABLES**

DESCRIPTION	AMOUNT
PUBLIC LAND DEDICATION REQUIRED (51.57 ACRES @ 12%)	6.19 acres
PROPOSED TRAIL DEDICATION DEFICIT	3.11 acres
<b>TOTAL</b>	<b>3.08 acres</b>

**FAR DEVELOPMENT ALLOWANCE**

SITE	AREA
UNITED PROPERTIES PARCEL: 548,882 SF AT 0.25 FAR =	137,223 SQUARE FEET
GAMES PARCEL: 225,898 SF AT 0.25 FAR =	56,418 SQUARE FEET
ARCHDIOCESE OF DENVER PARCEL: NO FAR IF DEVELOPED AS ZONE 1 USE.	
OTHERWISE 0.25 FAR MUST BE MAINTAINED: 1,187,452 SF AT 0.25 FAR =	296,863 SQUARE FEET
ARCHDIOCESE OF DENVER PARCEL: 148,190 SF AT 0.20 FAR =	29,638 SQUARE FEET
<b>TOTAL</b>	<b>520,340 SQUARE FEET</b>

NOTE: DESIGN WILL BE ENCOURAGED TO ESTABLISH CROSS ACCESS TO DILLON ROAD



**CONCEPTUAL ROADWAY CROSS SECTIONS**

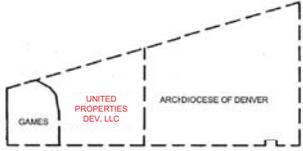
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Project Number: 175001  
Designed By: JDM  
Drawn By: CETB  
Checked By: JDM  
Reviewed By: JDM

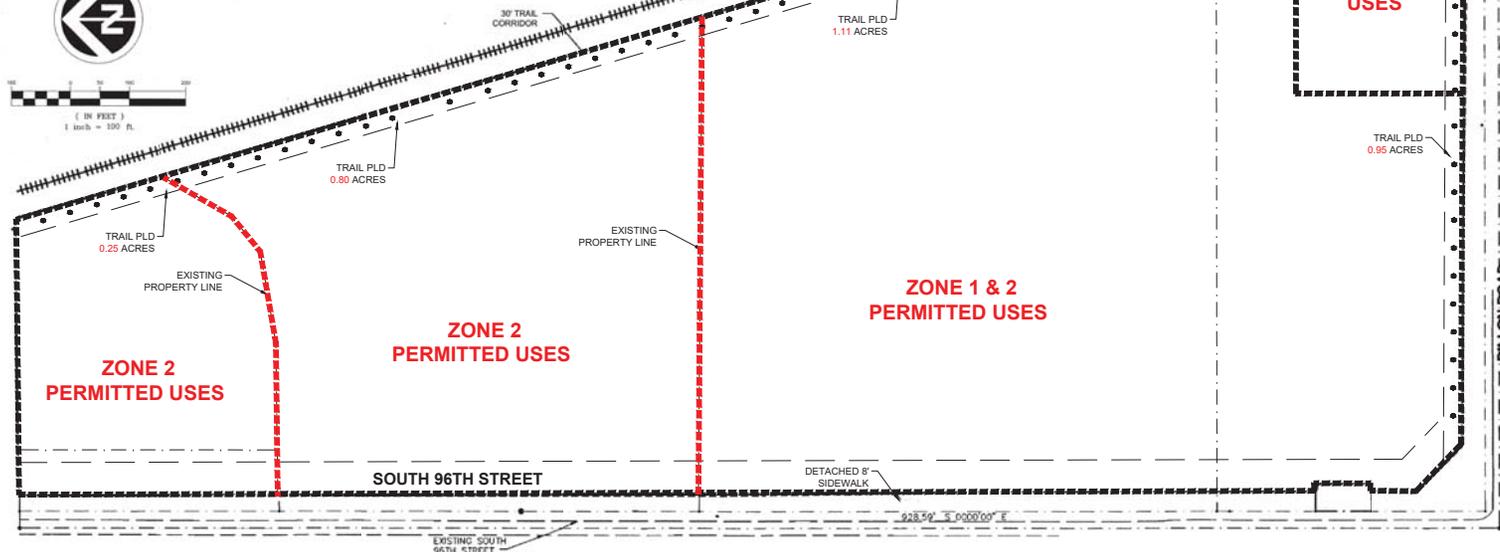
# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN

## 2ND AMENDMENT

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- UNITED PROPERTIES DEVELOPMENT, LLC
- ADRIAN GAMES

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THE ACCESS MOVEMENTS SHOWN ON THE PLD ARE SUBJECT TO CHANGE. AT ANY TIME IN THE FUTURE, IT IS DETERMINED BY THE CITY THAT CHANGE IS APPROPRIATE TO ENHANCE TRAFFIC FLOW ON ONE OR MORE SURROUNDING STREETS, OR TO MITIGATE AN UNSAFE SITUATION, UPON NOTIFICATION FROM THE CITY, THE PROPERTY OWNERS SHALL MAKE SUCH PHYSICAL CHANGES, AT THEIR COST, AS MAY BE REQUIRED BY THE CITY. EACH PRESENT AND FUTURE PROPERTY OWNER SHALL ACKNOWLEDGE IN WRITING THE FOREGOING AUTHORITY OF THE CITY. ALL ACCESS POINTS WILL BE PRIVATE. ACCESS DRIVES WITHIN THE DEVELOPMENT WILL HAVE CROSS ACCESS EASEMENTS AND MAINTENANCE AGREEMENTS. LEGAL EASEMENTS TO BE RECORDED AT PLATTING.

**DEVELOPMENT CONCEPT AND GENERAL NOTES**

- THE DEVELOPMENT CONCEPT IS TO ESTABLISH A RELIGIOUS INSTITUTION/SCHOOL CAMPUS AT THE INTERSECTION OF SOUTH 96TH STREET AND DILLON ROAD. A MIXED-USE COMMERCIAL/INDUSTRIAL DEVELOPMENT WITHIN THE CENTRAL PORTION OF THE DEVELOPMENT, WITH THE REMAINDER OF THE PARCELS USED FOR COMMERCIAL PURPOSES THAT ARE NOT IN CONFLICT WITH THE PRESENCE OF RELIGIOUS INSTITUTIONS AND A SCHOOL. THE DEVELOPMENT IS INTENDED TO PROVIDE SUPPORT SERVICES TO THE INDUSTRIAL/EMPLOYMENT AREA LOCATED TO THE EAST, AND BE A TRANSITION BETWEEN THAT DEVELOPMENT AND THE OPEN SPACE TO THE WEST. A LANDSCAPE BUFFER, BUILDING HEIGHTS, FLOOR AREA RATIOS AND PARKING REQUIREMENTS SHALL ALL BE USED TO FACILITATE THE TRANSITION FROM RURAL-OPEN SPACE TO THE DEVELOPED PROPERTY.
- EXCEPT WHERE AMENDED BY THIS GENERAL DEVELOPMENT PLAN, DEVELOPMENT WILL BE SUBJECT TO THE CITY OF LOUISVILLE COMMERCIAL DEVELOPMENT DESIGN STANDARDS AND GUIDELINES (CDSDS).
- DEVELOPMENT SHALL BE GOVERNED BY A GENERAL DEVELOPMENT PLAN AGREEMENT, SIGNED BY ALL OWNERS, ADDRESSING THE PHASING OF DEVELOPMENT, RESPONSIBILITY FOR INFRASTRUCTURE DESIGN, CONSTRUCTION AND COST AND ARCHITECTURAL DESIGN CRITERIA. THIS DEVELOPMENT AGREEMENT SHALL BE SUBMITTED TO, AND APPROVED BY, THE CITY OF LOUISVILLE IN CONJUNCTION WITH THE DEVELOPMENT APPLICATION AND PRELIMINARY PLAT PRIOR TO DEVELOPMENT OF ANY OF THE SUBJECT PROPERTY.
- RETAIL ESTABLISHMENTS SHALL BE LIMITED TO 70,000 SQUARE FEET.

**CITY COUNCIL CERTIFICATE**  
APPROVED THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 202\_\_\_\_, BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO.

ORDINANCE NO. \_\_\_\_\_ SERIES \_\_\_\_\_

MAYOR \_\_\_\_\_ CITY CLERK \_\_\_\_\_

**PLANNING COMMISSION CERTIFICATE**  
RECOMMENDED APPROVAL THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 202\_\_\_\_, BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO.

RESOLUTION NO. \_\_\_\_\_ SERIES \_\_\_\_\_

**BOULDER COUNTY CLERK AND RECORDER'S CERTIFICATE**  
THIS GDP WAS RECORDED IN THE OFFICE OF THE BOULDER COUNTY CLERK AND RECORDER ON \_\_\_\_\_ DAY OF \_\_\_\_\_, 202\_\_\_\_, UNDER RECEPTION NO. \_\_\_\_\_

- PERMITTED USES**
- ZONE ONE (approx. 16.2 acres)**
- RELIGIOUS INSTITUTIONS
  - SCHOOLS
  - ANCILLARY FACILITIES TYPICALLY AND COMMONLY ASSOCIATED WITH RELIGIOUS INSTITUTIONS AND SCHOOLS INCLUDING A RECTORY, ADMINISTRATIVE OFFICES, AND A CHILD CARE CENTER AS DETERMINED BY THE PLANNING COMMISSION AND CITY COUNCIL DURING THE PLD PROCESS.
- ZONE TWO (approx. 31.8 acres)**
- CONTINUAL OF THE EXISTING RESIDENTIAL USES ON THE PROPERTY.
  - RELIGIOUS INSTITUTION USE BY RIGHT.
  - ALL USES IN ZONE ONE - USE BY SPECIAL REVIEW USE
  - PROFESSIONAL, BUSINESS AND ADMINISTRATIVE OFFICES.
  - PROFESSIONAL MEDICAL OFFICES AND CLINICS.
  - FINANCIAL OFFICES AND BANKS
  - CULTURAL FACILITIES SUCH AS MUSEUMS, THEATERS, AND ART GALLERIES - USE BY SPECIAL REVIEW USE.
  - PEDESTRIAN PLAZAS, PEDESTRIAN WAYS, INCLUSIVE OF OUTDOOR AMENITIES AS OUTDOOR ART EXHIBIT FACILITIES AND PUBLIC ART.
  - OUTDOOR SPECIALTY USES, INCLUSIVE OF SIDEWALK CAFES AND OUTDOOR MARKET PLACES. OUTDOOR FLEA MARKETS ARE AN EXCLUDED USE IN ZONE 2 AND 3.
  - INDOOR RECREATIONAL/FITNESS FACILITIES - USE BY SPECIAL REVIEW USE.
  - OUTDOOR RECREATIONAL/FITNESS FACILITIES - USE BY SPECIAL REVIEW USE.
  - OUTDOOR COMMERCIAL AMUSEMENT - USE BY SPECIAL REVIEW USE. TEMPORARY EVENTS WITH DURATIONS OF TEN DAYS OR LESS IN ONE SEASON SHALL BE PROCESSED UNDER THE APPLICABLE TEMPORARY USE REVIEW STANDARDS AND CRITERIA.
  - RESTAURANTS AND CAFES.
  - FAST FOOD SERVICES IN CONJUNCTION WITH DRIVE THROUGH SERVICE SERVICE FACILITIES - USE BY SPECIAL REVIEW USE.
  - HOSPITALS - USE BY SPECIAL REVIEW USE.
  - ANIMAL HOSPITALS AND SMALL ANIMAL CLINICS - USE BY SPECIAL REVIEW USE.
  - KENNELS FOR THE BOARDING OR BREEDING OF DOMESTIC ANIMALS OR LIVESTOCK ARE AN EXCLUDED USE IN ALL ZONES.
  - AUTO SERVICE AND FUELING STATIONS - USE BY SPECIAL REVIEW USE.
  - AUTO SALES AND AUTO BODY SHOPS ARE EXCLUDED RURAL ZONES.
  - ASSISTED LIVING AND SKILLED NURSING FACILITIES.
  - RESIDENTIAL USES INCLUDING INDEPENDENT AND SENIOR LIVING ARE EXCLUDED.
  - CHILD CARE CENTERS - USE BY SPECIAL REVIEW USE.
  - RETAIL - PERSONAL SERVICE SHOPS.
  - CAR WASH - USE BY SPECIAL REVIEW USE.
  - RESEARCH/OFFICE AND CORPORATE USES, AND FACILITIES FOR THE MANUFACTURING, FABRICATION, PROCESSING, OR ASSEMBLY OF SCIENTIFIC OR TECHNICAL PRODUCTS, OR OTHER PRODUCTS, IF SUCH USES ARE COMPATIBLE WITH SURROUNDING AREAS.
- ZONE THREE (approx. 3.4 acres)**
- CHILD CARE CENTERS - USE BY SPECIAL REVIEW USE.
  - ALL USES PERMITTED IN ZONE ONE.
  - ASSISTED LIVING AND SKILLED NURSING FACILITIES - USE BY SPECIAL REVIEW USE.
  - RESIDENTIAL USES, INCLUDING INDEPENDENT AND SENIOR LIVING AREA EXCLUDED.

**OWNERSHIP CERTIFICATE**  
BY SIGNING THIS GDP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GDP. WITNESS OUR HANDS AND SEALS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 202\_\_\_\_.

OWNER

STATE OF COLORADO  
COUNTY OF \_\_\_\_\_  
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON THIS (DATE) BY (NAME AND TITLE OF POSITION): \_\_\_\_\_

(NOTARY'S OFFICIAL SIGNATURE)

(COMMISSION EXPIRATION)

**OWNERSHIP CERTIFICATE**  
BY SIGNING THIS GDP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GDP. WITNESS OUR HANDS AND SEALS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 202\_\_\_\_.

OWNER

STATE OF COLORADO  
COUNTY OF \_\_\_\_\_  
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON THIS (DATE) BY (NAME AND TITLE OF POSITION): \_\_\_\_\_

(NOTARY'S OFFICIAL SIGNATURE)

(COMMISSION EXPIRATION)

**OWNERSHIP CERTIFICATE**  
BY SIGNING THIS GDP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GDP. WITNESS OUR HANDS AND SEALS THIS \_\_\_\_\_ DAY OF \_\_\_\_\_, 202\_\_\_\_.

OWNER

STATE OF COLORADO  
COUNTY OF \_\_\_\_\_  
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME ON THIS (DATE) BY (NAME AND TITLE OF POSITION): \_\_\_\_\_

(NOTARY'S OFFICIAL SIGNATURE)

(COMMISSION EXPIRATION)

**AMENDMENTS**

- THIS FIRST AMENDMENT ALLOWS RELIGIOUS INSTITUTIONS IN ZONE 2 AS A USE BY RIGHT.
- THE SECOND AMENDMENT REDUCES THE BUILDING SETBACK DISTANCE FROM SOUTH 96TH STREET AND CHANGES THE PARKING LOT CONFIGURATION STANDARDS IN ZONE 2 AND 3. ELIMINATES LOCAL ROAD AND PROVIDES PRIVATE DRIVES WITH CROSS ACCESS BETWEEN ELEMENTS. ALIGNS ZONES WITH PROPERTY BOUNDARY AND ADD INDUSTRIAL AND CAR WASH USES.

**UNITED PROPERTIES**  
1331 17TH STREET, SUITE 804  
DENVER, CO 80202  
UNPROPERTIES.COM

**ST. LOUIS PARISH AND COMMERCIAL PARK  
GENERAL DEVELOPMENT PLAN  
2ND AMENDMENT**

**EES**  
ENVIRONMENTAL  
EVALUATION AND  
STATEMENT  
FOR THE  
ST. LOUIS PARISH AND  
COMMERCIAL PARK  
GENERAL DEVELOPMENT  
PLAN AND  
2ND AMENDMENT

901 S. Cherry St., Suite 800  
Denver, CO 80246  
303-572-7887 www.ees.us.com

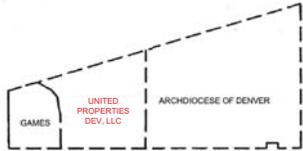
**DOCUMENT AMENDMENTS**

6	10/04/2020	ETHAN MATTAL	2ND GDP AMENDMENT
5	10/04/2020	ETHAN MATTAL	2ND GDP AMENDMENT
4	11/02/2019	ETHAN MATTAL	2ND GDP AMENDMENT
3	11/02/2019	ETHAN MATTAL	2ND GDP AMENDMENT
2	10/16/2017	ETHAN MATTAL	2ND GDP AMENDMENT
1	10/16/2017	ETHAN MATTAL	2ND GDP AMENDMENT

Project Number: 175001  
Drawn By: James B. JDM  
Checked By: CEIB  
Designed By: JDM  
Sheet Number: 1 of 2

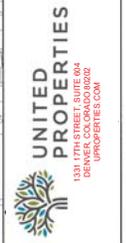
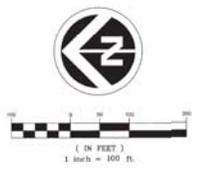
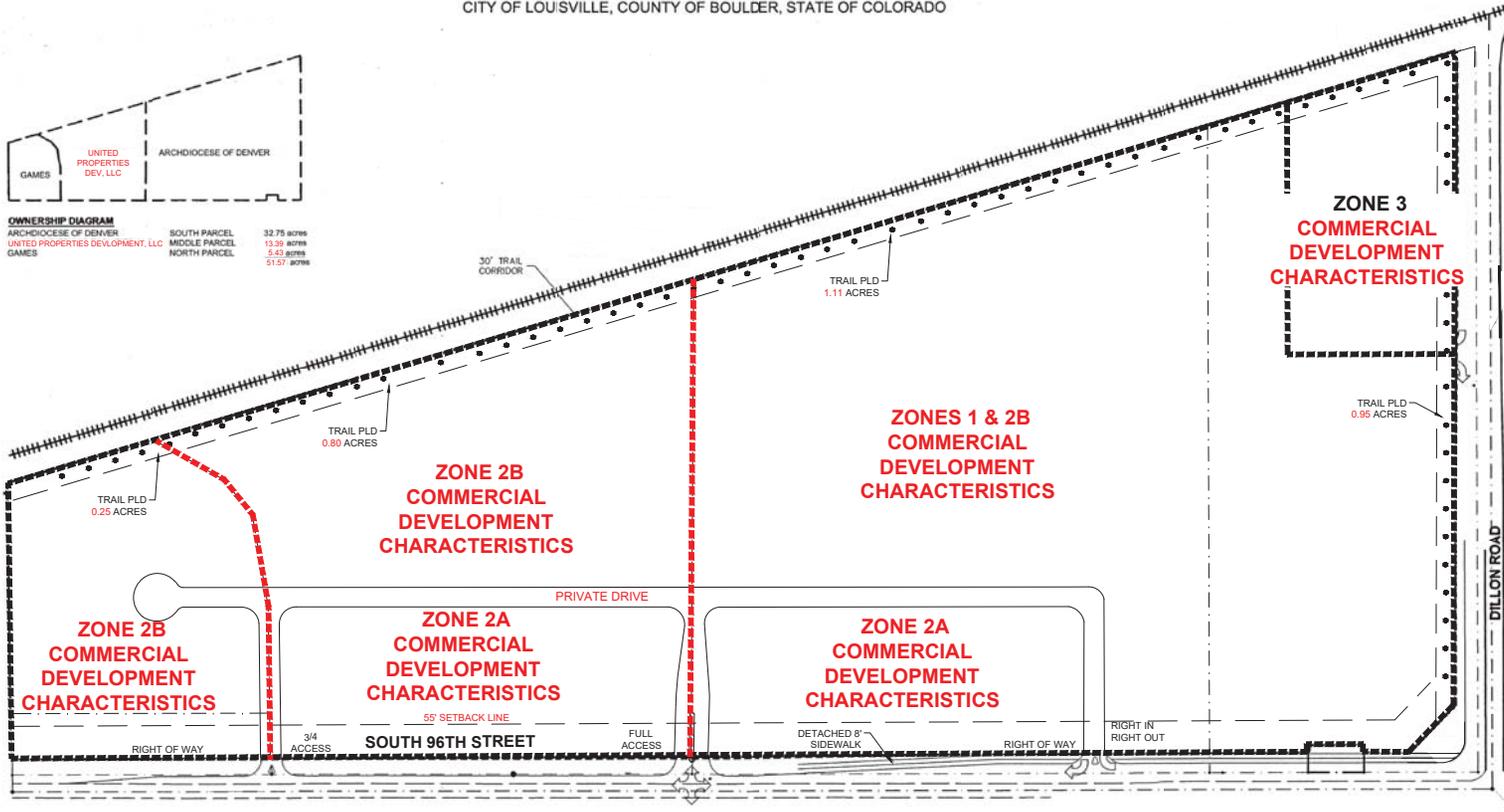
# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN 2ND AMENDMENT

THREE TRACTS OF LAND LOCATED IN A PORTION OF THE SW 1/4 OF SECTION 16,  
TOWNSHIP 1 SOUTH, RANGE 69 WEST, 6TH PRINCIPAL MERIDIAN,  
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



**OWNERSHIP DIAGRAM**

ARCHDIOCESE OF DENVER	SOUTH PARCEL	32.75 acres
UNITED PROPERTIES DEVELOPMENT, LLC	MIDDLE PARCEL	13.30 acres
GAMES	NORTH PARCEL	51.57 acres



**ST. LOUIS PARISH AND COMMERCIAL PARK  
GENERAL DEVELOPMENT PLAN  
2ND AMENDMENT**



**DOCUMENT AMENDMENTS**

No.	Date	Description
1	03/04/2020	FINAL SUBMITTAL - 2ND GDP AMENDMENT
2	03/04/2020	FINAL SUBMITTAL - 2ND GDP AMENDMENT
3	03/04/2020	FINAL SUBMITTAL - 2ND GDP AMENDMENT
4	11/02/2019	FINAL SUBMITTAL - 2ND GDP AMENDMENT
5	03/04/2020	FINAL SUBMITTAL - 2ND GDP AMENDMENT
6	03/04/2020	FINAL SUBMITTAL - 2ND GDP AMENDMENT

501 B Cherry St, Suite 300  
Glenfield, CO 80246  
303-672-7987 www.ees.us.com

**COMMERCIAL DEVELOPMENT CHARACTERISTICS**

**ZONE 2A**

SETBACKS:	55' FROM 96TH STREET, PER CODE OTHERWISE
PARKING:	ENHANCED LANDSCAPING TO BUFFER FROM S. 96TH STREET, PER CODE OTHERWISE
HEIGHT:	25' MAXIMUM FROM FINAL FINISH GRADE (RETAIL)
ARCHITECTURE:	SUBJECT TO PUD GUIDELINES

**ZONE 2B**

SETBACKS:	55' FROM 96TH STREET, PER CODE OTHERWISE
PARKING:	ENHANCED LANDSCAPING TO BUFFER FROM S. 96TH STREET, PER CODE OTHERWISE
HEIGHT:	40' MAXIMUM FROM FINAL FINISH GRADE
ARCHITECTURE:	SUBJECT TO PUD GUIDELINES

**ZONE 3**

SETBACKS:	PER MUNICIPAL CODE
PARKING:	PER MUNICIPAL CODE
HEIGHT:	35' MAXIMUM FROM FINAL FINISH GRADE
ARCHITECTURE:	SUBJECT TO PUD GUIDELINES

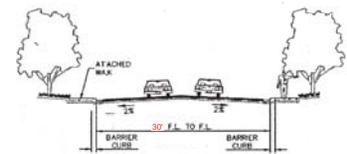
**TABLES**

PUBLIC LAND DEDICATION REQUIRED (51.57 ACRES @ 12%)	6.19 acres
PROPOSED TRAIL DEDICATION DEFICIT	3.11 acres
	3.08 acres

**FAR DEVELOPMENT ALLOWANCE**

<b>SITE</b>		
• UNITED PROPERTIES PARCEL: 548,882 SF AT 0.25 FAR =	137,223 SQUARE FEET	
• GAMES PARCEL: 225,898 SF AT 0.25 FAR =	56,474 SQUARE FEET	
• ARCHDIOCESE OF DENVER PARCEL: NO FAR IF DEVELOPED AS ZONE 1 USE	296,863 SQUARE FEET	
• OTHERWISE 0.25 FAR MUST BE MAINTAINED: 1,187,452 SF AT 0.25 FAR =	296,863 SQUARE FEET	
• ARCHDIOCESE OF DENVER PARCEL: 148,190 SF AT 0.20 FAR =	29,638 SQUARE FEET	
<b>TOTAL:</b>	<b>520,340 SQUARE FEET</b>	

NOTE: DESIGN WILL BE ENCOURAGED TO ESTABLISH CROSS ACCESS TO DILLON ROAD



**CONCEPTUAL ROADWAY CROSS SECTIONS**

**AMENDMENTS**

- THIS FIRST AMENDMENT ALLOWS RELIGIOUS INSTITUTIONS IN ZONE 2 AS A USE BY RIGHT.
- THE SECOND AMENDMENT REDUCES THE BUILDING SETBACK DISTANCE FROM SOUTH 96TH STREET AND CHANGES THE PARKING LOT CONFIGURATION STANDARDS IN ZONE 2 AND 3. ELIMINATES LOCAL ROAD AND PROVIDES PRIVATE DRIVES WITH CROSS ACCESS BETWEEN ELEMENTS. ALIGNS ZONES WITH PROPERTY BOUNDARY AND ADD INDUSTRIAL AND CAR WASH USES.

Project Number: 175001  
Designed By: JDM  
Drawn By: CETB  
Checked By: JDM  
Reviewed By: JDM



January 31, 2020

Ms. Alicia Rhymer  
United Properties  
1331 17<sup>th</sup> Street, Suite 604  
Denver, CO 80202

Re: Louisville Industrial Park  
Traffic Impact Analysis  
Louisville, CO  
LSC #180012

Dear Ms. Rhymer:

In response to your request, LSC Transportation Consultants, Inc. has prepared this traffic impact analysis for the proposed Louisville Industrial Park development. As shown on Figure 1, the site is located north of W. Dillon Road and east of S. 96<sup>th</sup> Street in Louisville, Colorado. This site was most recently studied in the April 16, 2018 *Ascent Church Traffic Impact Analysis* by LSC.

## REPORT CONTENTS

The report contains the following: the existing roadway and traffic conditions in the vicinity of the site including the lane geometries, traffic controls, posted speed limits, etc.; the existing weekday and Sunday peak-hour traffic volumes; the existing daily traffic volumes in the area; the typical weekday and Sunday site-generated traffic volume projections for the site; the assignment of the projected traffic volumes to the area roadways; the projected short-term and long-term background and resulting total traffic volumes on the area roadways; and recommendations to mitigate the impacts of the site.

## LAND USE AND ACCESS

The site is proposed to include a 20,000 square-foot church, a 600-student private school (K-8), about 347,400 square feet of light industrial use, a convenience market and gas station with 10 fueling pumps, a one-tunnel carwash, and about 5,000 square feet of retail space. Access is proposed from several locations as shown in the site plan in Figure 2.

## ROADWAY AND TRAFFIC CONDITIONS

### Area Roadways

The major roadways in the site's vicinity are shown on Figure 1 and are described below.

- **S. 96<sup>th</sup> Street** is a north-south, two-lane arterial roadway west of the site. The intersection with W. Dillon Road has four through lanes and is signalized with auxiliary turn lanes. The

posted speed limit in the vicinity of the site is 40 mph. It is planned to be a four-lane roadway adjacent to the site by 2040.

- **W. Dillon Road** is an east-west, two-lane arterial roadway south of the site. The intersection with S. 96<sup>th</sup> Street has four through lanes and is signalized with auxiliary turn lanes. The posted speed limit in the vicinity of the site is 45 mph. It is planned to be a four-lane roadway by 2040.

### **Existing Traffic Conditions**

Figures 3a and 3b show the existing weekday and Sunday traffic volumes, existing lane geometry, and the existing traffic controls in the vicinity of the site. The Sunday peak-hour and average daily traffic volumes are from the attached traffic counts conducted by Counter Measures in January, 2020. The weekday volumes are from August, 2019 and were included in the September, 2019 *Nawatny Ridge Traffic and Mobility Study* (Nawatny TIA) by Fox, Tuttle, Hernandez.

### **2024 and 2040 Background Traffic**

Figures 4a and 4b shows the estimated 2024 weekday and Sunday background traffic and Figures 5a and 5b show the estimated 2040 weekday and Sunday background traffic. The weekday background traffic volumes are consistent with those in the September, 2019 *Nawatny Ridge Traffic and Mobility Study* (Nawatny TIA) by Fox, Tuttle, Hernandez. The growth rate assumed in the Sunday scenario is similar to the weekday scenario.

### **Existing, 2024, and 2040 Background Levels of Service**

Level of service (LOS) is a quantitative measure of the level of congestion or delay at an intersection. Level of service is indicated on a scale from “A” to “F.” LOS A is indicative of little congestion or delay and LOS F is indicative of a high level of congestion or delay. Attached are specific level of service definitions for signalized and unsignalized intersections.

The intersections in the study area were analyzed to determine the existing, 2024, and 2040 background levels of service using Synchro. Table 1 shows the level of service analysis results. The level of service reports are attached.

- **S. 96<sup>th</sup> Avenue/W. Dillon Road:** This signalized intersection currently operates at an overall LOS “C” during the weekday morning peak-hour, LOS “D” during the weekday afternoon peak-hour, and LOS “C” during the Sunday peak-hour and is expected to do so through 2040 with the recommended improvements.

### **TRIP GENERATION**

Table 2 shows the estimated average weekday, weekday morning peak-hour, weekday afternoon peak-hour, average Sunday and Sunday peak-hour trip generation potential for the proposed site based on the rates from *Trip Generation, 10<sup>th</sup> Edition, 2017* by the Institute of Transportation Engineers (ITE).

The site is projected to generate about 6,248 external vehicle-trips on the average weekday, with about half entering and half exiting during a 24-hour period. During the morning peak-hour, which generally occurs for one hour between 6:30 and 8:30 a.m., about 680 vehicles would

enter and about 438 vehicles would exit the site. During the afternoon peak-hour, which generally occurs for one hour between 4:00 and 6:00 p.m., about 286 vehicles would enter and about 464 vehicles would exit. These estimates assume a pass-by trip reduction of 56 percent for the gas station trips and 34 percent of the retail trips.

The site is projected to generate about 2,036 external vehicle-trips on the average Sunday, with about half entering and half exiting during a 24-hour period. During the Sunday peak-hour, which generally occurs for one hour between 10:30 and 11:30 a.m., about 235 vehicles would enter and about 244 vehicles would exit the site. These estimates assume a pass-by trip reduction of 56 percent for the gas station trips and 34 percent of the retail trips.

### **TRIP DISTRIBUTION**

Figure 6 shows the estimated directional distribution of the site-generated traffic volumes on the area roadways. The estimates were based on the location of the site with respect to the regional population, employment, and activity centers; and the site's proposed land use.

### **TRIP ASSIGNMENT**

Figure 7a shows the estimated weekday primary site-generated traffic volumes based on the directional distribution percentages (from Figure 6) and the weekday trip generation estimate (from Table 2).

Figure 7b shows the estimated weekday pass-by site-generated traffic volumes based on the passby trip generation estimate (from Table 2).

Figure 8a shows the estimated Sunday primary site-generated traffic volumes based on the directional distribution percentages (from Figure 6) and the Sunday trip generation estimate (from Table 2).

Figure 8b shows the estimated Sunday pass-by site-generated traffic volumes based on the passby trip generation estimate (from Table 2).

### **2024 AND 2040 TOTAL TRAFFIC**

Figure 9a shows the 2024 total weekday traffic which is the sum of the 2024 weekday background traffic volumes (from Figure 4a) and the weekday site-generated traffic volumes (from Figures 7a and 7b). Figure 9a also shows the recommended 2024 lane geometry and traffic control.

Figure 9b shows the 2024 total Sunday traffic which is the sum of the 2024 Sunday background traffic volumes (from Figure 4b) and the Sunday site-generated traffic volumes (from Figures 8a and 8b). Figure 9b also shows the recommended 2024 lane geometry and traffic control.

Figure 10a shows the 2040 total weekday traffic which is the sum of the 2040 weekday background traffic volumes (from Figure 5a) and the weekday site-generated traffic volumes (from Figures 7a and 7b). Figure 10a also shows the recommended 2040 lane geometry and traffic control.

Figure 10b shows the 2040 total Sunday traffic which is the sum of the 2040 Sunday background traffic volumes (from Figure 5b) and the Sunday site-generated traffic volumes (from Figures 8a and 8b). Figure 10b also shows the recommended 2040 lane geometry and traffic control.

## **PROJECTED LEVELS OF SERVICE**

The intersections in Figures 9a through 10b were analyzed to determine the 2024 and 2040 total traffic levels of service. Table 1 shows the level of service analysis results. The level of service reports are attached.

- **S. 96<sup>th</sup> Street/W. Dillon Road:** This signalized intersection is expected to operate at an overall LOS “D” during the weekday morning and afternoon peak-hours and LOS “C” during the Sunday peak-hour through 2040 with the recommended improvements.
- **S. 96<sup>th</sup> Street/South RIRO Site Access:** All movements at this unsignalized intersection are expected to operate at LOS “D” or better during all peak-hours through 2040.
- **S. 96<sup>th</sup> Street/North Three-Quarter Site Access:** All movements at this unsignalized intersection are expected to operate at LOS “C” or better during all peak-hours through 2040.
- **S. 96<sup>th</sup> Street/Middle Access:** This signalized intersection is expected to operate at LOS “C” or better during all peak-hours through 2040. A traffic signal warrant is likely to be met with development of the convenience market and gas station and about 100,000 square feet of light industrial space.
- **W. Dillon Road/East RIRO Site Access:** All movements at this unsignalized intersection are expected to operate at LOS “C” or better during all peak-hours through 2040.

## **CONCLUSIONS AND RECOMMENDATIONS**

### **Trip Generation**

1. The site is projected to generate about 6,248 external vehicle-trips on the average weekday, with about half entering and half exiting during a 24-hour period. During the morning peak-hour, about 680 vehicles would enter and about 438 vehicles would exit the site. During the afternoon peak-hour, about 286 vehicles would enter and about 464 vehicles would exit. These estimates assume a pass-by trip reduction of 56 percent for the gas station trips and 34 percent of the retail trips.
2. The site is projected to generate about 2,036 external vehicle-trips on the average Sunday, with about half entering and half exiting during a 24-hour period. During the Sunday - peak-hour, about 235 vehicles would enter and about 244 vehicles would exit the site. These estimates assume a pass-by trip reduction of 56 percent for the gas station trips and 34 percent of the retail trips.

### **Projected Levels of Service**

3. The signalized S. 96<sup>th</sup> Street/W. Dillon Road intersection is expected to operate at LOS “D” or better during all peak-hours through 2040 with the recommended improvements.

- 4. The signalized S. 96<sup>th</sup> Street/Middle Site Access intersection is expected to operate at LOS "C" or better during all peak-hours through 2040 with the recommended improvements. A traffic signal warrant is likely to be met with development of the convenience market and gas station and about 100,000 square feet of light industrial space.
- 5. All movements at the unsignalized intersections analyzed are expected to operate at LOS "D" or better during all peak-hours through 2040.

**Conclusions**

- 6. The impact of the site can be accommodated by the existing and planned roadway improvements with the following recommended improvements

**Recommendations**

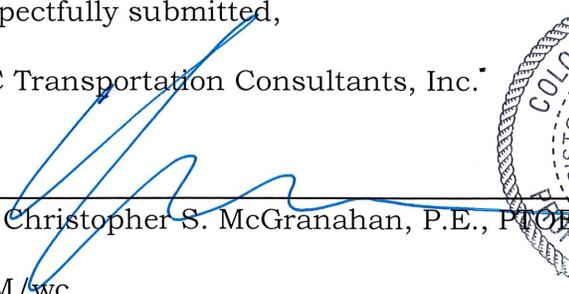
- 7. A second left-turn lane on each approach to the S. 96<sup>th</sup> Street/W. Dillon intersection is expected to be completed by the time the site reaches buildout in 2024 per the 2022 background traffic recommendations of the *Nawatny TIA* by Fox, Tuttle, Hernandez.
- 8. The recommend turn lanes at the site access intersections are shown in Figure 9a.
- 9. The westbound approach of the Full Movement Site Access to S. 96<sup>th</sup> Street should have separate left- and right-turn lanes. The westbound left-turn lane should be about 175 feet long by the time the site reaches buildout to accommodate buildout weekday afternoon peak-hour conditions.

\* \* \* \* \*

We trust our findings will assist you in gaining approval of the proposed Louisville Industrial Park development. Please contact me if you have any questions or need further assistance.

Respectfully submitted,

LSC Transportation Consultants, Inc.

By:  Christopher S. McGranahan, P.E., PTOE

CSM/wc



- Enclosure:
- Tables 1 and 2
  - Figures 1 - 10b
  - Existing Traffic Counts
  - Level of Service Definitions
  - Level of Service Printouts

**Table 1  
Intersection Levels of Service Analysis  
Louisville Industrial Park  
Louisville, CO  
LSC #180012; January, 2020**

Intersection Location	Traffic Control	Existing Traffic			2024 Background Traffic			2024 Total Traffic			2040 Background Traffic			2040 Total Traffic		
		Level of Service AM	Level of Service PM	Level of Service Sunday	Level of Service AM	Level of Service PM	Level of Service Sunday	Level of Service AM	Level of Service PM	Level of Service Sunday	Level of Service AM	Level of Service PM	Level of Service Sunday	Level of Service AM	Level of Service PM	Level of Service Sunday
<u>S. 96th Street/W. Dillon Road</u>	Signalized															
EB Left		E	D	A	D	D	E	E	D	D	D	D	D	D	D	D
EB Through		D	E	B	D	D	D	D	D	D	D	D	D	D	D	D
EB Right		A	B	A	A	B	A	A	B	A	B	C	A	B	D	A
WB Left		C	F	A	D	E	E	D	E	E	D	E	D	E	E	D
WB Through		D	D	B	D	D	D	D	D	D	D	D	D	D	D	D
WB Right		A	A	A	A	A	A	A	A	A	A	B	A	A	B	A
NB Left		C	B	C	E	E	D	E	E	D	E	E	D	E	E	D
NB Through		C	C	D	C	C	B	C	C	B	C	D	B	D	D	B
NB Right		A	A	A	A	A	A	A	A	A	A	A	A	A	A	A
SB Left		B	C	C	E	E	D	D	E	E	E	E	E	E	E	D
SB Through		C	C	D	C	C	B	D	C	B	D	C	B	D	C	B
SB Right		A	A	A	A	A	A	B	A	A	A	A	A	A	A	A
Entire Intersection Delay (sec./veh.)		28.4	40.7	24.1	32.6	35.5	30.2	36.3	36.5	30.4	34.9	42.0	31.3	39.6	45.1	31.6
Entire Intersection LOS		C	D	C	C	D	C	D	D	C	C	D	C	D	D	C
<u>S. 96th Street/South Access</u>	TWSC															
WB Right	RIRO	--	--	--	--	--	--	C	C	B	--	--	--	C	D	B
Critical Movement Delay (sec/veh)		--	--	--	--	--	--	15.4	18.9	10.5	--	--	--	18.5	31.0	11.3
<u>S. 96th Street/North Access</u>	TWSC															
WB Right	Three-Quarter	--	--	--	--	--	--	B	B	A	--	--	--	B	C	A
SB Left	Quarter	--	--	--	--	--	--	A	B	A	--	--	--	B	B	A
Critical Movement Delay (sec/veh)		--	--	--	--	--	--	11.0	13.3	9.3	--	--	--	13.1	21.0	9.6
<u>S. 96th Street/Middle Access</u>	Signalized															
WB Left		--	--	--	--	--	--	C	D	C	--	--	--	C	D	C
WB Right		--	--	--	--	--	--	A	A	A	--	--	--	A	A	A
NB Through		--	--	--	--	--	--	C	C	B	--	--	--	C	D	B
NB Right		--	--	--	--	--	--	A	A	A	--	--	--	A	A	B
SB Left		--	--	--	--	--	--	B	A	A	--	--	--	B	A	A
SB Through		--	--	--	--	--	--	A	A	A	--	--	--	A	A	A
Entire Intersection Delay (sec./veh.)		--	--	--	--	--	--	14.9	19.7	9.3	--	--	--	18.8	33.1	10.1
Entire Intersection LOS		--	--	--	--	--	--	B	B	A	--	--	--	B	C	B
<u>W. Dillon Road/East Access</u>	TWSC															
SB Right	RIRO	--	--	--	--	--	--	B	B	B	--	--	--	B	C	B
Critical Movement Delay (sec/veh)		--	--	--	--	--	--	12.6	14.1	10.1	--	--	--	13.6	15.3	11.0

**Table 2**  
**ESTIMATED TRAFFIC GENERATION**  
**Louisville Industrial Park**  
**Louisville, CO**  
**LSC #180012; January, 2020**

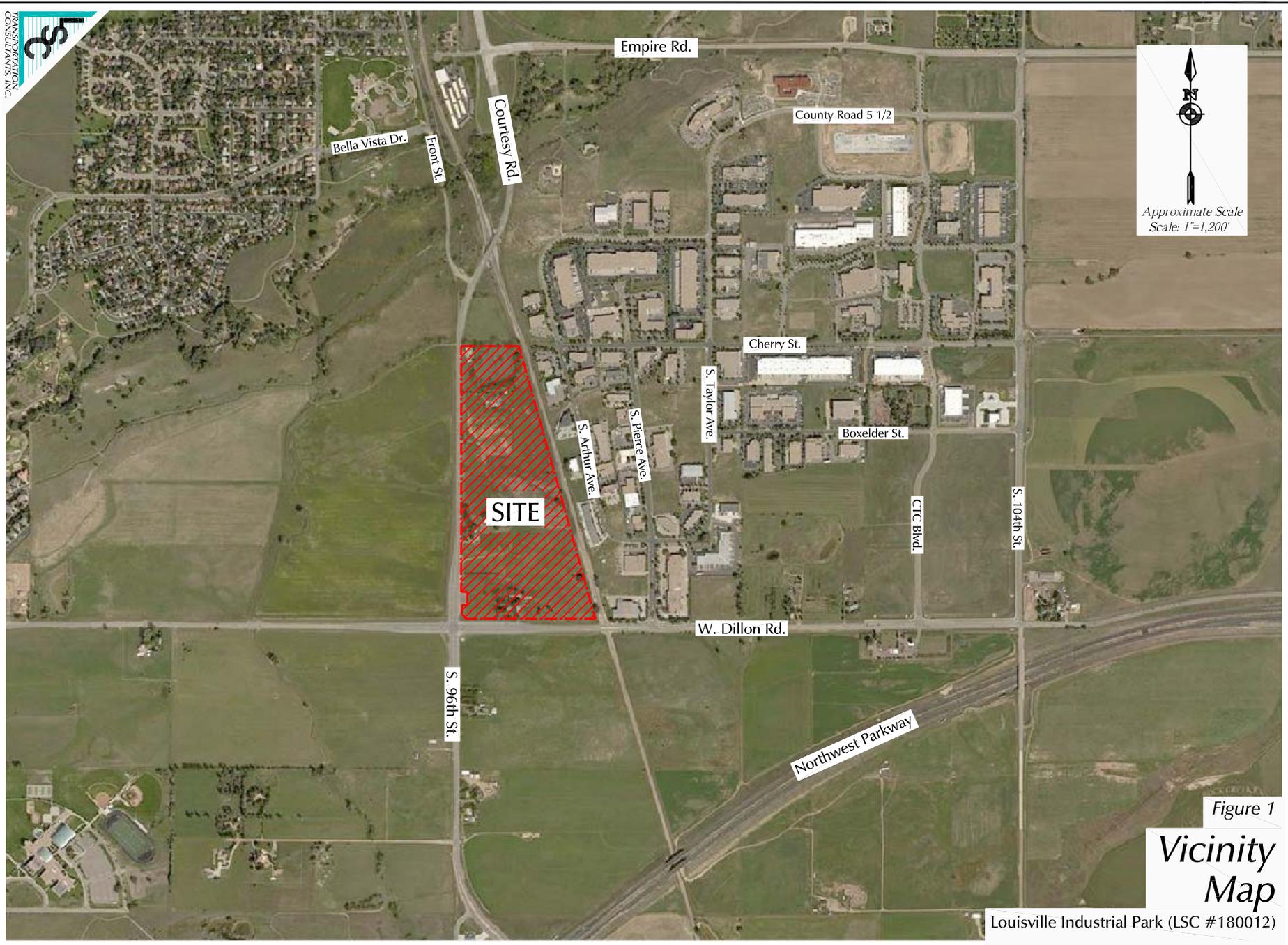
Land Use	Gross Floor Area	Generation Rates per Unit <sup>(1)</sup>					Vehicle-Trips Generated				
		Average Daily Traffic	Morning Peak-Hour		Evening Peak-Hour		Average Weekday Traffic	Morning Peak-Hour		Evening Peak-Hour	
			AM In	AM Out	PM In	PM Out		AM In	AM Out	PM In	PM Out
Church - Weekday <sup>(2)</sup>	20.00 KSF <sup>(3)</sup>	6.95	0.198	0.132	0.221	0.270	139	4	3	4	5
School <sup>(4)</sup>	600 students	4.11	0.501	0.410	0.120	0.140	2,466	300	246	72	84
Light Industrial <sup>(5)</sup>	347.40 KSF	4.96 *	0.616	0.084	0.082	0.548	1,723	214	29	28	190
Super Convenience Market/Gas Station <sup>(6)</sup>	10.00 VFP <sup>(7)</sup>	230.52 *	14.040	14.040	11.480	11.480	2,305	140	140	115	115
Car Wash <sup>(8)</sup>	1.00 Tunnel	387.5	19.375	19.375	38.750	38.750	388	19	19	39	39
Shopping Center <sup>(9)</sup>	5.00 KSF	156.80	0.583	0.357	5.684	6.157	784	3	2	28	31
<b>Weekday Gross Trip Generation Potential =</b>							<b>7,805</b>	<b>680</b>	<b>438</b>	<b>286</b>	<b>464</b>
<i>Passby Trip Reduction <sup>(10)</sup> =</i>							<i>1,557</i>	<i>80</i>	<i>80</i>	<i>75</i>	<i>75</i>
<b>Weekday Net Trip Generation Potential =</b>							<b>6,248</b>	<b>600</b>	<b>358</b>	<b>211</b>	<b>389</b>

Land Use	Gross Floor Area	Generation Rates per Unit <sup>(1)</sup>			Vehicle-Trips Generated		
		Average Sunday Traffic	Morning Peak-Hour		Average Sunday Traffic	Sunday Peak-Hour	
			AM In	AM Out		AM In	AM Out
Church - Sunday <sup>(2)</sup>	20.00 KSF <sup>(3)</sup>	27.63	4.795	5.195	553	96	104
School <sup>(4)</sup>	600 students	0	0.000	0.000	0	0	0
Light Industrial <sup>(5)</sup>	347.40 KSF	0	0.000	0.000	0	0	0
Super Convenience Market/Gas Station <sup>(6)</sup>	10.00 VFP <sup>(7)</sup>	233.34	9.304	9.304	2,333	93	93
Car Wash <sup>(8)</sup>	1.00 Tunnel	387.5	38.750	38.750	388	39	39
Shopping Center <sup>(9)</sup>	5.00 KSF	21.10	1.367	1.423	106	7	7
<b>Sunday Gross Trip Generation Potential =</b>					<b>3,379</b>	<b>235</b>	<b>244</b>
<i>Passby Trip Reduction <sup>(10)</sup> =</i>					<i>1,343</i>	<i>55</i>	<i>55</i>
<b>Sunday Net Trip Generation Potential =</b>					<b>2,036</b>	<b>180</b>	<b>189</b>

**Notes:**

- (1) Source: *Trip Generation*, Institute of Transportation Engineers, 10th Edition, 2017.
- (2) ITE Land Use No. 560 - Church
- (3) KSF = 1,000 square feet
- (4) ITE Land Use No. 534 - Private School (K-8)
- (5) ITE Land Use No. 110 - General Light Industrial; average rates
- (6) ITE Land Use No. 960 - Super Convenience Market/Gas Station - no Sunday rates available so 80% of Saturday rates were used.

- (7) VFP = Vehicle Fueling Positions
- (8) ITE Land Use No. 948 - Automatic Car Wash; AM peak assumed 1/2 of the PM rate; Daily weekday rate = 5 x PM rate; Sunday rate = weekday daily and PM rates.
- (9) ITE Land Use No. 820 - Shopping Center
- (10) 56% of gas station trips and 34% of retail trips are expected to be passby trips per the ITE Trip Generation Handbook, 3rd Edition.



North Arrow  
Approximate Scale  
Scale: 1"=1,200'

Figure 1  
**Vicinity  
Map**

Louisville Industrial Park (LSC #180012)

Approximate Scale  
Scale: NTS

Three-Quarter  
Movement Site Access

Full Movement  
Site Access  
(Future Traffic Signal)

RIRO Site Access

RIRO Site Access

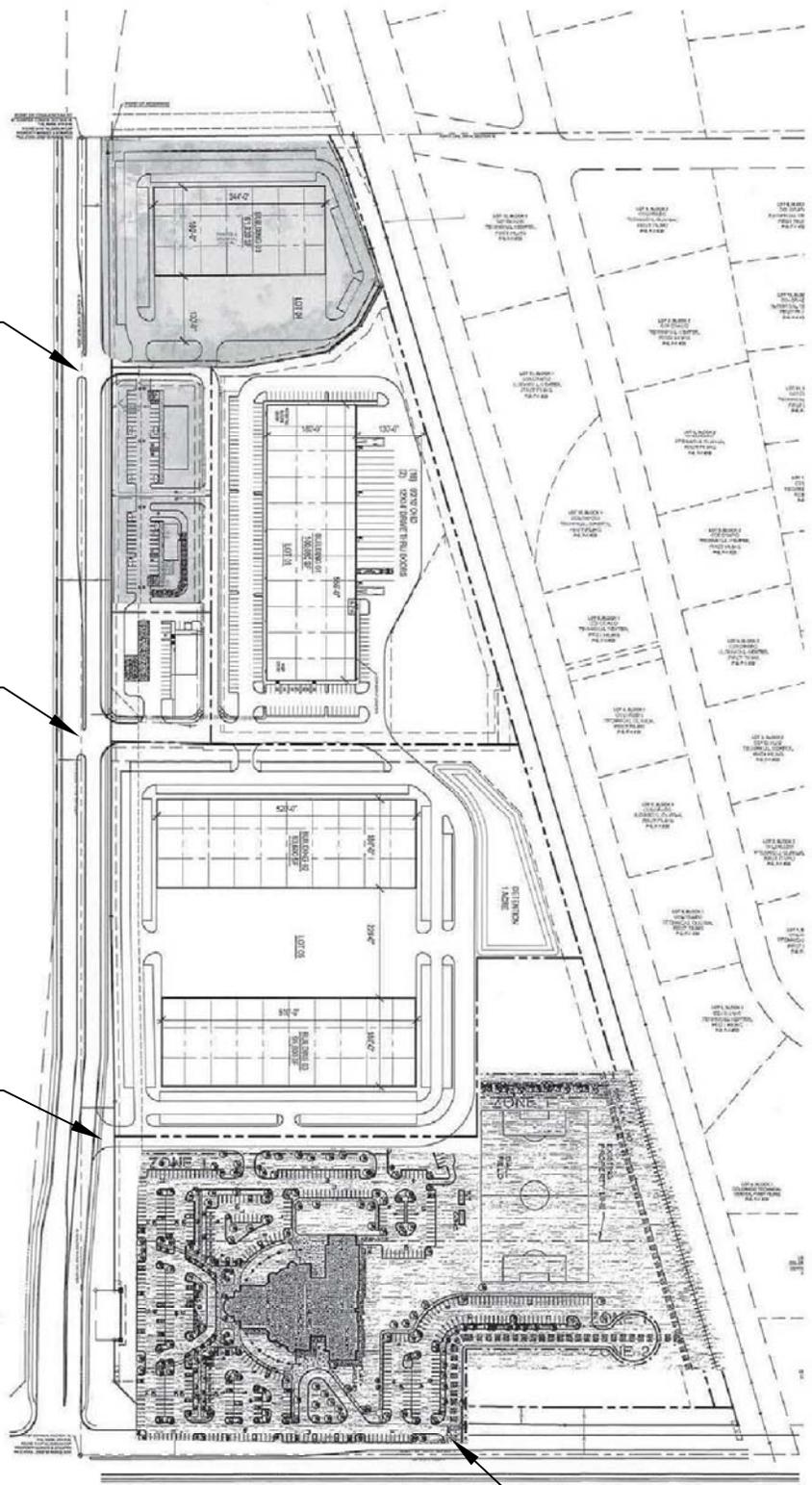


Figure 2

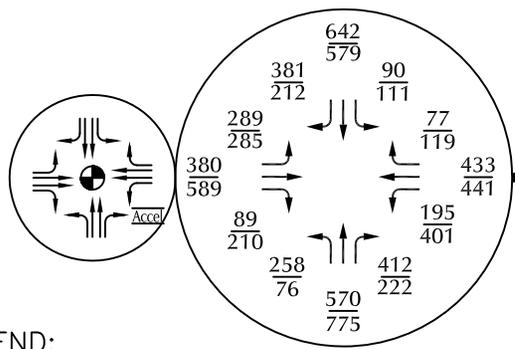
# Site Plan

Louisville Industrial Park (LSC #180012)





Approximate Scale  
Scale: 1"=1,200'



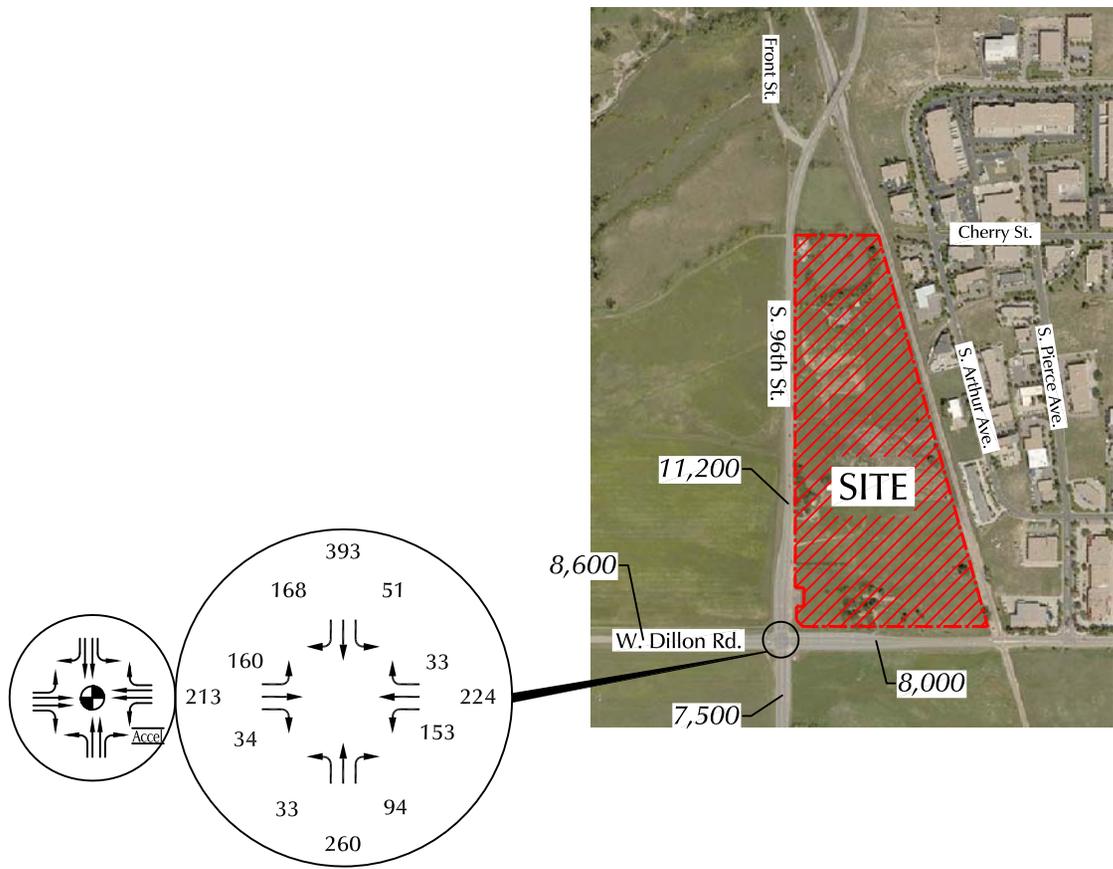
LEGEND:

- ⊥ = Stop Sign
- ⦿ = Traffic Signal
- ⓪ = Posted Speed Limit
- $\frac{26}{35}$  = Weekday AM Peak Hour Traffic / Weekday PM Peak Hour Traffic
- 500 = Weekday Daily Traffic

Figure 3a  
**Existing Weekday Traffic, Lane  
Geometry and Traffic Control**  
Louisville Industrial Park (LSC #180012)



Approximate Scale  
Scale: 1"=1,200'



LEGEND:

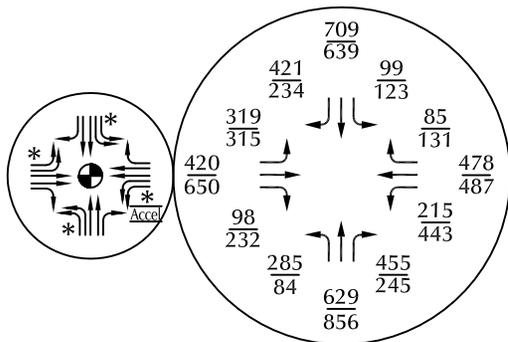
- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- 26 = Sunday AM Peak Hour Traffic
- 500 = Sunday Daily Traffic

Figure 3b  
**Existing Sunday Traffic, Lane  
Geometry and Traffic Control**  
Louisville Industrial Park (LSC #180012)



Approximate Scale  
Scale: 1"=1,200'

\* Recommendation for dual left-turn lanes on each approach was recommended in the 2022 and 2040 background traffic conditions from the September, 2019 Nawatny Ridge Traffic and Mobility Study by Fox, Tuttle Hernandez.



Note: Projections are consistent with those in the September, 2019 Nawatny Ridge Traffic and Mobility Study by Fox, Tuttle Hernandez.

LEGEND:

- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- $\frac{26}{35}$  = Weekday AM Peak Hour Traffic / Weekday PM Peak Hour Traffic
- 500 = Weekday Daily Traffic

Figure 4a

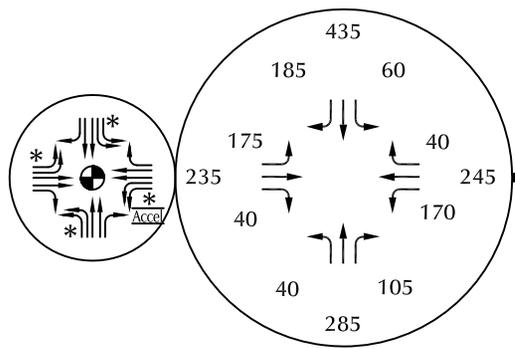
# Year 2024 Weekday Background Traffic, Lane Geometry and Traffic Control

Louisville Industrial Park (LSC #180012)



Approximate Scale  
Scale: 1"=1,200'

\* Recommendation for dual left-turn lanes on each approach was recommended in the 2022 and 2040 background traffic conditions from the September, 2019 Nawatny Ridge Traffic and Mobility Study by Fox, Tuttle Henrandez.



Note: Growth rate assumed to be similar to the weekday scenario.

LEGEND:

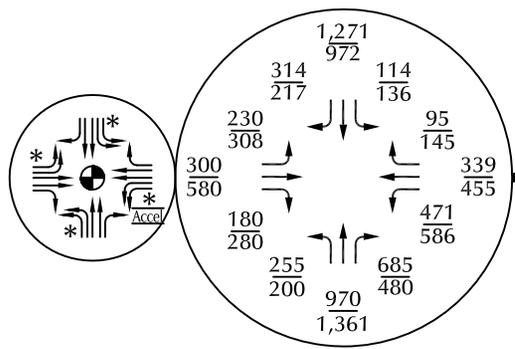
- ⊥ = Stop Sign
- ⊕ = Traffic Signal
- 26 = Sunday AM Peak Hour Traffic
- 500 = Sunday Daily Traffic

Figure 4b  
**Year 2024 Sunday Background Traffic,  
Lane Geometry and Traffic Control**  
Louisville Industrial Park (LSC #180012)



Approximate Scale  
Scale: 1"=1,200'

\* Recommendation for dual left-turn lanes on each approach was recommended in the 2022 and 2040 background traffic conditions from the September, 2019 Nawatny Ridge Traffic and Mobility Study by Fox, Tuttle Hernandez.



Note: Projections are consistent with those in the September, 2019 Nawatny Ridge Traffic and Mobility Study by Fox Tuttle Hernandez.

LEGEND:

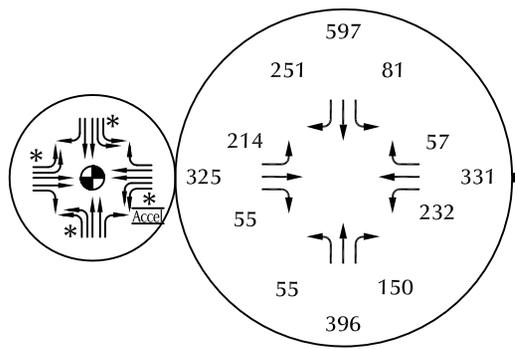
- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- $\frac{26}{35}$  = Weekday AM Peak Hour Traffic / Weekday PM Peak Hour Traffic
- 500 = Weekday Daily Traffic

Figure 5a  
**Year 2040 Weekday Background Traffic,  
Lane Geometry and Traffic Control**  
Louisville Industrial Park (LSC #180012)



Approximate Scale  
Scale: 1"=1,200'

\* Recommendation for dual left-turn lanes on each approach was recommended in the 2022 and 2040 background traffic conditions from the September, 2019 Nawatny Ridge Traffic and Mobility Study by Fox, Tuttle Henrandez.

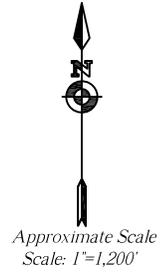
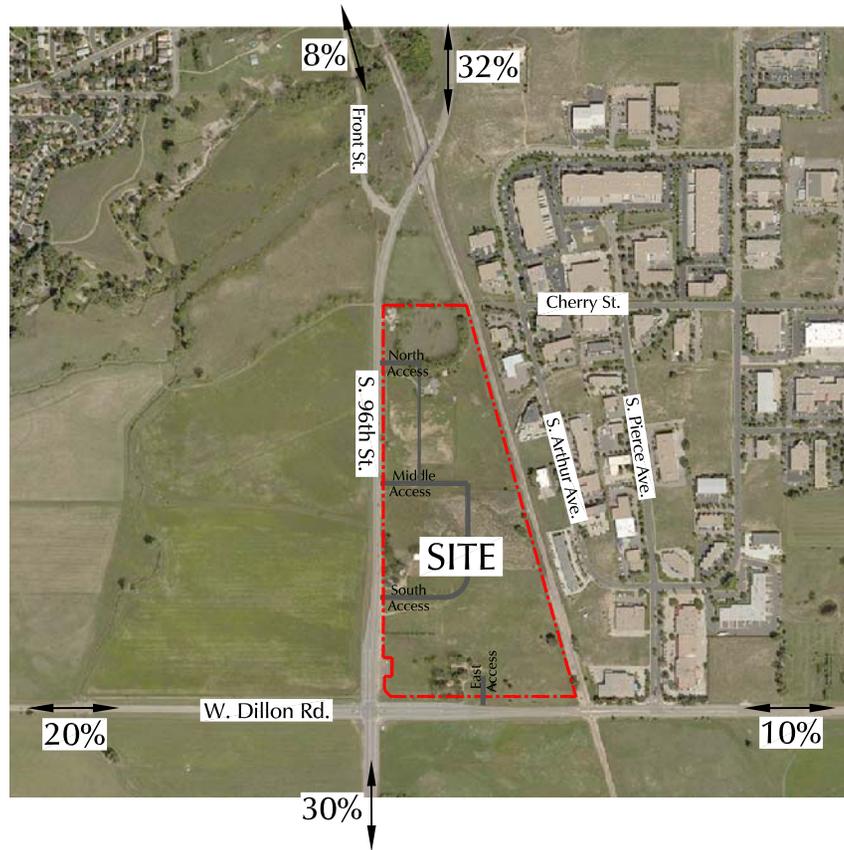


Note: Growth rates are assumed to be similar to the weekday scenario.

LEGEND:

- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- 26 = Sunday AM Peak Hour Traffic
- 500 = Sunday Daily Traffic

Figure 5b  
**Year 2040 Sunday Background Traffic,  
Lane Geometry and Traffic Control**  
Louisville Industrial Park (LSC #180012)

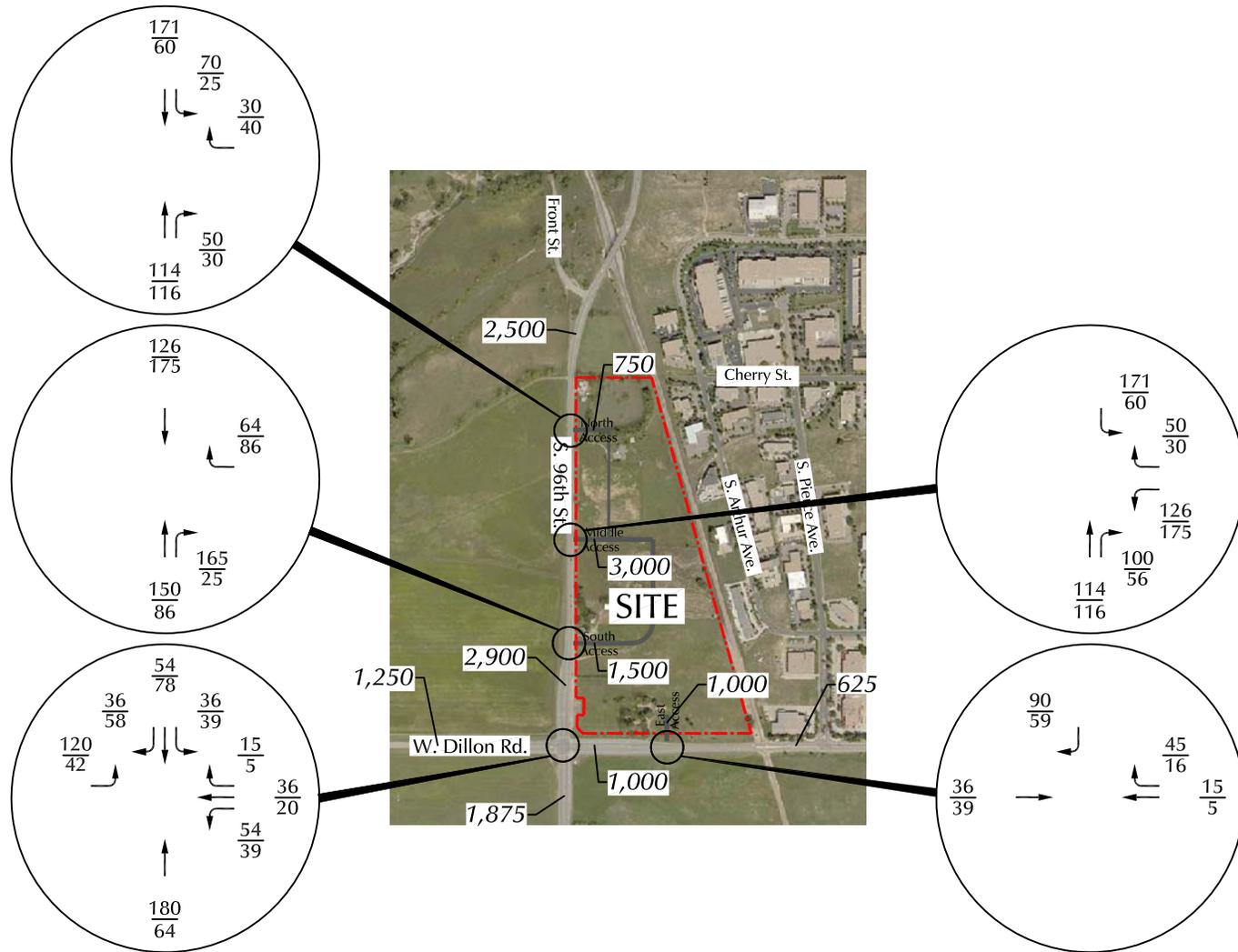


LEGEND:


  
 20% = Percent Directional Distribution

Figure 6  
*Directional Distribution  
of Site-Generated Traffic*

Louisville Industrial Park (LSC #180012)

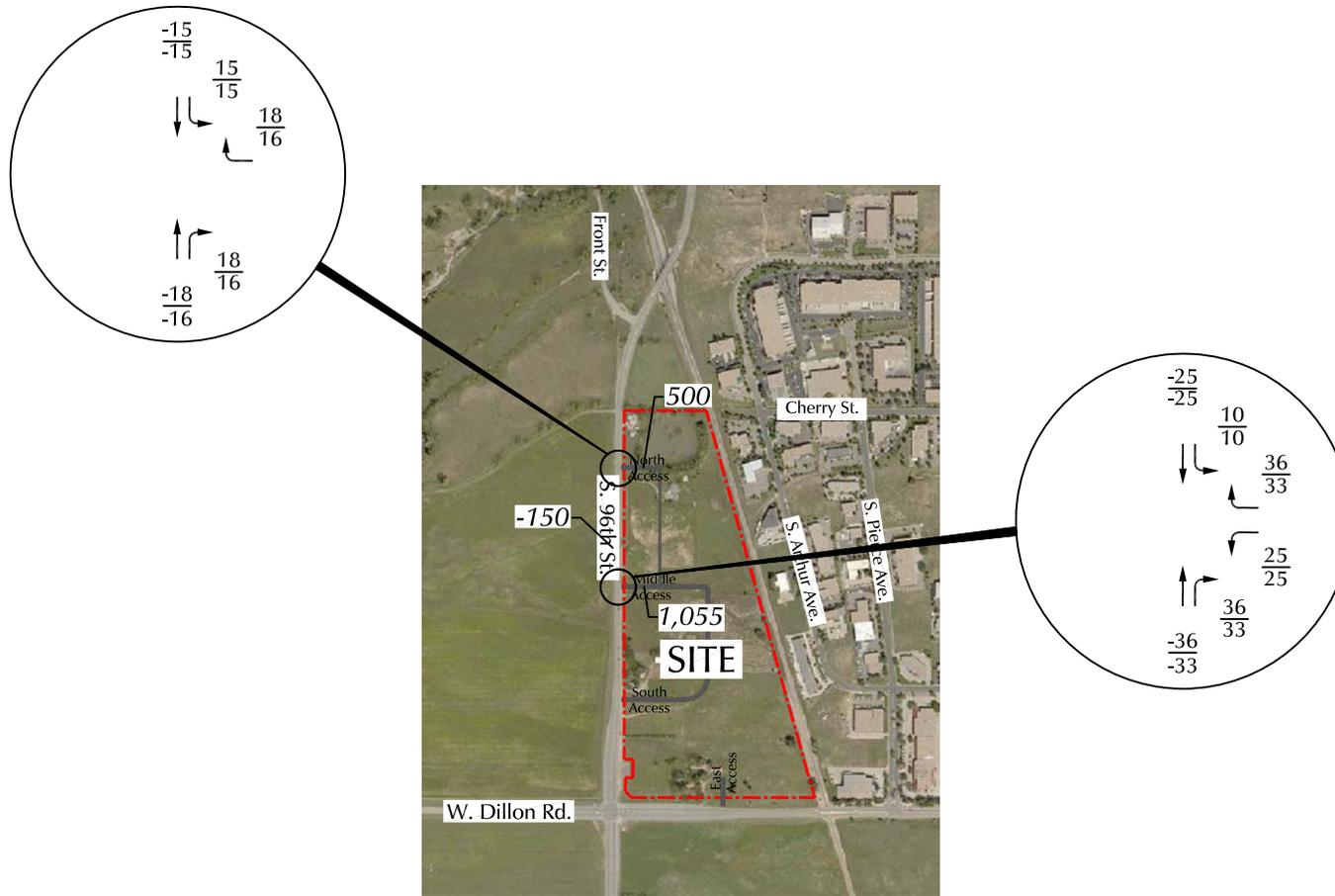


LEGEND:

- $\frac{26}{35}$  = Weekday AM Peak Hour Traffic
- $\frac{35}{26}$  = Weekday PM Peak Hour Traffic
- 500 = Weekday Daily Traffic

Figure 7a  
**Assignment of Weekday  
Primary Site-Generated Traffic**

Louisville Industrial Park (LSC #180012)

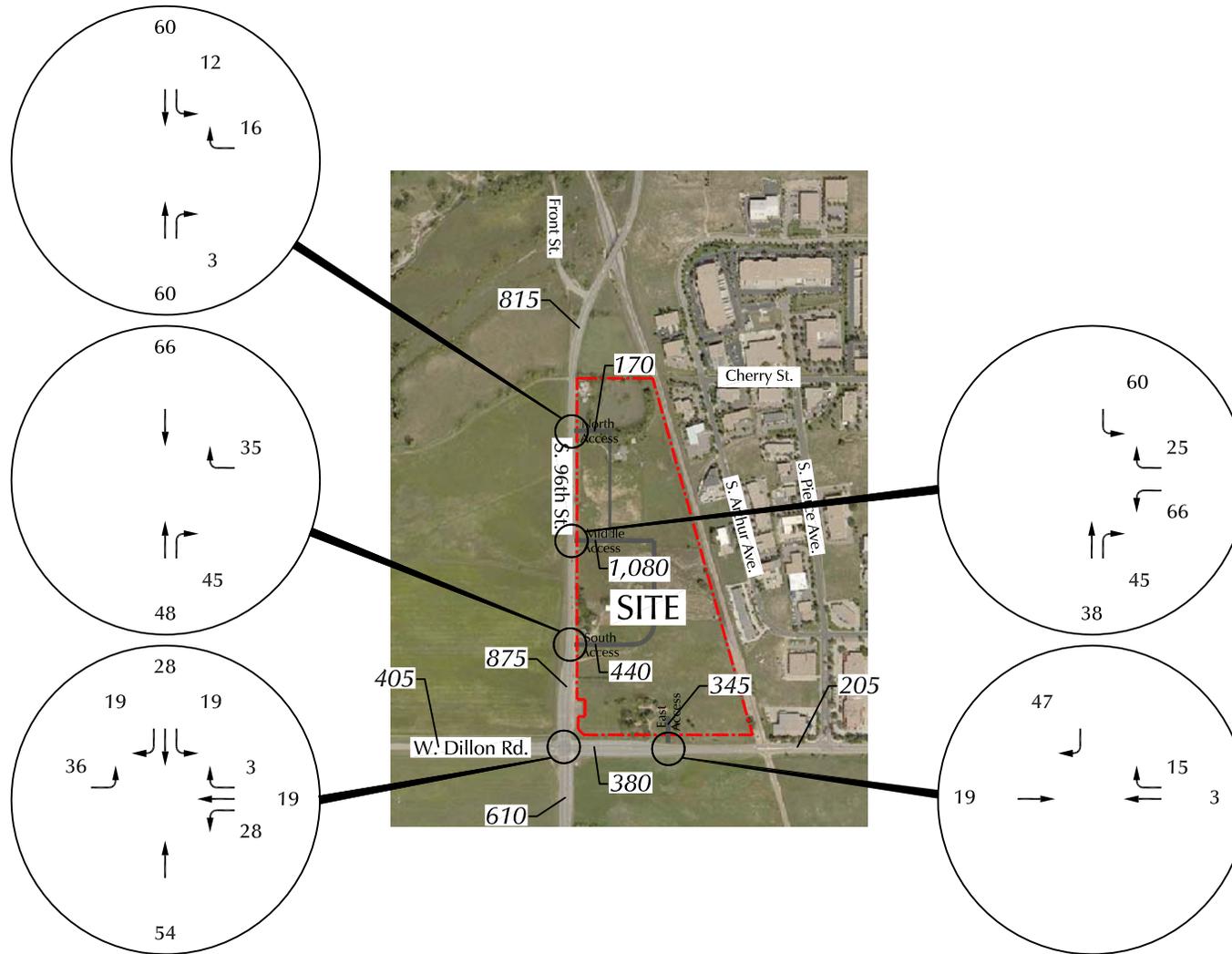


LEGEND:

- $\frac{26}{35}$  = Weekday AM Peak Hour Traffic
- $\frac{35}{26}$  = Weekday PM Peak Hour Traffic
- 500 = Weekday Daily Traffic

Figure 7b  
*Assignment of Weekday  
Passby Site-Generated Traffic*

Louisville Industrial Park (LSC #180012)

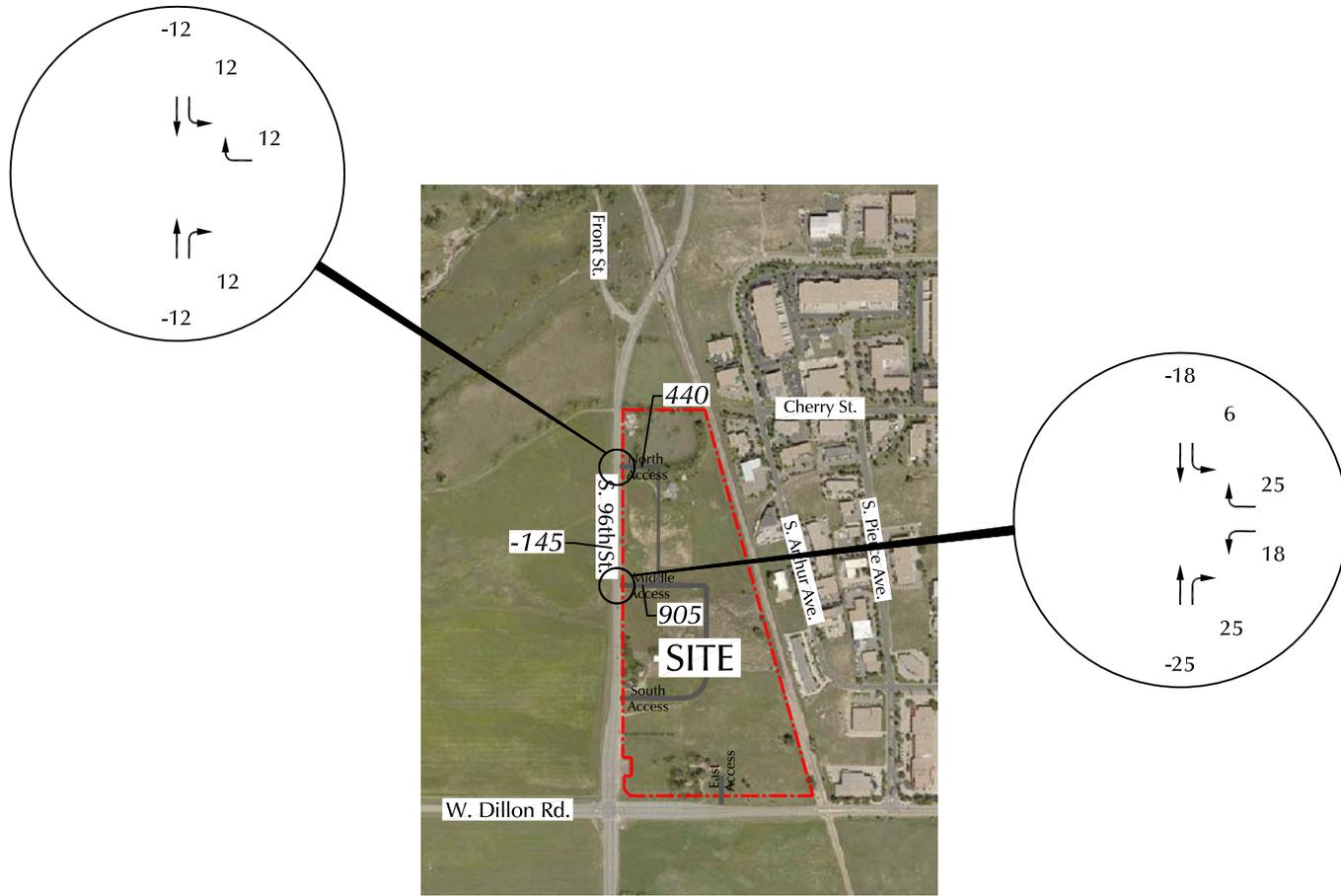


LEGEND:

- 26 = Sunday AM Peak Hour Traffic
- 500 = Sunday Daily Traffic

Figure 8a  
*Assignment of Sunday  
Primary Site-Generated Traffic*

Louisville Industrial Park (LSC #180012)



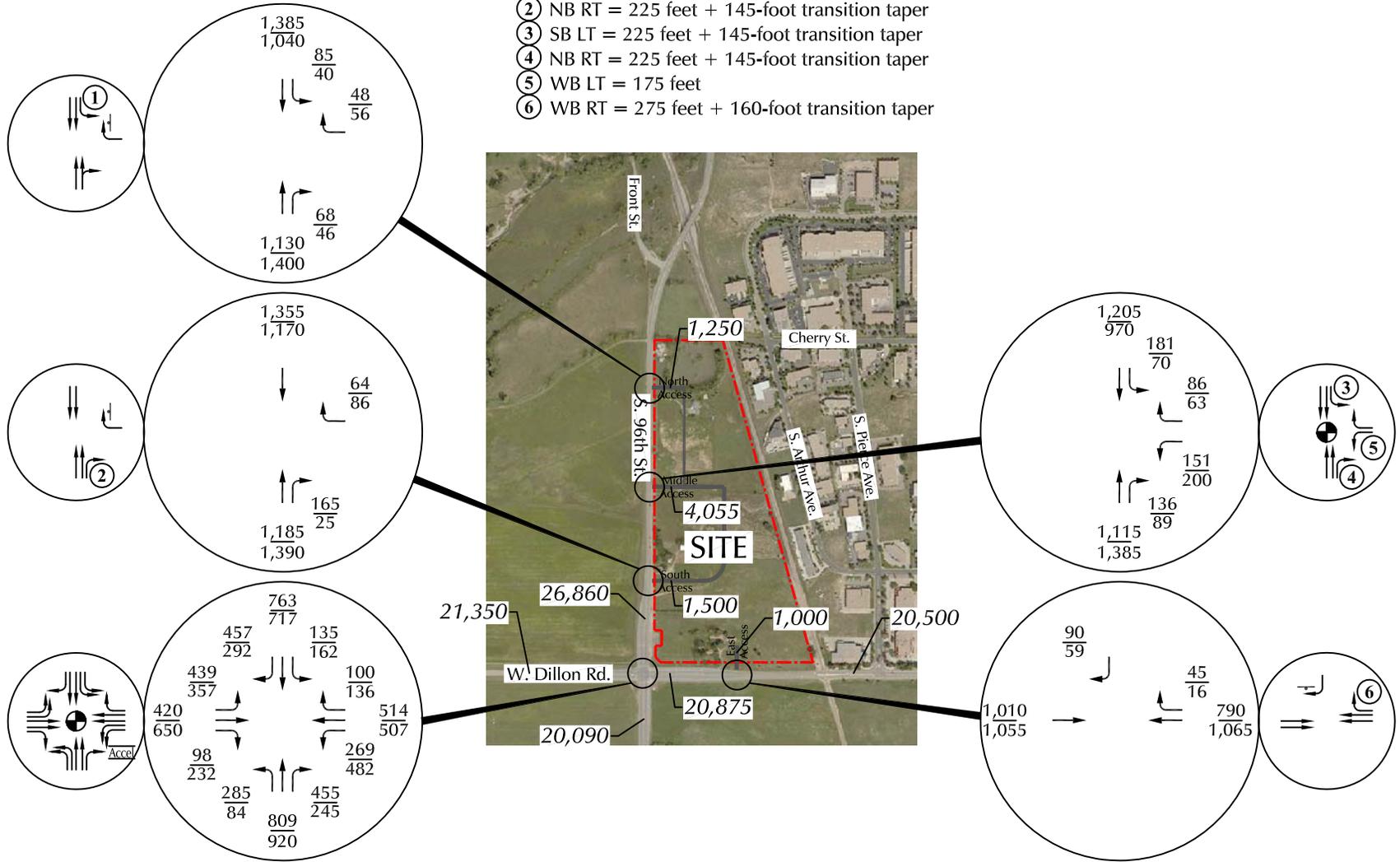
LEGEND:

- 26 = Sunday AM Peak Hour Traffic
- 500 = Sunday Daily Traffic

Figure 8b  
*Assignment of Sunday  
 Passby Site-Generated Traffic*

Louisville Industrial Park (LSC #180012)

- ① SB LT = 225 feet + 145-foot transition taper and 30:1 redirect taper
- ② NB RT = 225 feet + 145-foot transition taper
- ③ SB LT = 225 feet + 145-foot transition taper
- ④ NB RT = 225 feet + 145-foot transition taper
- ⑤ WB LT = 175 feet
- ⑥ WB RT = 275 feet + 160-foot transition taper



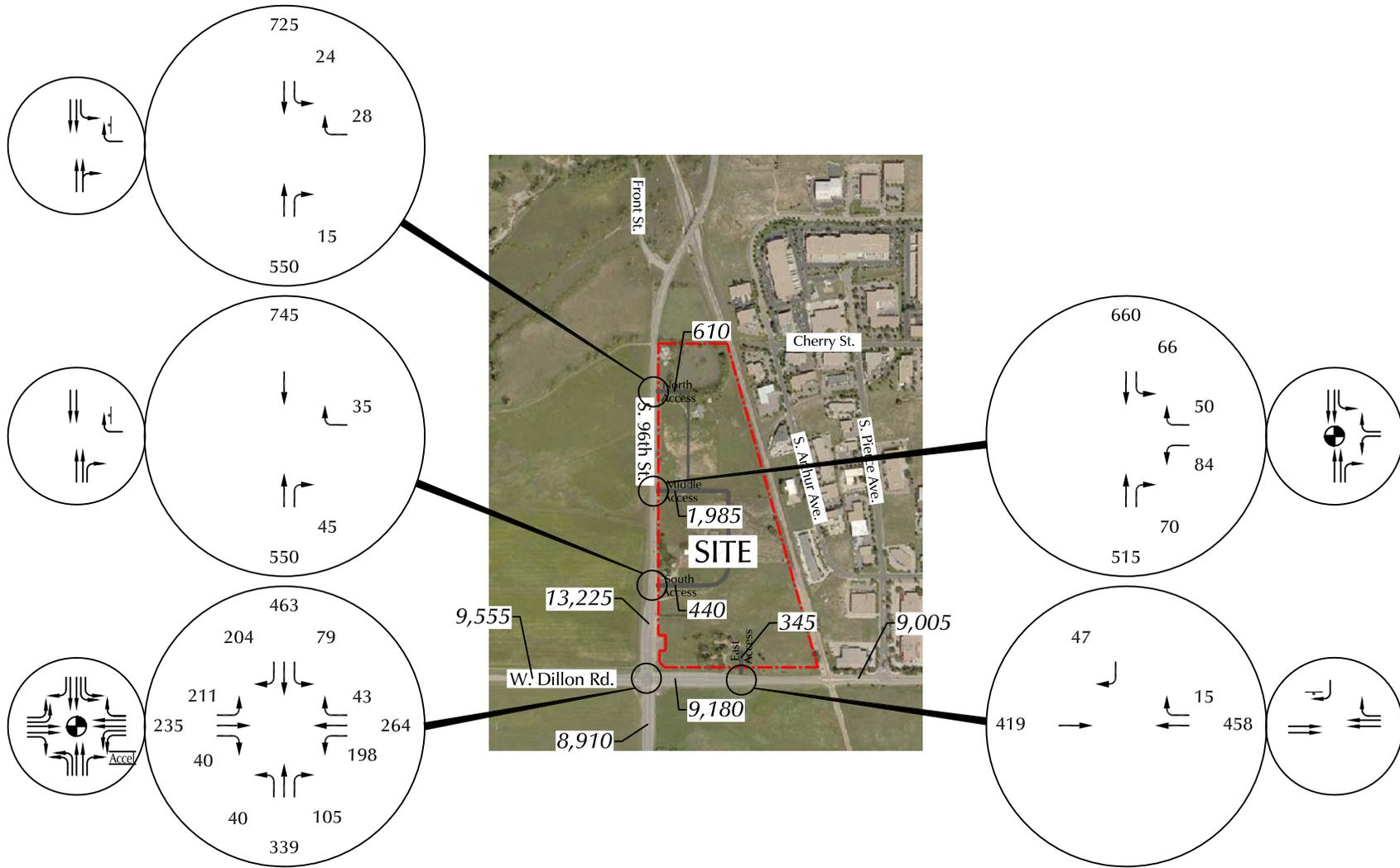
LEGEND:

- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- 26 = Weekday AM Peak Hour Traffic
- 35 = Weekday PM Peak Hour Traffic
- 500 = Weekday Daily Traffic

Figure 9a

## Year 2024 Weekday Total Traffic, Lane Geometry and Traffic Control

Louisville Industrial Park (LSC #180012)



LEGEND:

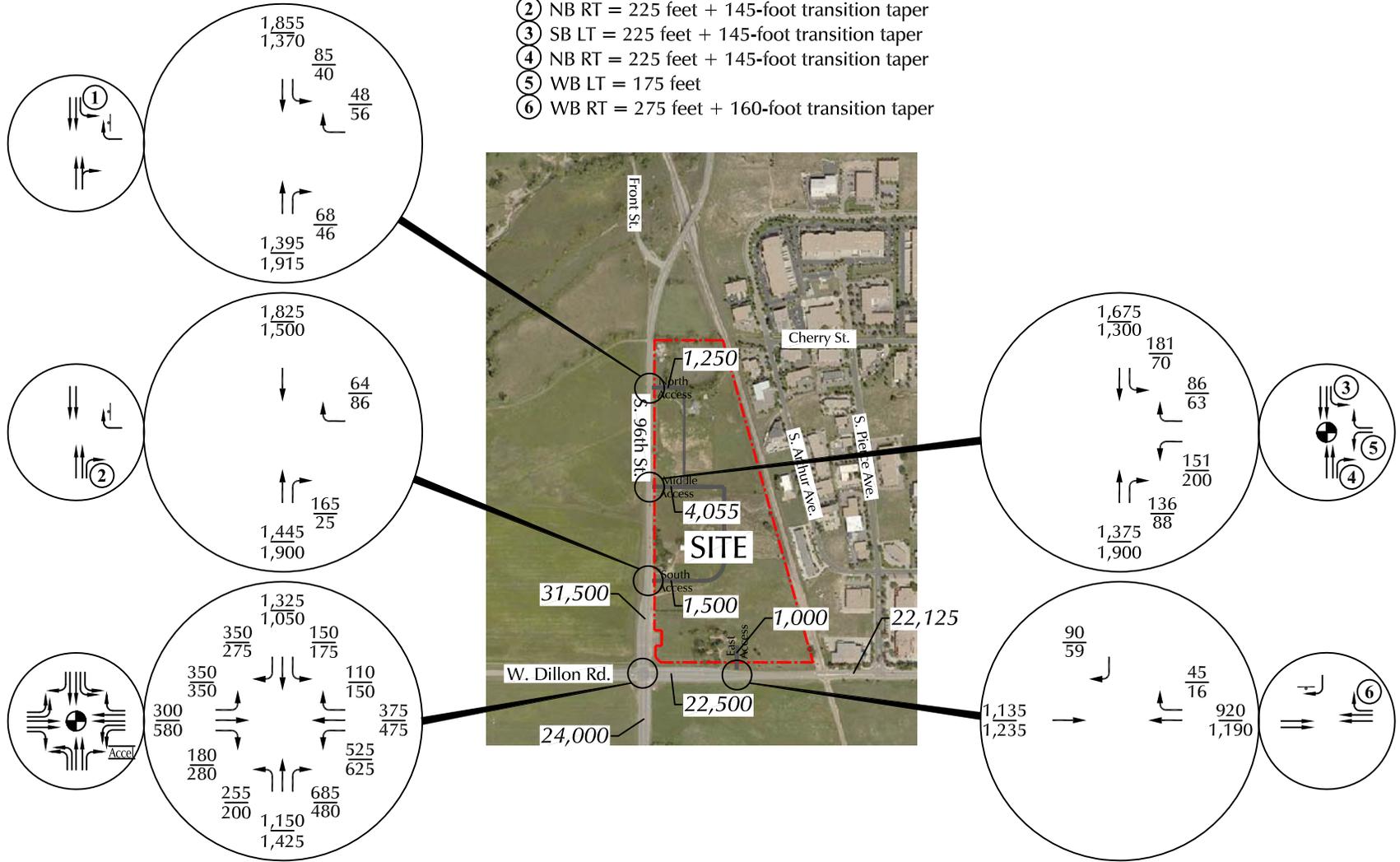
- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- 26 = Sunday AM Peak Hour Traffic
- 500 = Sunday Daily Traffic

Figure 9b

## Year 2024 Sunday Total Traffic, Lane Geometry and Traffic Control

Louisville Industrial Park (LSC #180012)

- ① SB LT = 225 feet + 145-foot transition taper and 30:1 redirect taper
- ② NB RT = 225 feet + 145-foot transition taper
- ③ SB LT = 225 feet + 145-foot transition taper
- ④ NB RT = 225 feet + 145-foot transition taper
- ⑤ WB LT = 175 feet
- ⑥ WB RT = 275 feet + 160-foot transition taper



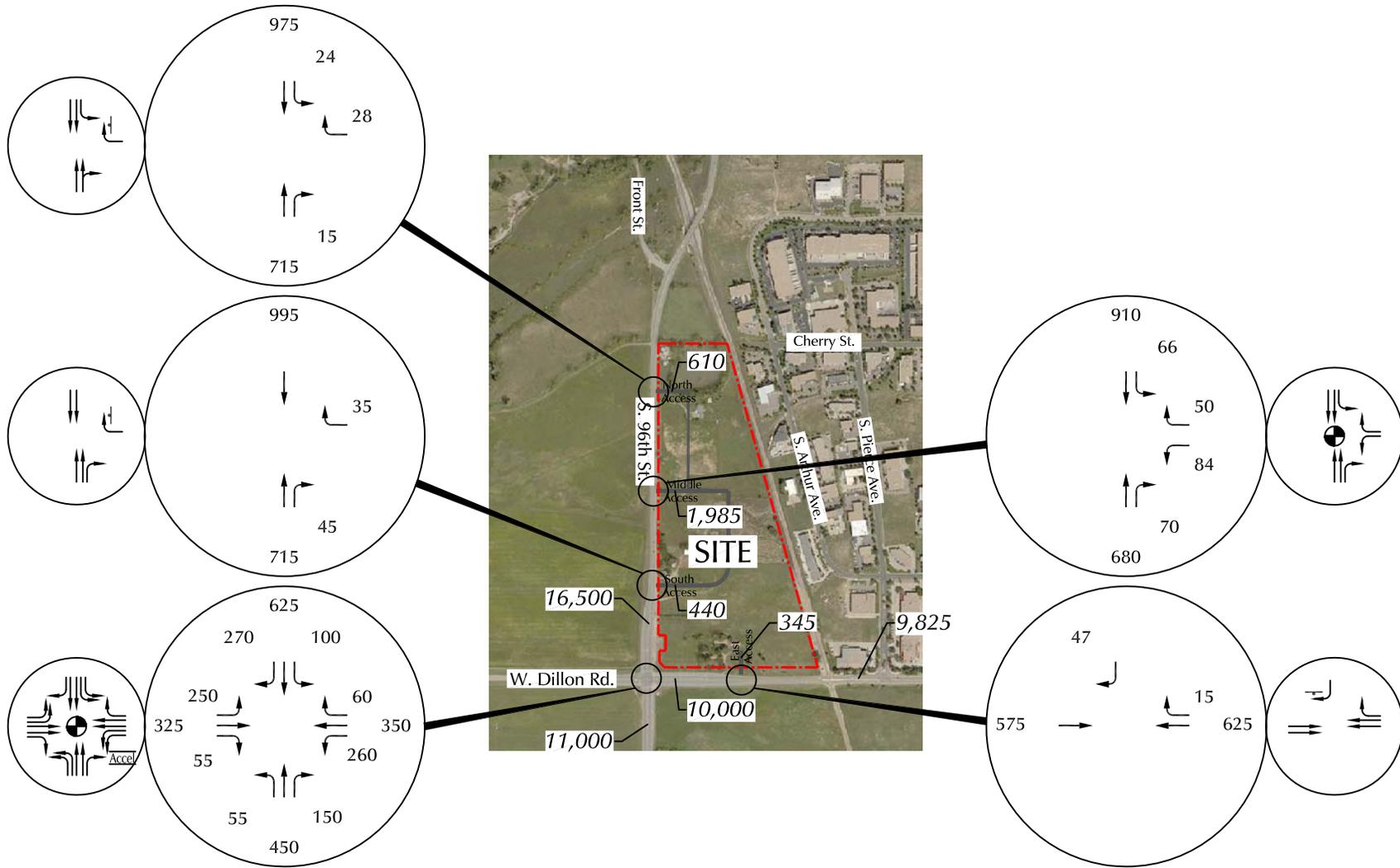
LEGEND:

- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- $\frac{26}{35}$  =  $\frac{\text{Weekday AM Peak Hour Traffic}}{\text{Weekday PM Peak Hour Traffic}}$
- 500 = Weekday Daily Traffic

Figure 10a

## Year 2040 Weekday Total Traffic, Lane Geometry and Traffic Control

Louisville Industrial Park (LSC #180012)



LEGEND:

- ⊥ = Stop Sign
- ⊙ = Traffic Signal
- 26 = Sunday AM Peak Hour Traffic
- 500 = Sunday Daily Traffic

Figure 10b  
**Year 2040 Sunday Total Traffic,  
 Lane Geometry and Traffic Control**  
 Louisville Industrial Park (LSC #180012)

**COUNTER MEASURES INC.**

1889 YORK STREET  
DENVER.COLORADO  
303-333-7409

N/S STREET: 96TH ST  
E/W STREET: DILLON RD  
CITY: LOUISVILLE  
COUNTY: BOULDER

File Name : 96THDILL 1-12-20  
Site Code : 00000016  
Start Date : 1/12/2020  
Page No : 1

Groups Printed- VEHICLES

Start Time	96TH ST Southbound				DILLON RD Westbound				96TH ST Northbound				DILLON RD Eastbound				Int. Total
	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	Left	Thru	Right	Peds	
Factor	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	
10:00 AM	8	71	42	0	17	40	5	0	11	61	19	0	27	28	7	0	336
10:15 AM	7	77	45	0	28	68	3	0	10	38	14	0	26	41	8	0	365
10:30 AM	13	81	42	0	28	54	2	0	6	61	8	2	48	49	6	0	400
10:45 AM	11	78	43	1	25	51	10	0	10	67	19	0	53	27	10	0	405
Total	39	307	172	1	98	213	20	0	37	227	60	2	154	145	31	0	1506
11:00 AM	9	98	45	0	25	33	10	0	10	61	13	0	52	36	6	0	398
11:15 AM	13	76	41	3	37	48	3	3	10	55	19	0	50	34	8	0	400
11:30 AM	13	75	34	0	42	46	5	0	4	57	14	0	41	47	15	0	393
11:45 AM	12	103	35	0	31	59	7	0	9	67	17	0	42	36	13	0	431
Total	47	352	155	3	135	186	25	3	33	240	63	0	185	153	42	0	1622
12:00 PM	13	86	41	2	42	57	10	0	8	58	25	0	42	52	8	0	444
12:15 PM	10	112	64	1	44	68	8	0	8	62	24	0	46	48	9	0	504
12:30 PM	20	109	36	1	28	38	2	0	11	67	23	0	23	42	10	0	410
12:45 PM	8	86	27	0	39	61	13	0	6	73	22	0	49	71	7	0	462
Total	51	393	168	4	153	224	33	0	33	260	94	0	160	213	34	0	1820
Grand Total	137	1052	495	8	386	623	78	3	103	727	217	2	499	511	107	0	4948
Apprch %	8.1	62.2	29.3	0.5	35.4	57.2	7.2	0.3	9.8	69.3	20.7	0.2	44.7	45.7	9.6	0.0	
Total %	2.8	21.3	10.0	0.2	7.8	12.6	1.6	0.1	2.1	14.7	4.4	0.0	10.1	10.3	2.2	0.0	

**COUNTER MEASURES INC.**

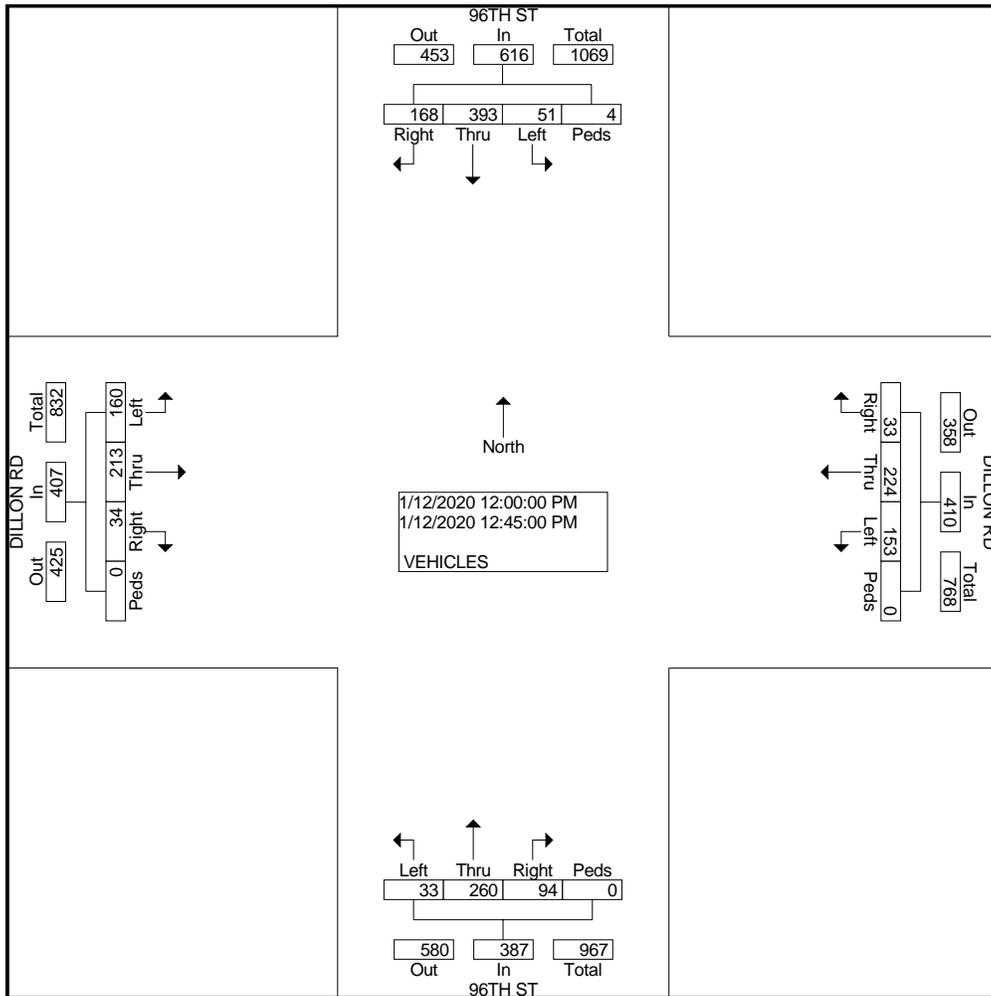
1889 YORK STREET  
DENVER.COLORADO  
303-333-7409

N/S STREET: 96TH ST  
E/W STREET: DILLON RD  
CITY: LOUISVILLE  
COUNTY: BOULDER

File Name : 96THDILL 1-12-20  
Site Code : 00000016  
Start Date : 1/12/2020  
Page No : 2

Start Time	96TH ST Southbound					DILLON RD Westbound					96TH ST Northbound					DILLON RD Eastbound					Int. Total
	Left	Thru	Rig ht	Ped s	App. Total	Left	Thru	Rig ht	Ped s	App. Total	Left	Thru	Rig ht	Ped s	App. Total	Left	Thru	Rig ht	Ped s	App. Total	
12:00 PM	51	393	168	4	616	153	224	33	0	410	33	260	94	0	387	160	213	34	0	407	1820
Percent	8.3	63.8	27.3	0.6		37.3	54.6	8.0	0.0		8.5	67.2	24.3	0.0		39.3	52.3	8.4	0.0		
12:15 PM	10	112	64	1	187	44	68	8	0	120	8	62	24	0	94	46	48	9	0	103	504
Peak Factor																					0.903
High Int.	12:15 PM					12:15 PM					12:30 PM					12:45 PM					
Volume	10	112	64	1	187	44	68	8	0	120	11	67	23	0	101	49	71	7	0	127	
Peak Factor					0.82					0.85					0.95					0.80	1

Peak Hour From 10:00 AM to 12:45 PM - Peak 1 of 1



## LEVEL OF SERVICE DEFINITIONS

From *Highway Capacity Manual*, Transportation Research Board, 2016, 6th Edition

### SIGNALIZED INTERSECTION LEVEL OF SERVICE (LOS)

<u>LOS</u>	<u>Average Vehicle Delay</u> sec/vehicle	<u>Operational Characteristics</u>
<b>A</b>	<10 seconds	Describes operations with low control delay, up to 10 sec/veh. This LOS occurs when progression is extremely favorable and most vehicles arrive during the green phase. Many vehicles do not stop at all. Short cycle lengths may tend to contribute to low delay values.
<b>B</b>	10 to 20 seconds	Describes operations with control delay greater than 10 seconds and up to 20 sec/veh. This level generally occurs with good progression, short cycle lengths, or both. More vehicles stop than with LOS A, causing higher levels of delay.
<b>C</b>	20 to 35 seconds	Describes operations with control delay greater than 20 and up to 35 sec/veh. These higher delays may result from only fair progression, longer cycle length, or both. Individual cycle failures may begin to appear at this level. Cycle failure occurs when a given green phase does not serve queued vehicles, and overflows occur. The number of vehicles stopping is significant at this level, though many still pass through the intersection without stopping.
<b>D</b>	35 to 55 seconds	Describes operations with control delay greater than 35 and up to 55 sec/veh. At LOS D, the influence of congestion becomes more noticeable. Longer delays may result from some combination of unfavorable progression, long cycle lengths, and high v/c ratios. Many vehicles stop, and the proportion of vehicles not stopping declines. Individual cycle failures are noticeable.
<b>E</b>	55 to 80 seconds	Describes operations with control delay greater than 55 and up to 80 sec/veh. These high delay values generally indicate poor progression, long cycle lengths, and high v/c ratios. Individual cycle failures are frequent.
<b>F</b>	>80 seconds	Describes operations with control delay in excess of 80 sec/veh. This level, considered unacceptable to most drivers, often occurs with over-saturation, that is, when arrival flow rates exceed the capacity of lane groups. It may also occur at high v/c ratios with many individual cycle failures. Poor progression and long cycle lengths may also contribute significantly to high delay levels.

## LEVEL OF SERVICE DEFINITIONS

From *Highway Capacity Manual*, Transportation Research Board, 2016, 6th Edition

### UNSIGNALIZED INTERSECTION LEVEL OF SERVICE (LOS)

Applicable to Two-Way Stop Control, All-Way Stop Control, and Roundabouts

LOS	Average Vehicle Control Delay	<u>Operational Characteristics</u>
A	<10 seconds	Normally, vehicles on the stop-controlled approach only have to wait up to 10 seconds before being able to clear the intersection. Left-turning vehicles on the uncontrolled street do not have to wait to make their turn.
B	10 to 15 seconds	Vehicles on the stop-controlled approach will experience delays before being able to clear the intersection. <u>The delay could be up to 15 seconds.</u> Left-turning vehicles on the uncontrolled street may have to wait to make their turn.
C	15 to 25 seconds	Vehicles on the stop-controlled approach can expect delays in the range of 15 to 25 seconds before clearing the intersection. Motorists may begin to take chances due to the long delays, thereby posing a safety risk to through traffic. <u>Left-turning vehicles on the uncontrolled street will now be required to wait to make their turn causing a queue to be created in the turn lane.</u>
D	25 to 35 seconds	<u>This is the point at which a traffic signal may be warranted for this intersection.</u> The delays for the stop-controlled intersection are not considered to be excessive. The length of the queue may begin to block other public and private access points.
E	35 to 50 seconds	The delays for all critical traffic movements are considered to be unacceptable. The length of the queues for the stop-controlled approaches as well as the left-turn movements are extremely long. <u>There is a high probability that this intersection will meet traffic signal warrants.</u> The ability to install a traffic signal is affected by the location of other existing traffic signals. Consideration may be given to restricting the accesses by eliminating the left-turn movements from and to the stop-controlled approach.
F	>50 seconds	The delay for the critical traffic movements are probably in excess of 100 seconds. The length of the queues are extremely long. Motorists are selecting alternative routes due to the long delays. <u>The only remedy for these long delays is installing a traffic signal or restricting the accesses.</u> The potential for accidents at this intersection are extremely high due to motorist taking more risky chances. If the median permits, motorists begin making two-stage left-turns.



Timings  
3: S. 96th Street & W. Dillon Road

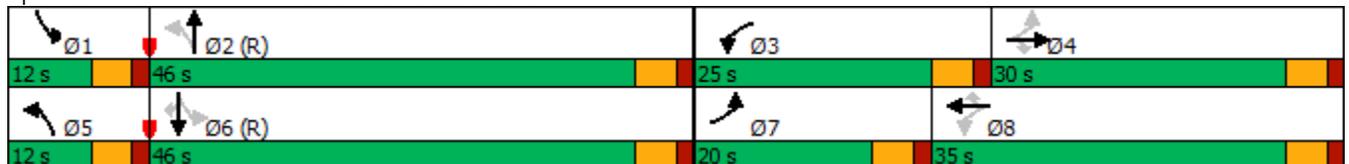
Existing  
PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	285	589	210	401	441	119	76	775	222	111	579	212
Future Volume (vph)	285	589	210	401	441	119	76	775	222	111	579	212
Turn Type	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Free	pm+pt	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4		4	8		8	2		Free	6		6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.5	21.0	21.0	17.0	21.0	21.0	10.5	21.0		10.5	21.0	21.0
Total Split (s)	20.0	30.0	30.0	25.0	35.0	35.0	12.0	46.0		12.0	46.0	46.0
Total Split (%)	17.7%	26.5%	26.5%	22.1%	31.0%	31.0%	10.6%	40.7%		10.6%	40.7%	40.7%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	0.0	1.0	0.0	0.0	1.0	0.0	0.0	1.0		0.0	1.0	0.0
Total Lost Time (s)	5.0	6.0	5.0	5.0	6.0	5.0	5.0	6.0		5.0	6.0	5.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effct Green (s)	38.9	23.1	24.1	49.1	28.4	29.4	48.7	40.8	113.0	49.9	43.2	44.2
Actuated g/C Ratio	0.34	0.20	0.21	0.43	0.25	0.26	0.43	0.36	1.00	0.44	0.38	0.39
v/c Ratio	0.77	0.88	0.46	1.15	0.54	0.25	0.25	0.66	0.15	0.50	0.47	0.30
Control Delay	37.9	58.6	10.1	125.4	39.1	6.9	18.7	33.5	0.2	24.8	28.6	4.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	37.9	58.6	10.1	125.4	39.1	6.9	18.7	33.5	0.2	24.8	28.6	4.3
LOS	D	E	B	F	D	A	B	C	A	C	C	A
Approach Delay		43.8			71.1			25.6			22.4	
Approach LOS		D			E			C			C	

Intersection Summary

Cycle Length: 113  
 Actuated Cycle Length: 113  
 Offset: 7 (6%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green  
 Natural Cycle: 90  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 1.15  
 Intersection Signal Delay: 40.7  
 Intersection LOS: D  
 Intersection Capacity Utilization 84.4%  
 ICU Level of Service E  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road



Timings  
3: S. 96th Street & W. Dillon Road

Existing  
Sunday Peak

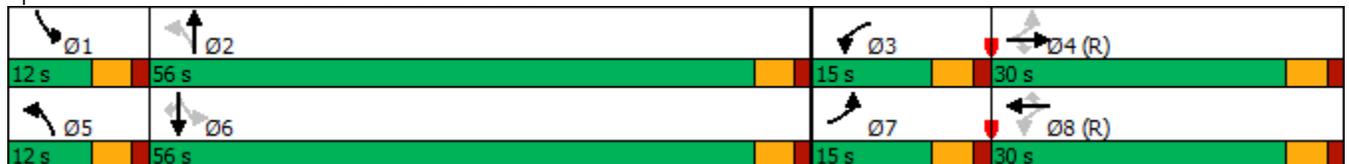
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	160	213	34	153	224	33	33	260	94	51	393	168
Future Volume (vph)	160	213	34	153	224	33	33	260	94	51	393	168
Turn Type	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Free	pm+pt	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases	4		4	8		8	2		Free	6		6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.5	21.0	21.0	10.5	21.0	21.0	10.5	21.0		10.5	21.0	21.0
Total Split (s)	15.0	30.0	30.0	15.0	30.0	30.0	12.0	56.0		12.0	56.0	56.0
Total Split (%)	13.3%	26.5%	26.5%	13.3%	26.5%	26.5%	10.6%	49.6%		10.6%	49.6%	49.6%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min		None	Min	Min
Act Effect Green (s)	69.5	59.4	59.4	69.0	59.1	59.1	24.6	19.1	113.0	25.7	21.5	21.5
Actuated g/C Ratio	0.62	0.53	0.53	0.61	0.52	0.52	0.22	0.17	1.00	0.23	0.19	0.19
v/c Ratio	0.23	0.12	0.04	0.22	0.13	0.04	0.18	0.47	0.06	0.22	0.63	0.41
Control Delay	9.9	16.2	0.1	9.8	16.4	0.1	30.2	44.2	0.1	31.0	46.2	8.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	9.9	16.2	0.1	9.8	16.4	0.1	30.2	44.2	0.1	31.0	46.2	8.2
LOS	A	B	A	A	B	A	C	D	A	C	D	A
Approach Delay		12.4			12.6			32.3			34.5	
Approach LOS		B			B			C			C	

Intersection Summary

Cycle Length: 113  
 Actuated Cycle Length: 113  
 Offset: 62 (55%), Referenced to phase 4:EBTL and 8:WBTL, Start of Green  
 Natural Cycle: 65  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.63  
 Intersection Signal Delay: 24.1  
 Intersection Capacity Utilization 46.8%  
 Analysis Period (min) 15

Intersection LOS: C  
 ICU Level of Service A

Splits and Phases: 3: S. 96th Street & W. Dillon Road



Timings  
3: S. 96th Street & W. Dillon Road

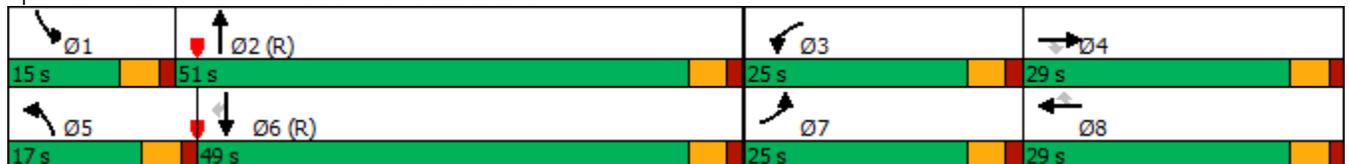
2024 Background  
AM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	319	420	98	215	478	85	285	629	455	99	709	421
Future Volume (vph)	319	420	98	215	478	85	285	629	455	99	709	421
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	29.0	29.0	25.0	29.0	29.0	17.0	51.0		15.0	49.0	49.0
Total Split (%)	20.8%	24.2%	24.2%	20.8%	24.2%	24.2%	14.2%	42.5%		12.5%	40.8%	40.8%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0		-1.0	-1.0	-1.0
Total Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	4.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effect Green (s)	17.7	26.4	26.4	14.3	22.9	22.9	14.3	53.6	120.0	9.7	49.1	49.1
Actuated g/C Ratio	0.15	0.22	0.22	0.12	0.19	0.19	0.12	0.45	1.00	0.08	0.41	0.41
v/c Ratio	0.67	0.58	0.24	0.56	0.75	0.23	0.74	0.42	0.31	0.38	0.52	0.50
Control Delay	54.9	44.7	7.6	54.9	53.3	6.2	63.1	24.9	0.5	56.0	29.2	5.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	54.9	44.7	7.6	54.9	53.3	6.2	63.1	24.9	0.5	56.0	29.2	5.2
LOS	D	D	A	D	D	A	E	C	A	E	C	A
Approach Delay		44.3			48.6			24.7			23.1	
Approach LOS		D			D			C			C	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.75  
 Intersection Signal Delay: 32.6  
 Intersection LOS: C  
 Intersection Capacity Utilization 63.4%  
 ICU Level of Service B  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road





Timings  
3: S. 96th Street & W. Dillon Road

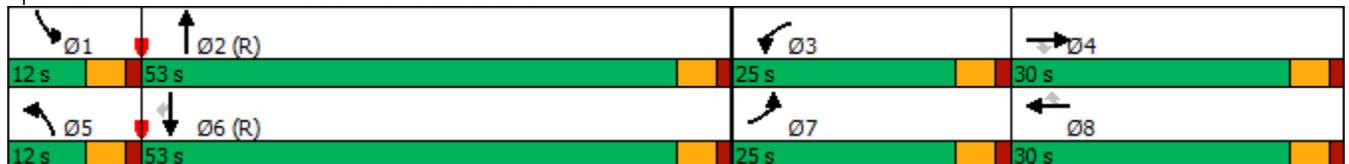
2024 Background  
Sunday Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	175	235	40	170	245	40	40	285	105	60	435	185
Future Volume (vph)	175	235	40	170	245	40	40	285	105	60	435	185
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	30.0	30.0	25.0	30.0	30.0	12.0	53.0		12.0	53.0	53.0
Total Split (%)	20.8%	25.0%	25.0%	20.8%	25.0%	25.0%	10.0%	44.2%		10.0%	44.2%	44.2%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0		-1.0	-1.0	-1.0
Total Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	4.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effect Green (s)	13.0	15.5	15.5	12.8	15.3	15.3	8.0	69.2	120.0	8.7	69.9	69.9
Actuated g/C Ratio	0.11	0.13	0.13	0.11	0.13	0.13	0.07	0.58	1.00	0.07	0.58	0.58
v/c Ratio	0.51	0.56	0.14	0.51	0.59	0.14	0.19	0.15	0.07	0.26	0.23	0.20
Control Delay	55.1	53.5	1.0	55.2	54.5	1.0	54.5	13.7	0.1	54.8	13.9	2.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	55.1	53.5	1.0	55.2	54.5	1.0	54.5	13.7	0.1	54.8	13.9	2.7
LOS	E	D	A	E	D	A	D	B	A	D	B	A
Approach Delay		49.5			50.1			14.1			14.5	
Approach LOS		D			D			B			B	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.59  
 Intersection Signal Delay: 30.2  
 Intersection LOS: C  
 Intersection Capacity Utilization 41.3%  
 ICU Level of Service A  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road



Timings  
3: S. 96th Street & W. Dillon Road

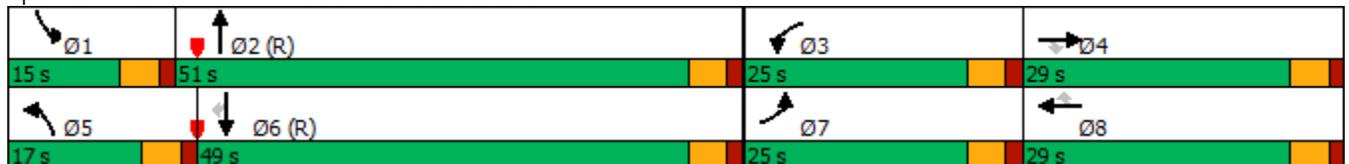
2024 Total  
AM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	439	420	98	269	514	100	285	809	455	135	763	457
Future Volume (vph)	439	420	98	269	514	100	285	809	455	135	763	457
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	29.0	29.0	25.0	29.0	29.0	17.0	51.0		15.0	49.0	49.0
Total Split (%)	20.8%	24.2%	24.2%	20.8%	24.2%	24.2%	14.2%	42.5%		12.5%	40.8%	40.8%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0		-1.0	-1.0	-1.0
Total Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	4.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effect Green (s)	20.2	27.4	27.4	16.2	23.5	23.5	13.5	50.0	120.0	10.3	46.9	46.9
Actuated g/C Ratio	0.17	0.23	0.23	0.14	0.20	0.20	0.11	0.42	1.00	0.09	0.39	0.39
v/c Ratio	0.81	0.55	0.23	0.62	0.79	0.27	0.79	0.58	0.31	0.49	0.59	0.56
Control Delay	60.0	43.9	7.9	54.6	54.8	8.7	67.2	29.7	0.5	54.0	35.8	11.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	60.0	43.9	7.9	54.6	54.8	8.7	67.2	29.7	0.5	54.0	35.8	11.2
LOS	E	D	A	D	D	A	E	C	A	D	D	B
Approach Delay		47.6			49.5			28.0			29.3	
Approach LOS		D			D			C			C	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 65  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.81  
 Intersection Signal Delay: 36.3  
 Intersection LOS: D  
 Intersection Capacity Utilization 69.3%  
 ICU Level of Service C  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road



HCM 6th TWSC  
6: S. 96th Street & South Access

2024 Total  
AM Peak

Intersection						
Int Delay, s/veh	0.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕	↗		↕
Traffic Vol, veh/h	0	64	1185	165	0	1355
Future Vol, veh/h	0	64	1185	165	0	1355
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	0	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	70	1288	179	0	1473

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	644	0	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-
Pot Cap-1 Maneuver	0	416	-	-	0
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	-	416	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	15.4	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	416
HCM Lane V/C Ratio	-	-	0.167
HCM Control Delay (s)	-	-	15.4
HCM Lane LOS	-	-	C
HCM 95th %tile Q(veh)	-	-	0.6

HCM 6th TWSC  
8: S. 96th Street & North Access

2024 Total  
AM Peak

Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕		↖	↕
Traffic Vol, veh/h	0	48	1130	68	85	1385
Future Vol, veh/h	0	48	1130	68	85	1385
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	0	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	52	1228	74	92	1505

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	651	0	0	1302
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	4.14
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	2.22
Pot Cap-1 Maneuver	0	*651	-	-	841
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %		1	-	-	1
Mov Cap-1 Maneuver	-	*651	-	-	841
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11	0	0.6
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	651	841
HCM Lane V/C Ratio	-	-	0.08	0.11
HCM Control Delay (s)	-	-	11	9.8
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.3	0.4

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

Timings  
12: S. 96th Street & Middle Access

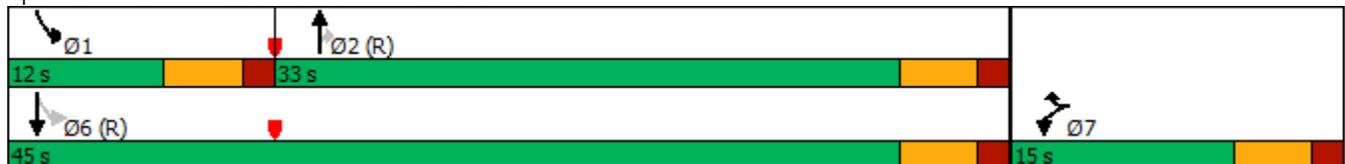
2024 Total  
AM Peak

Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	151	86	1115	136	181	1205
Future Volume (vph)	151	86	1115	136	181	1205
Turn Type	Prot	Prot	NA	Perm	pm+pt	NA
Protected Phases	7	7	2		1	6
Permitted Phases				2	6	
Detector Phase	7	7	2	2	1	6
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	23.0	23.0	23.0	23.0	10.0	23.0
Total Split (s)	15.0	15.0	33.0	33.0	12.0	45.0
Total Split (%)	25.0%	25.0%	55.0%	55.0%	20.0%	75.0%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag			Lag	Lag	Lead	
Lead-Lag Optimize?			Yes	Yes	Yes	
Recall Mode	None	None	C-Max	C-Max	None	C-Max
Act Effect Green (s)	9.1	9.1	31.4	31.4	43.2	44.2
Actuated g/C Ratio	0.15	0.15	0.52	0.52	0.72	0.74
v/c Ratio	0.61	0.29	0.65	0.16	0.58	0.50
Control Delay	33.8	8.6	24.1	7.4	13.6	5.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	33.8	8.6	24.1	7.4	13.6	5.4
LOS	C	A	C	A	B	A
Approach Delay	24.7		22.3			6.5
Approach LOS	C		C			A

Intersection Summary

Cycle Length: 60  
 Actuated Cycle Length: 60  
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBTL, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.65  
 Intersection Signal Delay: 14.9  
 Intersection Capacity Utilization 61.7%  
 Analysis Period (min) 15  
 Intersection LOS: B  
 ICU Level of Service B

Splits and Phases: 12: S. 96th Street & Middle Access



HCM 6th TWSC  
14: W. Dillon Road & East Access

2024 Total  
AM Peak

Intersection						
Int Delay, s/veh	0.6					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑	↑		↑
Traffic Vol, veh/h	0	1010	790	45	0	90
Future Vol, veh/h	0	1010	790	45	0	90
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	0	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1098	859	49	0	98

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	430
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	0	-	-	-	573
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	573
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	12.6
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	573
HCM Lane V/C Ratio	-	-	-	0.171
HCM Control Delay (s)	-	-	-	12.6
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.6



HCM 6th TWSC  
6: S. 96th Street & South Access

2024 Total  
PM Peak

Intersection						
Int Delay, s/veh	0.6					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↗↗	↗		↗↗
Traffic Vol, veh/h	0	86	1390	25	0	1170
Future Vol, veh/h	0	86	1390	25	0	1170
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	0	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	93	1511	27	0	1272

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	756	0	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-
Pot Cap-1 Maneuver	0	351	-	-	0
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	-	351	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	18.9	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	351
HCM Lane V/C Ratio	-	-	0.266
HCM Control Delay (s)	-	-	18.9
HCM Lane LOS	-	-	C
HCM 95th %tile Q(veh)	-	-	1.1

HCM 6th TWSC  
8: S. 96th Street & North Access

2024 Total  
PM Peak

Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕		↖	↕
Traffic Vol, veh/h	0	56	1400	46	40	1040
Future Vol, veh/h	0	56	1400	46	40	1040
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	0	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	61	1522	50	43	1130

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	786	0	0	1572
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	4.14
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	2.22
Pot Cap-1 Maneuver	0	*495	-	-	*740
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %		1	-	-	1
Mov Cap-1 Maneuver	-	*495	-	-	*740
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	13.3	0	0.4
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	495	*740
HCM Lane V/C Ratio	-	-	0.123	0.059
HCM Control Delay (s)	-	-	13.3	10.2
HCM Lane LOS	-	-	B	B
HCM 95th %tile Q(veh)	-	-	0.4	0.2

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

Timings  
12: S. 96th Street & Middle Access

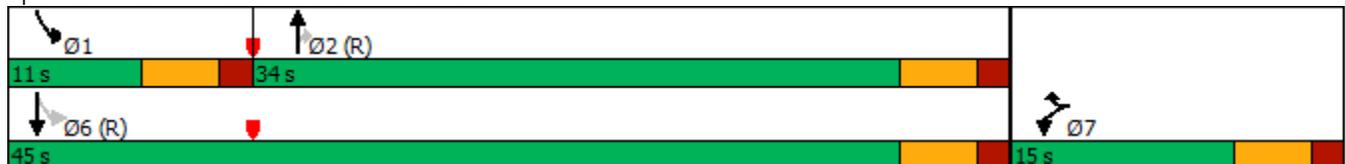
2024 Total  
PM Peak

Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	200	63	1385	89	70	970
Future Volume (vph)	200	63	1385	89	70	970
Turn Type	Prot	Prot	NA	Perm	pm+pt	NA
Protected Phases	7	7	2		1	6
Permitted Phases				2	6	
Detector Phase	7	7	2	2	1	6
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	23.0	23.0	23.0	23.0	10.0	23.0
Total Split (s)	15.0	15.0	34.0	34.0	11.0	45.0
Total Split (%)	25.0%	25.0%	56.7%	56.7%	18.3%	75.0%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag			Lag	Lag	Lead	
Lead-Lag Optimize?			Yes	Yes	Yes	
Recall Mode	None	None	C-Max	C-Max	None	C-Max
Act Effect Green (s)	9.7	9.7	33.7	33.7	40.3	40.3
Actuated g/C Ratio	0.16	0.16	0.56	0.56	0.67	0.67
v/c Ratio	0.76	0.22	0.76	0.10	0.26	0.44
Control Delay	43.8	8.7	28.2	6.3	5.8	5.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	43.8	8.7	28.2	6.3	5.8	5.4
LOS	D	A	C	A	A	A
Approach Delay	35.5		26.9			5.4
Approach LOS	D		C			A

Intersection Summary

Cycle Length: 60  
 Actuated Cycle Length: 60  
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBTL, Start of Green  
 Natural Cycle: 70  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.76  
 Intersection Signal Delay: 19.7  
 Intersection Capacity Utilization 66.0%  
 Analysis Period (min) 15  
 Intersection LOS: B  
 ICU Level of Service C

Splits and Phases: 12: S. 96th Street & Middle Access



HCM 6th TWSC  
14: W. Dillon Road & East Access

2024 Total  
PM Peak

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑	↑		↑
Traffic Vol, veh/h	0	1055	1065	16	0	59
Future Vol, veh/h	0	1055	1065	16	0	59
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	0	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1147	1158	17	0	64

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	579
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	0	-	-	-	458
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	458
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	14.1
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	458
HCM Lane V/C Ratio	-	-	-	0.14
HCM Control Delay (s)	-	-	-	14.1
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.5

Timings  
3: S. 96th Street & W. Dillon Road

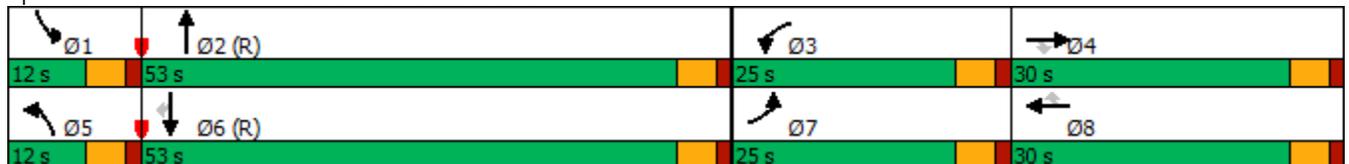
2024 Total  
Sunday Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	211	235	40	198	264	43	40	339	105	79	463	204
Future Volume (vph)	211	235	40	198	264	43	40	339	105	79	463	204
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	30.0	30.0	25.0	30.0	30.0	12.0	53.0		12.0	53.0	53.0
Total Split (%)	20.8%	25.0%	25.0%	20.8%	25.0%	25.0%	10.0%	44.2%		10.0%	44.2%	44.2%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0		-1.0	-1.0	-1.0
Total Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	4.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effect Green (s)	14.3	16.5	16.5	13.8	16.0	16.0	8.0	66.5	120.0	9.4	67.8	67.8
Actuated g/C Ratio	0.12	0.14	0.14	0.12	0.13	0.13	0.07	0.55	1.00	0.08	0.56	0.56
v/c Ratio	0.56	0.52	0.14	0.54	0.61	0.15	0.19	0.19	0.07	0.32	0.25	0.22
Control Delay	54.9	51.7	0.9	55.0	54.4	1.1	54.5	15.4	0.1	56.1	13.3	1.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	54.9	51.7	0.9	55.0	54.4	1.1	54.5	15.4	0.1	56.1	13.3	1.8
LOS	D	D	A	E	D	A	D	B	A	E	B	A
Approach Delay		48.9			50.1			15.3			14.7	
Approach LOS		D			D			B			B	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.61  
 Intersection Signal Delay: 30.4  
 Intersection LOS: C  
 Intersection Capacity Utilization 43.6%  
 ICU Level of Service A  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road



HCM 6th TWSC  
6: S. 96th Street & South Access

2024 Total  
Sunday Peak

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕	↗		↕
Traffic Vol, veh/h	0	35	550	45	0	745
Future Vol, veh/h	0	35	550	45	0	745
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	0	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	38	598	49	0	810

Major/Minor	Minor1	Major1	Major2
Conflicting Flow All	-	299	0
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	6.94	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	3.32	-
Pot Cap-1 Maneuver	0	697	-
Stage 1	0	-	-
Stage 2	0	-	-
Platoon blocked, %			
Mov Cap-1 Maneuver	-	697	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.5	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	697
HCM Lane V/C Ratio	-	-	0.055
HCM Control Delay (s)	-	-	10.5
HCM Lane LOS	-	-	B
HCM 95th %tile Q(veh)	-	-	0.2

HCM 6th TWSC  
8: S. 96th Street & North Access

2024 Total  
Sunday Peak

Intersection						
Int Delay, s/veh	0.4					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕		↖	↕
Traffic Vol, veh/h	0	28	550	15	24	725
Future Vol, veh/h	0	28	550	15	24	725
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	0	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	30	598	16	26	788

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	307	0	0	614
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	4.14
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	2.22
Pot Cap-1 Maneuver	0	*860	-	-	*1286
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %		1	-	-	1
Mov Cap-1 Maneuver	-	*860	-	-	*1286
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.3	0	0.3
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	860	* 1286
HCM Lane V/C Ratio	-	-	0.035	0.02
HCM Control Delay (s)	-	-	9.3	7.9
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0.1

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

Timings  
12: S. 96th Street & Middle Access

2024 Total  
Sunday Peak

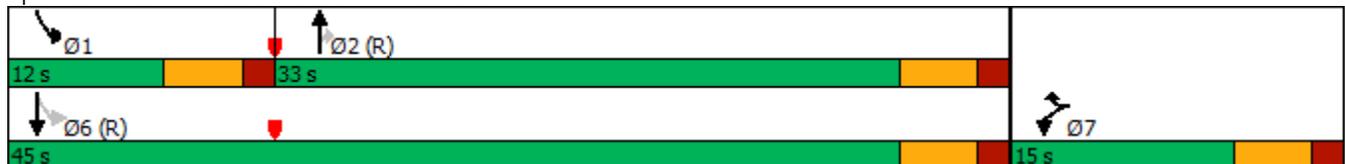
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	84	50	515	70	66	660
Future Volume (vph)	84	50	515	70	66	660
Turn Type	Prot	Prot	NA	Perm	pm+pt	NA
Protected Phases	7	7	2		1	6
Permitted Phases				2	6	
Detector Phase	7	7	2	2	1	6
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	23.0	23.0	23.0	23.0	10.0	23.0
Total Split (s)	15.0	15.0	33.0	33.0	12.0	45.0
Total Split (%)	25.0%	25.0%	55.0%	55.0%	20.0%	75.0%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag			Lag	Lag	Lead	
Lead-Lag Optimize?			Yes	Yes	Yes	
Recall Mode	None	None	C-Max	C-Max	None	C-Max
Act Effect Green (s)	8.1	8.1	38.0	38.0	44.0	45.0
Actuated g/C Ratio	0.14	0.14	0.63	0.63	0.73	0.75
v/c Ratio	0.38	0.21	0.25	0.07	0.11	0.27
Control Delay	27.8	9.6	14.3	9.8	3.7	3.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	27.8	9.6	14.3	9.8	3.7	3.7
LOS	C	A	B	A	A	A
Approach Delay	21.0		13.7			3.7
Approach LOS	C		B			A

Intersection Summary

Cycle Length: 60  
 Actuated Cycle Length: 60  
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBTL, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.38  
 Intersection Signal Delay: 9.3  
 Intersection Capacity Utilization 35.6%  
 Analysis Period (min) 15

Intersection LOS: A  
 ICU Level of Service A

Splits and Phases: 12: S. 96th Street & Middle Access



HCM 6th TWSC  
 14: W. Dillon Road & East Access

2024 Total  
 Sunday Peak

Intersection						
Int Delay, s/veh	0.5					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑	↑		↑
Traffic Vol, veh/h	0	419	458	15	0	47
Future Vol, veh/h	0	419	458	15	0	47
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	0	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	455	498	16	0	51

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	249
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	0	-	-	-	751
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	751
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	10.1
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	751
HCM Lane V/C Ratio	-	-	-	0.068
HCM Control Delay (s)	-	-	-	10.1
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.2



Timings  
3: S. 96th Street & W. Dillon Road

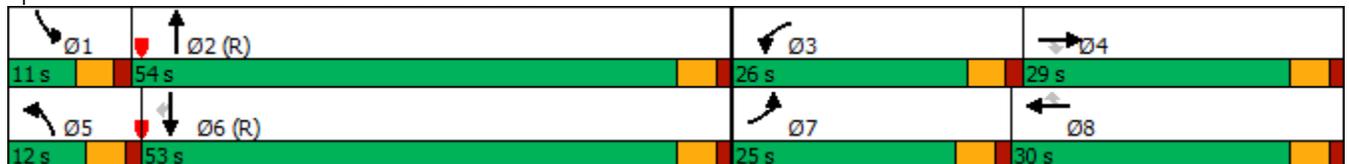
2040 Background  
PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	308	580	280	586	455	145	200	1361	480	136	972	217
Future Volume (vph)	308	580	280	586	455	145	200	1361	480	136	972	217
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	29.0	29.0	26.0	30.0	30.0	12.0	54.0		11.0	53.0	53.0
Total Split (%)	20.8%	24.2%	24.2%	21.7%	25.0%	25.0%	10.0%	45.0%		9.2%	44.2%	44.2%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0		-2.0	-2.0	-2.0
Total Lost Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0		3.0	3.0	3.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effect Green (s)	18.4	25.4	25.4	23.0	30.0	30.0	9.6	51.3	120.0	8.3	50.0	50.0
Actuated g/C Ratio	0.15	0.21	0.21	0.19	0.25	0.25	0.08	0.43	1.00	0.07	0.42	0.42
v/c Ratio	0.62	0.82	0.69	0.94	0.54	0.31	0.77	0.95	0.32	0.60	0.69	0.29
Control Delay	52.5	54.9	34.9	71.1	42.1	12.0	73.7	47.1	0.5	65.7	31.8	3.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	52.5	54.9	34.9	71.1	42.1	12.0	73.7	47.1	0.5	65.7	31.8	3.8
LOS	D	D	C	E	D	B	E	D	A	E	C	A
Approach Delay		49.5			52.7			38.8			30.7	
Approach LOS		D			D			D			C	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 90  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.95  
 Intersection Signal Delay: 42.0  
 Intersection LOS: D  
 Intersection Capacity Utilization 87.9%  
 ICU Level of Service E  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road



Timings  
3: S. 96th Street & W. Dillon Road

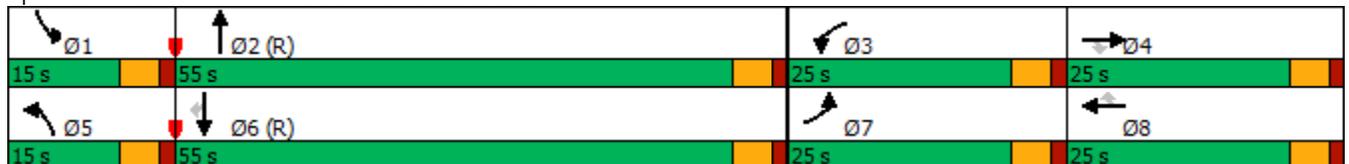
2040 Background  
Sunday Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	214	325	55	232	331	57	55	396	150	81	597	251
Future Volume (vph)	214	325	55	232	331	57	55	396	150	81	597	251
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	25.0	25.0	25.0	25.0	25.0	15.0	55.0		15.0	55.0	55.0
Total Split (%)	20.8%	20.8%	20.8%	20.8%	20.8%	20.8%	12.5%	45.8%		12.5%	45.8%	45.8%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0		-1.0	-1.0	-1.0
Total Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	4.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effect Green (s)	14.4	18.3	18.3	15.1	18.9	18.9	8.5	61.3	120.0	9.3	64.3	64.3
Actuated g/C Ratio	0.12	0.15	0.15	0.13	0.16	0.16	0.07	0.51	1.00	0.08	0.54	0.54
v/c Ratio	0.57	0.66	0.18	0.58	0.65	0.18	0.25	0.24	0.10	0.33	0.34	0.28
Control Delay	55.0	53.6	1.8	54.8	52.6	2.2	54.8	18.1	0.1	55.4	18.3	3.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	55.0	53.6	1.8	54.8	52.6	2.2	54.8	18.1	0.1	55.4	18.3	3.1
LOS	D	D	A	D	D	A	D	B	A	E	B	A
Approach Delay		49.3			48.8			17.0			17.4	
Approach LOS		D			D			B			B	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.66  
 Intersection Signal Delay: 31.3  
 Intersection Capacity Utilization 49.6%  
 Analysis Period (min) 15  
 Intersection LOS: C  
 ICU Level of Service A

Splits and Phases: 3: S. 96th Street & W. Dillon Road





HCM 6th TWSC  
6: S. 96th Street & South Access

2040 Total  
AM Peak

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕	↗		↕
Traffic Vol, veh/h	0	64	1445	165	0	1825
Future Vol, veh/h	0	64	1445	165	0	1825
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	0	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	70	1571	179	0	1984

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	786	0	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-
Pot Cap-1 Maneuver	0	335	-	-	0
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	-	335	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	18.5	0	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	335
HCM Lane V/C Ratio	-	-	0.208
HCM Control Delay (s)	-	-	18.5
HCM Lane LOS	-	-	C
HCM 95th %tile Q(veh)	-	-	0.8

HCM 6th TWSC  
8: S. 96th Street & North Access

2040 Total  
AM Peak

Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕		↖	↕
Traffic Vol, veh/h	0	48	1395	68	85	1855
Future Vol, veh/h	0	48	1395	68	85	1855
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	0	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	52	1516	74	92	2016

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	795	0	0	1590
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	4.14
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	2.22
Pot Cap-1 Maneuver	0	*495	-	-	*740
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %		1	-	-	1
Mov Cap-1 Maneuver	-	*495	-	-	*740
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	13.1	0	0.5
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	495	*740
HCM Lane V/C Ratio	-	-	0.105	0.125
HCM Control Delay (s)	-	-	13.1	10.6
HCM Lane LOS	-	-	B	B
HCM 95th %tile Q(veh)	-	-	0.4	0.4

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

Timings  
12: S. 96th Street & Middle Access

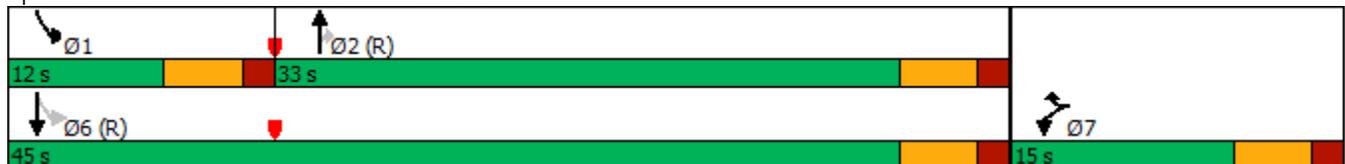
2040 Total  
AM Peak

Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	151	86	1375	136	181	1675
Future Volume (vph)	151	86	1375	136	181	1675
Turn Type	Prot	Prot	NA	Perm	pm+pt	NA
Protected Phases	7	7	2		1	6
Permitted Phases				2	6	
Detector Phase	7	7	2	2	1	6
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	23.0	23.0	23.0	23.0	10.0	23.0
Total Split (s)	15.0	15.0	33.0	33.0	12.0	45.0
Total Split (%)	25.0%	25.0%	55.0%	55.0%	20.0%	75.0%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag			Lag	Lag	Lead	
Lead-Lag Optimize?			Yes	Yes	Yes	
Recall Mode	None	None	C-Max	C-Max	None	C-Max
Act Effect Green (s)	9.1	9.1	31.4	31.4	43.2	44.2
Actuated g/C Ratio	0.15	0.15	0.52	0.52	0.72	0.74
v/c Ratio	0.61	0.29	0.81	0.16	0.60	0.70
Control Delay	33.8	8.6	32.9	5.1	16.1	7.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	33.8	8.6	32.9	5.1	16.1	7.9
LOS	C	A	C	A	B	A
Approach Delay	24.7		30.4			8.7
Approach LOS	C		C			A

Intersection Summary

Cycle Length: 60  
 Actuated Cycle Length: 60  
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBTL, Start of Green  
 Natural Cycle: 70  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.81  
 Intersection Signal Delay: 18.8  
 Intersection LOS: B  
 Intersection Capacity Utilization 68.9%  
 ICU Level of Service C  
 Analysis Period (min) 15

Splits and Phases: 12: S. 96th Street & Middle Access



HCM 6th TWSC  
14: W. Dillon Road & East Access

2040 Total  
AM Peak

Intersection						
Int Delay, s/veh	0.6					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑	↑		↑
Traffic Vol, veh/h	0	1135	920	45	0	90
Future Vol, veh/h	0	1135	920	45	0	90
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	0	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1234	1000	49	0	98

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	500
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	0	-	-	-	516
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	516
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	13.6
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	516
HCM Lane V/C Ratio	-	-	-	0.19
HCM Control Delay (s)	-	-	-	13.6
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.7

Timings  
3: S. 96th Street & W. Dillon Road

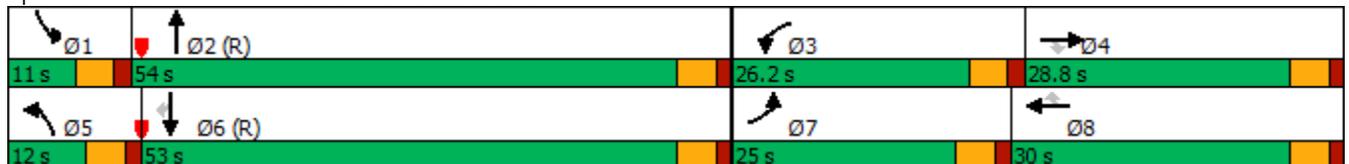
2040 Total  
PM Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	350	580	280	625	475	150	200	1425	480	175	1050	275
Future Volume (vph)	350	580	280	625	475	150	200	1425	480	175	1050	275
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	28.8	28.8	26.2	30.0	30.0	12.0	54.0		11.0	53.0	53.0
Total Split (%)	20.8%	24.0%	24.0%	21.8%	25.0%	25.0%	10.0%	45.0%		9.2%	44.2%	44.2%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0	-2.0		-2.0	-2.0	-2.0
Total Lost Time (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0		3.0	3.0	3.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effect Green (s)	19.4	25.2	25.2	23.2	29.0	29.0	9.5	51.0	120.0	8.6	50.1	50.1
Actuated g/C Ratio	0.16	0.21	0.21	0.19	0.24	0.24	0.08	0.42	1.00	0.07	0.42	0.42
v/c Ratio	0.66	0.81	0.69	0.98	0.58	0.33	0.76	0.99	0.32	0.74	0.74	0.35
Control Delay	52.9	54.9	36.0	79.2	43.7	14.2	73.1	54.6	0.5	66.7	33.4	5.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	52.9	54.9	36.0	79.2	43.7	14.2	73.1	54.6	0.5	66.7	33.4	5.3
LOS	D	D	D	E	D	B	E	D	A	E	C	A
Approach Delay		49.9			57.9			44.1			32.1	
Approach LOS		D			E			D			C	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 90  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.99  
 Intersection Signal Delay: 45.1  
 Intersection LOS: D  
 Intersection Capacity Utilization 91.6%  
 ICU Level of Service F  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road



HCM 6th TWSC  
6: S. 96th Street & South Access

2040 Total  
PM Peak

Intersection						
Int Delay, s/veh	0.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↗↗	↗		↗↗
Traffic Vol, veh/h	0	86	1900	25	0	1500
Future Vol, veh/h	0	86	1900	25	0	1500
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	0	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	93	2065	27	0	1630

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	1033	0	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-
Pot Cap-1 Maneuver	0	230	-	-	0
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	-	230	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	31	0	0
HCM LOS	D		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	230
HCM Lane V/C Ratio	-	-	0.406
HCM Control Delay (s)	-	-	31
HCM Lane LOS	-	-	D
HCM 95th %tile Q(veh)	-	-	1.9

HCM 6th TWSC  
8: S. 96th Street & North Access

2040 Total  
PM Peak

Intersection						
Int Delay, s/veh	0.5					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕		↖	↕
Traffic Vol, veh/h	0	56	1915	46	40	1370
Future Vol, veh/h	0	56	1915	46	40	1370
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	0	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	61	2082	50	43	1489

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	1066	0	0	2132
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	4.14
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	2.22
Pot Cap-1 Maneuver	0	*286	-	-	*428
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %		1	-	-	1
Mov Cap-1 Maneuver	-	*286	-	-	*428
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	21	0	0.4
HCM LOS	C		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	286	* 428
HCM Lane V/C Ratio	-	-	0.213	0.102
HCM Control Delay (s)	-	-	21	14.4
HCM Lane LOS	-	-	C	B
HCM 95th %tile Q(veh)	-	-	0.8	0.3

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

Timings  
12: S. 96th Street & Middle Access

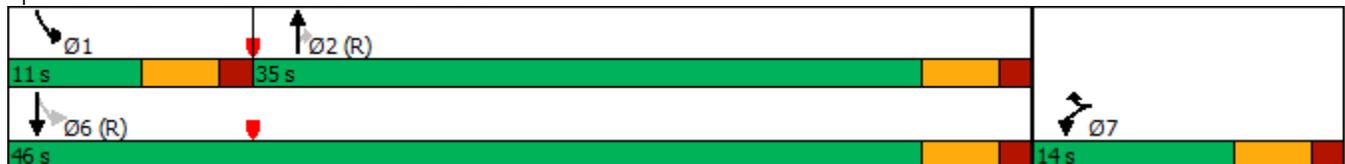
2040 Total  
PM Peak

Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	200	63	1900	88	70	1300
Future Volume (vph)	200	63	1900	88	70	1300
Turn Type	Prot	Prot	NA	Perm	pm+pt	NA
Protected Phases	7	7	2		1	6
Permitted Phases				2	6	
Detector Phase	7	7	2	2	1	6
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	23.0	23.0	23.0	23.0	10.0	23.0
Total Split (s)	14.0	14.0	35.0	35.0	11.0	46.0
Total Split (%)	23.3%	23.3%	58.3%	58.3%	18.3%	76.7%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag			Lag	Lag	Lead	
Lead-Lag Optimize?			Yes	Yes	Yes	
Recall Mode	None	None	C-Max	C-Max	None	C-Max
Act Effect Green (s)	9.0	9.0	34.4	34.4	41.0	41.0
Actuated g/C Ratio	0.15	0.15	0.57	0.57	0.68	0.68
v/c Ratio	0.82	0.23	1.02	0.10	0.26	0.58
Control Delay	52.1	9.3	52.7	3.8	5.4	6.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	52.1	9.3	52.7	3.8	5.4	6.2
LOS	D	A	D	A	A	A
Approach Delay	41.9		50.6			6.2
Approach LOS	D		D			A

Intersection Summary

Cycle Length: 60  
 Actuated Cycle Length: 60  
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBTL, Start of Green  
 Natural Cycle: 90  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 1.02  
 Intersection Signal Delay: 33.1  
 Intersection Capacity Utilization 77.6%  
 Analysis Period (min) 15  
 Intersection LOS: C  
 ICU Level of Service D

Splits and Phases: 12: S. 96th Street & Middle Access



HCM 6th TWSC  
 14: W. Dillon Road & East Access

2040 Total  
 PM Peak

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑	↑		↑
Traffic Vol, veh/h	0	1235	1190	16	0	59
Future Vol, veh/h	0	1235	1190	16	0	59
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	0	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	1342	1293	17	0	64

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	-
Stage 1	-	-	-
Stage 2	-	-	-
Critical Hdwy	-	-	-
Critical Hdwy Stg 1	-	-	-
Critical Hdwy Stg 2	-	-	-
Follow-up Hdwy	-	-	-
Pot Cap-1 Maneuver	0	-	-
Stage 1	0	-	-
Stage 2	0	-	-
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	-
Mov Cap-2 Maneuver	-	-	-
Stage 1	-	-	-
Stage 2	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	15.3
HCM LOS			C

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	414
HCM Lane V/C Ratio	-	-	-	0.155
HCM Control Delay (s)	-	-	-	15.3
HCM Lane LOS	-	-	-	C
HCM 95th %tile Q(veh)	-	-	-	0.5

Timings  
3: S. 96th Street & W. Dillon Road

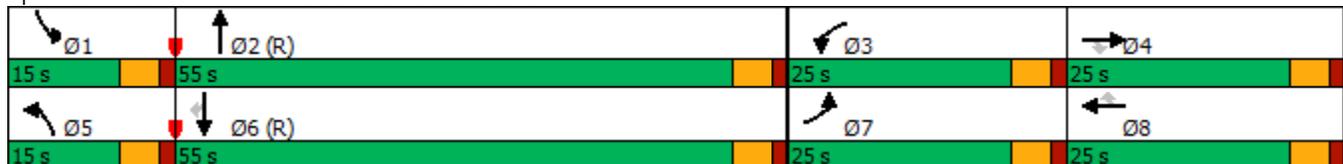
2040 Total  
Sunday Peak

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	250	325	55	260	350	60	55	450	150	100	625	270
Future Volume (vph)	250	325	55	260	350	60	55	450	150	100	625	270
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Free	Prot	NA	Perm
Protected Phases	7	4		3	8		5	2		1	6	
Permitted Phases			4			8			Free			6
Detector Phase	7	4	4	3	8	8	5	2		1	6	6
Switch Phase												
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0	5.0	5.0		5.0	5.0	5.0
Minimum Split (s)	10.0	20.0	20.0	10.0	20.0	20.0	10.0	20.0		10.0	20.0	20.0
Total Split (s)	25.0	25.0	25.0	25.0	25.0	25.0	15.0	55.0		15.0	55.0	55.0
Total Split (%)	20.8%	20.8%	20.8%	20.8%	20.8%	20.8%	12.5%	45.8%		12.5%	45.8%	45.8%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5	3.5	3.5		3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5		1.5	1.5	1.5
Lost Time Adjust (s)	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0	-1.0		-1.0	-1.0	-1.0
Total Lost Time (s)	4.0	4.0	4.0	4.0	4.0	4.0	4.0	4.0		4.0	4.0	4.0
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lag	Lag
Lead-Lag Optimize?	Yes		Yes	Yes	Yes							
Recall Mode	None	C-Max		None	C-Max	C-Max						
Act Effct Green (s)	15.8	18.4	18.4	16.1	18.8	18.8	8.5	59.6	120.0	9.8	63.1	63.1
Actuated g/C Ratio	0.13	0.15	0.15	0.13	0.16	0.16	0.07	0.50	1.00	0.08	0.53	0.53
v/c Ratio	0.60	0.65	0.18	0.61	0.69	0.19	0.25	0.28	0.10	0.39	0.36	0.30
Control Delay	54.6	53.3	1.8	54.6	54.3	2.9	54.8	19.4	0.1	51.8	17.6	3.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	54.6	53.3	1.8	54.6	54.3	2.9	54.8	19.4	0.1	51.8	17.6	3.1
LOS	D	D	A	D	D	A	D	B	A	D	B	A
Approach Delay		49.3			49.8			17.9			17.1	
Approach LOS		D			D			B			B	

Intersection Summary

Cycle Length: 120  
 Actuated Cycle Length: 120  
 Offset: 19 (16%), Referenced to phase 2:NBT and 6:SBT, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.69  
 Intersection Signal Delay: 31.6  
 Intersection LOS: C  
 Intersection Capacity Utilization 51.6%  
 ICU Level of Service A  
 Analysis Period (min) 15

Splits and Phases: 3: S. 96th Street & W. Dillon Road



HCM 6th TWSC  
6: S. 96th Street & South Access

2040 Total  
Sunday Peak

Intersection						
Int Delay, s/veh	0.2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕	↗		↕
Traffic Vol, veh/h	0	35	715	45	0	995
Future Vol, veh/h	0	35	715	45	0	995
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	0	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	38	777	49	0	1082

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	389	0	0	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	-
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	-
Pot Cap-1 Maneuver	0	610	-	-	0
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	-	610	-	-	-
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.3	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBT
Capacity (veh/h)	-	-	610
HCM Lane V/C Ratio	-	-	0.062
HCM Control Delay (s)	-	-	11.3
HCM Lane LOS	-	-	B
HCM 95th %tile Q(veh)	-	-	0.2

HCM 6th TWSC  
8: S. 96th Street & North Access

2040 Total  
Sunday Peak

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations		↗	↕		↖	↕
Traffic Vol, veh/h	0	28	715	15	24	975
Future Vol, veh/h	0	28	715	15	24	975
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	-	0	-	-	0	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	30	777	16	26	1060

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	-	397	0	0	793
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	6.94	-	-	4.14
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	3.32	-	-	2.22
Pot Cap-1 Maneuver	0	*807	-	-	1169
Stage 1	0	-	-	-	-
Stage 2	0	-	-	-	-
Platoon blocked, %		1	-	-	1
Mov Cap-1 Maneuver	-	*807	-	-	1169
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.6	0	0.2
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	807	1169
HCM Lane V/C Ratio	-	-	0.038	0.022
HCM Control Delay (s)	-	-	9.6	8.2
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.1	0.1

Notes  
 -: Volume exceeds capacity    \$: Delay exceeds 300s    +: Computation Not Defined    \*: All major volume in platoon

Timings  
12: S. 96th Street & Middle Access

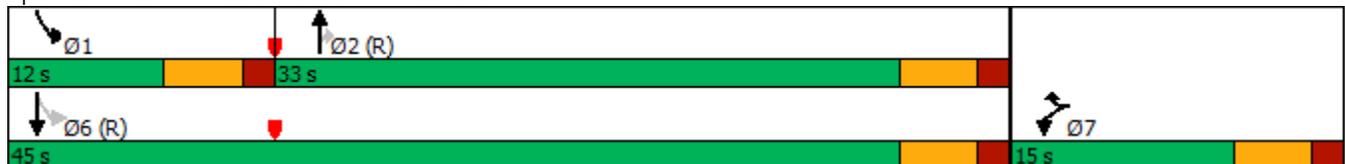
2040 Total  
Sunday Peak

						
Lane Group	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (vph)	84	50	680	70	66	910
Future Volume (vph)	84	50	680	70	66	910
Turn Type	Prot	Prot	NA	Perm	pm+pt	NA
Protected Phases	7	7	2		1	6
Permitted Phases				2	6	
Detector Phase	7	7	2	2	1	6
Switch Phase						
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	23.0	23.0	23.0	23.0	10.0	23.0
Total Split (s)	15.0	15.0	33.0	33.0	12.0	45.0
Total Split (%)	25.0%	25.0%	55.0%	55.0%	20.0%	75.0%
Yellow Time (s)	3.5	3.5	3.5	3.5	3.5	3.5
All-Red Time (s)	1.5	1.5	1.5	1.5	1.5	1.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lead/Lag			Lag	Lag	Lead	
Lead-Lag Optimize?			Yes	Yes	Yes	
Recall Mode	None	None	C-Max	C-Max	None	C-Max
Act Effect Green (s)	8.1	8.1	38.0	38.0	44.0	45.0
Actuated g/C Ratio	0.14	0.14	0.63	0.63	0.73	0.75
v/c Ratio	0.38	0.21	0.33	0.07	0.13	0.37
Control Delay	27.8	9.6	16.5	10.5	3.8	4.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	27.8	9.6	16.5	10.5	3.8	4.2
LOS	C	A	B	B	A	A
Approach Delay	21.0		15.9			4.2
Approach LOS	C		B			A

Intersection Summary

Cycle Length: 60  
 Actuated Cycle Length: 60  
 Offset: 0 (0%), Referenced to phase 2:NBT and 6:SBTL, Start of Green  
 Natural Cycle: 60  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.38  
 Intersection Signal Delay: 10.1  
 Intersection Capacity Utilization 40.1%  
 Analysis Period (min) 15  
 Intersection LOS: B  
 ICU Level of Service A

Splits and Phases: 12: S. 96th Street & Middle Access



HCM 6th TWSC  
14: W. Dillon Road & East Access

2040 Total  
Sunday Peak

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑	↑		↑
Traffic Vol, veh/h	0	575	625	15	0	47
Future Vol, veh/h	0	575	625	15	0	47
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	0	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	625	679	16	0	51

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	340
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-
Critical Hdwy	-	-	-	-	6.94
Critical Hdwy Stg 1	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-
Follow-up Hdwy	-	-	-	-	3.32
Pot Cap-1 Maneuver	0	-	-	-	656
Stage 1	0	-	-	-	0
Stage 2	0	-	-	-	0
Platoon blocked, %	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	-	656
Mov Cap-2 Maneuver	-	-	-	-	-
Stage 1	-	-	-	-	-
Stage 2	-	-	-	-	-

Approach	EB	WB	SB
HCM Control Delay, s	0	0	11
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	656
HCM Lane V/C Ratio	-	-	-	0.078
HCM Control Delay (s)	-	-	-	11
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0.3

## **Narrative - GDP 2nd Amendment**

The purpose of this letter is to outline the challenging history and existing site constraints for all three (3) property owners (Archdiocese, Adrian Games and Ascent Church) at the NE corner of S. 96<sup>th</sup> Street & Dillon Road in Louisville to enlist staff support for a General Development Plan (GDP) amendment which allows the development to move forward, providing public improvement benefits, sales tax revenue and additional jobs to the City.

### **HISTORY**

The St. Louis Parish and Commercial Park GDP was created in 2004, covering an area of 51.4 acres with three (3) property owners and breaking development into five (5) zones. The underlying zoning for the properties is PCZD, but the GDP established stricter use standards for the area. The Developer at the time was unable to bring development to fruition given the GDP site design requirements and financial burden put on the middle lot to fund all the infrastructure for the three (3) parcels.

In 2017, the GDP was further amended allowing for a change in permitted uses following Ascent Community Church's purchase of the middle lot. Ascent Church took the site through Preliminary Plat and PUD approval and had submitted for Final Plat and PUD approval when additional site development costs and further challenges with the GDP deemed the project to no longer be financially viable to move forward. Ascent now wishes to purchase and renovate their current location in Louisville, but it is contingent upon selling this property to United Properties and is supportive of the requests herein.

United Properties wishes to purchase the entire 13.73 acres from Ascent to construct an Industrial/Retail mixed-use development, complete the master development infrastructure that will serve all three (3) parcels and allow the development and adjacent property owner developments to move forward. This is all contingent upon securing critical necessary amendments to the current GDP and Plat/Final PUD approvals. The GDP amendment is being submitted first for approval to ensure permitted uses and necessary design guidelines needed for development to go forward are approved. United Properties will immediately follow it with a Final Plat, PUD and Special Review Use that details development plans, work with staff and bring forward to Planning Commission and City Council for final approval.

### **OVERVIEW**

The current GDP provides significant design and financial challenges, conflicts with current codes and standards and needs to be cleaned up to minimize/limit future amendments to allow the development to move forward for all three property owners. United Properties, along with the approval and support from Games and Archdiocese of Denver, are seeking a second amendment to the GDP that would do the following;

1. Reduce the building setback distance along S. 96<sup>th</sup> Street from 60' to 55'
2. Clarify building & parking lot configuration requirements along 96<sup>th</sup> Street to ensure parking is allowed in front of buildings with enhanced landscape screening techniques
3. Make the common access drives and roadways private versus public
4. Properly align zones and FAR requirements within property boundaries
5. Add industrial as a permitted use and car wash as permitted with special use review.
6. Increase building height from 35' to 40' for Industrial buildings only.

The proposed amendments meet the intent of the adopted 2013 Comprehensive plan for this corridor, the current commercial and industrial design standards and are consistent with the permitted uses in the underlying PCZD zoning on these properties. These amendments also allow the property owners to maximize developable area, meet minimum tenant market driven standards for users, reduce a portion of the financial hardships that have been placed on the Ascent parcel by combining the three (3) developments together and allow the development to finally move forward for all three (3) property owners in a timely manner.

Furthermore, surrounding cities and properties have underwent extensive growth that has changed the entire look, feel and operation of the 96<sup>th</sup> Street and Dillon corridors since the GDP was put in place in 2004. Both corridors have been identified as key commuter corridors with future expansion requirements to handle the existing and future traffic traveling through this area in the October 1, 2019 approved Transportation Master Plan. City Council has approved many plans, standards and surrounding developments in the past 15 years that conflict with the setback and parking orientation GDP standards that were set forth on these properties. The proposed GDP amendments make it more consistent with current standards and developments approved along these corridors and is strongly supported by all three (3) property owners.

## **SETBACKS**

The 2004 GDP required a 60-foot setback from S. 96<sup>th</sup> Street. This large setback is not been required by any other developments along 96<sup>th</sup> Street or Dillon Road and provides difficult constraints to achieve required infrastructure and minimum design standards for retailers and other permitted users.

All three property owners are dealing with the following site-specific constraints that challenge design layouts with the east/west dimensions within their parcel, making this large setback further challenge the development. Please see attached dimensioned conceptual plan attempting to aid in depicting the challenges:

- Angling of the properties make the east/west dimensions tighter as you move north for each parcel, but bound by same setback (Games & NE corner of Ascent parcel is most impacted)
- Each parcel is locked by railroad to the east, 96<sup>th</sup> Street to the west and property lines north/south limiting developable area with the required setbacks.
- Each property owner must convey neighboring properties detention across its site on the eastern side through a 20-35' drainage channel to ultimately outfall across Dillon road, on-site water quality and detention for their development within their parcel for the 100 year event that must be located on the east side given historical drainage patterns and grading, taking significant buildable area out of the east/west dimension on site. Grading against railroad and high-water table also limits allowable depth of ponds, thus requiring them to be larger.
- Access is limited to 96<sup>th</sup> Street only for 2 of the 3 parcels; therefore, common access drives and cross access roadways must be handled on Ascents parcel at the property lines and sized accordingly for multiple developments.
- Development was forced to receive 67.8 acres/120 cfs of off-site drainage conveyance from the west side of 96<sup>th</sup> Street, convey it through the development between the Ascent Church and Archdiocese property via a large pipe or channel, then channel along eastern property of the parcel and pipe to outfall to Dillon road. The acceptance of this large amount of off-site drainage has put a \$200K burden on property owners to absorb, required large drainage channels that have dictated design layouts and further limited buildable area throughout the development and prohibited logical public land dedication opportunities in this area to reduce cash in lieu costs.

- 30' Public land dedication for future trail required along the eastern property line losing additional developable space.
- Sanitary is located within Arthur Avenue and must be brought to the site by boring underneath the railroad, brought through to service each development in 30' utility easements. Water must be brought from west side of railroad, underneath the roadways to service each of the developments and connected back to Dillon Road.
- A high pressure gas line runs in 96<sup>th</sup> Street and Dillon road surrounding the site providing cost and crossing challenges on utilities/storm for this development.

United Properties is trying to accommodate a mixed-use development that meets the design and market standards for both retailers and industrial users within the developable area that remains after infrastructure and code requirements are met. They also need to maximize development financial feasibility of the project to support the \$3.1 million of public and private infrastructure that has been placed on the Ascent parcel to move this development forward. It is a balance that must be achieved between product types to meet market demands and ensure leasing and/or sale as well as success for the tenants.

The preferred and most marketable retail parcel dimensions are 225X225 feet (50,625 SF) and assume standard building setbacks of 25-30 feet from arterials. The minimum size pad you want to create along an arterial is 1 acre in size with 185' depth. The current retail parcels are at the minimum depths that we can propose and still market, layout and attract the likely retail users that go along, thrive and survive in this commercial commuter corridor. The 55' setback and 40' landscaping buffer proposed is the maximum we can provide to not deem these pads undevelopable and/or unmarketable and is more than preferred. If we were to apply current Commercial Design Standards, a 30' setback along 96<sup>th</sup> Street would be required for these pads, so the proposed 55' setback exceeds these requirements by 25'.

The 30' private road with 5' attached sidewalks on each side is bare minimum depth needed to safely accommodate delivery trucks and traffic that will be generated by the retail/industrial development and future development of surrounding parcels. Ascent Church's property is required to accommodate a 68,550 SF (1.57 acres) of detention area on the parcel and a 20-foot drainage channel to accept Games Parcel. The Archdiocese must provide a 35-foot drainage channel to accept upstream and off-site conveyance flows through the site.

The proposed 180' Industrial building depth and 130' truck court sizing proposed is critical to the success of the development. The site constraints caused by the existing detention design required to be in this area causes circulation challenges for the truck court on the east side of the building. The attached truck circulation diagram shows how the narrowed truck court depths on the northeast and southeast sides of the truck court prevent a full-size semi-truck's ability to fully maneuver to all dock door locations on the building. 130' is the bare minimum we can go to ensure successful operation and safe maneuvering as shown by the exhibit. Secondly, a building depth of 180' is critical for the success of the industrial and reducing this depth size will deter institutional type tenants from occupying the space. Many tenants are programmatic with their layout requirements, and the 180' depth allows for maximum interior efficiencies for office, lab and racking layout design. Even shrinking the building depth by 5-10' throws off the bay sizing which would cause constraints in interior layouts. The new product that has been developed by Etkin Johnson in CTC is 180' deep for this same reason. United Properties has developed nearly 3 million square feet of industrial product in the Denver market and have leased to tenants including FedEx, Breakthru Beverage, Panera, Coca-Cola, and others. Our industrial expertise and market knowledge on tenant requirements have aided in our successful leasing track record. Most recently, we developed two, 180' deep buildings at Interpark Broomfield that were successfully leased to Swisslog, GC Imports and MKS Instruments, who all moved into the project because

they could gain significant operational efficiencies in the buildings. The current proposed site configuration, including both building depth and truck court depth, is important to ensure successful leasing of the project once development is complete and obtaining necessary rents to support overall public and private infrastructure costs on this development.

Current Industrial development design standards and guidelines require a 60' setback from arterials. The Industrial buildings will be placed behind the retail development on the current Ascent parcel and therefore would be 311 feet from 96<sup>th</sup> Street far exceeding current design standards if they were applied. United Properties is in discussion with the Archdiocese to expand Industrial development into a portion of there site. If that happens, it is likely that Industrial would be placed 55' from the arterial, but enhanced architecture and landscaping would be provided to offset the 5' reduction.

Reducing the 60' setback along S. 96<sup>th</sup> Street to 55' will not be detectable to the human eye but has significant impact to the success of the project. The reduction allows parcel to achieve minimum necessary dimensions needed east/west to develop, while still providing a 40' landscaping buffer along 96<sup>th</sup> Street for great landscaping opportunities and enhancements that will facilitate a gradual transition between the rural area to the west and the developed area to the east as well as meet the intention of the comprehensive plan. In addition, landscaping buffers in excess of 40' will be provided on the entrances of the access drives to soften appearance of asphalt and enhanced building architecture will be provided to ensure an aesthetically pleasing entrance into the City of Louisville. Each parcel with work with Staff to ensure these requirements are met and City Council will approve through the Final PUD process.

## **BUILDING AND PARKING LOT ORIENTATIONS**

The current GDP language states: buildings adjacent to or fronting to S. 96th Street to be located so as to primarily place the building between S. 96th Street and the parking lot. Parking lots extending beyond the shadow of the building shall be shielded from S. 96th Street using landscaping and berms that are a minimum of 30" above the parking level. We feel the current language within the GDP allows for parking to be placed in front of the buildings with enhanced landscaping techniques, but request language be changed to further clarify "Parking lots adjacent to south 96<sup>th</sup> Street shall be shielded from 96<sup>th</sup> Street using enhanced landscaping techniques such that is effectively buffered" given staff comments and concerns.

In addition, urban design configuration requirements would further challenge and already tight east/west design dimensions for each development, requiring a greater reduction in the setback than 55' and landscaping buffer to be provided and is contradictive to maintaining a "rural entryway into the City" as proposed in the Comprehensive Plan. In addition, requiring urban design standards in this highly vehicular/commuter area with limited to no foot traffic will negatively impact the marketability and success of establishing retail development along S. 96<sup>th</sup> Street. Retailers tend to see significant impact to sales when parking is placed behind buildings along arterials.

Therefore, amending the GDP to clarify the parking lot and building configuration requirement within this zone would provide more aesthetically pleasing street fronts leading into the City and provide consistency with other developments that have been approved along the Dillon and S. 96<sup>th</sup> Street corridors. Also, placing the backside of the building along S. 96th Street exposes the mechanical, electrical, and garbage facilities to the street and does not allow for a transitional

zone between rural west and developed east and it creates additional access constraints for the overall development.

In order to maintain this GDP area as a transition zone between the rural area to the west and the developed area to the east while providing some flexibility in site design, we are proposing to require a buffer in the form of enhanced landscape techniques to be used along S. 96th Street in order shield the parking. The site will sit 3-4 feet below the roadway and effectively be screened by the visual eye through grades, but in addition development specific enhanced landscaping techniques will occur in the newly defined 55-foot setback area from S. 96th Street and be detailed in Final PUD plans.

### **PRIVATE ROADS INSTEAD OF LOCAL COLLECTORS**

The design and traffic study support three (3) common access drives to S. 96<sup>th</sup> Street that will serve all three (3) parcels and a future RI/RO access to Dillon Road. The RI/RO and ¾ turn access point and full movement access will be constructed with the United Properties development. The future 96<sup>th</sup> Street RI/RO and Dillon RI/RO would be constructed with the Archdiocese develops.

Since the access drives will be phased and serve the three (3) properties only, they are better served as private roads instead of local connectors. A private drive section detail has been included in the GDP, which includes attached sidewalks and will be further detailed in Final PUD plans and once future property owner developments are known.

The property owners will establish common access roads, grant cross-access rights and maintenance obligations of these areas through separate legal agreements to be recorded with PUD and plat approvals. This reduces cost and design for all three (3) property owners eliminating local street section requirements, increases buffer capabilities along S. 96<sup>th</sup> Street, puts the control of guaranteed maintenance and snow plow removal of these roads into the property owners' hands and reduces the city's long-term maintenance costs.

### **ZONE AND FAR REALIGNMENT TO PROPERTY BOUNDARIES**

The current GDP has three (3) different zones and then subdivides those zones further across all three parcels. It is very confusing, does not align with property boundaries and bifurcates the parcels significantly. The property owners prefer to simplify the zones and FAR by aligning it with property boundaries going forward to alleviate confusion, allow for greater flexibility within the property boundaries and reduce further amendments of the GDP. Furthermore, each site-specific development plan must still come in front of City Council for approval to ensure quality development and standards are being met.

On Page 2 Zone 2 is further broken into 2A and 2B to show the delineation between the 25-foot height requirements for Retail and 40-foot for the Industrial buildings and the intent to transition height higher as you move away from the arterial towards CTC for Ascent's parcel.

The FAR requirements proposed for each property are as follows:

Ascent Parcel - .25 FAR = 137,223 SF of proposed retail/industrial building on 548,892 SF

Games Parcel - .25 FAR = 56,416 SF of proposed building on 225,666 SF

Archdiocese – No FAR limits if developed as school/church (consistent with current GDP) and .25 if portion is developed for any other permitted use = 296,863 SF building/1,187,452 SF

The Comprehensive Plan has intent to maintain a .25 FAR. The CTC development directly east these properties averages .3 or higher. We believe the FAR requirements being requested are consistent with intention of the comprehensive plan, comparable to surrounding development and provides a complimentary balance between building and green space on each parcel.

### **ADDITIONAL PERMITTED USES**

United Properties wishes to develop the Ascent Church piece with Industrial building behind the proposed retail lots and has interest from a tunnel car wash user. The underlying zoning on the site is PCZD. The GDP further restricted the allowed permitted uses within PCZD zoning at the time of approval in 2004 not listing all uses in PCZD as allowed. The proposed additional permitted uses are consistent with the underlying PCZD zoning that allows for research/office and corporate uses, facilities for the manufacturing, fabrication, processing or assembly of scientific or technical products, or other products and automobile service stations. We believe car washes fall within this broader language but would prefer clarification written into the approved GDP. Although they would be allowed uses within the GDP, the Final PUD and Special Review would come in front of Council for approval detailing specifics on those development.

We further believe this is consistent with the 2013 Comprehensive Plan, which states that S. 96<sup>th</sup> Street and Dillon Road Rural Special District serves as the rural gateway to the City of Louisville and will include a mix of commercial, institutional and industrial uses. Retail along S. 96<sup>th</sup> Street with industrial development in the back would be consistent with surrounding development along S. 96<sup>th</sup> Street and Dillon Road and serve as a continued transition to the existing industrial park approved and developed east of the railroad tracks.

With Ascent Church opting not to move forward on the development, industrial is a logical use to develop behind the retail pad users and support costs for the development to move forward for all three property owners. Given the other three (3) corners will remain open space limiting future densities in the area and direct access to the existing industrial park is prohibited by the railroad, the addition of workers to this corner will help drive retail development on the pads along S. 96<sup>th</sup> Street.

### **INCREASE MAX HEIGHT FOR INDUSTRIAL BUILDINGS**

The Current GDP had a maximum height of 35 feet. Along with the request to add Industrial as a permitted use, we are requesting the maximum height be increased to 40' for this allowed use. This is consistent with the Industrial Development Design Standards and Guidelines and what code allows. It is also consistent with the buildings constructed in CTC east of our development.

### **CONCLUSION**

Approval of the GDP amendments is critical for this development to move forward. This development will provide infrastructure and identified transportation master plan immediate needs and give surrounding properties the opportunity to finally move forward: 30' of land dedication along each eastern property line adjacent to the railroad for the construction of the future trail, significant cash in lieu payment for the public land dedication deficit that could fund the trail connection, expansion of south 96<sup>th</sup> Street on the east side to add one lane, curb/gutter and a 5' detached sidewalks, sanitary service extension from Arthur Avenue, under the railroad

to service the developments and future development in the area and water main extensions from Dillon Road to S. 96<sup>th</sup> Street.

We encourage the City to support the necessary amendments to allow development to move forward for all three (3) property owners bound by this aged 2004 GDP to bring public improvement benefits, jobs, sales tax dollars and much needed services to the City.

Thank you,

United Properties, Ascent Church Community, Adrian Games and the Archdiocese of Denver



the development. The systems are designed to capture and convey the 100-year storm to the detention pond. For phase 1, the development will focus on the Ascent PUD parcel, however the facilities installed are designed to be easily converted to regional systems when the future regional pond is implemented.

2. Offsite runoff is accepted into the drainage systems.

#### PUD

Tributary offsite runoff to the Ascent PUD area is accepted through the facilities to the site pond on Lot 1. The offsite tributary runoff is from the east half of South 96<sup>th</sup> Street. Drainage from 96<sup>th</sup> will continue in this pattern in the initial phase and future phases. The discharge of 25 CFS of the combined Ascent PUD/future Parcel 2 development is added to the CH-2. The flow as ended because this is detained release overlapping the offsite peak flow. The total runoff from the PUD and the tributary historic offsite basins will be less than historic at regulatory rates for the PUD.

#### OFF-SITE RUNOFF THROUGH PARCEL 1

A phased storm conveyance system is being proposed to accommodate runoff from the Ascent Church site and from off-site runoff west of S. 96<sup>th</sup> Street (City of Louisville open space). The proposed system will accept flow from Off-Site Basin F through an existing 43”x68” HERCP that runs under S. 96<sup>th</sup> Street. Off-Site Basin F produces approximately 120 cfs of flow. Off-Site Basin F corresponds with the 65.6 acre Basin 2 and Design Point 28 in the “Drainageway G Outfall Systems Plan Update” completed by Ayres Associates on October 2006.

From the existing 43”x68” HERCP, runoff is directed east. This runoff will be conveyed in an open channel or through 60” RCP (or as otherwise sized for future surface conditions) to the northeast corner of the Archdiocese (Parcel 1) property. At this point the 25 cfs from the Ascent PUD and future Parcel 2 developments enter the storm system. The combined flows of 145 cfs is directed south through a proposed in an open channel or through 66” RCP (or as otherwise sized for future surface conditions) to the Drainageway G improvements. The storm sewer system outfall initial design was to match the invert elevation of 5338.32 for the proposed dual 36” RCP that runs under Dillion Road into Drainageway G, however those initial conceptual inverts of the pipes under Dillon Rd need to be lower for a successful project. Invert elevations for the dual 36” RCP were obtained from the “Louisville Quiet Zone Dillion Road Storm Sewer Layout” prepared by Felsburg Holt & Ullevig, print date 9/26/2018.

3. Various tables, charts, exhibits and supporting information is presented in the appendix of the report. In general, the included documentation is from the City’s Criteria and the UDFCD DCM along with supporting information and culvert charts. Additional documentation includes spreadsheets developed by JLB Engineering that follow the

# MASTER DRAINAGE PLAN

DECEMBER 21, 2018



HISTORIC

ST. LOUIS PARISH AND  
COMMERCIAL PARK - FILING NO.  
1 AND ASCENT COMMUNITY  
CHURCH PUD

### BASIN RUNOFF SUMMARY

BASIN ID	DESIGN PT	AREA (AC)	DIRECT		CUMULATIVE		DETAINED (1/2")	DESCRIPTION
			Q <sub>d</sub> (cfs)	Q <sub>u</sub> (cfs)	Q <sub>c</sub> (cfs)	Q <sub>t</sub> (cfs)		
HIST	3	58	13	51	-	-	n	Historic Site Runoff
E	40	117.1	31	130	-	-	n	West of site basin to south
F	50	67.8	19	120	-	-	n	West of site basin to north
10	9	209.9	69	208	-	-	n	Offsite basin to southwest
11A	10	14.4	5	69	-	-	n	Offsite basin to southwest
Hist	3	467	137	537	-	-	n	Total tributary basin

### BASIN AREA SUMMARY

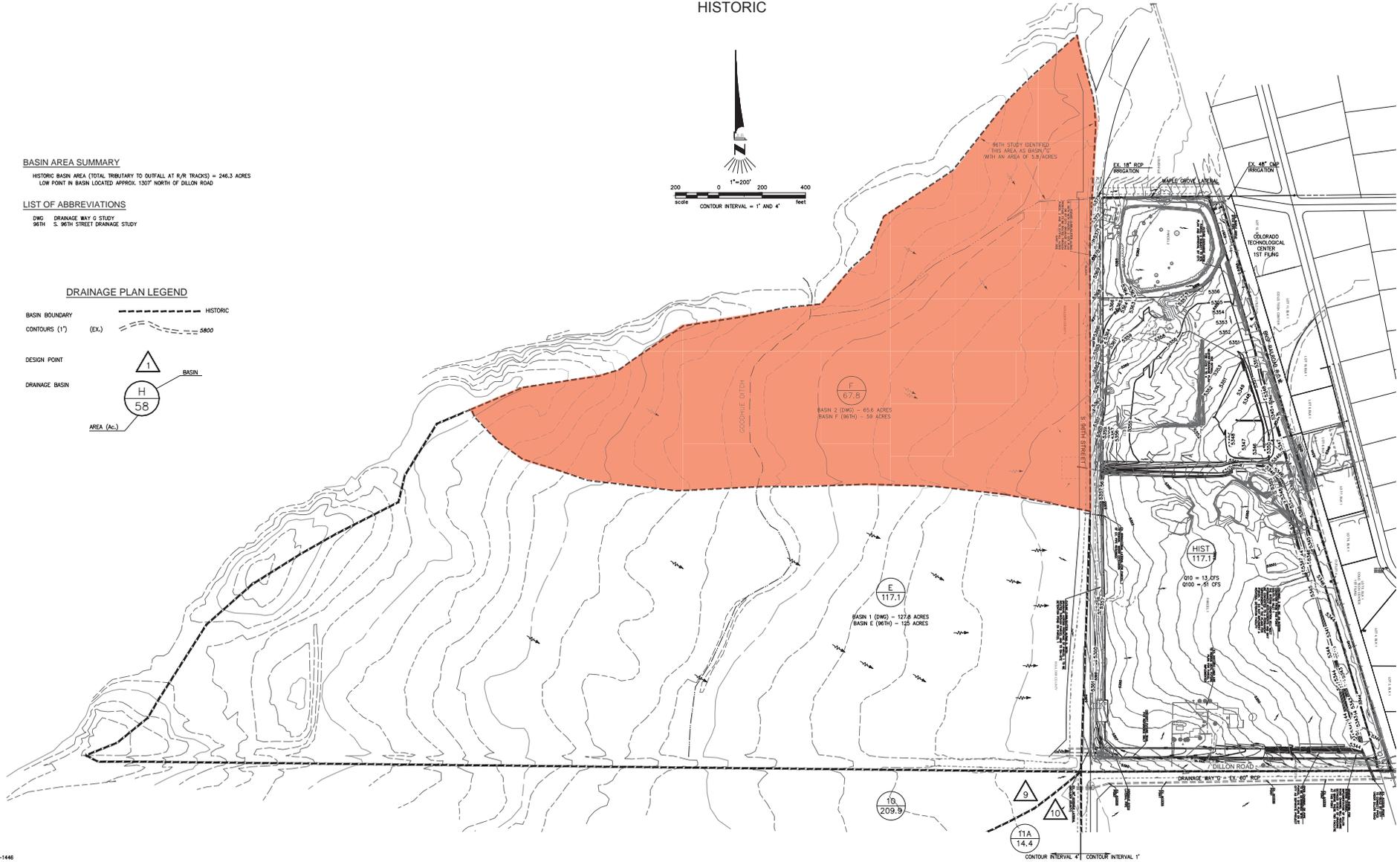
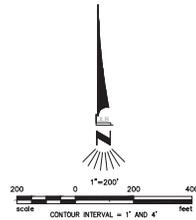
HISTORIC BASIN AREA (TOTAL TRIBUTARY TO OUTFALL AT R/R TRACKS) = 246.3 ACRES  
LOW POINT IN BASIN LOCATED APPROX. 1307' NORTH OF DILLON ROAD

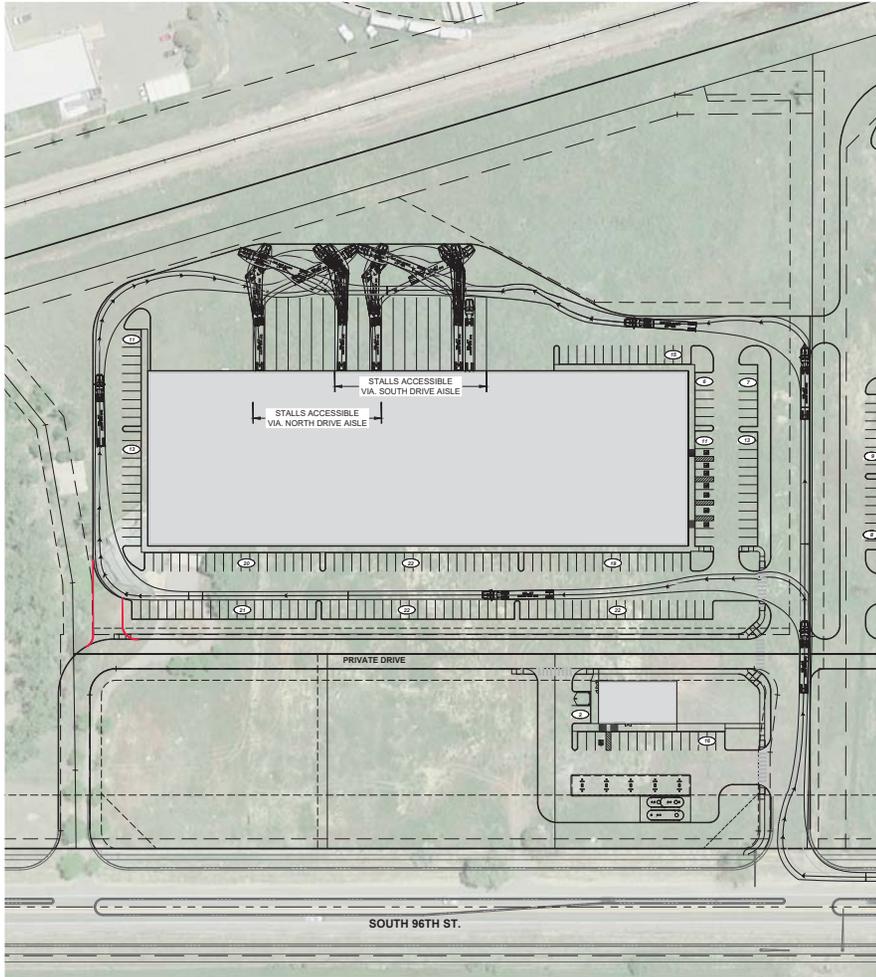
### LIST OF ABBREVIATIONS

DWG DRAINAGE WAY STUDY  
BETH S. 96TH STREET DRAINAGE STUDY

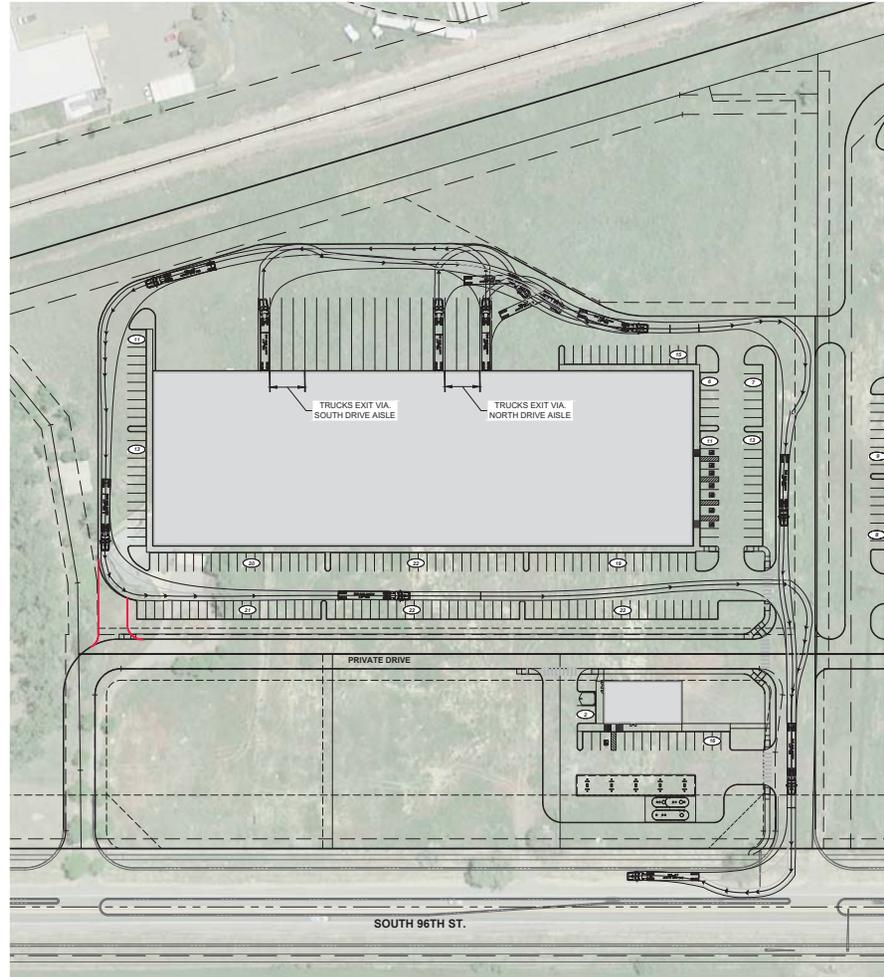
### DRAINAGE PLAN LEGEND

- BASIN BOUNDARY HISTORIC
- CONTOURS (1") (EX) 5800
- DESIGN POINT
- DRAINAGE BASIN
- AREA (AC)

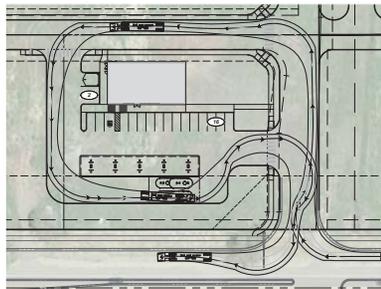




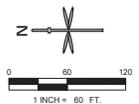
**WB-67 TRUCK ROUTE  
ENTERING INDUSTRIAL SITE**



**WB-67 TRUCK ROUTE  
EXITING INDUSTRIAL SITE**



**WB-50 FUELING TRUCK ROUTE - COMMERCIAL SITE**



NO.	REVISION	BY	DATE

**EES**  
ENGINEERING AND  
SOLUTIONS, INC.  
501 S Cherry St, Suite 300  
Denver, CO 80246  
303-672-7897 www.ees.us.com

**UNITED PROPERTIES**

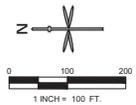
CONCEPTUAL SITE PLAN  
**96TH & DILLON - COMMERCIAL  
DEVELOPMENT - COMMERCIAL**  
96TH & DILLON, LOUISVILLE, COLORADO  
**TRUCK TURN EXHIBIT**

PROJECT NO: UPR01181  
DESIGNED BY: CRO  
DRAWN BY: CRO  
DATE: 01/08/20

**EX-2**



**OVERALL DEVELOPMENT AREA**  
SCALE: 1" = 100'



No.	REVISION	BY	DATE

**EES**  
ENGINEERING AND  
ENVIRONMENTAL  
SOLUTIONS, INC.  
501 S Cherry St, Suite 300  
Denver, CO 80246  
303-572-7567 www.ees-usa.com

**UNITED PROPERTIES**

**CONCEPTUAL SITE PLAN  
96TH & DILLON - COMMERCIAL  
DEVELOPMENT**  
96TH & DILLON, LOUISVILLE, COLORADO

**OVERALL DEVELOPMENT PLAN**

PROJECT NO:	UPR011181
DESIGNED BY:	CRO
DRAWN BY:	CRO
DATE:	02/27/20

**EX-1**



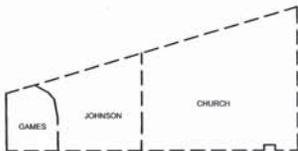




# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN

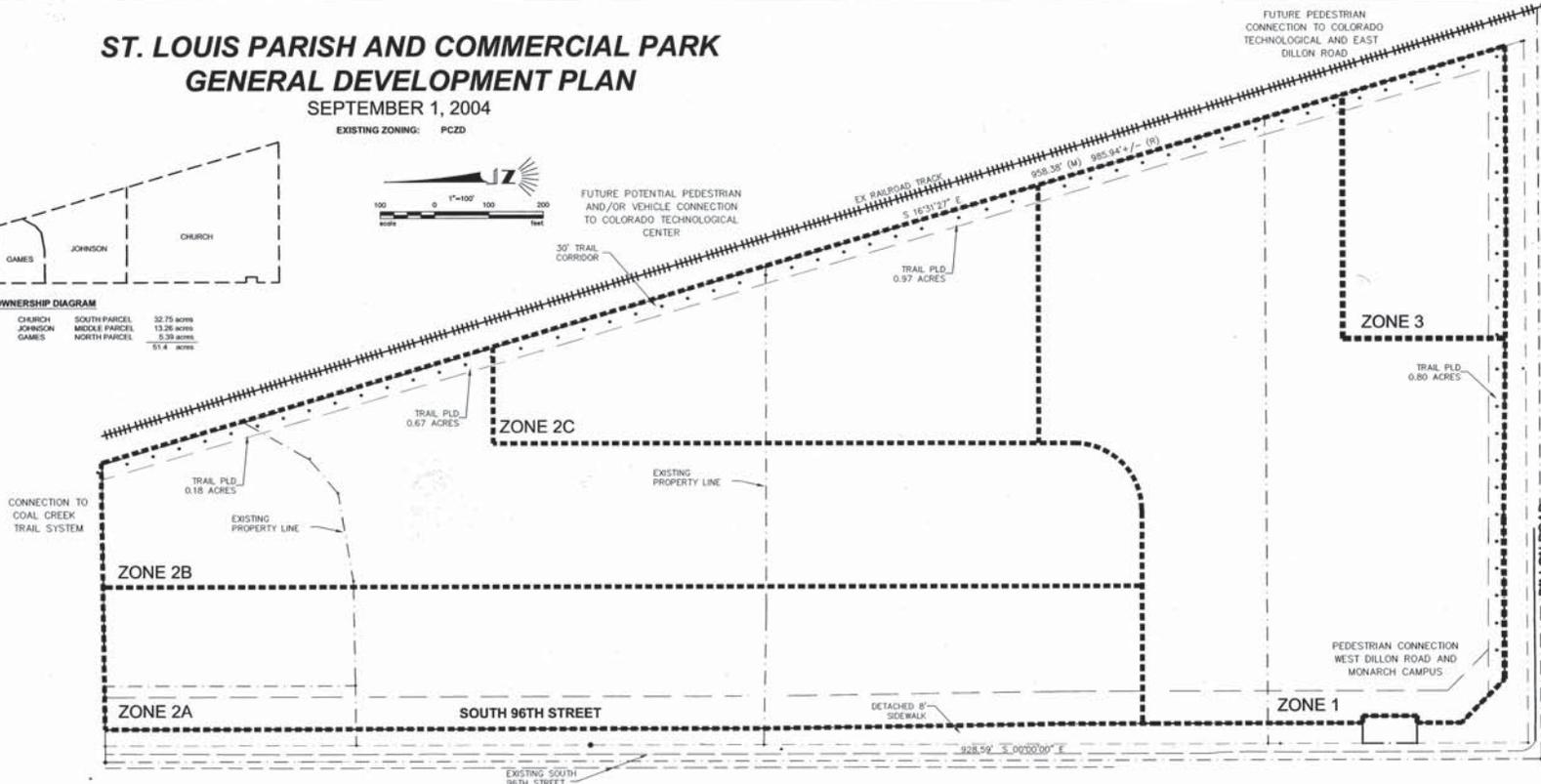
SEPTEMBER 1, 2004

EXISTING ZONING: PCZD



### OWNERSHIP DIAGRAM

CHURCH	SOUTH PARCEL	32.75 acres
JOHNSON	MIDDLE PARCEL	13.30 acres
GAMES	NORTH PARCEL	5.39 acres
	<b>TOTAL</b>	<b>51.4 acres</b>



### DEVELOPMENT CONCEPT AND GENERAL NOTES

1. THE DEVELOPMENT CONCEPT IS TO ESTABLISH A COMMERCIAL CORRIDOR AT THE INTERSECTION OF SOUTH 96TH STREET AND DILLON ROAD WITH THE REMAINDER OF THE PARCELS OWNED BY THE COMMERCIAL TRUST AND ARE LOCATED TO THE EAST AND IN A TRANSITION WITH THE TRAIL DEVELOPMENT AND THE TRAIL SPACE TO THE WEST. A LANDSCAPE BUFFER ALONG THE EXISTING CORRIDOR SHOULD BE MAINTAINED AND ENHANCED WITH THE REMAINDER OF A CORRIDOR AND SHOULD BE REDESIGNED TO PROVIDE A BUFFER BETWEEN THE TRAIL DEVELOPMENT AND THE TRAIL SPACE TO THE WEST. A LANDSCAPE BUFFER ALONG THE EXISTING CORRIDOR SHOULD BE MAINTAINED AND ENHANCED WITH THE REMAINDER OF A CORRIDOR AND SHOULD BE REDESIGNED TO PROVIDE A BUFFER BETWEEN THE TRAIL DEVELOPMENT AND THE TRAIL SPACE TO THE WEST. A LANDSCAPE BUFFER ALONG THE EXISTING CORRIDOR SHOULD BE MAINTAINED AND ENHANCED WITH THE REMAINDER OF A CORRIDOR AND SHOULD BE REDESIGNED TO PROVIDE A BUFFER BETWEEN THE TRAIL DEVELOPMENT AND THE TRAIL SPACE TO THE WEST.

### PERMITTED USES

- ZONE ONE (approx. 16.2 acres)**
    1. CHURCH
    2. SCHOOLS
    3. RECREATION FACILITIES, TYPICALLY AND COMMONLY ASSOCIATED WITH CHURCHES AND SCHOOLS INCLUDING A RECREATION, ADMINISTRATIVE OFFICES AND A CHILD-CARE CENTER AS DETERMINED BY THE PLANNING COMMISSION AND CITY COUNCIL DURING THE PLAN PROCESS.
  - ZONE TWO (approx. 31.8 acres)**
    1. CONTINUATION OF THE EXISTING RESIDENTIAL USES ON THE PROPERTY.
    2. ALL USES IN ZONE ONE. **USE BY SPECIAL REVIEW USE.**
    3. PROFESSIONAL AND ADMINISTRATIVE OFFICES.
    4. PROFESSIONAL, MEDICAL, OFFICES AND CLINICS.
    5. FINANCIAL OFFICES AND BANKS.
    6. GYMNASIUMS, FACILITIES SUCH AS MUSEUMS, THEATERS, AND ART GALLERIES. **USE BY SPECIAL REVIEW USE.**
    7. PEDESTRIAN PLAZAS, PEDESTRIAN WALKS, INCLUDING OR OUTDOOR AMUSEMENT AND OUTDOOR ART EXHIBIT FACILITIES AND PUBLIC ART.
    8. OUTDOOR RECREATION FACILITIES, INCLUDING OF TENNIS COURTS AND OUTDOOR MARKET PLACES. OUTDOOR PLAY AREAS ARE INCLUDED USE IN ZONES 2 AND 3.
    9. INDOOR RECREATION FACILITIES, INCLUDING INDOOR AND OUTDOOR TENNIS AND SOCCER FACILITIES SHALL BE A PERMITTED USE.
    10. OUTDOOR RECREATION FACILITIES. **USE BY SPECIAL REVIEW USE.**
    11. OUTDOOR COMMERCIAL MANAGEMENT. **USE BY SPECIAL REVIEW USE.** LEASING AND EVENTS WITH A DURATION OF TEN DAYS OR LESS IN ONE SEASON SHALL BE PROCESSED UNDER THE APPLICABLE TEMPORARY USE REVIEW GUIDELINES AND COSTS.
    12. RECREATION USES INCLUDING INDEPENDENT AND SENIOR LEASING ARE EXCLUDED.
    13. FAST FOOD SERVICE IN CONNECTION WITH DRIVE THROUGH SERVICE. **USE BY SPECIAL REVIEW USE.**
    14. NIGHTCLUBS. **USE BY SPECIAL REVIEW USE.**
    15. ANIMAL HOSPITALS AND SMALL ANIMAL CLINICS. **USE BY SPECIAL REVIEW USE.**
    16. KENNELS FOR THE REARING OR BREEDING OF DOMESTIC ANIMALS OR LIVESTOCK AND AN ENCLOSED USE WALL ZONES.
    17. AUTO SERVICE AND PARTS AND REPAIRS. **USE BY SPECIAL REVIEW USE.**
    18. AUTO SALES AND AUTO BODY REPAIR ARE EXCLUDED IN ALL ZONES.
    19. ASSISTED LIVING AND SKILLED NURSING FACILITIES.
    20. RESIDENTIAL USES INCLUDING INDEPENDENT AND SENIOR LEASING ARE EXCLUDED.
    21. CHURCH CENTERS. **USE BY SPECIAL REVIEW USE.**
    22. RETAIL, PERSONAL SERVICE USES. **USE BY SPECIAL REVIEW USE.**
    23. RETAIL, RETAIL ESTABLISHMENTS DEDICATED TO 30,000 SQUARE FEET OR GREATER TO A SINGLE USER IS AN EXCLUDED USE IN ZONE 2A.
    24. RETAIL, RETAIL ESTABLISHMENTS DEDICATED TO 30,000 SQUARE FEET OR GREATER TO A SINGLE USER IS AN EXCLUDED USE IN ZONES 2B AND 2C.
- ZONE THREE (approx. 3.4 acres)**
  1. CHURCH CENTERS. **USE BY SPECIAL REVIEW USE.**
  2. ALL USES PERMITTED IN ZONE ONE.
  3. ASSISTED LIVING AND SKILLED NURSING FACILITIES. **USE BY SPECIAL REVIEW USE.**
  4. RESIDENTIAL USES, INCLUDING INDEPENDENT AND SENIOR LEASING ARE EXCLUDED.

### HEIGHT, YARD AND BULK REQUIREMENTS

- ZONE ONE**
  - \*HEIGHT TO BE MEASURED FROM FINAL FINISH GRADE.
  - \*ALL SETBACKS SHALL COMPLY TO CONFORM WITH CITY OF LOUISVILLE ZONING REGULATIONS.
  - \*FLOORING AMOUNT TO CONFORM WITH CITY OF LOUISVILLE REGULATIONS.
- ZONE TWO AND THREE**
  - \*COMMERCIAL DEVELOPMENT SHALL NOT EXCEED A FLOOR AREA RATIO OF 0.25 WITH THE FLOOR DISTRIBUTION ON SITE WITHIN SUBDIVISIONS EXCEEDED ON SHEET 2.
  - \*HEIGHT TO BE MEASURED FROM FINAL FINISH GRADE.
  - \*ALL BUILDINGS ADJACENT TO OR FRONTING ON SOUTH 96TH STREET SHALL NOT EXCEED TWENTY-FIVE FEET HEIGHT AND ALL OTHER BUILDINGS SHALL CONFORM WITH THE CITY OF LOUISVILLE HEIGHT REGULATIONS.
  - \*BUILDINGS ADJACENT TO OR FRONTING ON SOUTH 96TH STREET SHALL BE LOCATED SO AS TO PRIMARILY PLACE THE BUILDING BETWEEN SOUTH 96TH STREET AND THE PARKING LOT. PARKING LOTS LOCATED BEHIND THE IMPROVEMENT OF THE BUILDING SHALL BE SHELTERED FROM SOUTH 96TH STREET USING LANDSCAPING AND BARRIERS THAT ARE A MINIMUM OF 30' ABOVE THE PARKING LOT LEVEL.
  - \*PARKING AMOUNT TO CONFORM WITH CITY OF LOUISVILLE REGULATIONS.
- SITE INFORMATION**
  - OWNERSHIP**
    - \*CATHOLIC ARCHDIOCESE OF DENVER: ST. LOUIS CATHOLIC CHURCH
    - \*HOWARD AND MARY JOHNSON
    - \*GAMES
  - DEDICATIONS**
    - \*ALL DEDICATIONS FOR SOUTH 96TH STREET AND DILLON ROAD RIGHTS-OF-WAY ARE COMPLETED PRIOR TO THIS ZONING.
    - \*PURSUANT TO COMPREHENSIVE PLAN POLICY, THE LAND DEDICATION REQUIRED BY THE SUBDIVISION REGULATIONS SHALL BE PRIMARILY USED FOR THE PURPOSES OF TRAIL, LEASING AND SERVICES TO SERVE THE SUBDIVISION AS A TRAIL, LEASING CORRIDOR SHALL BE PROVIDED ALONG THE EAST AND SOUTH BOUNDARIES TO THE PROPERTY AND A LANDSCAPED BUFFER SHALL BE PROVIDED ALONG SOUTH 96TH STREET. THE FORM OF DEDICATION, RESPONSIBILITY FOR CONSTRUCTION AND RESPONSIBILITY FOR MAINTENANCE SHALL BE DETERMINED AT THE TIME OF SUBDIVISION.
  - ACCESS MANAGEMENT**

**CITY COUNCIL**  
APPROVED THIS 21 DAY OF SEPTEMBER, 2004 BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, CO. RESOLUTION NO. 14 SERIES 2004  
*Sign on Copy at Boulder County*  
MAYOR \_\_\_\_\_ CITY CLERK \_\_\_\_\_

**PLANNING COMMISSION CERTIFICATE**  
APPROVED THIS 13 DAY OF JULY, 2004 BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, CO. RESOLUTION NO. 14 SERIES 2004

**CLERK AND RECORDER CERTIFICATE**  
I HEREBY CERTIFY THAT THIS INSTRUMENT WAS FILED IN MY OFFICE AT \_\_\_\_\_ O'CLOCK \_\_\_\_\_ M. THIS 5<sup>th</sup> DAY OF May, 2010 AND IS RECORDED IN PLAN FILE # \_\_\_\_\_ RECEPTION FEE \$ \_\_\_\_\_ PAID \$ 200

**OWNERSHIP SIGNATURE BLOCK**  
BY SIGNING THIS GDP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GDP. WITNESS OUR HANDS AND SEALS

*James J. Chaput, Archbishop*  
*Judy Gordon*  
*Judy Gordon*



PREPARED BY:  
**JLB**  
743 PEAR COURT  
LOUISVILLE, COLORADO 80027  
303.564.1534

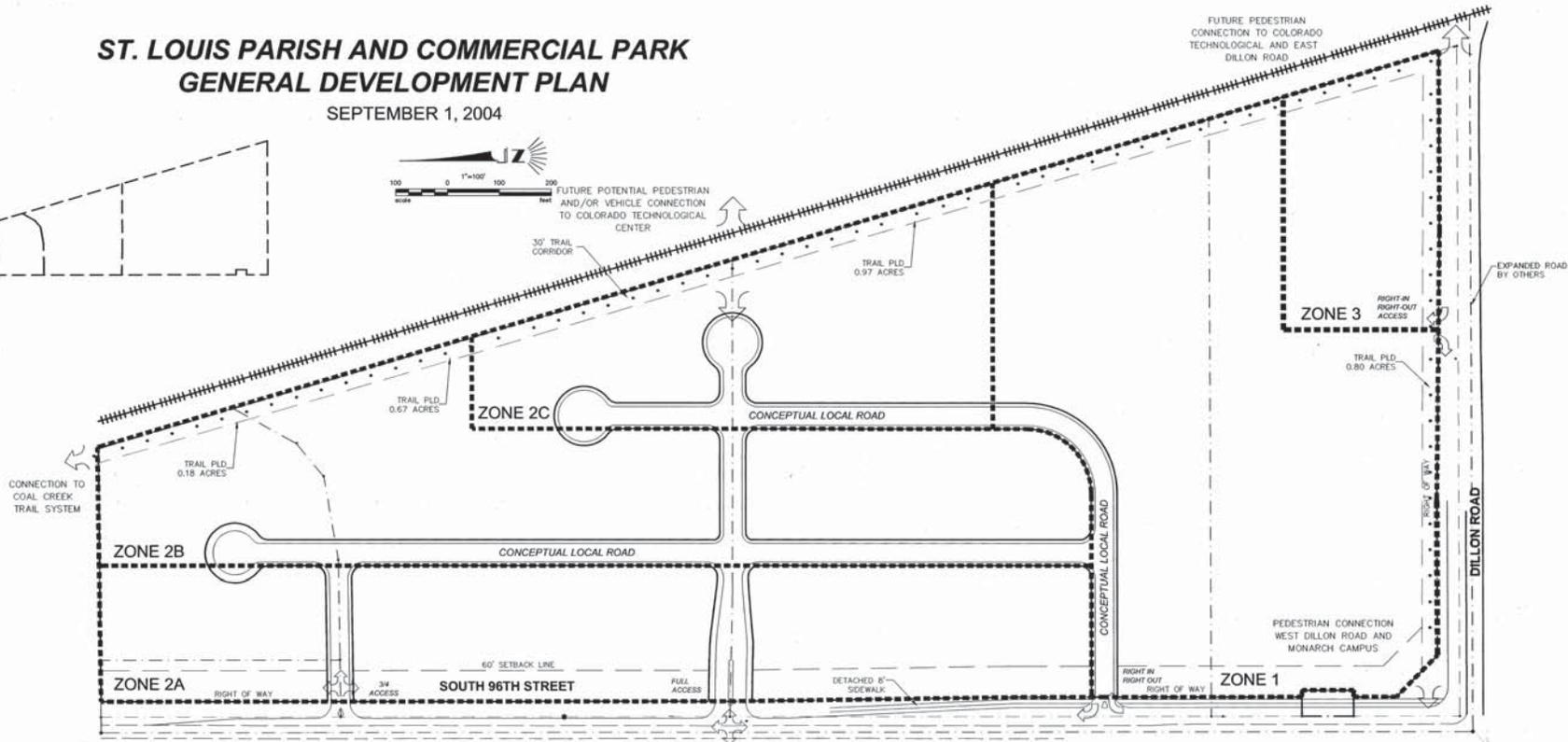
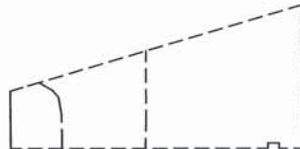
**GDP-1**  
SHEET 1 OF 2

JLB Project No. 113  
File: Rezone\113\development plan.dwg

DRAWING NUMBER  
DRAWING NUMBER  
DRAWING NUMBER  
St. Louis Parish & Commercial Park  
Commercial Park  
SHEET 1 OF 2

# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN

SEPTEMBER 1, 2004



### COMMERCIAL DEVELOPMENT CHARACTERISTICS

ZONE	FLOOR AREA RATIO:	SETBACKS:	PARKING:	HEIGHT:	ARCHITECTURE:	COVERAGE:
ZONE 2A	0.17	60' FROM 96TH STREET, PER CODE OTHERWISE BEHIND BUILDINGS, BEHIND AND/OR LANDSCAPE WHERE VISIBLE FROM 96TH	PER MUNICIPAL CODE	25' MAXIMUM FROM FINAL FINISH GRADE	SINGLE STORY PITCHED ROOF	84,640 square feet
ZONE 2B	0.20	PER MUNICIPAL CODE	PER MUNICIPAL CODE	35' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PUD GUIDELINES	111,252 square feet
ZONE 2C	0.245	PER MUNICIPAL CODE	PER MUNICIPAL CODE	35' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PUD GUIDELINES	80,801 square feet
ZONE 3	0.20	PER MUNICIPAL CODE	PER MUNICIPAL CODE	35' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PUD GUIDELINES	29,838 square feet

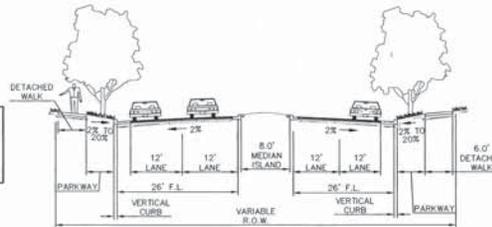
### TABLES

PUBLIC LAND DEDICATION	
REQUIRED (51.4 ACRES @ 12%)	6.17 acres
PROPOSED TRAIL DEDICATION	2.82 acres
DEFICIT	3.35 acres

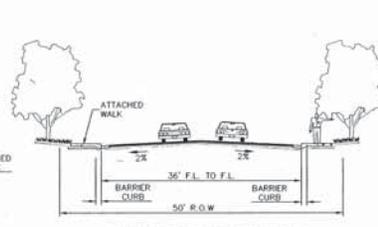
### SUMMARY OF POTENTIAL DEVELOPMENT

SITE	ZONE 2A:	ZONE 2B:	ZONE 2C:	ZONE 3:	TOTAL:
	84,640 square feet	111,252 square feet	80,801 square feet	29,838 square feet	306,531 square feet

ZONE 1 (CHURCH) NOT INCLUDED



LOCAL STREET SECTION (VARIABLE ROW)  
DETACHED SIDEWALK



LOCAL STREET SECTION (50 FT. ROW)  
ATTACHED WALK

### CONCEPTUAL ROADWAY CROSS SECTIONS

PREPARED BY  
**JLB**  
743 PEAR COURT  
LOUISVILLE, COLORADO 80027  
303.664.1834

DC-1  
SHEET 2 OF 2

JLB Project No. 1137  
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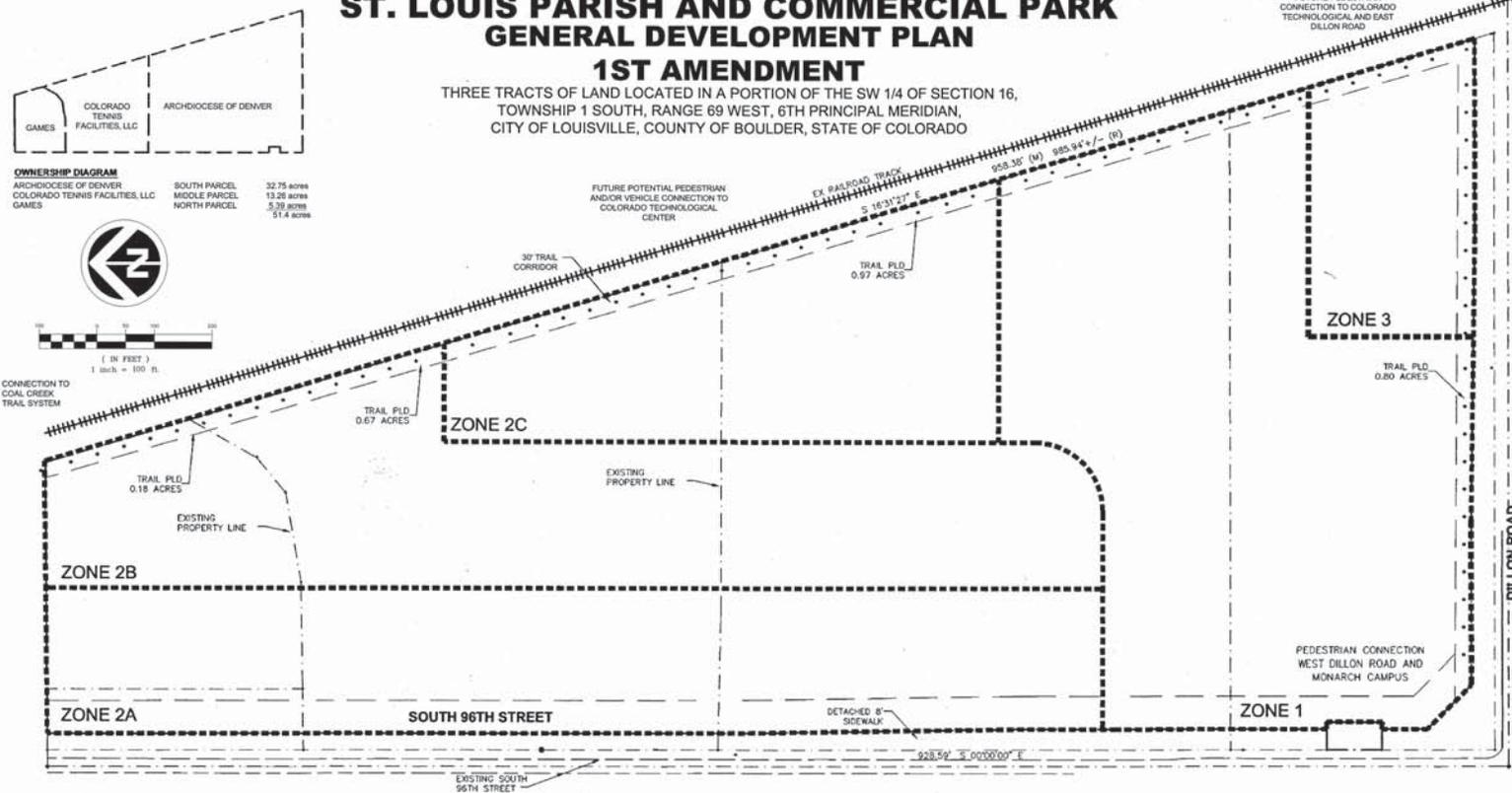
DRAWING NUMBER  
DRAWING NUMBER  
DRAWING NUMBER  
St. Louis Parish & Commercial Park  
2 of 2

03837827  
 Page 1 of 3  
 07-18-2020  
 Boulder County Clerk

# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN

## 1ST AMENDMENT

THREE TRACTS OF LAND LOCATED IN A PORTION OF THE SW 1/4 OF SECTION 16,  
 TOWNSHIP 1 SOUTH, RANGE 89 WEST, 6TH PRINCIPAL MERIDIAN,  
 CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



**HEIGHT, YARD AND BULK REQUIREMENTS**

**ZONE ONE**

- HEIGHT TO BE MEASURED FROM FINAL FINISH GRADE.
- ALL SETBACKS AND LOT COVERAGE TO CONFORM WITH CITY OF LOUISVILLE ZONING REGULATIONS.
- PARKING AMOUNT TO CONFORM WITH CITY OF LOUISVILLE REGULATIONS.

**ZONE TWO AND THREE**

- COMMERCIAL DEVELOPMENT SHALL NOT EXCEED A FLOOR AREA RATIO OF 0.20 WITH THE FAR DISTRIBUTED ON SITE WITH SUBAREAS IDENTIFIED ON SHEET 2.
- HEIGHT TO BE MEASURED FROM FINAL FINISH GRADE.
- BUILDINGS ADJACENT TO, OR FRONTING TO SOUTH 96TH STREET SHALL NOT EXCEED TWENTY-FIVE (25) FEET IN HEIGHT AND ALL OTHER BUILDINGS SHALL CONFORM WITH THE CITY OF LOUISVILLE HEIGHT REGULATIONS.
- BUILDINGS ADJACENT TO, OR FRONTING TO SOUTH 96TH STREET SHALL BE LOCATED 50 AS TO PRIMARILY PLACE THE BUILDING BETWEEN SOUTH 96TH STREET AND THE PARKING LOT. PARKING LOTS EXTENDING BEYOND THE SHADOW OF THE BUILDING SHALL BE SHIELDED FROM SOUTH 96TH STREET USING LANDSCAPING AND BERMS THAT ARE A MINIMUM OF 30" ABOVE THE PARKING LOT LEVEL.
- PARKING AMOUNT TO CONFORM WITH CITY OF LOUISVILLE REGULATIONS.

**SITE INFORMATION**

**OWNERSHIP**

- CATHOLIC ARCHDIOCESE OF DENVER/ST. LOUIS CATHOLIC CHURCH
- COLORADO TENNIS FACILITIES, LLC
- ADRIAN GAMES

**DEDICATIONS**

- ALL DEDICATIONS FOR SOUTH 96TH STREET AND DILLON ROAD RIGHTS-OF-WAY ARE COMPLETED PRIOR TO REZONING.
- PURSUANT TO COMPREHENSIVE PLAN POLICY, THE LAND DEDICATION REQUIRED BY THE SUBDIVISION REGULATIONS SHALL BE PRIMARILY USED FOR NEIGHBORHOOD PARKS, TRAILS LINKAGES AND BUFFERS TO SERVE THE SUBDIVISION A TRIAL LINKAGE CORRIDOR SHALL BE PROVIDED ALONG THE EAST AND SOUTH BOUNDARIES TO THE PROPERTY, AND A LANDSCAPED BUFFER SHALL BE PROVIDED ALONG SOUTH 96TH STREET. THE FORM OF DEDICATION, RESPONSIBILITY FOR CONSTRUCTION AND RESPONSIBILITY FOR MAINTENANCE SHALL BE DETERMINED AT THE TIME OF SUBDIVISION.

**ACCESS MANAGEMENT**

THE ACCESS MOVEMENTS SHOWN ON THE PUD ARE SUBJECT TO CHANGE. AT ANYTIME IN THE FUTURE, IT IS DETERMINED BY THE CITY THAT CHANGE IS APPROPRIATE TO ENHANCE TRAFFIC FLOW ON ONE OR MORE SURROUNDING STREETS, OR TO MITIGATE AN UNSAFE SITUATION, UPON NOTIFICATION FROM THE CITY, THE PROPERTY OWNERS SHALL MAKE SUCH PHYSICAL CHANGES, AT THEIR COST, AS MAY BE REQUIRED BY THE CITY. EACH PRESENT AND FUTURE PROPERTY OWNER SHALL ACKNOWLEDGE IN WRITING THE FOREGOING AUTHORITY OF THE CITY.

**DEVELOPMENT CONCEPT AND GENERAL NOTES**

- THE DEVELOPMENT CONCEPT IS TO ESTABLISH A RELIGIOUS INSTITUTION/SCHOOL CAMPUS AT THE INTERSECTION OF SOUTH 96TH STREET AND DILLON ROAD, A SECOND RELIGIOUS INSTITUTION CAMPUS WITHIN THE CENTRAL PORTION OF THE DEVELOPMENT, WITH THE REMAINDER OF THE PARCELS USED FOR COMMERCIAL PURPOSES THAT ARE NOT IN CONFLICT WITH THE PRESENCE OF RELIGIOUS INSTITUTIONS AND A SCHOOL. THE DEVELOPMENT IS INTENDED TO PROVIDE SUPPORT SERVICES TO THE INDUSTRIAL/EMPLOYMENT AREA LOCATED TO THE EAST, AND BE A TRANSITION BETWEEN THAT DEVELOPMENT AND THE OPEN SPACE TO THE WEST. A LANDSCAPE BUFFER, BUILDING HEIGHTS, FLOOR AREA RATIOS AND PARKING REQUIREMENTS SHALL ALL BE USED TO FACILITATE THE TRANSITION FROM RURAL/OPEN SPACE TO THE DEVELOPED PROPERTY.
- EXCEPT WHERE AMENDED BY THIS GENERAL DEVELOPMENT PLAN, DEVELOPMENT WILL BE SUBJECT TO THE CITY OF LOUISVILLE COMMERCIAL DEVELOPMENT DESIGN STANDARDS AND GUIDELINES (CDDSG).
- DEVELOPMENT SHALL BE GOVERNED BY A GENERAL DEVELOPMENT PLAN AGREEMENT, EXECUTED BY ALL OWNERS, ADDRESSING THE PHASING OF DEVELOPMENT, RESPONSIBILITY FOR INFRASTRUCTURE DESIGN, CONSTRUCTION AND COST, AND ARCHITECTURAL DESIGN CRITERIA. THIS DEVELOPMENT AGREEMENT SHALL BE SUBMITTED TO, AND APPROVED BY, THE CITY OF LOUISVILLE IN CONJUNCTION WITH THE DEVELOPMENT APPLICATION AND/OR PRELIMINARY PLAT PRIOR TO DEVELOPMENT OF ANY OF THE SUBJECT PROPERTY.
- RETAIL ESTABLISHMENTS SHALL BE LIMITED TO 70,000 SQUARE FEET.

- PERMITTED USES**
- ZONE ONE (approx. 16.2 acres)**
- RELIGIOUS INSTITUTIONS
  - SCHOOLS
  - ANCILLARY FACILITIES TYPICALLY AND COMMONLY ASSOCIATED WITH RELIGIOUS INSTITUTIONS AND SCHOOLS INCLUDING A RECTORY, ADMINISTRATIVE OFFICES, AND A CHILD CARE CENTER AS DETERMINED BY THE PLANNING COMMISSION AND CITY COUNCIL DURING THE PUD PROCESS.
- ZONE TWO (approx. 31.8 acres)**
- CONTINUAL OF THE EXISTING RESIDENTIAL USES ON THE PROPERTY.
  - RELIGIOUS INSTITUTIONS USE BY RIGHT.
  - ALL USES IN ZONE ONE - USE BY SPECIAL REVIEW USE.
  - PROFESSIONAL AND ADMINISTRATIVE OFFICES
  - PROFESSIONAL MEDICAL OFFICES AND CLINICS
  - FINANCIAL OFFICES AND BANKS
  - CULTURAL FACILITIES SUCH AS MUSEUMS, THEATERS, AND ART GALLERIES - USE BY SPECIAL REVIEW USE.
  - PEDESTRIAN PLAZAS, PEDESTRIAN WAYS, INCLUSIVE OF OUTDOOR AMENITIES AS OUTDOOR ART EXHIBITS FACILITIES AND PUBLIC ART
  - OUTDOOR SPECIALTY USES, INCLUSIVE OF SIDEWALK CAFES AND OUTDOOR MARKET PLACES. OUTDOOR FLEA MARKETS ARE AN EXCLUDED USE IN ZONES 2 AND 3.
  - INDOOR RECREATIONAL/FITNESS FACILITIES, INDOOR AND OUTDOOR TENNIS AND SOCCER FACILITIES SHALL BE A PERMITTED USE.
  - OUTDOOR RECREATIONAL/FITNESS FACILITIES - USE BY SPECIAL REVIEW USE.
  - OUTDOOR COMMERCIAL AMUSEMENT - USE BY SPECIAL REVIEW USE. TEMPORARY EVENTS WITH DURATION OF TEN DAYS OR LESS IN ONE SEASON SHALL BE PROCESSED UNDER THE APPLICABLE TEMPORARY USE REVIEW STANDARDS AND CRITERIA.
  - RESTAURANTS AND CAFES
  - FAST FOOD SERVICES IN CONJUNCTION WITH DRIVE THROUGH SERVICE SERVICE FACILITIES - USE BY SPECIAL REVIEW USE.
  - HOSPITALS - USE BY SPECIAL REVIEW USE.
  - ANIMAL HOSPITALS AND SMALL ANIMAL CLINICS - USE BY SPECIAL REVIEW USE.
  - KENNELS FOR THE HOarding OR BREEDING OF DOMESTIC ANIMALS/LIVESTOCK ARE AN EXCLUDED USE IN ALL ZONES.
  - AUTO SERVICE AND FUELING STATIONS - USE BY SPECIAL REVIEW USE.
  - AUTO SALES AND AUTO BODY SHOPS ARE EXCLUDED IN ALL ZONES.
  - ASSISTED LIVING AND SKILLED NURSING FACILITIES.
  - RESIDENTIAL USES INCLUDING INDEPENDENT AND SENIOR LIVING ARE EXCLUDED.
  - CHILD CARE CENTERS - USE BY SPECIAL REVIEW USE.
  - RETAIL - PERSONAL SERVICE SHOPS
  - RETAIL - RETAIL ESTABLISHMENTS DEDICATING 30,000 SQUARE FEET OR GREATER TO A SINGLE USER IS AN EXCLUDED USE IN ZONE 2A.
  - RETAIL - RETAIL ESTABLISHMENTS DEDICATING 30,000 SQUARE FEET OR GREATER TO A SINGLE USER IS AN EXCLUDED USE IN ZONE 2B AND 2C.
  - USE BY SPECIAL REVIEW USE IN ZONE 2B AND 2C.
- ZONE THREE**
- CHILD CARE CENTERS - USE BY SPECIAL REVIEW USE.
  - ALL USES PERMITTED IN ZONE ONE.
  - ASSISTED LIVING AND SKILLED NURSING FACILITIES - USE BY SPECIAL REVIEW USE.
  - RESIDENTIAL USES, INCLUDING INDEPENDENT AND SENIOR LIVING ARE EXCLUDED.

**CITY COUNCIL CERTIFICATE**  
 APPROVED THIS 17th DAY OF October 2017 BY THE CITY COUNCIL OF THE CITY OF LOUISVILLE, COLORADO.

**PLANNING COMMISSION CERTIFICATE**  
 RECOMMENDED APPROVAL THIS 14th DAY OF September 2017 BY THE PLANNING COMMISSION OF THE CITY OF LOUISVILLE, COLORADO.

RESOLUTION NO. 21 SERIES 217

**BOULDER COUNTY CLERK AND RECORDERS CERTIFICATE:**

THIS PUD WAS RECORDED IN THE OFFICE OF THE BOULDER COUNTY CLERK AND RECORDER ON \_\_\_\_ DAY OF \_\_\_\_ 2017 UNDER RECEPTION NO. \_\_\_\_\_



**OWNERSHIP CERTIFICATE:**  
 BY SIGNING THIS GOP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GOP. WITNESS OUR HANDS AND SEALS THIS 22nd DAY OF Jan 2018.

OWNER: COLORADO TENNIS FACILITIES, LLC

By: Duke Alan Faluch, General Manager

STATE OF COLORADO  
 COUNTY OF Boulder  
 THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS (DATE) BY (NAME AND TITLE OF POSITION): DUKE PALUCH, AS GENERAL MANAGER OF COLORADO TENNIS FACILITIES, LLC.

(NOTARY'S OFFICIAL SIGNATURE)  
 August 29, 2020  
 (COMMISSION EXPIRATION)



**OWNERSHIP CERTIFICATE:**  
 BY SIGNING THIS GOP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GOP. WITNESS OUR HANDS AND SEALS THIS 23rd DAY OF January 2018.

OWNER: ADRIAN D. GAMES BY KURT BRANKAGE, AS ATTORNEY-IN-FACT FOR ADRIAN D. GAMES

STATE OF COLORADO  
 COUNTY OF Boulder  
 THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS (DATE) BY (NAME AND TITLE OF POSITION): KURT BRANKAGE, AS ATTORNEY-IN-FACT FOR ADRIAN D. GAMES

(NOTARY'S OFFICIAL SIGNATURE)  
 JANUARY 12, 2020  
 (COMMISSION EXPIRATION)



**OWNERSHIP CERTIFICATE:**  
 BY SIGNING THIS GOP, THE OWNER ACKNOWLEDGES AND ACCEPTS ALL THE REQUIREMENTS AND INTENT SET FORTH BY THIS GOP. WITNESS OUR HANDS AND SEALS THIS \_\_\_\_ DAY OF \_\_\_\_ 2017.

OWNER: THE ARCHDIOCESE OF DENVER, A COLORADO CORPORATION, BY AND THROUGH AND FOR THE BENEFIT OF ST. LOUIS CATHOLIC PARISH

BY: VERA REVEREND RANDY DELUCA, V.G. M. ATTORNEY-IN-FACT FOR SAMUEL JOSEPH ADRIAN, ARCHBISHOP

STATE OF COLORADO  
 COUNTY OF Denver  
 THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS (DATE) BY (NAME AND TITLE OF POSITION): RANDY DELUCA, V.G. M. ATTORNEY-IN-FACT FOR SAMUEL JOSEPH ADRIAN, ARCHBISHOP

(NOTARY'S OFFICIAL SIGNATURE)  
 12-17-2021  
 (COMMISSION EXPIRATION)

ANDREESA WHITLEY  
 NOTARY PUBLIC  
 STATE OF COLORADO  
 MY COMMISSION EXPIRES DECEMBER 31, 2021

**AMENDMENTS**

- THIS FIRST AMENDMENT ALLOWS RELIGIOUS INSTITUTIONS IN ZONE 2 AS A USE BY RIGHT.

**ENGINEERING CONSULTANTS**  
 Contact: Jason D. Margraf, PE  
 6505 S. Teabon - Louisville, CO 80116-0500  
 Email: jdmargraf@engcon.com

**ST. LOUIS PARISH AND COMMERCIAL PARK  
 GENERAL DEVELOPMENT PLAN  
 1ST AMENDMENT**

**ASCENT COMMUNITY CHURCH**  
 550 S MacQuinn Blvd  
 Louisville, CO 80116  
 Tel: 303-518-8004

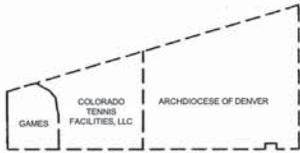
NO.	DATE	REVISION
1	10/17/2017	3RD SUBMITTAL
2	09/14/2017	2ND SUBMITTAL
3	08/02/2017	1ST SUBMITTAL

Project Number: 175001  
 Pre-Drawn By: JDM  
 Checked By: JDM  
 Sheet Number: 1 of 2

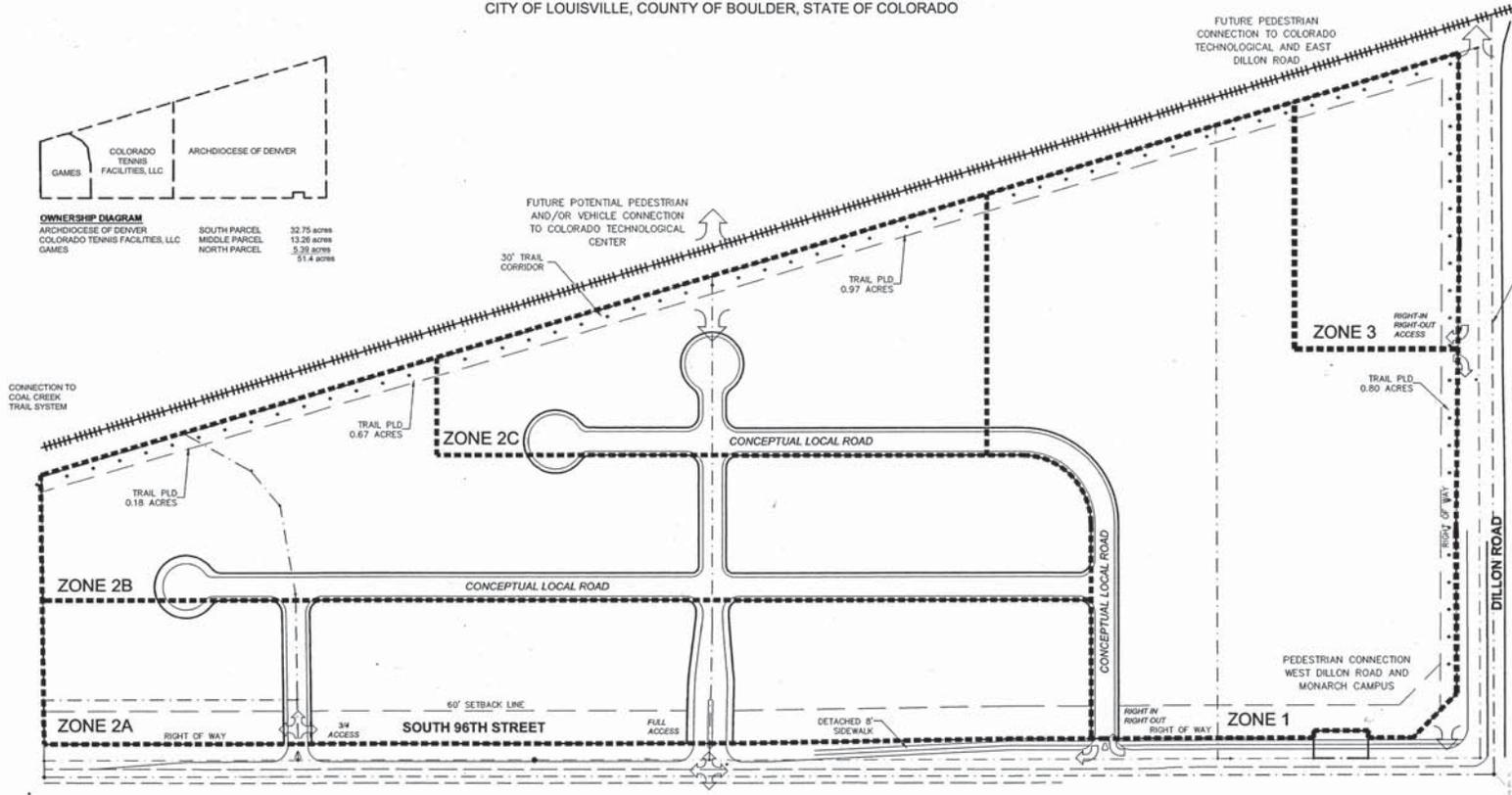
# ST. LOUIS PARISH AND COMMERCIAL PARK GENERAL DEVELOPMENT PLAN

## 1ST AMENDMENT

THREE TRACTS OF LAND LOCATED IN A PORTION OF THE SW 1/4 OF SECTION 16,  
TOWNSHIP 1 SOUTH, RANGE 69 WEST, 6TH PRINCIPAL MERIDIAN,  
CITY OF LOUISVILLE, COUNTY OF BOULDER, STATE OF COLORADO



**OWNERSHIP DIAGRAM**  
ARCHDIOCESE OF DENVER  
COLORADO TENNIS FACILITIES, LLC  
GAMES



**ENGINEERING CONSULTANTS**  
Contact: Jason D. Margraf, PE  
6055 S. McCaslin Blvd  
Louisville, CO 80027  
Tel: 303-519-8084  
Fax: 303-519-8084  
Email: jdmargraf@engcon.com

**ST. LOUIS PARISH AND COMMERCIAL PARK  
GENERAL DEVELOPMENT PLAN  
1ST AMENDMENT**

**ASCENT COMMUNITY CHURCH**  
500 S. McCaslin Blvd  
Louisville, CO 80027  
Tel: 303-519-8084

NO.	DESCRIPTION	DATE
1	PRELIMINARY	10/1/2017
2	1ST SUBMITTAL	10/1/2017
3	2ND SUBMITTAL	10/1/2017
4	3RD SUBMITTAL	10/1/2017
5	ORIGINAL ISSUE DATE	10/1/2017

**DOCUMENT AMENDMENTS**

NO.	DESCRIPTION	DATE
1	1ST AMENDMENT	10/1/2017

**COMMERCIAL DEVELOPMENT CHARACTERISTICS**

ZONE	FLOOR AREA RATIO:	SETBACKS:	PARKING:	HEIGHT:	ARCHITECTURE:	COVERAGE:
ZONE 2A	0.17	60' FROM 96TH STREET, PER CODE OTHERWISE	BEHIND BUILDINGS, BERM AND/OR LANDSCAPE WHERE VISIBLE FROM 96TH	25' MAXIMUM FROM FINAL FINISH GRADE	SINGLE STORY PITCHED ROOF	84,640 square feet
ZONE 2B	0.20	PER MUNICIPAL CODE	PER MUNICIPAL CODE	35' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PLD GUIDELINES	111,252 square feet
ZONE 2C	0.245	PER MUNICIPAL CODE	PER MUNICIPAL CODE	35' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PLD GUIDELINES	80,801 square feet
ZONE 3	0.20	PER MUNICIPAL CODE	PER MUNICIPAL CODE	35' MAXIMUM FROM FINAL FINISH GRADE	SUBJECT TO PLD GUIDELINES	29,838 square feet

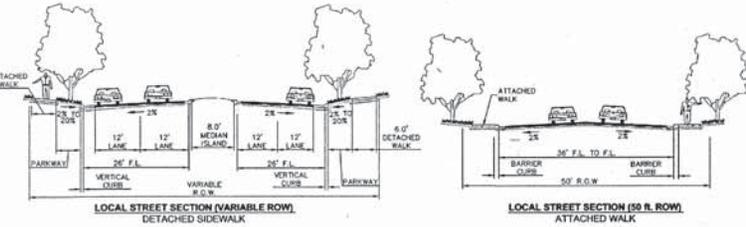
**TABLES**

DESCRIPTION	AMOUNT
PUBLIC LAND DEDICATION REQUIRED (11.4 ACRES @ 12%)	6.17 acres
PROPOSED TRAIL DEDICATION	2.62 acres
DEFICIT	3.55 acres

**SUMMARY OF POTENTIAL DEVELOPMENT**

SITE	ZONE 2A:	ZONE 2B:	ZONE 2C:	ZONE 3:	TOTAL:
	84,640	111,252	80,801	29,838	306,531
	square feet				

ZONE 1 (CHURCH) NOT INCLUDED



**CONCEPTUAL ROADWAY CROSS SECTIONS**

**AMENDMENTS**  
THIS FIRST AMENDMENT ALLOWS RELIGIOUS INSTITUTIONS IN ZONE 2 AS A USE BY RIGHT.

Project Number: 175001  
Drawn By: JDM  
Checked By: JDM  
Sheet Number: 2 of 2



# ASCENT COMMUNITY CHURCH

Dear Planning Commission and City Council,

Thanks SO much for your service to Louisville! We are grateful for you guys and have always valued your partnership in continuing to make the City a great place to live.

I know many of you are new to your roles, and I thought it would be helpful to give background information on Ascent's role regarding the future of Louisville. As the owner of some important parcels (1326 96<sup>th</sup> St. and 550 McCaslin), Ascent takes our responsibility very seriously in helping improve Louisville's revenue sustainability and increasing residents' enjoyment of these properties. I am writing to explain Ascent's hopes for 550 McCaslin, the opportunities at 1326 96<sup>th</sup> St. and challenges we need your help with in order to overcome.

## **550 McCaslin**

This property has been the home of our church for the past six years. When we signed the lease on this vacant big box warehouse, we assumed it would be a short-term solution. Ascent Church never considered buying this property until it became apparent that our ownership might actually help the City's economy by reactivating commercial sales tax in the area. Absent zoning that allows residential, no developer is willing to scrape the giant warehouse and start over. Obviously, a 10-year vacancy shows no other large big box store is going to move in and there is little to no financial sense to scrape the building and build smaller retail outlets. In fact, as you are aware, quite the opposite is happening as Kohl's shuttered their nearby location to open a new store in Lafayette. Unfortunately, this now leaves another large retail building sitting vacant on McCaslin.

*Ascent sees an opportunity to catalyze and fuel redevelopment and attract new retail users to this area with our plan to utilize the less desirable parts of the building for our church and multi-use space, while the more appealing commercial areas can finally be reactivated.* Our purchase makes commercial development financially viable, and we have a retail developer ready to move forward. We are already in the design phase for the complete overhaul of this building, and we are excited for this location to become a key gathering spot and commercial center for the community.

The retail developer has listened intently to the desires of residents, Staff and Council. I'm excited to report that the first drawings are back for the property and include an exciting mix of restaurants, retail, indoor and outdoor public gathering space and other uses. We're excited to start showing these concepts in the very near future. It was evident in the last election cycle how much Louisville's residents want to see movement forward at "Parcel O." We believe, in coordination and cooperation with the City, it's possible to commence construction as early as this fall.

## **1326 96<sup>th</sup> St.**

Assuming 550 McCaslin would eventually be purchased by a commercial developer, Ascent purchased 1326 96<sup>th</sup> St. Ascent intended to make this property its permanent home until a) we saw the opportunity to create the win-win scenario described above on McCaslin and b) the challenges of the 96<sup>th</sup> St. property became too much for Ascent, not a developer by nature, to manage. We have spent considerable time and money investing in a plan for redevelopment of this property, and have worked diligently with staff and the two adjacent property owners to tee up this property for the future. However, after several years of exhaustive efforts to move this forward, we recognize the development challenges are beyond our abilities, but can be overcome by an experienced commercial developer.

As you know, Planning Commission and City Council have an upcoming vote on a GDP Amendment proposed by the potential buyer of this property. Passage of this amendment is critical to the future of both 1326 96<sup>th</sup> St. and 550 McCaslin.

Knowing the history of St. Louis Parish and Commercial Park (SLPCP) is important to inform the upcoming vote.

***The GDP Hinders Development on a Single Property & Requires Considerable Infrastructure Improvements with the First Development***

Nearly 20 years ago, City Council approved a rezoning of SLPCP expanding allowable uses to include commercial retail and office. In exchange for the increase in allowable uses, the property owners agreed to a system in which the plat and PUD require unified planning by all three owners. Because of this, no single property can be developed independently without full cooperation from adjoining property owners. The result explains the multiple failures by developers at this site.

The challenge lies in the fact that the probability of all three owners being ready to simultaneously develop is very remote. Unified development means infrastructure planning and costs for all 51+ acres ends up falling on one owner – namely, the one who is ready before the others. If adjacent property owners are not ready to develop at the same time, obtaining funding for the required infrastructure improvements are nearly impossible. The infrastructure requirements are very challenging and include an extremely long water line, boring sewer under the BNSF tracks and the conveyance of stormwater from open space to the west to the extreme southeast corner eventually piping under Dillon Road. Previous would-be developers and Ascent did not have the capital, capacity or expertise to manage these requirements single-handedly.

***Retail is Not Viable Under Current GDP Requirements***

In exchange for tying the properties together and creating this challenging infrastructure burden, retail zoning was permitted. The location, with very little residential density in proximity, will not support businesses that sell clothes, groceries, etc. Successful businesses on this site will offer products and services catering to people in transit. Ascent fielded many inquiries with the desire to build gas, coffee and convenience retail applications to capture the high traffic volume in the corridor.

All successful retail requires strong visibility (as some of the struggling areas of McCaslin have proven). *The SLPCP's 60-foot building setback is unacceptable to every potential buyer we talked to.* United Properties is requesting to minimally adjust the setback to 55 feet. All other potential retail buyers were unwilling to pursue the property unless the standard setback in Louisville's commercial guidelines (30 feet) were used. Please note, if a 60 foot setback is required, no retail developer will buy this property.

We feel this virtually imperceptible setback modification is a minimal request for the significant benefits that will come to fruition with the development of this site.

The development regulations set forth under the 2004 GDP are outdated and thus do not facilitate, catalyze, nor create a sustainable retail corridor. The required orientation of the parking, setback, etc. have a massive impact on the success of those businesses. Forcing these conditions, that are not retail-friendly, risks the failure of these businesses.

If the old 2004 GDP scenario is enforced, as is, Ascent, the Archdiocese and Adrian Games (land owners) are getting the worst of both scenarios. Not only are we forced to coordinate our development, the exchanged value of retail zoning is worthless. Having said that, we have spent hundreds of thousands of extra dollars (literally), legal work and thousands of personnel hours in an effort to spur development at this location. The answer is not to separate the properties at this point, it is to make the minimal adjustments our buyer, United Properties, is requesting and move things forward.

### ***United Properties***

We've been very impressed by this company and its willingness to put the tremendous amount of work in to complete this development. Alicia Rhymer and her team have worked very well with all three property owners and City staff and are willing to carry the financial infrastructure burden. This is a rare buyer with a great track record of successful development across the Front Range and beyond.

UP is asking for minor changes to the setback and parking orientation. They are offering an increased landscaping package that helps with the buffer to open space, and are willing to work with the City to choose one of many aesthetic design packages. Again, UP is willing to work with a setback that is much greater than any other retail zone in Louisville. City guidelines require a 30 foot setback and UP, in consideration of the open space buffer, is willing to work with 55 feet. Please consider this minimal request to finally launch development of this property forward.

### ***If Retail is Not Enabled to Succeed, The City Will Not Receive Revenue From This Property***

With United Property's current proposal, the City has an opportunity to capture significant tax revenue from this location. As the corridor grows, more traffic is using the area and Louisville will benefit greatly from capturing customers travelling past this site.

Our belief is that if United Properties' GDP Amendment is unsuccessful, the only viable buyers for the property are non-sales tax producing entities. The inquiries we receive from a retail perspective are very similar to United Properties' plans (gas, coffee, etc.). UP is the only buyer we encountered willing to go the extra mile with infrastructure and request minimal changes to the 2004 GDP.

If this GDP Amendment and subsequent Final PUD is not approved, the future of the property will not produce revenue for the City.

### ***Other Effects***

An additional reality of the 96<sup>th</sup> St. property is its relationship to 550 McCaslin. *Ascent needs the funds from the sale of 1326 96<sup>th</sup> St. in order to proceed with the re-development at 550 McCaslin.* We estimate that a loss of the United Properties sale will result in a minimum two-year delay. Honestly, it could be

much longer, because the viable buyers will be slim. In this scenario, the City will lose revenue at *both* 96<sup>th</sup> St. and 550 McCaslin. Revenue will not be the only loss. Residents will miss out on the planned gathering space, restaurants and retail planned at 550 as well as the dedicated trail space included in the 96<sup>th</sup> St. sale. Additionally, the 96<sup>th</sup> St. development will bring jobs to Louisville in both the retail and office/industrial realms. These are key long-awaited changes residents of Louisville are wanting to see take place.

We urge you to approve this GDP Amendment, with the proposed minor adjustments to the development standards and forthcoming Final Plat/PUD this summer for the St. Louis Parish and Commercial Park. Approval of these application will very positively impact the character of the City we love.

Best,

Jim Candy  
Co-Pastor  
Ascent Church

## Lisa Ritchie

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**From:** John Cartwright <john.c.cartwright@comcast.net>  
**Sent:** Saturday, February 29, 2020 12:20 PM  
**To:** Lisa Ritchie  
**Subject:** United Property's proposed development at 96th Street

Dear Ms. Ritchie,

I am writing the Louisville Planning Commission and City Council in support of United Property's proposed development on 96th Street. I have read through the [Narrative](#) and, on the whole, believe that this project is in the best interests of the City of Louisville and its residents.

I have been a resident of Louisville since 1995 and very much appreciate our city and its small-town feel. However, I am becoming increasingly concerned about the imbalance between our retail and property tax base. United Property's proposal and its retail/industrial opportunities are appealing from that perspective. Also the various restrictions described in the Narrative seem to be precluding any productive use for this land and benefit to the City. In addition I appreciate the provision included in the proposal to expand the City's trail system.

Thank you for your time and for conveying my support for this proposal to the the Planning Commission and City Council.

Sincerely,

John Cartwright  
120 W. Pine St.  
Louisville, CO 80027

## Lisa Ritchie

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**From:** Leanne Hamlin <leanne.hamlin@yahoo.com>  
**Sent:** Friday, February 28, 2020 9:14 AM  
**To:** Lisa Ritchie  
**Subject:** GDP Amendment

Lisa,

I am writing to voice my support of a revision of the 96th St & Dillon Road GDP and the United Properties development plan within that property.

My husband Dennis & I currently reside at 2356 Dogwood Circle and have been residents of Louisville since 1986. So obviously we love it here! I know Louisville is consistently ranked as one of the best places to live in America and we really believe that's true. However we are concerned about the population increasing without a similar surge in retail tax dollars. So we're definitely in favor of United Properties plan for industrial & retail in a portion of the property. We often drive down 96th on our way in/out of town so having retail along the way would be nice.

The other thing that's great about this proposal is the trail expansion. We try to walk as much as possible and love the all of the trails throughout the city. Additions to connect existing trails or add new ones are an added bonus.

We encourage the City to support the amendments needed to move forward.

Thank you,

Dennis & Leanne Hamlin

## Lisa Ritchie

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**From:** meekbrien <meekbrien@comcast.net>  
**Sent:** Saturday, February 29, 2020 6:09 PM  
**To:** Lisa Ritchie  
**Subject:** United Property's proposed development at 96th

Dear Planning Commission and City Council,

We have been Louisville residents for the past 28+ years and love living here. We love the convenience of trails, parks, open space, shopping and restaurants that our city has to offer and were sad to see Kohl's leave.

Since we have lived here, we have seen many beneficial improvements made to the city and feel that the proposed development of the property located at 1326 96<sup>th</sup> Street would be one more enhancement that would benefit the residents of Louisville. Over the past several years, the traffic along 96<sup>th</sup> has increased with more people using this roadway for both work and pleasure. We feel that having the convenience of a gas station and other services in this area would not only benefit the residents of Louisville, but also the surrounding communities. Additionally, this will increase the cities retail tax dollars to allow for the continued improvement of our city and the quality of life in Louisville.

Thank you,  
Deanna Meek-Brien  
Terry Brien  
835 W. Conifer Court  
Louisville, CO 80027

## Lisa Ritchie

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**From:** Richard Morgan <richardmorgan644@gmail.com>  
**Sent:** Friday, February 28, 2020 6:22 PM  
**To:** Lisa Ritchie; Planning Commission; City Council  
**Subject:** GDP Amendment for 1326 96th Street

Members of Planning Commission and City Council.

My name is Richard Morgan. My family and I reside at 644 W Pine Street, Louisville, and have for the past 20 years. I understand that the Planning Commission will be discussing a GDP Amendment for the parcel located at 1326 96th Street at its March 12 meeting. I am writing to express my support for this amendment.

The area around Dillon Road and 96th Street presents a tremendous opportunity for the City to convert developable land into a revenue generating asset for the City, and it adds much needed retail services to the southwest quadrant of our town. Tax revenue from retail sales and other construction activity along McCaslin has been in decline, affected primarily by Sam's Club and Kohl's departures, and perhaps soon, Lowe's. Amending the approved uses of 1326 96th Street to include industrial and retail is compatible with the Colorado Technology Center (CTC) and activates a portion of road that already carries a steady volume of vehicle traffic. CTC is quickly approaching complete build-out. New inventory will attract innovative companies and high quality jobs. Offering retail services along the east side of 96th Street provides added convenience to our residents that doesn't exist there today, and it does not adversely impact the view plane over the open space and mountain vistas to the west.

I also understand that the applicant is United Properties. UP is an experienced developer with completed industrial projects in Broomfield. UP is offering to dedicate some of this land to the City's trail system. I know UP to be a thoughtful developer that will listen to the needs of Louisville residents. Presently, it's difficult for tenants of CTC and Louisville residents to access our incredible open space, let alone travel into Old Town without a vehicle, forcing pedestrians and cyclists to share a highway with a posted speed limit of 40 mph.

City Council's Economic Vitality Commission's Strategic Goal is "dedicated to producing reliable revenue to support City services which enhance our quality of life by fostering an economic environment that generates high quality jobs, innovative companies, and a diversity of businesses, employees, and customers." I submit that the subject GDP Amendment promotes Louisville's values by attracting companies, jobs and new sources of tax revenue, and provides retail convenience to our citizens. Thank you for considering the applicant's request to amend the GDP. This is a positive development for Louisville, and I support United Properties' request.

Thank you,  
Richard Morgan

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Richard Morgan  
303.956.8188 (cell)  
[www.linkedin.com/in/morganrichardb](http://www.linkedin.com/in/morganrichardb)

## Lisa Ritchie

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**From:** Kathrena Mountjoy <kathrena\_mountjoy@hotmail.com>  
**Sent:** Tuesday, February 25, 2020 10:10 AM  
**To:** Lisa Ritchie  
**Subject:** Retail on 96th

Dear Lisa!

We lived for many years raising our kids in south Louisville and now reside in Outlook In Steel Ranch. We love this development which the city allowed. We have run up and down 95th/96th for years and years watched the Dillion rd area lie more and more run down. We need a gas station in there for starters and other retail would be great for our beloved Louisville tax revenue esp since Kohl's left. Let's get retail under construction on hwy 42 / 96th 😊

Thank you. K Mountjoy 1868 Kalel.

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Sent from my iPhone

## Lisa Ritchie

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**From:** KEN and LEANNE <KANDLPRESLEY@msn.com>  
**Sent:** Sunday, March 1, 2020 2:32 PM  
**To:** Lisa Ritchie  
**Subject:** United Property development proposal for 96th St and Dillon

To Louisville Planning Commission and City Council

My wife and I are writing to express our strong support for United Property's (UP), development proposal for property on 96th St north of Dillon as explained in their GDP Second Amendment narrative. I believe their requested changes, as explained in that narrative and associated documents, should be approved in their entirety.

In fact, we would even encourage the planning commission and city council to be proactive and open up one particular restriction further than UP has requested, this being the 60 ft setback requirement which UP has requested by relieved to only 55ft. Given the nature of other development along 96th St we believe a setback of 40 ft or less would in no way harm or impair the city's interests in controlling such setbacks and would lead to greater commercial success for the development which would benefit the city.

My wife and I have been Louisville residents for 12 years and are very familiar with the 96th street area that is the subject of the development proposal. We drive that corridor regularly. Based on our knowledge of the area we believe the combined uses which UP has planned for the property seem completely consistent with the nature of the area. They further seem consistent with how that area is steadily developing, in particular the nature of 96th street as a growing commuter corridor. Retail services such as gas stations, convenience marts, car washes, etc are severely lacking in the area and as the corridor further develops will be of great benefit to the people that travel through.

Locating industrial space back from the road and closer to the rail line is a very logical approach. The 40ft requested building height seems very reasonable given the nature of the Tech Center development.

In short, the UP plans seem well thought out and we believe will benefit the city and the people who use the 96th street corridor. We see significant benefits to the city from tax revenue, from the infrastructure that would enable development of the other two lots, from trail system expansion, and from the convenience of new retail in that area. Please approve the request and please consider being proactive in the interest of project success and further relieving the 60 ft setback requirement beyond the 55ft UP request.

Regards

Kenneth and Leanne Presley  
809 Rock Rose Ct.  
Louisville, CO